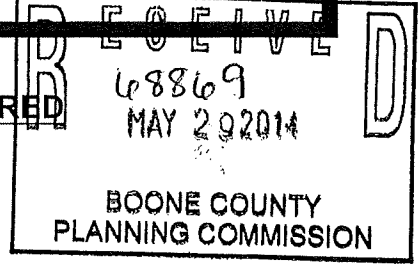


APPLICATION FORM

CONCEPT DEVELOPMENT PLAN
BOONE COUNTY PLANNING COMMISSION



FIVE (5) COPIES OF SUBMITTED DRAWINGS ARE REQUIRED

(See Boone County Zoning Regulations)
SECTION A (To be completed by applicant)

- 1. Check One: [] Concept Development Plan... [X] Change in an Approved Concept Development Plan... [] Long Range Planning Committee Review... [] Long Range Planning Committee Review... [] Zone Change Committee Review... [] Zoning Administrator Review... [] Zoning Administrator Review...
2. Name of Project Thornwilde Village
3. Location of Project Francisville KY - Corner of Thornwilde Dr and Rt 237
4. Total Acreage of Site 8 +/- Acres
5. Current Zoning RPD
6. Date of Previous Zoning Map Amendment or Approved Concept Development Plan (if applicable) April 1994
7. Is the site subject to a specialized Land Use Study approved by the Boone County Planning Commission? (If so, indicate the name of the Study) N/A
8. Proposed Uses (please specify each use) RPD zone allowing Single Family Attached and/or Multi-Family uses
9. Proposed Building Intensities (please specify) 120 units, multi story type buildings attached single family and/or multi-family residential/apartment units
10. Have you submitted a Concept Development Plan? Yes
11. Are you also applying for: [] Conditional Use Permit [] Dimensional Variance
12. Name of Applicant(s) Thornwilde Inc. Phone Number 859 814-8186 Fax No. E-Mail drees@terraceholdings.
13. Address of Applicant(s) 510 Graves Ave Ste 206 Erlanger, KY 41018
14. Name of Property Owner(s) The Drees Company Phone Number 578-4200 Fax No. E-Mail
15. Address of Property Owner(s) 211 Grandview Dr Ft. Mitchell, KY 41017
16. Are there any existing buildings on the site? Yes How many? 1
17. Deed Book Page No. Group No.
18. Have you had a pre-application meeting with BCPC Staff? Yes, 2/25/2014

(over)

19. Please check the following organizations/agencies which you have discussed the proposed development with in the last several months:

- Boone County Water District
- Florence Public Services Dept.
- Duke Energy
- Sanitation District #1
- Cincinnati Bell
- Owen Electric Cooperative, Inc.
- Boone County Public Works Department
- Kentucky Transportation Cabinet
- Boone County Building Department
- Northern Kentucky Health District
- USDA NRCS/Boone County Conservation District
- KY Division of Water
- Local School District
- Local Fire District
- Other: _____

20. Concept Development Plan Jurisdiction/Location

- Unincorporated Boone County
- Florence
- Walton Union

21. ORIGINAL Property Owner's Signature

(Faxed, Photocopied or Scanned Signatures will NOT be Accepted)

The Drees Co. By: Michael Schoetzlbatt

22. ORIGINAL Applicant's Signature

(Faxed, Photocopied or Scanned Signatures will NOT be Accepted)

THORNWILDE INC by: Paul Drees

SECTION B (To be completed by BCPC Staff)

1. Date Received 5/29/14 Fee Received \$2,102.⁰⁰ R# 68869

2. Check what has been submitted:

- Application
- Fee
- Legal Description
- Concept Development Plan
- Addresses of Adjoining Property Owners
- No. of copies of plan received **

3. Is application complete? YES NO

4. Staff Reviewer KEVIN WALL

5. Committee Chairperson BEN BRANDSTETTER

6. Scheduled Public Hearing Date 7/2/14

7. Boone County Planning Commission Action:

- Approved
- Approved With Conditions 8/6/14
- Denied

8. Other: _____

Boone County Planning Commission
Boone County Administration Building
2950 Washington Street, Room 317
P.O. Box 958
Burlington, Kentucky 41005
Phone (859) 334-2196 - Fax (859) 334-2264
plancom@boonecountyky.org - E-Mail
www.boonecountyky.org - Web Page

Note: See Boone County Planning Commission Fee Schedule for Concept Development Plan Fees. An application consists of all fees paid in full, submitted drawings and a completed application form.

EXHIBIT

“A”

STAFF REPORT

Request of Thornwilde Inc. (applicant) for The Drees Company (owner) for a Change in an Approved Concept Development Plan in a Residential Planned Development (RPD) zone for a 8.648 acre site located on the northwest corner of the North Bend Road/Thornwilde Drive intersection, including the tract at 1207 North Bend Road, Boone County, Kentucky. The request is for a Change in an Approved Concept Development Plan to allow a combination of commercial, attached single family residential, and multi-family residential uses.

July 2, 2014

PROPOSAL

This proposal is for a Change in an Approved Concept Development Plan for the above referenced tract. The tract is part of the original Concept Development Plan for Thornwilde Subdivision, which included 287 detached single family dwelling units, 240 attached single family dwelling units, and approximately 26,000 square feet of commercial space, all on approximately 217 acres. The original Concept Development Plan was conditionally approved in 1994 (approval documents are attached). The specific site in question was approved for the commercial space and eleven (11) detached single family lots. The commercial space on the original plan is configured in a strip mall fashion along North Bend Road with an outlot at the North Bend Road/Thornwilde Drive intersection.

The current application is to reconfigure the area into an integral "village" concept with approximately 16,000 square feet of commercial space (ground floor footprint area), and 120 attached single family dwelling units and/or multi-family dwelling units. The buildings are arranged along a main street/drive that loops between access points on North Bend Road and Thornwilde Drive ("Village Drive"). The specific buildings shown on the Concept Development Plan include:

- A. Three commercial buildings along the North Bend Road frontage with footprints that measure approximately 2,181 square feet, 5,106 square feet, and 4,105 square feet. Patio/plaza areas are shown around these three buildings.
- B. An approximate 4,325 square foot commercial building along Thornwilde Drive at the proposed intersection with Village Drive. One of the plan illustrations in the project narrative labels this building as a day care center with an outdoor play area.
- C. A total of eight (8) attached single family residential and/or multi-family residential buildings in the center, north, and west parts of the site. These include both townhouse and apartment style structures which range in size from "4-6" units to 24 units each.

The applicant has stated that both types of residential structures would be two or three stories high. The commercial buildings are anticipated to be one level, but could be multi-story.

Based on the 120 dwelling unit figure noted in the project narrative, the residential density on the project site (in addition to the commercial building area) is 13.88 units per acre. The approved residential density in the current overall Thornwilde development would increase from 1.9 units per acre (1,037 units on 545 acres) to 2.1 units per acre (1,146 units on 545 acres). The original residential density from the 1994 approval was 2.4 units per acre (527 units on 217 acres in addition to the commercial building area).

The project narrative states "Village Drive will strive to be a public dedicated street; however depending on width and setback requirements it may have to be private." Parking spaces are proposed along Village Drive where buildings would immediately adjoin the street/drive. These include both striped commercial style parking spaces and parking in the individual driveways for the townhouse style buildings. A landscaped median is shown in the eastern part of Village Drive. Secondary drives and parking areas branch off of Village Drive and are shown in between the proposed buildings. Sidewalks are conceptually shown on the Concept Development Plan.

The applicant provided a "Flexibility Narrative" which states:

The submission of this concept plan shall have the flexibility to allow the attached/multi-family product to replace the commercial building located by the entrance off of Thornwilde Drive. Adding multiple stories above the storefronts, on part or all of the commercial buildings, can be an option to allow for a variety of uses such as, commercial, professional office and/or multi-family residential.

The concept plan is a preliminary layout and thus the design and site plan must have the flexibility to change as the engineering, market conditions and site characteristics are finalized over time.

Regarding building design, the project narrative states "design elements, color and materials that will be used in the commercial design will be used in the attached/multi-family design. The following collection of pictures shows materials, quality and scale for the type of product that will be used as Thornwilde Village." The referenced pictures are included in the attached project narrative.

Public water and sanitary sewer service are proposed. Landscaping, signage, storm water management, and detailed traffic impact analysis are not specifically addressed in the application materials.

SITE HISTORY

As described above, the site is part of the original Thornwilde Subdivision Concept Development Plan that was conditionally approved in 1994 (approval documents are attached). The original plan included 287 detached single family dwelling units, 240 attached single family dwelling units, and approximately 26,000 square feet of commercial space, all on approximately 217 acres.

Changes and additions to other parts of Thornwilde Subdivision were conditionally approved in 1995, 1997, and 2003. In addition to the commercial space, a total of 1,037 dwelling units (673 detached single family dwelling units and 364 attached single family dwelling units) are approved on a total of 545 acres.

ADJACENT LAND USES AND ZONING

The land uses and zoning adjacent to the site include the following.

- A. Detached single family residences on estate/frontage lots are located to the north along the west side of North Bend Road (RSE). A subdivision of detached single family residences is located further to the north (Wyndemere Place Subdivision, SR-1).
- B. Detached single family residences on estate/frontage lots and a stable are located to the east and northeast across North Bend Road (A-2 and RSE). A subdivision of detached single family residences is located further to the northeast (Deer Creek Subdivision, SR-1). The site for an approved mixed use development is located further to the east and southeast (Rivers Pointe Estates, SR-3/C-1/O-1/PD).
- C. Detached single family residences on frontage lots, and a church and cemetery, are located to the south along the west side of North Bend Road (RSE and RPD). A subdivision of detached single family residences is located further to the south (Tree Tops Subdivision, SR-1).
- D. An open space area for Thornwilde Subdivision is located to the immediate west, and a detached single family phase of Thornwilde Subdivision is located further to the west (RPD).

SITE CHARACTERISTICS

As stated above, the site contains 8.648 acres based on the legal description provided by the applicant. The site has approximately 675 feet of frontage along North Bend Road and approximately 820 feet of frontage along Thornwilde Drive. A detached single family residence is located in the approximate center of the site's North Bend Road frontage. A street intersection into the site was constructed as part of Thornwilde Drive ("Sedgewicke Drive").

The site is largely wooded and hilly. The high point of the site is along the North Bend Road frontage at approximately el 816, and the low point is along the west boundary at approximately el 730. Public sanitary sewer lines exist both on and around the subject site. Public water mains exist in the adjoining Thornwilde Drive right-of-way and on the east side of the North Bend Road right-of-way. Soil types on the site include Cynthiana flaggy silty clay loam (CyF), Rossmoyne silt loam (RsB), and Jessup silt loam (JeD).

RELATIONSHIP TO COMPREHENSIVE PLAN

The 2010 Boone County Comprehensive Plan's Future Land Use Map provides the following designations for the site (percentage figures are based on drawn GIS polygons; figures add to 99% due to rounding):

Commercial © for approximately 23 percent of the site at the intersection of North Bend Road and Thornwilde Drive. This area spans across approximately two thirds of the site's North Bend Road frontage. The Commercial designation is described as "retail, corporate and professional office, interchange commercial, indoor commercial recreation, restaurants, services, etc."

Suburban Residential (SR) for approximately 53 percent of the site in the center, along the north property line, and across the northern one third of the site's North Bend Road frontage. This designation is described as "single family housing of up to four units per acre. This classification does not preclude low density or estate residential developed as a formal subdivision."

Developmentally Sensitive (DS) for approximately 23 percent of the site along the west property line. This designation is described as "areas that have an existing slope of twenty percent or greater for a height of 20 meters (67.6 feet), or have unique soil or flooding characteristics which limit the ability of an area to support urban development, or contain significant wooded areas, creeks, wildlife habitat, or other natural features that are important to a site's stability and visual character. In addition, developmentally sensitive areas may, in the future, be defined and

protected due to historical or visual importance. Any development on land identified as Developmentally Sensitive must be carefully assessed by the developer and the Planning Commission to determine the ability of the land to support the proposed project. The land use priority for areas designated Developmentally Sensitive is preservation of the existing environment, as opposed to development of the land. This shall act as a guideline for any project proposed in Developmentally Sensitive areas; specific sites designated as Developmentally Sensitive require more detailed inventories of soil, slope, wildlife habitat, vegetation, and other possible physical constraints as part of the development plans. These inventories shall be used, along with engineering studies, to determine whether an area designated Developmentally Sensitive can be developed in a fashion which is sensitive to the existing site characteristics. The Developmentally Sensitive areas of Boone County have been identified by utilizing United States Department of Agriculture Natural Resources Conservation Service (formerly Soil Conservation Service) data and mapping, through the Boone County Geographic Information System (GIS). The degree of accuracy of these areas is intended to locate general areas of concern. Actual site assessments will determine the exact boundaries of Developmentally Sensitive areas in Boone County at the time of development.”

The Land Use Element provides the following quotes that relate to the proposal and/or general area (“17. North Hebron,” pg. 180).

- A. With planned public sanitary sewer improvements in the Sand Run Creek and Elijah Creek watersheds, including sewer lift stations, there will continue to be considerable growth pressures. Development should be clustered on the ridge top areas because of the limited capacity of the existing roadway network, the Developmentally Sensitive areas, and the importance of the stream valleys as greenbelts. Lower density development, such as Rural Density Residential, should provide a transition into the Developmentally Sensitive areas.

With the provision of adequate infrastructure, some Suburban Density Residential development and small amounts of attached housing may extend onto the ridge top areas. Development would need to retain the bluff line vegetation in order to minimize the visual and environmental impacts of residential development. Planned Development or clustered housing should be used to insure proper development of this area, due to environmental and site characteristics. Much of the remaining land in this section contains Developmentally Sensitive and scenic hillsides, and should stay wooded in order to create open space between future developments.

- B. Conventional layout subdivisions should be discouraged because of their inefficient circulation systems and excessive site grading requirements. The stream valleys and ridge bluff edges in this section should remain in their current state to provide public open space for the residential development on the ridge tops. Overall, this section should experience growth because of its proximity to I-275 and the Burlington-Hebron areas.

In general, Suburban Residential development should be concentrated on land nearest to KY 237, as indicated on the Future Land Use Map. Unlike most developing areas of Boone County, the north river area contains only one primary means of access, being North Bend Road. In the event that a bridge is proposed between Boone County and Hamilton County, a full study of the impacts should be made. These impacts could include effects on labor supply and transpositions, increased truck traffic in Boone County, and impacts on existing and planned residential areas.

The Land Use Element's Future Land Use Development Guidelines provide the following general comments that relate to the proposal.

- A. Developments in Boone County should begin with an assessment of the existing site features to determine positive and useful attributes, as well as features which should or can be preserved. Development design should incorporate the use of these attributes for the benefit of the development and the County as a whole, rather than leveling an entire site to meet a pre-conceived project design. Development plans should identify such areas, and delineate disturb limits to protect those areas that have been defined. True Open Space subdivision design should be considered to blend new subdivisions in with areas that have a rural character ("Utilization of Existing Vegetation and Topography," pp. 161 and 162).
- B. As described in the Goals and Objectives, different development densities can occur in Boone County as long as the development is designed in a proper manner and the infrastructure exists or is planned to support the development. The Goals and Objectives also acknowledge that fluctuations in the economy and housing market should be considered when evaluating the uses and design of developments.

Business, commercial, or industrial parks should be designed and developed to allow future residential or other uses to access through them where the corridor may extend to large developable tracts or may provide a future road connection. A roadway that is suitable for mixed use development should serve side streets for businesses rather than serving individual businesses directly, include an urban curb and gutter design, and provide pedestrian amenities and green space along the

right-of-way. In this way, future residential development could be developed beyond the initial development, but be readily marketed ("Development Layout, Lot Sizes, and Setbacks," pg. 162).

- C. Developments in Boone County must recognize the potential impacts upon adjoining land uses and incorporate a transition of land uses, building setbacks, and/or landscaping to minimize these impacts. Potential impacts include visual, noise or vibrations, odors, dust, smoke, and light. Buffering to mitigate these impacts should be an integral part of the design of proposed projects; existing site features should be used in meeting this guideline. Developments should provide buffering along public roadways, to soften the visual impact. Appropriate wooded areas and stream valleys should remain as open space within developments and between developments. Developments proposed adjacent to planned or established open spaces should provide pedestrian access where appropriate. Natural green space benefits the community as well as encourages developers to create innovative development designs through clustering of buildings and impermeable area. Typically, buffering is required and provided between unlike land uses; however, as the development of Boone County fills in previously rural areas, different lot sizes and designs of residential development sometimes impact each other. Where an appropriate gradation of lot size and setbacks cannot be designed into a proposed residential subdivision development of a significantly higher density than existing adjacent residential uses, deliberate vegetation buffering may need to be incorporated into the design to help protect low density residential areas. Buffer areas should use and supplement existing site features where possible. ("Buffering," pp. 162 and 163).
- D. Developments in Boone County must include landscaping to accompany the proposed project. Retention of existing healthy vegetation is considered a component of landscaping and is encouraged. This landscaping should be designed to improve the public view of a development, and should be incorporated into parking lots and other vehicle circulation areas, as well as within open spaces and around structures. Landscaping is intended to minimize the visual impacts of the development from adjoining properties and roadways.

Developments along major roadways in Boone County must include landscaping between the development and the right-of-way in order to promote the aesthetic appearance from the roads and to facilitate the compatibility of differing land uses ("Landscaping," pg. 163).

- E. Developments in Boone County must recognize the potential impacts of associated traffic on adjoining properties and transportation systems. The need exists to protect the capacity of the existing roadway network, and to plan improvements to

accommodate new development and travel patterns. Access management provisions include the coordination of curb cuts, adequate corner clearance and site distance for access points, adequate space between access points, shared access points and parking facilities, provisions for access connections to adjoining properties, and dedication of public right-of-way ("Access Management," pg. 163).

- F. Developments in Boone County must be designed, where appropriate, to improve the County's transportation network system of roadways, and functional classifications must be used in the planning for and designing of new developments. Collector roadways should be extended and developed to provide for the safe movement of traffic through and between subdivisions. Development along existing arterials and collector roadways should not have direct driveway access, but be served by a local street. Appropriate road connections should be constructed to provide alternate routes for traffic to and through specific projects and to enhance the capacity of existing streets. The traditional grid system can provide an alternative to typical suburban design. Parallel and frontage roads should be used to minimize impacts of individual sites on collector and arterial roadways. Appropriate pedestrian networks should be incorporated into the design of developments which will generate or experience significant pedestrian use. These networks should not only provide internal paths, but should provide connections to adjoining uses where appropriate. Public open space and recreation sites should be connected to each other by bicycle and pedestrian paths where appropriate. Transit Oriented Development (TOD) is discussed in detail in the Transportation Element, and should be examined as a future direction in Boone County development patterns ("Transportation and Pedestrian Network," pp. 163 and 164).
- G. Developments in Boone County should give consideration to the overall design of the area. Site, landscape, and building design should be a primary concern at the early stages of the development, with an emphasis on the aesthetic impact of the proposed use. The minimal use of signs is encouraged; signage should be adequate to identify a specific development, but should not be used as a means to compete for motorist attention.

Neo-traditional residential and commercial development should be encouraged to provide quality development that blends with the natural or historic character of parts of Boone County ("Design, Signs, and Historic Preservation," pg. 164).

The Population Element outlines population estimates based on OKI transportation analysis zone "regions." The population of the "Hebron" region where the subject site is located is estimated to change to 15,086 in 2020, and to 17,886 in 2030.

The Business Activity Element makes the following statements regarding the overall area ("Recommended Areas of Commercial Activity," pp. 69 and 70).

- A. The Hebron area will experience additional commercial development along KY 237 and KY 20 as the northern area increases in population. The existing commercial center along KY 20, just east of KY 237, will probably not expand significantly because the new highways, I-275 and KY 237, have bypassed it. Commercial development in Hebron will center around the intersection of new KY 237 and KY 20 with a limited amount of convenience and neighborhood commercial uses north of the I-275 interchange to serve the SouthPark Industrial Park, Park West Industrial Park, and the growing number of residential subdivisions along North Bend Road. Park West is also the home of a Foreign Trade Zone (FTZ No. 47) which is an area exempt from Customs payment, taxes, and duties normally associated with international trade. Such FTZ's are designed to stimulate economic growth while encouraging companies to keep their operations in the United States.

With the amount of residential construction expected in the North River area, larger scale commercial developments, such as drug stores and grocery stores will be needed. Commercial development in this area should be designed to serve the northern part of Boone County. This commercial growth should coordinate with the existing and proposed industrial and residential developments, especially in terms of access.

- B. The various small communities should continue to serve the commercial needs of their surrounding areas, and any regulations or incentives that affect these areas should be designed to support small businesses. New businesses, as they are constructed or as existing structures are remodeled, should be developed with sensitivity to the historical character as well as the current character of the area.
- C. In general, large residential developments may need to provide for neighborhood-scale commercial activities. Neighborhood commercial activities may include small grocery or convenience stores, family restaurants, professional offices or small retail operations of general merchandise. These neighborhood commercial activities should be scaled to serve only their surrounding residential subdivisions, and designed to fit in with their surroundings.

The Housing Element makes the following statements that relate to the project or the overall area.

- A. A variety of housing opportunities within the established urban areas and within developing areas would encourage some of the higher income residents to reside there and thus encourage mixing of different income levels. Therefore, single-family

housing should remain an ingredient of the urban areas instead of being targeted only for rural or suburban areas. Large, left-over parcels in urban service areas are well suited for planned residential development that includes high density housing. However, established single-family housing areas in urban settings should not experience an influx of multi-family or duplex construction unless adequate buffering or proper development design can be provided ("Population Needs," pg. 79).

- B. Large lot areas, including Union, will see multi-family construction, although this higher density development should occur close to highway arterials and urban services and be sensitively developed in terms of building height, setbacks, mass, and visual impact. Throughout Boone County, high density developments should be close to thoroughfares to achieve a gradation of building site dimensions and land uses outlined in the Future Land Use Plan.

Multi-family housing developments should have convenient access to commercial districts or should provide their own supporting commercial uses. The developments should be designed to offer the shortest trips to the most people. High density residential developments help to hold down the cost of individual units due to many variables including lower land cost per unit, lower land development cost and building unit costs per unit. Clustering of the dwelling units allows this by requiring less site work and utility construction. Better design can correlate with more density, and walkable, multi-modal communities. This also provides a supply of affordable housing for county residents ("Housing Types," pp. 79 and 80).

- C. It is important to offer a variety of housing units, not only in terms of economics, but also because of the county's demographics. As stated previously, the youthful population, divorcees, and elderly often select a multi-family unit or manufactured home to satisfy their housing needs. In the case of the young, this housing provides their first independent housing unit. In order for Boone County to retain its youth, the county must provide both jobs and housing, otherwise, an out-migration of the young and educated population will occur. Divorcees often select multi-family housing immediately following a separation. Elderly select these units for reasons usually related to economics and the need for a secure environment near their daily needs. The variety of multi-family housing includes nursing homes, retirement housing, apartments, townhouses, and condominiums all ranging greatly in price per unit. These types of units are increasing in demand outside of the established urban areas. As the county's population ages and land values rise, the need for higher density residential and planned unit developments will likely increase ("Housing Types," pg. 80).

- D. High density residential areas should be located sufficiently near and with convenient access to major streets, highways, and shopping and public facilities. A progression of densities of residential uses from high (multi-family) to low (single family) shall be encouraged. Where traditional progressions of high to low net density are not possible through creative development design, an appropriate and attractive visual transition should be achieved. This could include existing vegetation or new landscaping and/or fencing. Existing vegetation (such as wooded fence rows) should be retained as much as possible to provide buffer strips and stormwater filters. This serves a dual purpose in that woodland is becoming relatively scarce in the eastern uplands of the county, and these areas buffer different land uses. Housing that is proposed on smaller, remnant tracts needs to be carefully designed.

Housing development density has become an issue of concern to many existing residents in established, large lot subdivisions in suburban Boone County. Many of these areas developed during the 1960's to the 1980's when land prices were relatively low, few utilities were present, the surrounding land was agricultural, and urban/suburban development had not yet extended out to these areas. Many of these older subdivisions contain two-acre lots or larger. By contrast, most new subdivisions in Boone County are developing at around three units per acre and sponsor sewer construction if sanitary sewer is not currently available to the site. The issue arises when these new higher density subdivisions are proposed near established low density areas and centers mainly around development impacts on infrastructure and the residential character of the area ("Housing Densities," pg. 81).

- E. North Bend Road has been upgraded to better serve development in that corridor, including the continuation of Thornwilde and the new Rivers Pointe Estates. The flat North Bend Bottom area and other river bottoms should be reserved primarily for their present agriculture, or for appropriate residential, recreational, or employment uses that further enhance the riverfront resource without placing development in flood prone areas. Hillside development guidelines should be followed in this area to properly locate structures and avoid complete clearing of natural vegetation along the tops of hillsides so that development does not overwhelm the public view. The Williams Road area should be evaluated in the Land Use Element for future residential growth, if infrastructure can be provided ("North River/I-275 Corridor Area," pp. 83 and 84).

The "Summary of the 2006 Boone County Transportation Plan Recommendations" section in the Transportation Element lists "I-275 Interchange at Graves/Williams Rd" and "Graves Rd improvements" as Recommended Capacity Projects that are in the general area (pg. 147). The Transportation Element also provides a description of the "Northeast Boone County Major Investment Study" (pp. 156 and 157). "The widening of North Bend Road (KY

237) north of I-275" was a "prioritized" recommendation in this Study.

The 2010 Boone County Comprehensive Plan Goals and Objectives include the following pertinent statements.

- A. Proper planning is achieved through accepted land use management tools and coordination with public infrastructure ("Overall," Goal).
- B. Development issues shall be viewed in terms of promoting overall quality of life. Mixing of residential and other land uses shall be encouraged within appropriately planned and designed neighborhood developments ("Overall," Objective 2).
- C. Proper design principles shall be applied in development ("Overall," Objective 3).
- D. Future growth shall be accompanied by adequate infrastructure and services. Existing infrastructure and services shall be monitored, maintained, and improved as needed ("Overall," Objective 4).
- E. Boone County shall strive to achieve both a diversity and balance in land use. Land uses and zoning decisions shall strive to balance the rights of landowners with the rights of the general public in a fair and impartial manner ("Overall," Objective 6).
- F. The needs of Boone County's population are measured and fulfilled through the provision of orderly growth ("Population," Goal).
- G. New development or redevelopment shall attempt to design sites utilizing existing topography and vegetation, and encourage preservation of the existing character of the land. Development design should minimize grading work as opposed to clearing the entire site. Existing trees and pasture land shall be evaluated during the design and construction process as a visual and economic benefit to the community ("Environment," Objective 2).
- H. Existing vegetation shall be considered as both an important site characteristic and a community resource ("Environment," Objective 9).
- I. Boone County businesses are developed in appropriate locations and are compatible with their surrounding areas ("Business Activity," Goal).
- J. Compact, efficient development patterns shall be encouraged for business districts (e.g. industrial, commercial and office) with appropriately sized and well maintained buffer spaces between the business use and other land usage ("Business Activity," Objective).

- K. Commercial uses shall be designed and located to coordinate with the surrounding land uses and shall have safe and effective access and ample parking spaces ("Business Activity," Commercial [Retail and Office] Objective 1).
- L. The large scale mixing of commercial and non-commercial uses shall occur in planned developments or in areas where consideration has been given to assure compatibility with surrounding land uses. The location of large scale commercial uses shall take into consideration existing or planned infrastructure conditions so as to protect the existing and future population and businesses. Small scale mixing of commercial uses within residential areas shall be carefully assessed and properly placed so as to enhance the neighborhood. Architectural Design Review and consistent sign standards shall be implemented on a corridor by corridor basis, when justified by legitimate land use planning principles ("Business Activity," Commercial [Retail and Office] Objective 2).
- M. A broad range of housing opportunities shall be provided which meet the needs and desires for all household types ("Housing," Objective 1).
- N. Housing supply in Boone County shall be balanced against present and planned commercial, industrial, and education needs. Primary and Secondary education capacity shall be evaluated during housing development review ("Housing," Objective 4).
- O. In order to offer the citizens of Boone County maximum choice in living environment, residential developments shall be judged primarily on the impact on infrastructure, buffering, and development design with only secondary consideration given to the type of dwelling units ("Housing," Objective 5).
- P. The interior street system of new residential developments shall insure continuity with adjacent built or planned neighboring areas. Residential developments shall be encouraged to provide interconnections between sections of their developments and with adjacent developments to promote safe and easy transportation access and a sense of neighborhood interaction. Based on certified traffic analysis, collector and sub-collector streets shall be evaluated to determine design without individual lot or driveway access. Lot and home buyers shall have access to approved plans regarding future connections. The interior street system shall also coordinate with and continue the evolving multi-modal transportation system (pedestrian and bicycle facilities, mass transit stops) ("Housing," Objective 8).
- M. New or redeveloped residential areas shall be designed to establish clearly defined neighborhoods with a mix of retail, public, and recreational uses. Established neighborhoods shall be protected and enhanced, and deteriorated neighborhoods

shall be regenerated ("Housing," Objective 9).

- Q. Mixed use, higher density neighborhoods shall be encouraged near existing or proposed mass transit areas, commercial districts, and public facilities (i.e. parks) and also have convenient access to major streets and highways ("Housing," Objective 10).
- R. Residential developments shall be encouraged to plan and build in a manner which is compatible not only with the general housing density and design planned for the area but also with the existing conditions of the site, including the suitability of adjoining lands for appropriate access, and significant site features ("Housing," Objective 12).
- S. Traditional and innovative design as well as mixed use options shall be encouraged throughout Boone County in order to accommodate market demands ("Housing," Objective 14).
- T. Safe, efficient and environmentally sound public services and facilities shall exist for all development ("Public Services and Facilities," Goal).
- U. Mixed use, higher density neighborhoods shall be designed to accommodate safe pedestrian access to mass transit, major streets and highways, public facilities, and parks ("Transportation," 1st Goal, Objective 9).
- V. The local transportation system in Boone County shall be maintained and improved so that overall safety and level of service will be enhanced. Inefficiencies in the system will be identified and addressed. New land development shall be closely coordinated with the local transportation system to assure efficiency and continuity with the regional multi-modal transportation system ("Transportation," 2nd Goal).
- W. New connector streets shall be developed where needed and feasible thus lessening the total dependence on arterial, collector, and local streets. Existing connector streets shall be improved where needed ("Transportation," 2nd Goal, Objective 2).
- X. Proper vehicular and pedestrian access to adjoining property shall be provided and/or retained when a property develops or redevelops ("Transportation," 2nd Goal, Objective 3).
- Y. Developers of planned, mixed-use neighborhoods shall design such projects so that residential, retail, office, and compatible light-industrial land uses can be served by transit stations or stops, pedestrian and bicycle facilities, open space, and public uses ("Transportation," 3rd Goal, Objective 1).

- Z. Redevelopable and infill sites shall be encouraged to develop as walkable, mixed-use districts ("Transportation," 3rd Goal, Objective 3).

BOONE COUNTY TRANSPORTATION PLAN 2030

- A. North Bend Road is illustrated as a "Six Year Highway Plan Project" on the "Operational Improvement Plan Project Locations Map" (Exhibit 5-2, pg. 5-4).
- B. Exhibit 6-1 (pg. 6-2) lists the following "Recommended Projects" that are in the general area.

Map ID 6 - New I-275 Interchange: "New I-275 interchange in vicinity of Graves Road/Williams Road/Watts Road." This is listed as a high priority project (less than 10 years) in Exhibit 6-13 "Recommended Transportation Plan Project Prioritization" (pg. 6-20).

Map ID 7 - Graves Road: "Realign with Bullittsville Road and widen/improve from KY 20 Petersburg Road to KY 237 North Bend Road." This is listed as a medium priority project (11 - 20 years) in Exhibit 6-13 "Recommended Transportation Plan Project Prioritization" (pg. 6-20).

- C. Exhibit 6-10 (pg. 6-14) lists the "Year 2030 Recommended Transportation Plan Levels of Service - Two Lane State Routes" for the segments of North Bend Road between Graves Road and Sand Run Road, and between Sand Run Road and KY 8, as LOS C.
- D. Exhibit 6-18 "Multi-Use Trail Network" (pg. 6-29) illustrates a "planned" trail along the section of North Bend Road which adjoins the subject site.

STAFF COMMENTS

1. COMPREHENSIVE PLAN

The governing bodies need to determine whether the proposal is in agreement with the 2010 Boone County Comprehensive Plan. The Comprehensive Plan's Future Land Use Map designates the site as Commercial (approximately 23 percent), Suburban Residential (approximately 53 percent), and Developmentally Sensitive (approximately 23 percent) (figures add to 99% due to rounding). The Developmentally Sensitive designation does not prohibit development, nor like the other designations does it outline specific contemplated land uses. Rather, the somewhat lengthy description (outlined previously) includes the key phrase "any

development on land identified as Developmentally Sensitive must be carefully assessed by the developer and the Planning Commission to determine the ability of the land to support the proposed project," meaning that each site and proposal

needs to be evaluated on its own merits. It also states "the land use priority for areas designated Developmentally Sensitive is preservation of the existing environment, as opposed to development of the land."

The Suburban Residential designation which applies to approximately one half of the site is described as "single family housing of up to four units per acre. This classification does not preclude low density or estate residential developed as a formal subdivision." The proposal includes single family attached and/or multi-family dwelling units at a density of 13.88 units per acre for this specific site, based on 120 units on 8.648 acres (per the attached "Flexibility Narrative" provided by the applicant, these figures are potentially higher). The residential density for the overall Thornwilde Subdivision would increase to 2.1 units per acre. Standard #1 in Section 1607 "Land Use Standards" of the Boone County Zoning Regulations which applies to EPD/RPD zone proposals states that a planned development "shall generally conform" to the Comprehensive Plan, and that the intensity "should be generally consistent with the Future Land Use Map designations for the area." This standard also states that "deviation from the Comprehensive Plan is dependent upon whether the site is part of an urban service area for public infrastructure and upon the provision of public facilities, services and roads by the applicant to mitigate the impact of the proposed development on these infrastructure systems." Input from Sanitation District #1 regarding the current lack of sanitary sewer capacity is outlined below under "Section 1607 Land Use Standards," Subsection 5.

Similar language is also provided in Section 1605 "Maximum Intensity and Density" of the zoning regulations. Additionally, Section 1605 states that the maximum building intensity for commercial uses shall "generally" be a maximum of 15,000 square feet per acre, and that "greater intensities than these can be permitted if multi-story buildings and multi-level parking are utilized and the amount of green space can be shown to increase. Necessary infrastructure, including fire protection must be adequately provided."

The applicable Land Use Element text ("17. North Hebron," pg. 180) does not discuss this specific site, but it does discuss the overall area. It generally recommends Suburban Residential uses, creating "open space between future developments" by maintaining wooded areas on "Developmentally Sensitive and scenic hillsides," and maintaining "stream valleys and ridge bluff edges." It also notes that "with planned public sanitary sewer improvements in the Sand Run Creek and Elijah Creek watersheds, including sewer lift stations, there will continue to be

considerable growth pressures,” and that “ unlike most developing areas of Boone County, the north river area contains only one primary means of access, being North Bend Road.”

Regarding commercial development in the area, the Business Activity Element (“Recommended Areas of Commercial Activity,” pp. 69 and 70) states “the Hebron area will experience additional commercial development along KY 237 and KY 20 as the northern area increases in population. The existing commercial center along KY 20, just east of KY 237, will probably not expand significantly because the new highways, I-275 and KY 237, have bypassed it. Commercial development in Hebron will center around the intersection of new KY 237 and KY 20 with a limited amount of convenience and neighborhood commercial uses north of the I-275 interchange to serve the SouthPark Industrial Park, Park West Industrial Park, and the growing number of residential subdivisions along North Bend Road.” It also states “with the amount of residential construction expected in the North River area, larger scale commercial developments, such as drug stores and grocery stores will be needed. Commercial development in this area should be designed to serve the northern part of Boone County. This commercial growth should coordinate with the existing and proposed industrial and residential developments, especially in terms of access.”

Regarding Thornwilde Subdivision specifically, the Housing Element (“North River/I-275 Corridor Area,” pp. 83 and 84) states “North Bend Road has been upgraded to better serve development in that corridor, including the continuation of Thornwilde and the new Rivers Pointe Estates.” The Housing Element also makes the following statements which pertain to multi-family and attached housing in a more general sense.

- A. Large, left-over parcels in urban service areas are well suited for planned residential development that includes high density housing. However, established single-family housing areas in urban settings should not experience an influx of multi-family or duplex construction unless adequate buffering or proper development design can be provided (“Population Needs,” pg. 79).
- B. Throughout Boone County, high density developments should be close to thoroughfares to achieve a gradation of building site dimensions and land uses outlined in the Future Land Use Plan (“Housing Types,” pp. 79 and 80).
- C. Multi-family housing developments should have convenient access to commercial districts or should provide their own supporting commercial uses. The developments should be designed to offer the shortest trips to the most people. High density residential developments help to hold down the cost of

individual units due to many variables including lower land cost per unit, lower land development cost and building unit costs per unit. Clustering of the dwelling units allows this by requiring less site work and utility construction. Better design can correlate with more density, and walkable, multi-modal

communities. This also provides a supply of affordable housing for county residents ("Housing Types," pp. 79 and 80).

- D. As the county's population ages and land values rise, the need for higher density residential and planned unit developments will likely increase ("Housing Types," pg. 80)
- E. High density residential areas should be located sufficiently near and with convenient access to major streets, highways, and shopping and public facilities. A progression of densities of residential uses from high (multi-family) to low (single family) shall be encouraged. Where traditional progressions of high to low net density are not possible through creative development design, an appropriate and attractive visual transition should be achieved. This could include existing vegetation or new landscaping and/or fencing. Existing vegetation (such as wooded fence rows) should be retained as much as possible to provide buffer strips and stormwater filters. This serves a dual purpose in that woodland is becoming relatively scarce in the eastern uplands of the county, and these areas buffer different land uses. Housing that is proposed on smaller, remnant tracts needs to be carefully designed ("Housing Densities," pg. 81).

Several of these statements are reinforced by the Comprehensive Plan's Goals and Objectives which are outlined in the body of this report, such as advocating opportunities for varying types of dwelling units. Proper buffering and the retention of existing vegetation are also mentioned in the Future Land Use Development Guidelines, Housing Element, and Goals and Objectives.

Appropriate transitional uses or progression of densities are recommended by the Housing Element ("Housing Densities," pg. 81) and the Future Land Use Development Guidelines ("Buffering," pp. 162 and 163). The transitional use concept is also described in the "Development Layout, Lot Sizes, and Setbacks" section of the Future Land Use Development Guidelines (pg. 162).

2. RPD ZONE STANDARDS

Concept Development Plan proposals in the RPD zone are to be assessed against the standards in Sections 1607, 1608, and 1609 of the Boone County Zoning

Regulations (full text is attached). Section 1610 "Findings Necessary For A Zoning Map Amendment Or Concept Development Plan In An EPD Or RPD District" states "each application shall conform to the criteria in Section 308 of the Boone County Zoning Regulations in presenting the basis for granting a Zoning Map Amendment or approval of a Concept Development Plan in an EPD or RPD district. In addition,

each application shall be in conformance with the standards and requirements stated in this Article."

Staff offers the following comments regarding the standards in Sections 1607, 1608, and 1609.

Section 1607 Land Use Standards

1. This standard states that the type, character, and intensity of uses shall generally conform with the Comprehensive Plan. The Comprehensive Plan and how this standard relates to the proposed residential density are discussed in depth above.

Condition #5.e of the 1994 Concept Development Plan approval (3/16/94 Committee Report) states that "the proposed commercial development shall be limited to Commercial One (C-1) uses." The applicant should clarify whether any changes to the uses permitted by this condition are intended. Other than seating for a potential eating and drinking establishment(s) and play area for the day care center, no outdoor commercial activities such as fuel sales or display of goods are indicated on the graphic Concept Development Plan. The entirety of condition #5 of the 1994 Concept Development Plan approval outlines various stipulations which apply to the commercial phase. The current zoning regulations handle several of the topics mentioned in condition #5 in a manner more detailed than the condition.

As part of the discussion regarding consistency with the Future Land Use Map, this standard states "residential densities, however, shall be concentrated on portions of the site to leave open space on other portions. When applicable, the design mechanisms recommended in the 'Development Layout, Lot Sizes, and Setbacks' section of the Comprehensive Plan's Land Use Element may be employed." Open space is proposed along the north and west boundaries, with the majority of this space in the west part of the site in the creek valley. The applicant should provide a break down of the areas that will be designated as permanent open space, outline the amount of open space, any improvements or amenities which are planned for the open space (paths, play equipment, shelter, etc), and whether the existing tree cover will be retained. Healthy, mature vegetation which is retained can be credited towards landscaping requirements. As a point of reference, conditions #4 and #7 of the 1994 Concept Development Plan approval pertain to retention of existing tree cover, grading limits, and open space.

If the existing vegetation is not retained along the north property line adjoining the active development area, Staff recommends that berming be added to provide more height and substance to the buffering for the adjoining property. The parking area at the front of the development is shown approximately 25 feet from the north

property line, and the closest residential building is shown approximately 30 feet from this property line. Based on the number of proposed dwelling units, the normal landscape requirement along the north property line is Buffer Yard C per Article 36 of the zoning regulations (60' wide area w/ specified plantings or 30' wide area w/ specified plantings and a 6' high berm, fence, or masonry wall).

The "Development Layout, Lot Sizes, and Setbacks" section of the Comprehensive Plan's Land Use Element (pg. 162) includes language that correlates to the proposal, although it assumes a larger development than just this specific 8.648 site. This section states "business, commercial, or industrial parks should be designed and developed to allow future residential or other uses to access through them where the corridor may extend to large developable tracts or may provide a future road connection. A roadway that is suitable for mixed use development should serve side streets for businesses rather than serving individual businesses directly, include an urban curb and gutter design, and provide pedestrian amenities and green space along the right-of-way. In this way, future residential development could be developed beyond the initial development, but be readily marketed." For the main street loop through the development ("Village Drive"), the applicant should explain the intended street and sidewalk section so that conformance with this language can be determined. Because Village Drive is the main route through the development with buildings immediately along the entire alignment, Staff recommends that sidewalks be provided on both sides (note - they would be required on both sides regardless if it were a public street).

2. This standard states that the development shall efficiently utilize the site, incorporate existing natural features into the site design, and include natural areas and Developmentally Sensitive areas identified on the Future Land Use Map as open space for recreational or common use. The Concept Development Plan shows "efficient" use of the site based on the type of project that is proposed. The active development is clustered in the east and central parts of the site in a dense "village" arrangement. The one "scenic asset" or "natural feature" of the types mentioned in this standard is the wooded hillside, which would necessitate substantial grading and removal of vegetation if it were to be developed for most typical projects. The configuration of the open space on the Concept Development Plan correlates to the Developmentally Sensitive area delineated on the Future Land Use Map. As outlined under #1 above, details regarding the open space still need to be provided by the applicant.

3. This standard states in part that “where appropriate, the planned development shall include developable land that is unbuilt and be available for purchase or donation to public agencies for public recreation or public facility building sites.” The proposed development site is small and does not have any unbuilt areas or parts that

are worthwhile for most conventional types of public facilities. A portion of the overall Thornwilde Subdivision development has already been conveyed to the school district for a public elementary school (Thornwilde Elementary School).

4. This standard states “the planned development shall not hinder nor deter development of surrounding undeveloped properties in accordance with the adopted Comprehensive Plan.” This proposal will not affect the ability of any adjoining property to develop. The area to the west in the creek valley is permanent open space that is owned by the Thornwilde Subdivision HOA (RPD zone). The approximate 3.9 acre site to the north (RSE zone) is designated as Suburban Residential in the south-central part of the tract and along the North Bend Road frontage, and as Developmentally Sensitive for the remainder (refer to attached Future Land Use Map excerpt). Staff’s only comment regarding the future development of the neighboring tract to the north is that the governing bodies should determine whether or not any type of vehicular connection between the two properties is warranted (driveway connection, emergency access, etc.).

5. This standard states that the development shall be located in an area where adequate transportation, public services, utilities, and facilities are or will be available. The project narrative acknowledges current limitations in the area-wide sanitary sewer system. Joe Verst, P.E., Project Engineer, Planning & Design Group for Sanitation District #1, has provided two e-mails regarding the lack of sanitary sewer capacity (6/25/14 e-mail thread is attached). The first e-mail states:

The flow from this development would go by gravity directly to the Sand Run pump station, which is out of capacity. If submitted to SD1, we would deny capacity to this development and we would put them on a waiting list.

There are currently four other developments on the waiting list in front of Thornwilde Village (Treetops Phase 4, Treetops Phase 5, Dominion Trail at North Pointe Phase 2 & Breckenridge at North Point).

If capacity became available because of upgrades by SD1, capacity would be granted based on the order of the waiting list. Given SD1's current budget, there would likely not be any upgrades in this vicinity by SD1 in the near future and any upgrades would be at the developers cost.

The second e-mail states:

SD1 has also performed an extensive analysis on the entire area tributary (including 6 upstream pump stations) to the Sand Run Pump Station. In the process, SD1 has outlined construction phases that could take place that would allow additional capacity for various locations within the tributary area.

The Thornwilde Village falls within the Phase 1 upgrades area. By replacing 8" gravity sewer upstream (SE) of the Sand Run PS with 815 ft. of 36" sewer (inline EQ) and 885 ft. of 12" sewer, capacity for 181 people could be generated and be available for this site.

However, this would come at a construction cost of \$495,000. As mentioned earlier, this would be paid for by the developer.

Regarding other public utilities and services, written comments on the proposal were requested from Boone County Schools, Boone County Water District, and the Hebron Fire Protection District. Such comments have not been received as of this writing. Any written comments received from these agencies will be forwarded to the Zone Change Committee for review.

Regarding the street/drive network, the project narrative states that "Village Drive will strive to be a public dedicated street; however, depending on width and setback requirements it may have to be private." Village Drive is the one vehicular route in the development which will function largely like a conventional "street," whereas the remaining ones are private parking lot driveways. If the proposal is approved, the applicant should remove the existing intersection into the site from Thornwilde Drive (Sedgewicke Way) and restore the curb line along the north side of the street, as the proposed Village Drive/Thornwilde Drive intersection is further to the west. Written comments from the Boone County Public Works Department on these topics are expected to be available at the public hearing for this request.

Aside from whether or not Village Drive is publically dedicated, Staff agrees that based on the mixed-use village concept, and the direction from the "Development Layout, Lot Sizes, and Setbacks" section of the Comprehensive Plan's Land Use Element mentioned above, this street/drive should be designed with an urban section including box curb and gutter, and sidewalks on both sides. When considering the close proximity of several of the buildings to Village Drive, Staff recommends that integral curb/walks be provided, at least for the eastern part of development.

Laura T. Mitchell, P.E., Permits Supervisor for the Kentucky Transportation Cabinet, District 6, has commented that "the proposed entrance on KY 237 is at a location that is by permit meaning that we would entertain the option of a curb cut at that location. The approval of such curb cut would be contingent on the completion of a traffic study at the applicant's expense. We would look to the traffic study to determine required improvements to KY 237 before approving any permits" (6/16/14 e-mail is attached). Paragraph 12 "Traffic Impact" of the project narrative states that

the development is estimated to generate approximately 90 trips during peak traffic hours, although no documentation was provided to show how this figure was determined, or whether this was the AM or PM peak. Staff recommends that a basic traffic analysis which explains the peak hour traffic generation, turning movements, any level of service changes, and any necessary road improvements to mitigate or

accommodate the new development be provided for the Zone Change Committee to evaluate. The information required by the KTC for the North Bend Road access point could be covered in the analysis as well.

Section 1608 Compatibility Assessment Standards

This section states “the proposed uses and Concept Development Plan in each planned development district shall be assessed in terms of compatibility of the existing conditions of the site. Each applicant or developer is required to address the following issues in a written and/or graphic manner.” These issues are outlined as a set of nine topics that are to be explained in the Concept Development Plan. For the current proposal, the topics outlined in section 1608 are generally explained in the project narrative. Staff feedback on these issues is provided throughout the Staff Comments section of this report.

Section 1609 Design Standards

1. Height: The applicant has stated that both types of residential structures would be two or three stories high. The commercial buildings are anticipated to be one level, but could be multi-story. The existing residence to the immediate north is a one story ranch style house, and the existing residence to the south across Thornwilde Drive is a bi-level that is approximately one and half stories high at the highest point.

Provided adequate landscape buffering is provided, Staff is not opposed to the two to three story building height proposed for the residential structures. No specific height is proposed for the commercial structures. In order to relate to the tight-knit “village” concept proposed, yet moderate the apparent scale of the development from North Bend Road, Staff suggests limiting the height of the commercial buildings to two or three stories. The height approved for the commercial structures in the nearby Rivers Pointe Estates development is three stories.

2. Placement or Location of Buildings: The buildings and related vehicular areas are proposed to be placed in a cluster that is substantially in the central and east parts of the site. The building frontages are staggered from the adjoining public streets. Open space is proposed along the north property line, partly along the south property

line adjoining Thornwilde Drive, and in the west part of the site in/adjoining the creek valley. While asymmetric in configuration, the parking area along North Bend Road consists basically of just one driveway which is double loaded with parking spaces.

The basic arrangement is similar to the "Village Center" approved as part of the

nearby Rivers Pointe Estates, although that phase of the development is shown in more of a rectilinear grid form than the Thornwilde proposal. Mature landscaping on berming currently exists along the site's Thornwilde Drive frontage. This landscaping is part of the entry "statement" for the overall Thornwilde Subdivision and includes the curved brick entry monument at the intersection with North Bend Road. The applicant should explain whether this landscaping is proposed to be retained or explain any proposal to change or remove it. While Staff does not endorse the removal of this street frontage landscaping, if it were to be removed Staff would recommend that the 4,105 square foot commercial building and the southern-most 24 unit residential building be moved closer to Thornwilde Drive to create more of street-wall effect (with the vehicular areas placed away from Thornwilde Drive), with the addition of walk connections and pedestrian-oriented lighting and other street furniture.

3. Preliminary Building Design or Architecture: This standard states that large developments should be "designed in a compatible architectural style," "have a consistent design treatment on all facades," and "shall include traditional, regionally influenced architectural forms and allow variations within it." It also allows "contemporary or transitional styles of comparable quality" to be considered.

Regarding building design, the project narrative states "design elements, color and materials that will be used in the commercial design will be used in the attached/multi-family design. The following collection of pictures shows materials, quality and scale for the type of product that will be used as Thornwilde Village." Staff has no fundamental issues with the design concepts presented in the submitted photos, although there would need to be a formal mechanism to assure that these concepts were followed.

The nearby Rivers Pointe Estates development used a similar approach in presenting a collection of photographs to illustrate the intended architectural themes and quality level. To assure compliance with the representations made in the Concept Development Plan booklet, the following condition applied (#13 in the 9/1/10 Committee Report). A comparable condition could be considered for the current proposal as well.

The design related aspects of the development, including architectural design for the townhouses, multi-family buildings, and village center buildings, building orientation in the village center, signage, landscaping, site design details, and related matters, shall generally follow the text, photos, and graphic exhibits in the Concept Development Plan booklet. Compliance with this condition shall be determined by the Zoning Administrator, who shall review individual proposals in the development for consistency with the thematic and qualitative commitments and representations in the Concept Development Plan booklet.

4. Scale and Pedestrian Orientation: As noted before, the buildings are arranged in a tight cluster, mainly in close proximity to Village Drive. The residential buildings are multi-story, and the commercial buildings could potentially be multi-story which would help define a street-wall corridor. Street trees and the pedestrian amenities mentioned above (walks on both sides of Village Drive, on at least one side of the remaining drives, and along building frontages; street furniture such as benches and trash receptacles) would help further create a pedestrian environment. Section 3327 of the zoning regulations requires walks to be provided along building frontages, and walk connections to be provided between buildings, parking areas, and adjoining streets.

This standard also mentions the relationship of the proposed building designs to the creation of a pedestrian orientation. Based on the examples provided in the project narrative, Staff has no issue with the building concepts presented relative to this standard.

5. Elevation: As noted above, the site is hilly, particularly in the western part adjoining the HOA open space. The buildings and parking areas will essentially be terraced as the site slopes from North Bend Road towards the HOA open space to the west. The site at the concept level is conducive to this approach. Provided the overall site grading is designed to fit the buildings and parking areas into the topography versus on top of it, this standard will be met. This approach would also soften the view of the improvements from the site to the north and as seen from the south, especially if the existing berm and mature landscaping are kept along Thornwilde Drive.

6. Historic and Prehistoric Features: Staff is not aware of any historic or prehistoric features on this site.

7. Paved Surfaces: This standard discusses the interrelationship of open space, landscaping, and paved areas including sidewalks and the interior road system. The basic point of the standard is the amount of paving versus open space. As noted before, paved areas are generally provided in/around the building cluster, and open space is proposed along the north and west boundaries, with the majority of the space in the west part of the site in the creek valley. Additional information needed from the applicant regarding the open space is outlined above.

This standard also mentions the use of decorative pavement (such as for outdoor plazas, crosswalks, focal points, etc.) and the demarcation of the main entry points with decorative features such as landscaping, entry walls/columns, etc. The applicant should explain these aspects of the proposal so that conformance with this standard can be determined. These items can be included in the streetscape details.

8. Fences, Walls and Landscaped Berms: Berming is discussed above. No fences or walls are proposed. Staff has not identified any specific need for fences or walls unless they were part of the landscape buffer along the north property line in case the existing vegetation was substantially removed.

9. Landscaping: A specific landscape plan was not provided with the application. The previously mentioned landscape related topics include delineating the existing vegetation to be retained, the treatment of the buffer area along the north property line (partly dependent upon how much existing vegetation is retained), the intended treatment of the existing landscaping and related improvements along Thornwilde Drive, streetscape along Village Drive, and the treatment of the two main entry points into the development. Aside from these items, the development will need to meet the normal requirements of Article 36 of the zoning regulations.

10. Open Space and Recreational Uses: Open space and its potential treatment are discussed in several places above, including the additional details which need to be provided by the applicant. This standard outlines a number of ways that a proposed development can provide open space and other community amenities. No active facilities such as a pool, clubhouse, sports court, and the like are proposed, although "outdoor pedestrian gathering areas" are mentioned in the project narrative. Relative to the existing facilities in Thornwilde Subdivision, the project narrative states "since the site is distinctly separate from the remainder of the subdivision, the residents living at Thornwilde Village will not be a part of the HOA; therefore, no impact on any existing facilities." As reinforced by this standard, community facilities and amenities are intended to be a substantial part of an RPD zone development. The applicant should further explain how the proposal meets this requirement.

11. Signage: No specific signage program was provided in the Concept Development Plan materials. Relative to the base signage requirements in Article 34 "Signs" of the zoning regulations, the standards for either the Commercial One (C-1) District (Section 3413) or Small Community (SC) Overlay District and Walton Downtown (WD) District (Section 3450) would be appropriate for the commercial phase, with the SC/WD requirements being generally more restrictive. These requirements deal with subjects such as size, height, placement, and quantity of certain types of signs.

Regarding the cosmetic aspects of signage, a specific program or listing of the types or characteristics of permitted and prohibited signs should be provided for the Zone Change Committee to consider. As an example, the signage stipulations from the nearby Rivers Pointe Estates Concept Development Plan booklet are attached (pp. 38 and 53). While many of the specific provisions in the Rivers Pointe Estates development would not apply based on the size and other differences between the two projects, the format can be used as a model. The cosmetic aspects of signage

were included in the design issues condition for Rivers Pointe Estates discussed under #3 "Preliminary Building Design or Architecture" above.

12. Utilities: Public utilities are discussed above. The applicant should verify whether all utilities are planned to be underground.

13. Detention/Retention Ponds or Lakes: Storm water management was not outlined in the Concept Development Plan materials. The applicant should describe the intended storm water management facilities.

14. Transportation Design: Transportation, traffic, and sidewalk issues are discussed above. In addition, because Village Drive is intended to function as a street versus a parking lot driveway, Staff recommends that the two opposing, offset intersections in the center of the development (at the two 24 unit residential buildings, 8 unit residential building, and commercial building) be realigned into a four way intersection.

15. Multi-Modal Transportation System: Sidewalks/pedestrian paths are discussed above. Bike racks are required to be provided per section 3327 of the zoning regulations.

16. Location of Land Uses: The issues outlined in this standard are addressed above.

3. OTHER ISSUES


A. The acreage of the site needs to be confirmed. Based on the legal description provided for this application, the site contains 8.648 acres. The application form states that it contains "±" 8 acres. In contrast, the site contains approximately 12 acres based on varying figures in the tax record, and based on a drawn polygon in the Planning Commission's GIS system. If the project site is larger than noted in the application materials, the proposed residential density noted above for this tract will actually be lower.

- B. For the applicant's information, an alternate street name will need to be selected for "Village Drive." There is an existing street already in the community with this name.
- C. Also for the applicant's information, an exhaustive site plan type review was not conducted due to the conceptual nature and small scale of the plan. The project will still need to meet all applicable requirements at the site plan stage.

CONCLUSION

The Boone County Planning Commission and the Boone County Fiscal Court need to evaluate the application in terms of the requirements of Article 16 "Employment Planned Development District - Residential Planned Development District" of the Boone County Zoning Regulations, the 2010 Boone County Comprehensive Plan, and the potential impacts on the existing and planned uses in the area. The Future Land Use Map will need to be amended to reflect higher density attached/multi-family residential uses on the tract if this request is approved.

Respectfully submitted,



Kevin T. Wall, AICP
Director, Zoning Services

Attachments:

- Location map
- Aerial photograph with zoning
- 2035 Future Land Use Map excerpt
- Topographic map
- 1994 Concept Development Plan excerpt, 3/16/94 Committee Report, Planning Commission Resolution R-94-004-A, minutes from 5/17/94 Fiscal Court meeting
- Sections 1607, 1608, and 1609 of Boone County Zoning Regulations
- 6/25/14 e-mail thread from Joe Verst, P.E., Project Engineer, Planning & Design Group, Sanitation District #1

- 6/16/14 e-mail from Laura T. Mitchell, P.E., Permits Supervisor, Kentucky Transportation Cabinet, District 6
- "Signage" excerpts from Rivers Pointe Estates Concept Development Plan booklet (pp. 38 and 53)
- Application materials including project narrative and plan sheets
- written public comment received to date

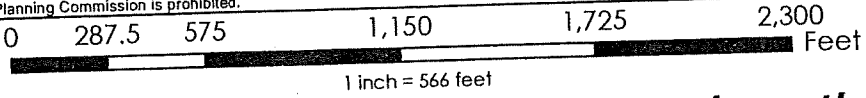
Location

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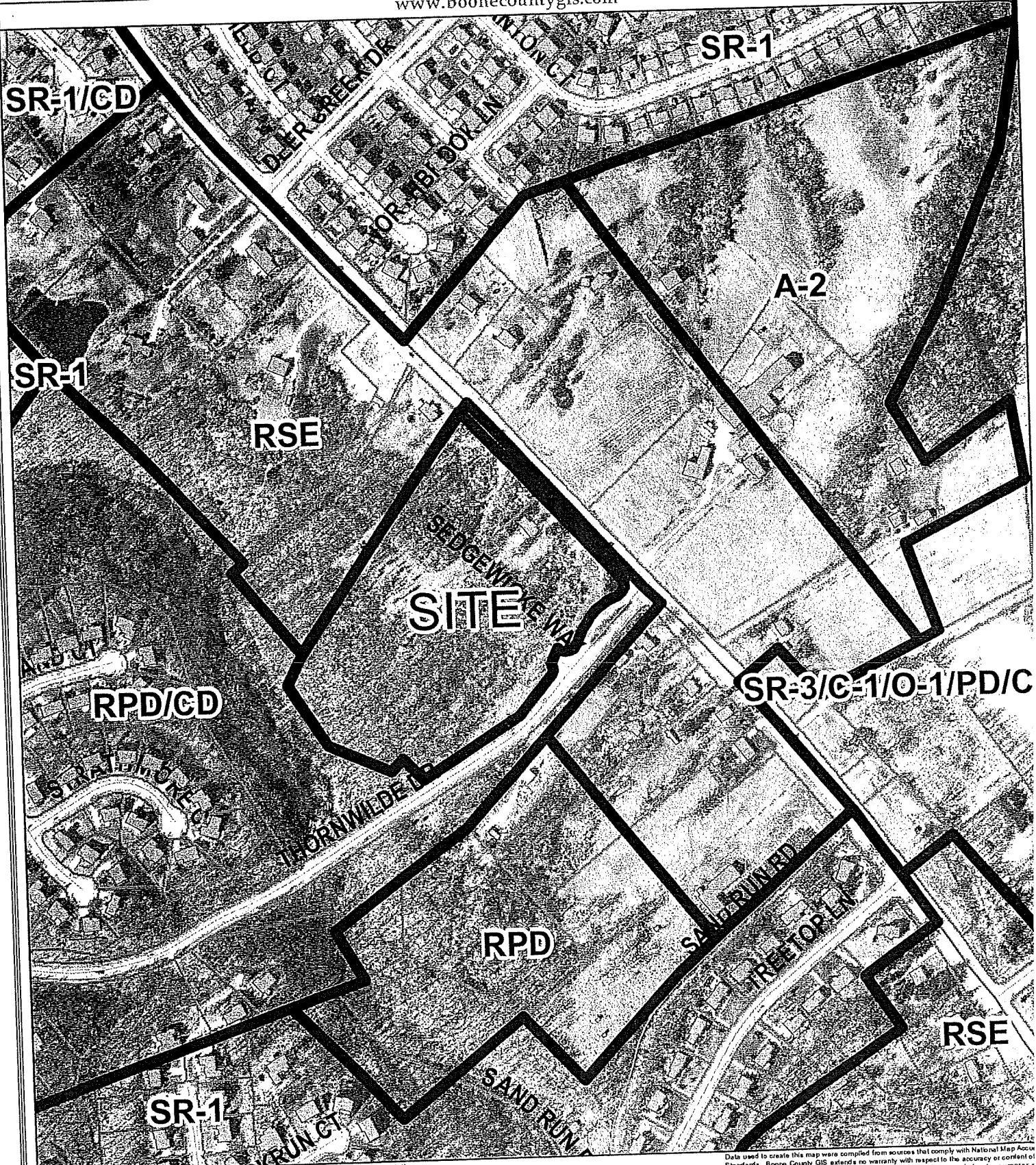
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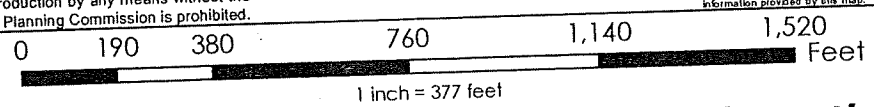
Zoning

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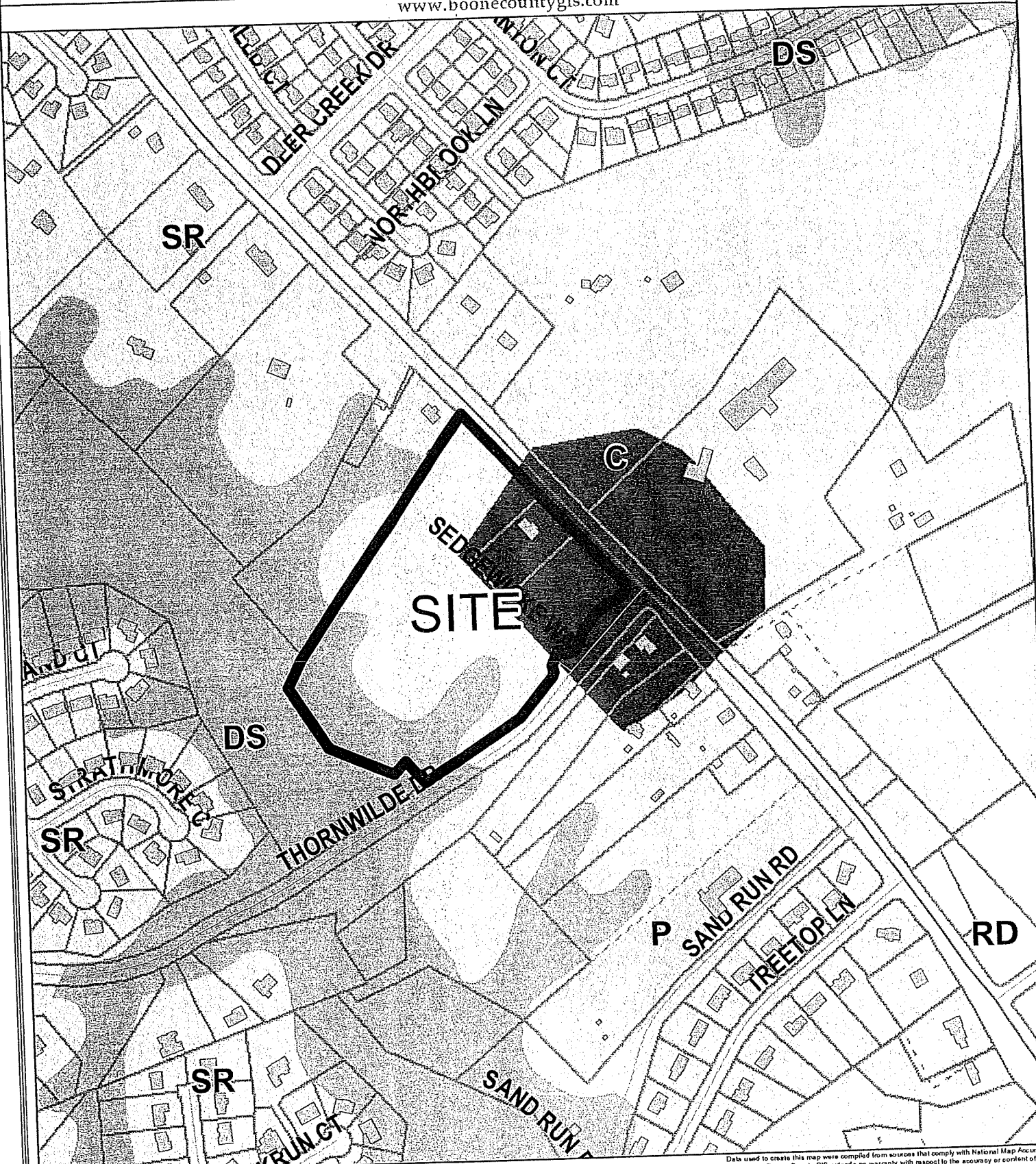
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Boone County GIS - Putting Northern Kentucky on the Map

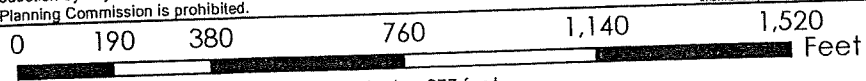
Future Land Use

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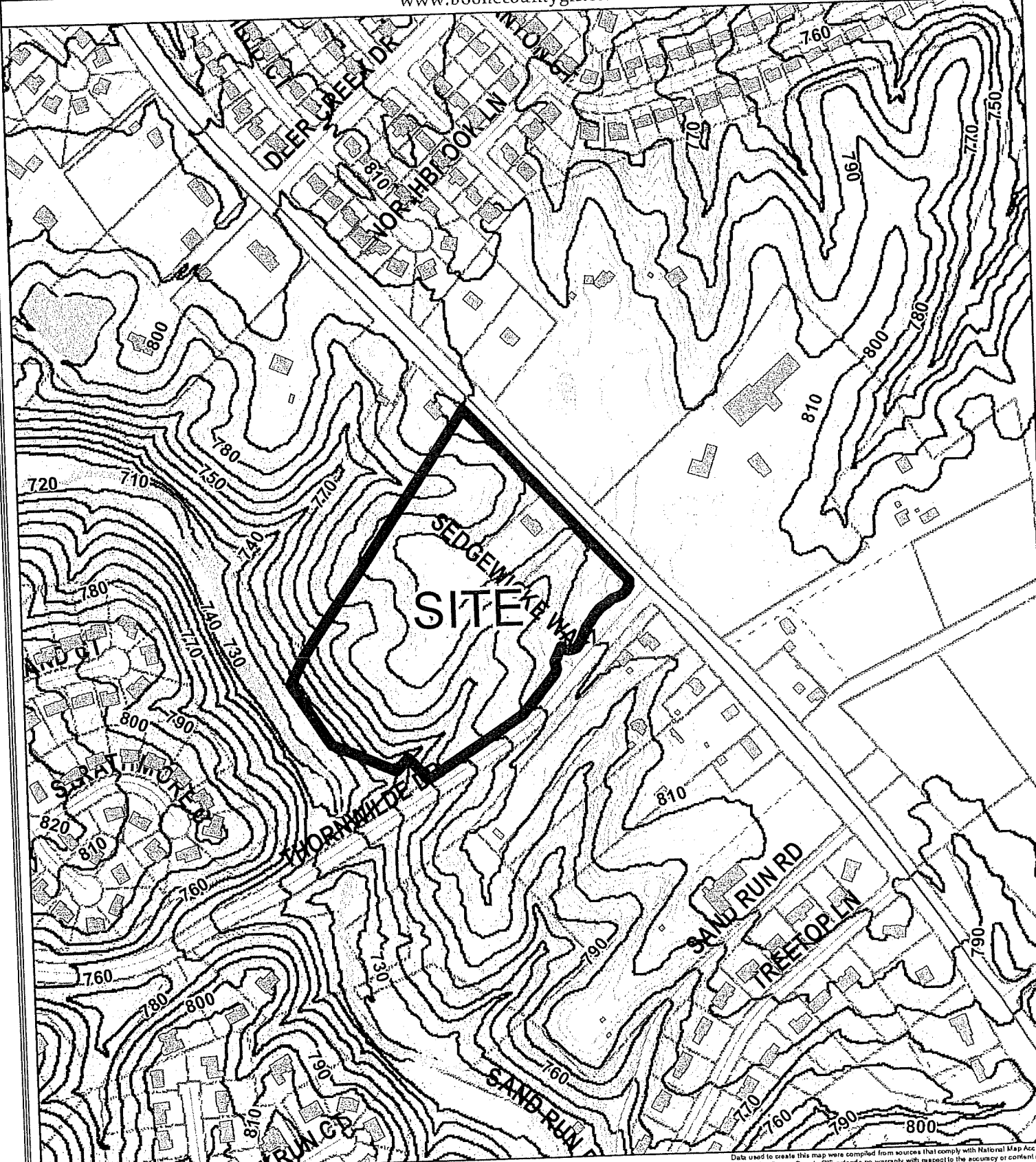


Boone County GIS - Putting Northern Kentucky on the Map



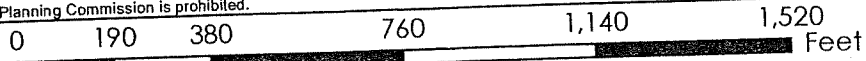
Topography

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Boone County GIS - Putting Northern Kentucky on the Map

MES E. HERFMANN
DB 424 Pg. 125

JOHN E. VENTKUS
DB 363 Pg. 67



ROBERT & CHARLENE CONRAD
DB 260 Pg. 51

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COMMITTEE REPORT

TO: Boone County Planning Commission

FROM: Barry Neltner, Chairman

DATE: March 16, 1994

RE: Request of The Drees Company (applicant) for Earl Souther and The Drees Company (owner) a Approval of a Concept Development Plan on a 217 acre parcel located on the west side of North Bend Road, Boone County Kentucky. The request is to allow Thornwilde Subdivision with carriage homes, single family homes, retail and community facilities in the Residential Planned Development Zone (RPD).

REMARKS:

We the Committee, recommend approval of the request based upon the following findings of fact and with the following conditions:

Findings of Fact

1. The request generally conforms to the adopted 1990 Boone County Comprehensive Plan, and specifically with the Future Land Use Map which indicates Suburban Density Residential for the applicant's site. The proposed densities are compatible with the Comprehensive Plan and are located on the site to preserve existing tree stands. The planned development generally conforms to the Comprehensive Plan with respect to type, character and intensity of use and impact on public facilities, and services. However, the Committee believes there will need to be a cooperative effort to improve the existing condition of North Bend Road.
2. The proposed development efficiently utilizes the site and protects the scenic assets and natural features by incorporating existing trees, streams and topographic features into the site design.
3. The planned development does not hinder nor deter development of surrounding undeveloped properties in accordance with the Comprehensive Plan.

Conditions

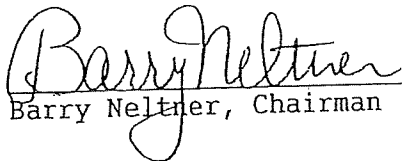
The applicant is being asked to agree to and include these items as part of the Concept Development Plan:

1. The applicant shall be required to install a northbound left turn lane on North Bend Road at the Thornwilde Drive intersection after 150 lots have been approved by the Boone County Planning Commission through the Improvement Plan Review process. This improvement shall meet the requirements of the Kentucky Transportation Cabinet and require their ultimate approval. The applicant shall also request that the Kentucky Department of Transportation Cabinet assess the present condition of North


- Bend Road to determine the realignment of the road at a later date, as well as the possible reduction of the speed limit. Understanding that there may not be sufficient right-of-way south of the project, the applicant will not be required to install the turn lane until the Boone County Fiscal Court or the Kentucky Department of Transportation Cabinet acquires the right-of-way or easement needed for the improvement.
2. The public dedicated section of Thornwilde Road shall be designed as indicated on the submitted Conceptual Plan with limited access.
 3. Detailed erosion control plans shall be submitted with the subdivision Improvement or Grading Plans for each phase of the development. These plans shall include detailed time schedules for grading work and re-establishment of ground cover on all graded areas. Additional erosion control measures may be required if deemed necessary during the review process to protect the creeks on the site.
 4. Every effort shall be made to retain as many existing trees as possible on the site. Therefore, grading limits shall be established during the Improvement or Grading Plan stage for each phase of the development. These grading limits will be used to identify tree cover that will be retained and enable measures to be taken to protect these trees during construction.
 5. The commercial development shall be evaluated during the Site Plan Review procedure to determine the potential visual impacts. This evaluation shall examine items such as berming, landscaping, lighting, types of materials used, building design and the configuration of the building as ways of mitigating potential visual impacts. This evaluation may require additional measures to be taken to mitigate potential visual impacts to the adjoining residential uses. However, the following will represent the minimum conditions that shall be applied to the commercial development:
 - a. Berming shall be provided at a height of six (6) feet along the south and north property lines of the subdivision which screen the commercial building from the adjoining residential homes. These berms shall also be planted to provide year round visual screening.
 - b. All lighting shall be installed so that it shines away from the existing residential properties, and the site shall not be illuminated at night other than security lighting.
 - c. Access to the commercial development shall be as indicated on the Conceptual Plan.

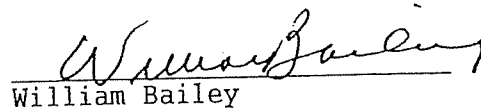
- d. A ten (10) feet landscaping strip shall be required along the North Bend Road frontage between the parking and the property line for the proposed commercial building. This landscaping strip shall be planted to reduce the view of the pavement and parking areas of this commercial development.
 - e. The proposed commercial development shall be limited to Commercial One (C-1) uses.
6. The section of Thornwilde Drive that provides access to the proposed Carriage Homes shall not be constructed beyond the first intersection as indicated on the conceptual plan until an emergency access or second means of access is constructed.
7. The areas which were indicated as open space on the conceptual plan shall remain undeveloped. The applicant shall also pursue negotiations with the County or other agencies/organizations regarding the donation of this property for passive recreation uses.

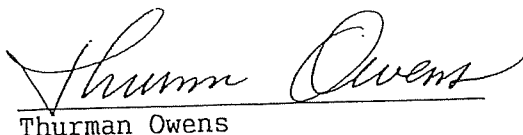
A copy of the Public Hearing minutes accompanies the findings and recommendation serving as a summary of the evidence and testimony presented by the proponents and opponents of this request.

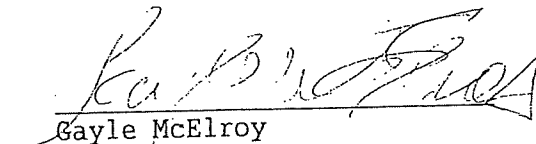

Barry Neltner, Chairman

Fred Burch


Phil Damstrom


William Bailey


Thurman Owens


Gayle McElroy

RESOLUTION R-94-004-A

A RESOLUTION OF THE BOONE COUNTY PLANNING COMMISSION RECOMMENDING APPROVAL FOR A REQUEST OF THE DREES COMPANY (APPLICANT) FOR EARL SOUTHER AND THE DREES COMPANY (OWNERS) FOR A CONCEPT DEVELOPMENT PLAN IN A RESIDENTIAL PLANNED DEVELOPMENT (RPD) ZONE ON A 217 ACRE SITE GENERALLY LOCATED ON THE WEST SIDE OF NORTH BEND ROAD, BOONE COUNTY, KENTUCKY, AND PROVIDING THE RECOMMENDATION BE FORWARDED TO BOONE COUNTY FISCAL COURT, BURLINGTON, KENTUCKY.

WHEREAS, the Boone County Planning Commission received a request for a Concept Development Plan in a Residential Planned Development (RPD) zone for a 217 acre site generally located on the west side of North Bend Road, Boone County, Kentucky, which is more particularly described below; and,

WHEREAS, the Boone County Planning Commission as the planning unit for the unincorporated Boone County, Kentucky, was requested to and has conducted a Public Hearing serving as a due process trial-type hearing and made findings recommending approval, with conditions, for a Concept Development Plan in an Residential Planned Development (RPD) zone for a 217 acre site generally located on the west side of North Bend Road, Boone County, Kentucky.

NOW, THEREFORE, BE IT RESOLVED BY THE BOONE COUNTY PLANNING COMMISSION AS FOLLOWS:

SECTION I

That the request for a Concept Development Plan for the real estate which is more particularly described below shall be and is hereby recommended for approval, with conditions, in a Residential Planned Development (RPD) zone for a 217 acre site generally located on the west side of North Bend Road, Boone County, Kentucky. The real estate which is the subject of this request for a Concept Development Plan in a Residential Planned Development (RPD) zone is more particularly described in DEED BOOKS 136, 456 and 466, PAGE NOS. 82, 190, 27 and 30, respectively (as supplied by the applicant) as recorded in the Boone County Clerk's office.

SECTION II

That as a basis for the recommendation of approval for a Concept Development Plan in a Residential Planned Development (RPD) zone on a 217 acre site, findings of fact and conditions of the Boone County Planning Commission as set forth in its minutes and official records for this request shall be and are hereby incorporated by reference as if fully set out in this Resolution and marked as "Exhibit A."

The Committee recommended approval for this request based on the findings of fact and conditions as set forth in the Committee Report and marked as "Exhibit B."

SECTION III

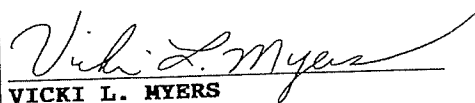
That a copy of this Resolution recommending approval for a Concept Development Plan in a Residential Planned Development (RPD) zone generally on the west side of North Bend Road, Boone County, Kentucky, shall be forwarded to Boone County Fiscal Court, Burlington, Kentucky, having jurisdiction over the property for its action on the recommendation of the Boone County Planning Commission.

PASSED AND APPROVED ON THIS 6TH DAY OF APRIL, 1994.

APPROVED:


BARRY NELTNER
TEMPORARY PRESIDING OFFICER

ATTEST:


VICKI L. MYERS
OFFICE MANAGER

BN:vlm

Industrial/Office Uses = 20,000 s.f./acre
Commercial Uses = 15,000 s.f./acre

Greater intensities than these can be permitted if multi-story buildings and multi-level parking are utilized and the amount of green space can be shown to increase. Necessary infrastructure, including fire protection must be adequately provided.

Generally, the development intensity permitted in Residential Planned Development districts shall be consistent with the Future Land Use Map of the adopted Boone County Comprehensive Plan. Proposed developments shall be planned in clusters and provide an innovative design as outlined in the objectives and standards of this article. Greater overall development intensity may be granted if the development includes dedication of appropriate developable land for public facilities and/or public recreation uses in addition to preserving developmentally sensitive areas as open space. Any increase in intensity over that recommended by the Comprehensive Plan must be shown by the applicant to be appropriate in terms of infrastructure, adjacent land uses and zoning, and provision of public land.

SECTION 1606

Pre-Application And Procedure

The developer shall meet with the Boone County Planning Commission staff prior to the submission of a Concept Development Plan. The purpose of the meeting will be to discuss early and informally the purpose and effect of this order and the criteria and standards which may apply, and to familiarize the developer with the detailed objectives of the Comprehensive Plan and its elements, this order and this article.

Submittal and Contents of Application for Approval of a Zoning Map Amendment to EPD or RPD or Concept Development Plan in an existing RPD or EPD District.

The Planning Commission shall require that the Concept Development Plan and an application form be submitted for property located in a proposed EPD and RPD zone. The Concept Development Plan may be filed with the Planning Commission by the owner(s), owner by contract (option) or lessee of property for which the planned development is proposed. The contents of the Concept Development Plan submittal shall include the items listed in Section 303.

The Planning Commission and legislative body or Fiscal Court may recommend or approve the proposed Planned Development district subject to any conditions or qualifications found merited in the Concept Development Plan or other conditions on the site. The legislative body's or Fiscal Court's approval in principle of the Concept Development Plan shall constitute a commitment to the general layout of the project, the planned land use types and intensities, design treatments and other particulars as shown on the Concept Development Plan. Approval in principle shall not be construed to endorse engineering feasibility or plan elements that do not meet the development standards of this order or the Boone County Subdivision Regulations unless an exception is specifically granted through the review and approval of the Concept Development Plan. The Planning Commission may seek assistance in making its recommendation from any appropriate source.

Public Hearing by Planning Commission

Upon receipt of an application for approval of the planned development district request or Concept Development Plan, the Planning Commission shall hold at least one public hearing, within thirty (30) days of receipt of the application, after notice as required for amendment under Kentucky Revised Statutes, Chapter 424, as stated in Section 305 of this order.

SECTION 1607

Land Use Standards

No application or Concept Development Plan shall be approved for an Employment Planned Development district or a Residential Planned Development district under the provisions of this article unless each district and application fulfills the following standards.

1. The planned development shall generally conform to the adopted Comprehensive Plan with respect to type, character and intensity of use and impact on public facilities, services and road network. The

intensity of planned residential developments, in particular, should be generally consistent with the Future Land Use Map designations for the area. Residential densities, however, shall be concentrated on portions of the site to leave open space on other portions. When applicable, the design mechanisms recommended in the "Development Layout, Lot Sizes, and Setbacks" section of the Comprehensive Plan's Land Use Element may be employed. Deviation from the Comprehensive Plan is dependent upon whether the site is part of an urban service area for public infrastructure and upon the provision of public facilities, services and roads by the applicant to mitigate the impact of the proposed development on these infrastructure systems;

2. The planned development shall efficiently utilize the site, and shall protect and preserve the scenic assets and natural features by incorporating existing trees, streams, tree lines along property lines, cemeteries, and topographic features into the site design. The retention of such features may also fulfill portions of the Open Space and Recreational Uses and Landscaping requirements in Section 1609. The development shall include the provision that designated natural areas and developmentally sensitive areas as identified on the Future Land Use maps in the adopted Boone County Comprehensive Plan will constitute permanent open space or be dedicated to public or semi-public organizations for recreational or common use;
3. Where appropriate, the planned development shall include developable land that is unbuilt and be available for purchase or donation to public agencies for public recreation or public facility building sites. This provision of land can constitute one reason for increased development densities on portions of the site or even an increased overall development intensity in relation to the Boone County Comprehensive Plan, provided other public facilities and services are adequately provided;
4. The planned development shall not hinder nor deter development of surrounding undeveloped properties in accordance with the adopted Comprehensive Plan;
5. The planned development shall be located in an area in which transportation, police and fire protection, other public facilities and public utilities, including sanitary sewerage are provided, or will be available and adequate for the uses proposed. The applicant shall make provision for such facilities or utilities which are not presently available or shall construct such facilities for the eventual connection into a public system.

This flexible approach to planned development is intended to provide an opportunity and incentive to the developer to achieve excellence in physical, social and economic planning. To be granted either one of the two types of planned development zoning districts, the property owner or developer must demonstrate the achievement of the above objectives and standards. In addition, a property owner or developer's request for a planned development would be assessed in terms of its impact on the existing conditions of the surrounding land uses, utility systems, road network and public facilities and services. This demonstration must be in written and/or graphic form as part of the Concept Development Plan submittal. In summary, the property owner or developer is responsible to present why the subject plan is unique and requires special consideration that can only be fulfilled by the EPD or RPD zoning district. Equally as important is that the property in question could be unique with certain qualities and/or amenities that warrant the special consideration of the EPD or RPD district. Proposed developments that cannot be shown to meet these standards may be more appropriately developed under conventional zoning districts.

SECTION 1608

Compatibility Assessment Standards

The proposed uses and Concept Development Plan in each planned development district shall be assessed in terms of compatibility of the existing conditions of the site. Each applicant or developer is required to address the following issues in a written and/or graphic manner.

1. How does the proposed project affect the natural features of the site (e.g. soils, topography, drainage areas)?;
2. Describe in detail the existing road network (e.g. closest arterial road and/or interstate access) that would be used to travel to this proposed project. What is the condition of this road system? What is the

- expected impact of the proposed development on the public road system?;
3. Where is the closest public sanitary sewer line and water line, which could serve this site? What is the capacity of these lines and what would be needed to accommodate this development with existing lines?;
 4. Indicate the location of schools, which would serve the proposed development. What impact would this development have on the school system? Indicate the availability and adequacy of police and fire protection services;
 5. Demonstrate how the proposed project will impact vehicular traffic or parking in the area and at what level would traffic or parking conditions become unsafe;
 6. Demonstrate how the proposed project will impact the view for adjoining property owners;
 7. Demonstrate how the proposed project will impact storm water runoff;
 8. Demonstrate how the proposed project will impact the demand for additional governmental services. (e.g. fire protection, police protection, etc.);
 9. Demonstrate how the proposed project will affect the need for off-site public utilities.

SECTION 1609

Design Standards

The proposed uses and Concept Development Plan in each planned development district shall be assessed in terms of the design standards contained in this section. Each applicant or developer is required to demonstrate how the proposed Concept Development Plan fulfills the following design standards in a written and/or graphic manner. The Concept Development Plan shall fulfill the following criteria unless a portion of the criteria do not apply or relate, in whole or part, to a specific proposal. The examples listed in this section are for illustrative purposes. Specific solutions used to fulfill these criteria are contingent upon the size, scale, site conditions, design, uses and impacts of a proposed development.

1. Height: The height of any structure shall be compatible with the existing buildings in the area, and proposed structures within the project site, and meet airport clearance regulations.
2. Placement or location of buildings: Consideration should be given to the most appropriate location of buildings based upon access and direct visibility. The location of any building should not interrupt a continuous visual pattern from public view or a public street. Building location should be planned to accomplish a transition with the landscaped areas, pedestrian areas, and parking areas. Building setbacks can vary in order to allow appropriate building location. The setback on the perimeter of the planned development district shall be large enough to protect the privacy and amenity of adjoining uses both existing and anticipated.
3. Preliminary Building Design or Architecture: Building design should be designed in a compatible architectural style if part of a large commercial, residential, industrial or office center. Individual buildings not part of larger development should be compatible with surrounding structures. Sites which are subject to architectural requirements through adopted land use studies shall follow said requirements. All structures shall have a consistent design treatment on all facades. The unified architectural style or building design shall also apply to materials, colors, roof types and entrances, and shall include traditional, regionally influenced architectural forms and elements and shall allow variations within it. Traditional styles such as Georgian, Federal, I-House, Cape Cod, Craftsman, Tudor, Queen Anne, Italianate, early 20th century commercial structures, and local farm structures may be used as models. Contemporary or transitional styles of comparable quality may also be considered. The architectural theme shall also relate to existing structures on the project site and adjacent sites, especially if such existing structures are historic.

For attached or multi-family residential developments and commercial or office developments, the

predominant building materials shall have a solid appearance, such as could be achieved with brick, stone, and architectural grade cast concrete products designed to replicate natural materials. Roof designs shall have a finished appearance through the use of three dimensional pitched roof forms with architectural grade roofing and/or the use of defined parapets with cornice lines. Long building facades and roof planes shall be interrupted through the use of three dimensional jogs in the building footprint and secondary roof forms such as hips, dormers, and gables. Such buildings shall include architectural detailing for cosmetic enhancement, largely use natural colors, and use a consistent design treatment on all facades. The use of architectural guidelines or building prototypes is required for all multi-phased projects.

Developments should be mixed-use in character with multi-level buildings where commercial services are proposed. Walkability must be planned for when locating commercial and residential uses in proximity to each other. Office and residential uses are strongly advocated above commercial uses in business districts to decrease dependence on the automobile.

4. Scale and Pedestrian Orientation: Consideration shall be given to the scale of each building and architectural detail, which relates to the size of an individual and the natural environment. In general, planned developments shall have a pedestrian scale and orientation. In rendering this decision, emphasis should be given to building mass and how it relates to open space.

In general, planned developments shall have a pedestrian orientation, where it is possible to live, work, shop, and play in the same immediate vicinity without a required dependence on the automobile. This may be accomplished through the use of comprehensive pedestrian circulation networks including multi-purpose paths and walks along main routes and open spaces such as stream corridors, between major destinations within the development and adjoining areas, secondary walk connections to the multi-purpose paths, the creation of a designed pedestrian environment including street trees in addition to other required landscaping, decorative street lights and other street furniture, and seating areas, and the use of integral curb walks where appropriate along streets. Disruptions in major paths due to street and drive intersections shall be minimal. Additionally in commercial areas, pedestrian orientation can be accomplished by placing buildings in close proximity to the street with parking areas to the side and rear of buildings, mixing uses within the same multi-story buildings, building entrances directly facing streets with reduced setbacks, architectural design which employs display windows, projecting signs, and awnings at street level, and designed outdoor seating and gathering spaces at the street level. Each development proposal must demonstrate in detail how the project will be made walkable throughout.

5. Elevation: The elevation of any site or structure should be determined by the grade of any existing or proposed public road. Consideration should be given to the relationship between the width and height of the front, side and rear elevation of any building from a public road. Exposed foundations and building elevations from a public street should be minimized.
6. Historic and Prehistoric Features: Historic features on the project site shall be retained, utilized, and incorporated into the overall project design if physically and economically feasible.
7. Paved Surfaces: Emphasis should be placed on preserving at a reasonable rate the amount of open space. The interior road system, sidewalk system, and parking lot should be designed with this in mind. Paved surface refers to cement, brick pavement, asphalt, cobblestone, or other similar type surfaces. Parking areas should be treated with decorative elements, fence, or wall extensions, plantings, berms as other means so as to screen parking areas from public view or soften their visual impact. Large parking lots shall be designed with landscaped islands. Parking lots and driveway aisles should be either asphalt or concrete. Landscaped entrances with trees and bushes should be encouraged near the entrances of any commercial, retail or industrial development and any structures used to demarcate entry points shall visually correlate to the planned architectural theme by the use of consistent design details, materials, and colors.
8. Fences, Walls and Landscaped Berms: Consideration should be given to brick walls, fences and landscaped berms, which would produce a continuous cohesive wall of enclosure on a lot line or

adjacent to a road. These fences, walls and/or berms shall be designed and maintained in relationship with character of the surrounding land uses and structures.

9. Landscaping: Consideration should be given to the predominance of a quality and quantity of landscaping and an emphasis shall be given to streetscape areas, buffer zones, and the provision of significant landscaping (in terms of size of landscaped areas, and quantity and quality of landscape materials) within the developed portions of the site. Landscaping details will be evaluated on the entire site and surrounding any proposed structure and shall be reviewed in relation to adjacent properties. A special attempt should be made to preserve the existing vegetation and scenic areas, if any and encourage additional open space for the public's benefit. Existing topography, significant tree cover, and water courses and water bodies shall be largely preserved and incorporated into the project design, where appropriate and consistent with the remainder of this article. Plantings should be used to soften the impact of parking and loading areas. Properly designed street tree plantings may be permitted to fulfill some landscaping requirements as part of an overall amenity package. Plant material should be selected on the basis of texture and color and for its ultimate growth to provide a visual screen for the public. The use of landscape design guidelines is required for multi-phased projects.
10. Open Space and Recreational Uses: Residential Planned Developments shall make a provision for preserving open space and devoting an appropriate portion of the site to meet the recreational (either active or passive recreation) needs of the residential population at a minimum, and such open spaces are strongly encouraged in Employment Planned Developments. These spaces may be provided in the form of linear parks, parks, plazas, arcades, commons, trails, sports courts or other athletic and recreational areas, outdoor areas for the display of sculptural elements, etc. Land reservations for community facilities may be considered in lieu of useable open space. The use of single loaded streets to provide linear parks with multi-purpose paths, activity or scenic areas, or to protect stream corridors, is strongly advocated. Any site proposed to be publically dedicated or donated for park or open space purposes shall comply with the appropriate legislative body's requirements for acceptance of such dedications or donations.
11. Signage: Signage should be designed to protect the visual order of any site and to minimize the impact of adjacent properties. Consideration should be given to the number, location, size and height of any building mounted sign or free-standing sign. A consistent signage theme shall be provided within a planned development. Except on residential areas, building mounted signs shall be the predominate signage on the project site. Freestanding signs shall be monument style and of a limited size and height. Signage shall visually correlate to the planned architectural theme by the use of consistent design details, materials, and colors. The use of signage design guidelines is required for multi-phased projects.
12. Utilities: All utility service lines to the building and possible main lines to or within the site shall be located and installed underground. Alternatives may be possible if this requirement is economically infeasible.
13. Detention/Retention Ponds or Lakes: Large detention/retention ponds or lakes should be discouraged in the front yard or in an area subject to public view from a public street if not used both for stormwater management and aesthetic purposes. Smaller detention/retention ponds or lakes appropriately designed and landscaped should be encouraged and distributed throughout the site and may appear in the front yard or in an area subject to public view from a public street. Surface drainage and floodwater retention shall be planned to not adversely impact the adjoining properties.
14. Transportation Design: Principal vehicular access shall be from major streets and access points shall be designed to encourage smooth traffic flow with controlled turning movements and minimum hazards to vehicular or pedestrian traffic. Maximum separation of vehicular traffic on major roads from pedestrian and recreational areas shall occur. The provision of transportation connections (street connections, pedestrian paths, multi-purpose trails, and sidewalks, bicycle facilities) shall be provided in all planned developments unless physically unfeasible or unsafe. This shall include connections to adjoining properties and developments, and inter-connectivity within the development itself, and contain minimal use of cul-de-sacs or other dead-end types of streets only when necessary. Transportation connections shall account for the County's adopted Transportation Plan and any adopted greenway, trails, bikeway, and/or pedestrian plans.
15. Multi-Modal Transportation System: Planned developments shall incorporate multi-modal transportation

elements in the development, depending on the foreseeable needs of future residents and users of the site, and the relationship of the project site to the community at large. Such multi-modal elements may include provisions for mass transit stops or stations, car pooling lots, pedestrian and bicycle paths and lanes, bicycle parking areas, etc. Multi-modal facilities are encouraged to be combined with the pedestrian systems and open spaces described above.

16. Location of Land Uses: Land uses shall be located primarily for convenience to the service area and compatibility with the adjoining land uses. If conflicting land uses are intended to be located near each other, measures shall be taken to mitigate the impact of the uses upon each other.

SECTION 1610

Findings Necessary For A Zoning Map Amendment or Concept Development Plan Approval In An EPD Or RPD District

Each application shall conform to the criteria in Section 308 of the Boone County Zoning Regulations in presenting the basis for granting a Zoning Map Amendment or approval of a Concept Development Plan in an EPD or RPD district. In addition, each application shall be in conformance with the standards and requirements stated in this Article.

SECTION 1611

Action by Planning Commission and Legislative Unit

Within ninety (90) days of receipt of an application for a Concept Development Plan and after the public hearing required in this article has been conducted, the Planning Commission shall determine whether the Concept Development Plan is consistent with the intent, purpose, requirements and standards of this order; whether the proposed development advances the general welfare of the community and neighborhood, and whether the benefits, combination of various land uses, innovation of physical design, and the inter-relationship with the land uses in the surrounding area justify any deviation from standard district regulations as mentioned in the objectives section of this article. The Commission shall submit a recommendation to the legislative body or Fiscal Court for either approval or denial of the Concept Development Plan. The Planning Commission may seek assistance in making its recommendation from any appropriate source.

Final action for approval in principle, as described in Section 1606, or denial of an EPD or RPD zoning district or Concept Development Plan shall be made by the appropriate legislative body or Fiscal Court. After recommendation by the Planning Commission all materials constituting the record and recommendations of the Planning Commission shall be transmitted to the legislative body or Fiscal Court. Within ninety (90) days from the Commission's recommendation, the legislative body or Fiscal Court may take action to approve or deny the EPD or RPD zoning map designation or Concept Development Plan application. Approval shall be by ordinance. When an EPD or RPD zoning designation is approved, the subject property so zoned shall be respectively designated "EPD" or "RPD" on the Official Zoning Map. Upon approval of a Concept Development Plan to utilize either classification, the notation "CD" shall be placed on the Official Zoning Map. A Certificate of Land Use Restriction giving notice of the approval for the Concept Development Plan or EPD or RPD zoning district designation shall be filed within 30 days of final action as explained in Section 280. If disapproved, the legislative body or Fiscal Court shall state the reasons for disapproval.

SECTION 1612

Subsequent Reviews After Concept Development Plan Approval

After the appropriate legislative body or Fiscal Court has approved a Concept Development Plan in an EPD or RPD zoning District, the applicant or property owner may proceed through the subdivision review or site plan review approval process by the Boone County Planning Commission. Subdivision review requirements can be found in the Boone County Subdivision Regulations and site plan review requirements are outlined in the Boone County Zoning Regulations.

Kevin Wall

From: Verst, Joe <jverst@sd1.org>
Sent: Wednesday, June 25, 2014 10:24 AM
To: Kevin Wall
Cc: Greg Stacy; Greg Haggard
Subject: Re: Thornwilde Village concept development plan

Kevin,

SD1 has also performed an extensive analysis on the entire area tributary (including 6 upstream pump stations) to the Sand Run Pump Station. In the process, SD1 has outlined construction phases that could take place that would allow additional capacity for various locations within the tributary area.

The Thornwilde Village falls within the Phase 1 upgrades area. By replacing 8" gravity sewer upstream (SE) of the Sand Run PS with 815 ft. of 36" sewer (inline EQ) and 885 ft. of 12" sewer, capacity for 181 people could be generated and be available for this site.

However, this would come at a construction cost of \$495,000. As mentioned earlier, this would be paid for by the developer.

Regards,
Joe Verst, P.E.
Project Engineer
Planning & Design Group
SD1
1045 Eaton Drive
Ft. Wright, KY 41017
Telephone: 859-578-7467
Fax: 859-578-6897
Email: jverst@sd1.org

On Tue, Jun 24, 2014 at 4:54 PM, Verst, Joe <jverst@sd1.org> wrote:

> Kevin,
>
> The flow from this development would go by gravity directly to the
> Sand Run pump station, which is out of capacity. If submitted to SD1,
> we would deny capacity to this development and we would put them on a waiting list.
>
> There are currently four other developments on the waiting list in
> front of Thornwilde Village (Treetops Phase 4, Treetops Phase 5,
> Dominion Trail at North Pointe Phase 2 & Breckenridge at North Point).
>
> If capacity became available because of upgrades by SD1, capacity
> would be granted based on the order of the waiting list. Given SD1's
> current budget, there would likely not be any upgrades in this
> vicinity by SD1 in the near future and any upgrades would be at the developers cost.
>
> Regards,

>
>
>
> Joe Verst, P.E.
> Project Engineer
> Planning & Design Group
> SD1
> 1045 Eaton Drive
> Ft. Wright, KY 41017
> Telephone: 859-578-7467
> Fax: 859-578-6897
> Email: jverst@sd1.org
>
>
> On Tue, Jun 24, 2014 at 11:32 AM, Kevin Wall <KWall@boonecountyky.org>
> wrote:
>>
>> Thanks.
>>
>>
>>
>> From: Verst, Joe [<mailto:jverst@sd1.org>]
>> Sent: Tuesday, June 24, 2014 11:32 AM
>> To: Kevin Wall
>> Cc: Greg Stacy; Greg Haggard
>> Subject: Re: Thornwilde Village concept development plan
>>
>>
>>
>> Kevin,
>>
>>
>>
>> I will take a look and provide feedback later today. You are correct,
>> there is capacity issues in the Hebron area. I will see if this
>> concept plan will be affected by the capacity limitations.
>>
>>
>>
>> Regards,
>>
>>
>> Joe Verst, P.E.
>>
>> Project Engineer
>>
>> Planning & Design Group
>>
>> SD1
>>
>> 1045 Eaton Drive
>>

>> Ft. Wright, KY 41017

>>

>> Telephone: 859-578-7467

>>

>> Fax: 859-578-6897

>>

>> Email: jverst@sd1.org

>>

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>>

>> On Mon, Jun 23, 2014 at 10:13 AM, Kevin Wall

>> <KWall@boonecountyky.org>

>> wrote:

>>

>> Joe:

>>

>>

>>

>> Have you had a chance to review the above referenced project? We are
>> especially interested in SD1's feedback in light of the current North
>> Hebron sanitary sewer issues.

>>

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>>

>> Thanks,

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>

Kevin Wall

From: Mitchell, Laura (KYTC-D06) <Laura.Mitchell@ky.gov>
Sent: Monday, June 16, 2014 1:10 PM
To: Kevin Wall
Cc: Callan-Ramler, Carol (KYTC-D06)
Subject: Thornewilde Village Change in Concept

Hi Kevin,

Rob Hans and Carol Callan-Ramler in our office forwarded on the concept development plan for the Thornewilde Village Change in Concept Development work being addressed by the Boone County Planning Commission. The proposed entrance on KY 237 is at a location that is by permit meaning that we would entertain the option of a curb cut at that location. The approval of such curb cut would be contingent on the completion of a traffic study at the applicant's expense. We would look to the traffic study to determine required improvements to KY 237 before approving any permits.

Please let me know if you need any additional info.

Thanks,
Laura

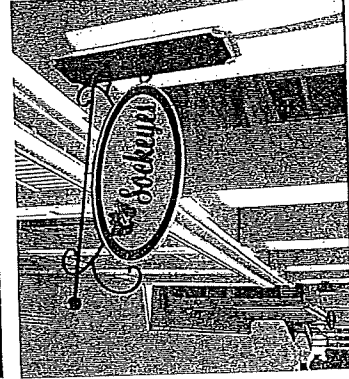
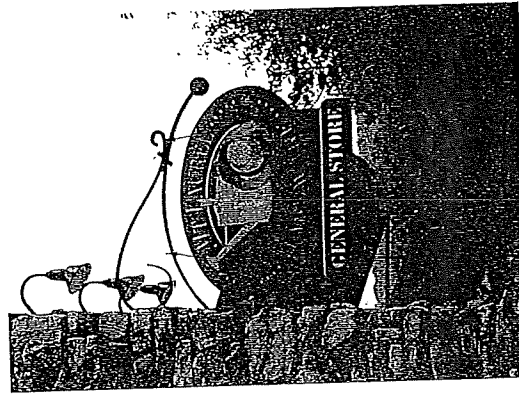
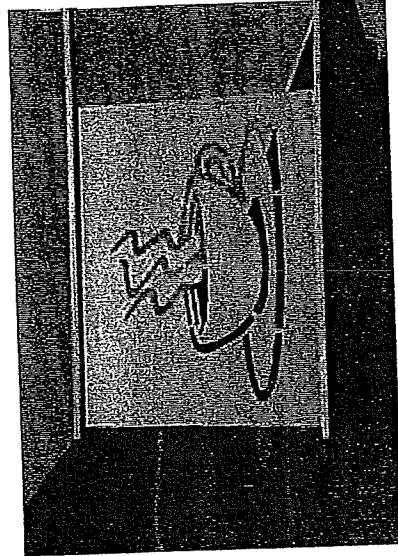
Laura T. Mitchell, P.E.
Permits Supervisor
District 6, Covington
Kentucky Transportation Cabinet
Laura.Mitchell@ky.gov

Historic and Prehistoric Features

There are no known historic or prehistoric features on the site. The Boone County Planning office was consulted.

Signage

Signage at Rivers Pointe Estates will be of high quality craftsmanship and design. Entry signs will be classic and subtle and may incorporate pillars, low walls, water features and/or iconic relic-like structures. Commercial signage will primarily consist of building-mounted signage (shade, awning, wall-mounted, hanging, etc.) along with a low-scale single tenant monument sign. Street signs may vary from the County standard to provide a consistent high quality decorative element within the community. The developer shall provide all signage, including traffic and street signs, upon Boone County approval. The developer and ultimately the association shall maintain all private signage in the public right-of-way. See Design Guidelines.



SIGNAGE

Signage at Rivers Pointe Estates should reinforce the architectural style of the building and express the presence of a commercial establishment. High quality sign designs should culminate and enrich the streetscape experience for pedestrians without detracting from the spatial coherence and quality of the streets and public spaces. Signs at Rivers Pointe Estates are envisioned to be scaled appropriately and crafted carefully to promote the overall theme of a rural village.

All signage at Rivers Pointe Estates should conform with Article 34 of the Boone County Zoning Ordinance as a minimum standard with the following exceptions, exclusions and additions:

Include Francisville Historical Marker as approved signage type within public right-of-way along either KY237 (North Bend Road) or main entry boulevard.

Permitted signage may include decorative banners, planing baskets, pennants, posters, ribbons, flags, streamers, spinners, and strings of lights across the public right-of-way and mounted on decorative street lamp posts for the sole purpose of community identity, seasonal decoration and community festivals. Any such elements must be maintained by the HOA and may not display a commercial advertisement or message and should not require a Temporary Advertising Permit per Section 34.020.

Portable Sandwich Board/A-frame signs should be permitted along commercial frontage in the Village Center of Rivers Pointe Estates without requirement for permit per Section 34.50, paragraph 5.

Entrance signs shall be permitted at both Stahl Road property entrances and the KY237/North Bend Road main property entrance.

The following unmentioned types of signage should be permitted to meet the overall allowed Building Mounted Signage area:

- Blade or hanging signs projecting from the facade of the building
- Window graphics; either printed or applied to storefront glazing

A Free-Standing/Aluminum sign per Article 34 shall be permitted along KY237/North Bend Road advertising the on-site Country Store.

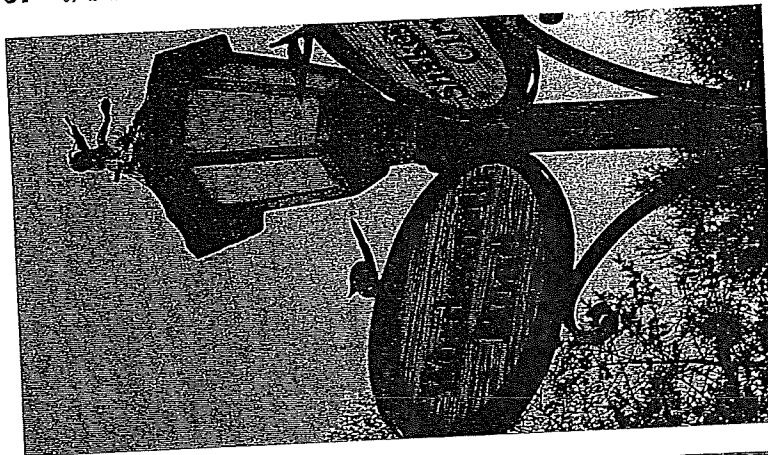
An Electronic Message Board (EMMB) will be permitted within Rivers Pointe Estates for the purpose of displaying community messages; no commercial advertisement will be allowed. Electronic Message Board is intended for internal community use and will not be visible from Stahl Road. The EMMB will be located and oriented to serve the community and not the general public. Signage area requirements and minimum limitations of changing messages should conform with Article 34 as a minimum standard. Messages should be managed by the HOA or their designated representative.

Custom street, traffic and monument signs will be allowed in public right-of-way and maintained by developer and ultimately the Home Owners' Association. Such signage will meet MUTCD regulations.

"No Soliciting" and "Open House" signs may be allowed at all community entrances.

Signage not permitted includes:

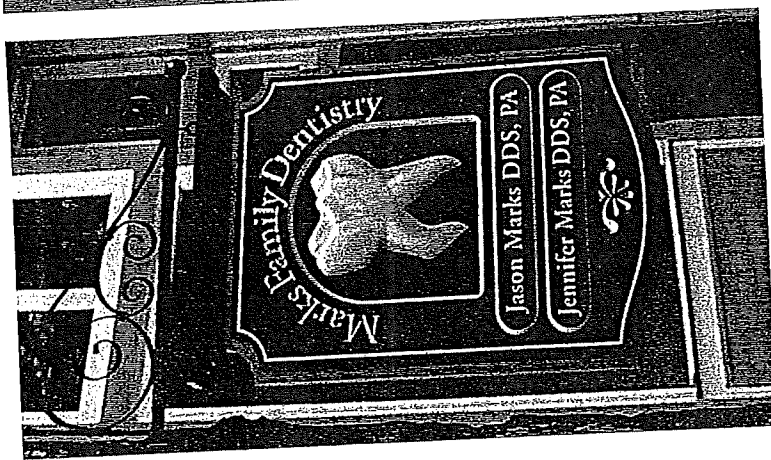
- Flat panel blade signs with only painted or vinyl graphics
- Individual metal channel letters with internally illuminated Plexiglas fronts
- Surface mounted box or cabinet type signs (any signage with exposed electrical connectors)



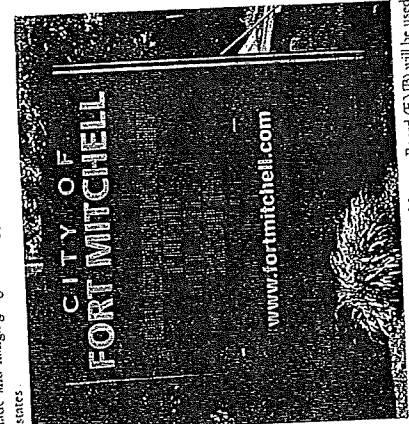
Well crafted street signage expresses community identity.



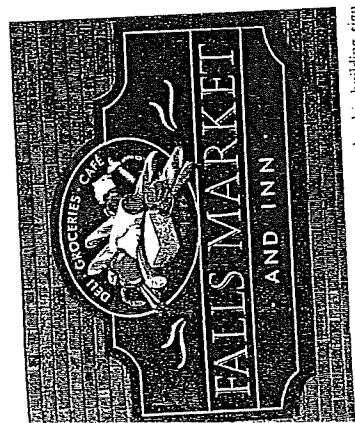
A free standing monument sign will advertise an on-site Country Store at Rivers Pointe Estates.



Blade and hanging signs are appropriate types for Rivers Pointe Estates.



An appropriately scaled Electronic Message Board (EMMB) will be used to display community messages within Rivers Pointe Estates.



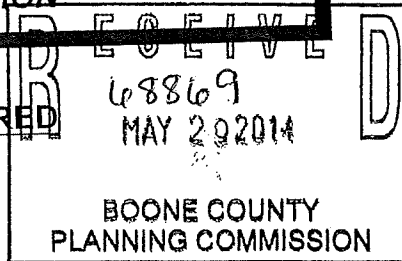
Appropriate scale, and craftsmanship make this building sign appropriate for the pedestrian oriented Village Center at Rivers Pointe Estates.



Appropriate community directional signage as an appropriate example for Rivers Pointe Estates.

APPLICATION FORM

CONCEPT DEVELOPMENT PLAN
BOONE COUNTY PLANNING COMMISSION



FIVE (5) COPIES OF SUBMITTED DRAWINGS ARE REQUIRED

(See Boone County Zoning Regulations)
SECTION A (To be completed by applicant)

1. Check One:
 - Concept Development Plan (Includes Utilization of an Underlying Zone in a Planned Development)
 - Change in an Approved Concept Development Plan
 - Long Range Planning Committee Review (As stated in the Houston-Donaldson Study)
 - Long Range Planning Committee Review (As stated in the Union Town Plan)
 - Zone Change Committee Review of previously approved Concept Development Plan Conditions or Zoning Map Amendment Conditions
 - Zoning Administrator Review (As stated in the Central Florence Strategic Plan, An Update of the Parkway Corridor Study)
 - Zoning Administrator Review (As stated in the Mall Road District Study)
2. Name of Project Thornwilde Village
3. Location of Project Francisville KY - Corner of Thornwilde Dr and Rt 237
4. Total Acreage of Site 8 +- Acres
5. Current Zoning RPD
6. Date of Previous Zoning Map Amendment or Approved Concept Development Plan (if applicable) April 1994
7. Is the site subject to a specialized Land Use Study approved by the Boone County Planning Commission? (If so, indicate the name of the Study) N/A
8. Proposed Uses (please specify each use) RPD zone allowing Single Family Attached and/or Multi-Family uses
9. Proposed Building Intensities (please specify) 120 units, multi story type buildings attached
single family and/or multi-family residential/apartment units
10. Have you submitted a Concept Development Plan? Yes
11. Are you also applying for:
 - N/A Conditional Use Permit
 - N/A Dimensional Variance
12. Name of Applicant(s) Thornwilde Inc.
Phone Number 859 814-8186 Fax No. _____ E-Mail drees@terraceholdings
13. Address of Applicant(s) 510 Graves Ave Ste 206
Erlanger, KY 41018
City _____ State _____ Zip _____
14. Name of Property Owner(s) The Drees Company
Phone Number 578-4200 Fax No. _____ E-Mail _____
15. Address of Property Owner(s) 211 Grandview Dr
Ft. Mitchell, KY 41017
City _____ State _____ Zip _____
16. Are there any existing buildings on the site? Yes
How many? 1
17. Deed Book _____ Page No. _____ Group No. _____
18. Have you had a pre-application meeting with BCPC Staff? Yes, 2/25/2014

19. Please check the following organizations/agencies which you have discussed the proposed development with in the last several months:

- Boone County Water District
- Florence Public Services Dept.
- Duke Energy
- Sanitation District #1
- Cincinnati Bell
- Owen Electric Cooperative, Inc.
- Boone County Public Works Department
- Kentucky Transportation Cabinet
- Boone County Building Department
- Northern Kentucky Health District
- USDA NRCS/Boone County Conservation District
- KY Division of Water
- Local School District
- Local Fire District
- Other: _____

20. Concept Development Plan Jurisdiction/Location
 Unincorporated Boone County Walton Union
 Florence

21. ORIGINAL Property Owner's Signature The Drees Co. By: Michael Schoettker
 (Faxed, Photocopied or Scanned Signatures will NOT be Accepted)

22. ORIGINAL Applicant's Signature THORNWILDE INC by: Paul Drees
 (Faxed, Photocopied or Scanned Signatures will NOT be Accepted)

SECTION B (To be completed by BCPC Staff)

1. Date Received 5/29/14 Fee Received \$2,102.⁰⁰ R# 68869
2. Check what has been submitted:
 Application Fee Legal Description
 Concept Development Plan Addresses of Adjoining Property Owners
 No. of copies of plan received **
3. Is application complete? YES NO
4. Staff Reviewer KEVIN WALL
5. Committee Chairperson _____
6. Scheduled Public Hearing Date 7/2/14
7. Boone County Planning Commission Action:
 Approved Approved With Conditions
 Denied
8. Other: _____

Boone County Planning Commission
 Boone County Administration Building
 2950 Washington Street, Room 317
 P.O. Box 958
 Burlington, Kentucky 41005
 Phone (859) 334-2196 - Fax (859) 334-2264
 plancom@boonecountyky.org - E-Mail
 www.boonecountyky.org - Web Page

Note: See Boone County Planning Commission Fee Schedule for Concept Development Plan Fees. An application consists of all fees paid in full, submitted drawings and a completed application form.



Thornwilde, Inc.

510 Graves Ave Suite 206
Erlanger, Ky 41018

Office (859) 814-8186
Fax (859) 360-1761

Thornwilde Village

May 29, 2014

Boone County Planning Commission
2950 Washington St, Rm 317
P.O. Box 958
Burlington, KY 41005

Re: Thornwilde Subdivision – RPD Concept Plan Revision

Dear Boone County Commissioners:

We would like to request a review and recommendation of approval to the Concept Plan Revision of the Thornwilde subdivision RPD zone. This request is for an approximately 8-acre area located near the corner of Rt 237 and Thornwilde Drive - locally referred to as Francisville City limits.

Based on the submitted Thornwilde Subdivision concept plan, it is our opinion that the rezoning and plan revision are in agreement with the previously adopted Boone County Comprehensive Plan and is consistent with the RPD approved zoning. We respectfully request such a finding and recommendation from the Planning Commission.

Thank you for your time and consideration of this request.

Sincerely,

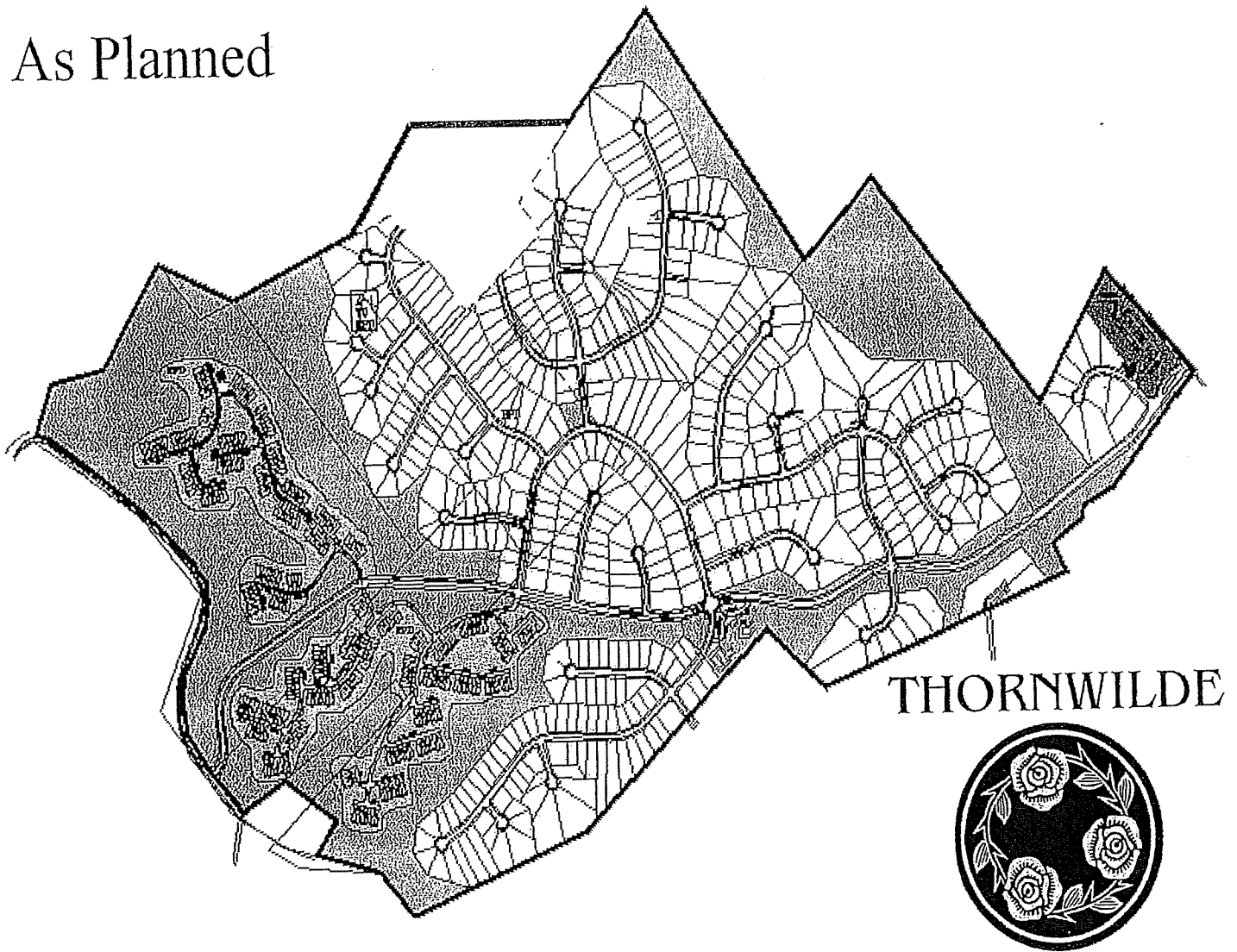
Philip Drees
President
Thornwilde Inc.

Thornwilde Village
Concept Plan Change
Francisville, KY

Thornwilde History: In 1994, Boone County approved Thornwilde Subdivision as a RPD zone. The original Thornwilde concept plan showed a total of 527 units on 217 acres, of which approximately half was going to be attached units (condo, townhomes or a higher density type product). The original concept plan also showed a 4-acre commercial area located at the corner of Thornwilde Dr and Rt 237. The density planned for Thornwilde was 2.5 units per acre with a wide variety of open space and community facilities.

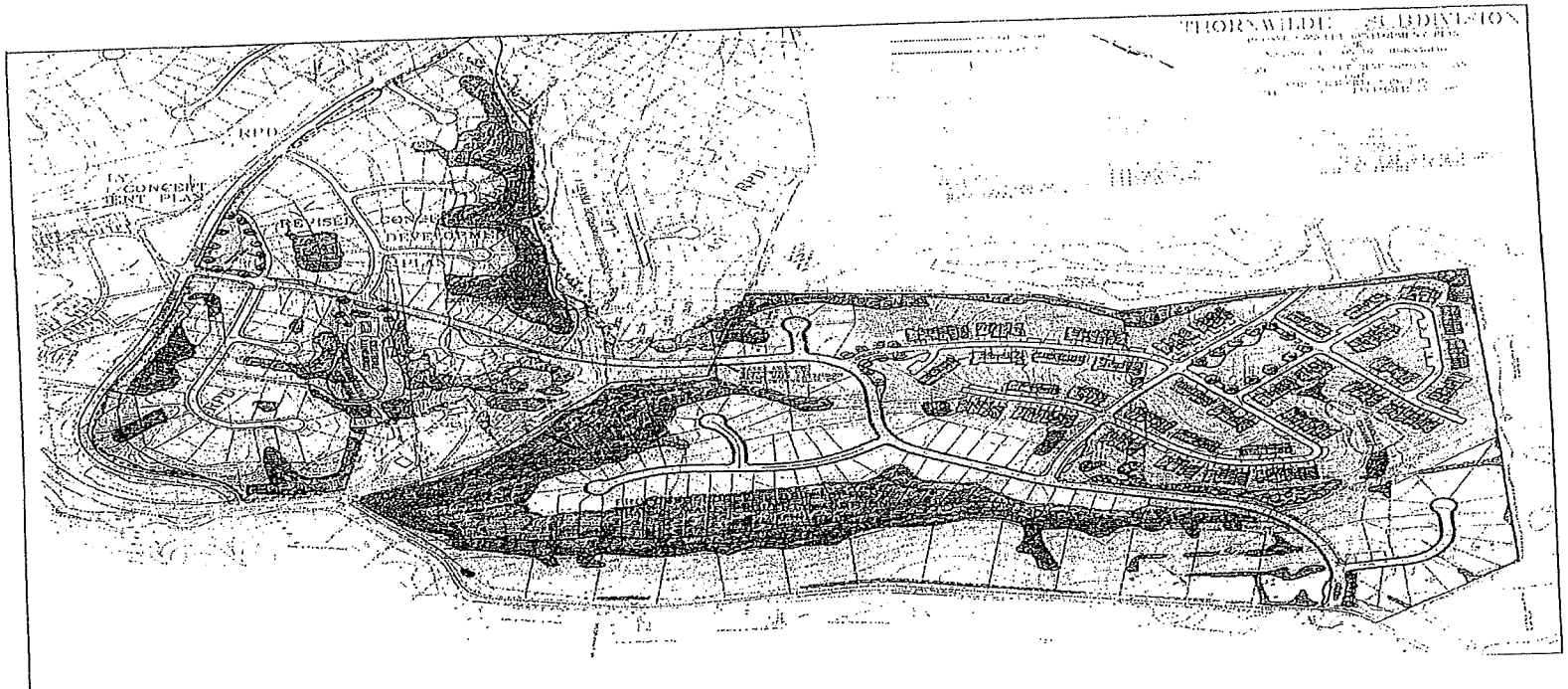
The following diagram shows the original Thornwilde layout.

As Planned



The flexibility of the RPD zone has allowed the Thornwilde concept plan to change over time to meet the needs of Boone County residents. The last concept plan revision approved in 2003 shows

a total of 1,037 homes on 545 acres (364 Attached units and 673 single family houses). Included in this revision was a new community facility/pavilion and pool, a continuation of the open space, pocket parks and a trail walkway system. The added amenities continued and enhanced the overall Thornwilde experience. Below is a diagram showing the approved 2003 concept plan revision.

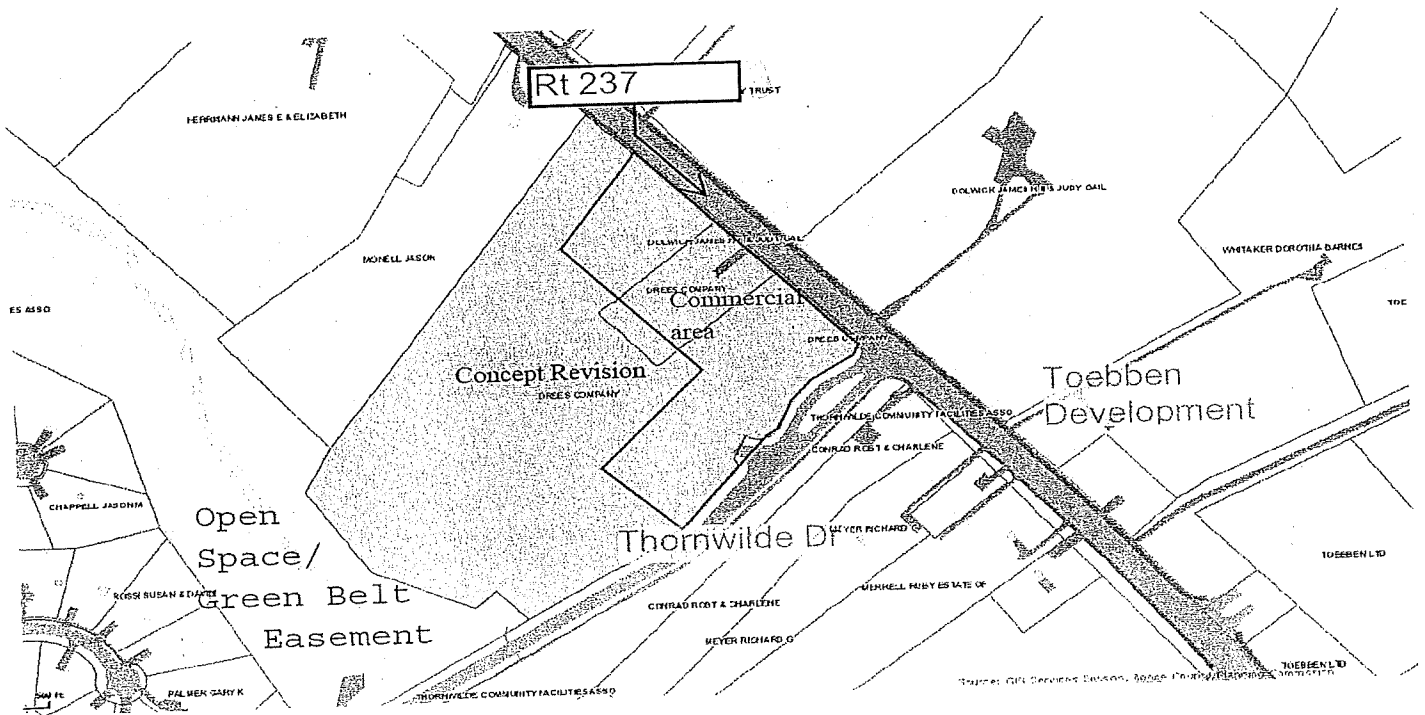


Today - The current plan remains very similar to the original approved submission. The change now includes a majority of the attached product as shown in the Southeast corner of the Thornwilde subdivision (above Diagram) has been replaced by the Thornwilde Elementary School as shown on the current diagram below.

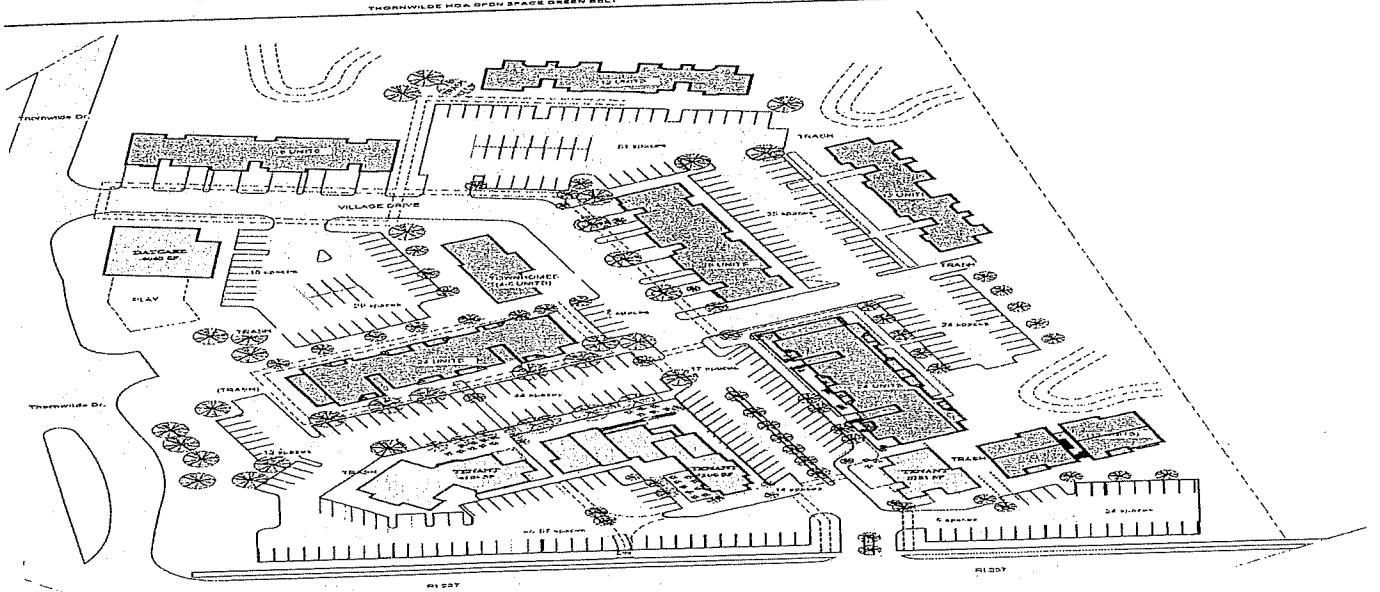


Thornwilde Subdivision Outline
Thornwilde Elementary School
Concept Plan Revision

1. Proposal: Request approval of a concept-plan revision of approximately 8 acres, from a RPD concept plan showing detached single family lots to a RPD concept plan showing Single-Family Attached and/or Multi-Family Residential.
2. Location: Located near the corner of Rt 237 and Thornwilde Drive and locally referred to as Francisville. The site is bordered by future commercial development at Thornwilde Dr. and Rt. 237— which was already approved by Boone County in 1994. Rt. 237 has been recently upgraded from a two-lane country road to a major collector road - three lanes wide with bicycle lanes and sidewalks on both sides of the road in anticipation of the Boone County growth. On the North side, the property is bordered by one house on four acres. The West property line abuts the Thornwilde open space/green belt and the South property line abuts Thornwilde Dr. (see plan below)



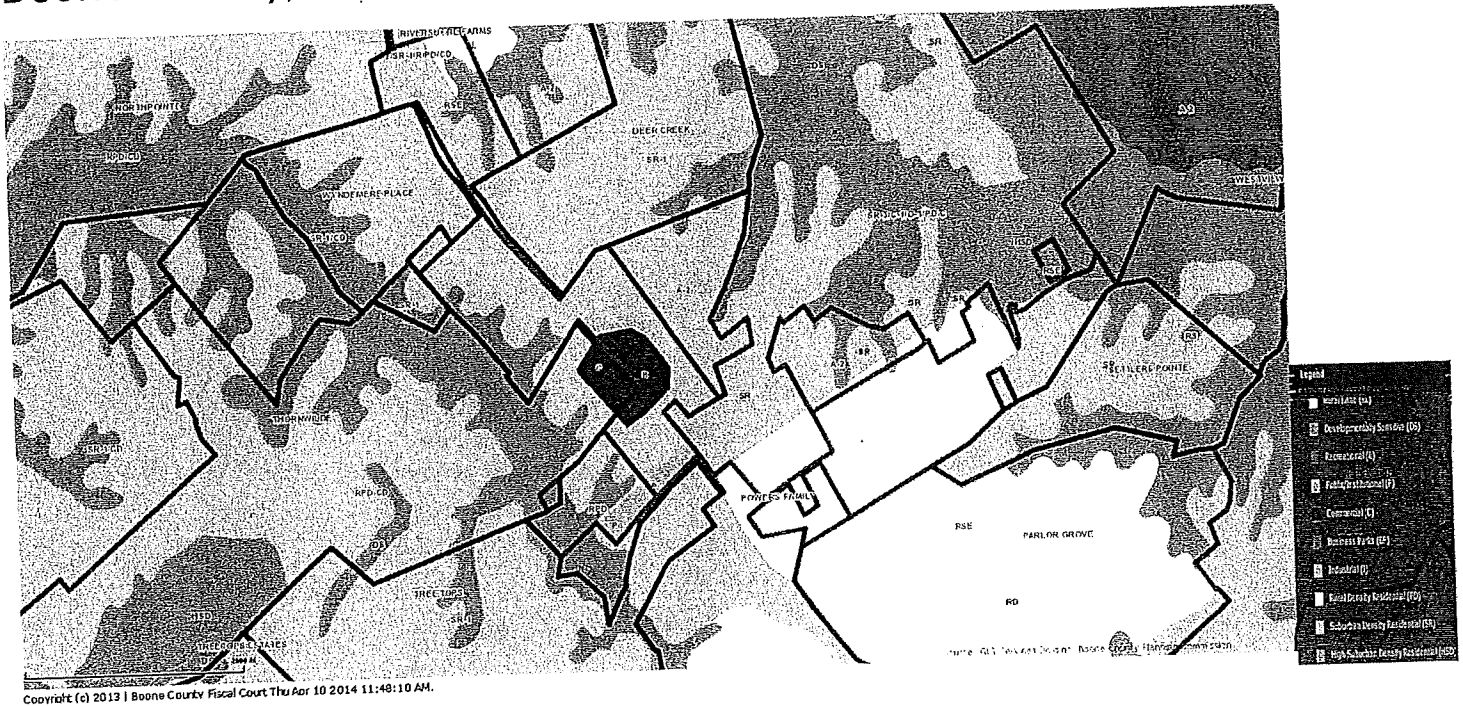
3. Land Use: The proposed plan revisions changes from Single-Family detached to Attached single family and/or Multi-Family units. The propose change will directly relate to the already approved commercial area in a way that will create a mix-use development to be known as "Thornwilde Village". (see plan below)



4. Density/Intensity of Use: The original approved Thornwilde Development plan showed a density of 2.5 units/ac even though the Boone County RPD and SR-1 zone allows up to 4 units/ac. Currently, Thornwilde is at a density of 1.85 units/ac - 979 units/528 acres - nets out the new school location changes. The new plan of approximately 8 acres will replace 11 single family lots with approximately 120 attached units. This will change Thornwilde's density to 2 units/ac still well below the original concept plan of 2.5 units/Ac.
5. Building locations: The location of all attached product is shown on the submitted concept plan. Setbacks vary and parking fields are minimized along "Village Drive". Emphasis of building placement close to the commercial area is important to creating a walkable, pedestrian-friendly neighborhood center.
6. Buffering: The original approved concept plan didn't show much buffering between the commercial and single-family. It had two distinct entrance points for each use which caused a disconnect between uses. The concept revision invites the different uses into a "Village" setting therefore buffering between the commercial and attached product is not necessary. Natural buffering is provided along the West and North property line as topo and grades will necessitate the buildings to step down with grades. Also, the West property line is buffered by the already existing open space and the creek line separates all existing Thornwilde single-family homes by a considerable distance.
7. Vehicle and Pedestrian Circulation: Village Drive allows free flow of ingress and egress from the site while providing walkable access from the Thornwilde public right-of-ways and bike access off of Rt 237. Village Drive will strive to be a public dedicated street; however depending on width and setback requirements it may have to be private. The current access point on Thornwilde Drive will move south to allow a safer distance from the curved Thornwilde entrance and intersection of Rt 237.
8. Sanitary and Water Service: Public sanitary connection will tie into the Sand Run pump station once capacity issues have been address by the Sanitation District. Extension of the water distribution system will tie into the existing system serving the area.
9. Soil Erosion Control: Best management practices will be incorporated into the site improvement plans at time of submission.
10. School Impact: Minimal impact of additional school age children is expected as most units will be occupied by empty-nesters, single adults and young couples with no children.
11. Thornwilde HOA Impact: Since the site is distinctly separate from the remainder of the subdivision, the residents living at Thornwilde Village will not be a part of the HOA; therefore, no impact on any existing facilities.
12. Traffic Impact: The proposed development is estimated that approximately 90 trips during peak traffic hours. Since the site is directly off of Rt 237 it is anticipated that none of this traffic will impact the majority of the streets in Thornwilde, including Thornwilde Drive because it will be easier to enter and exit the site off the RT.237 entrance than to navigate an extra turn if using the Thornwilde entrance.
13. Comprehensive Plan and RPD zone: The Thornwilde Development in the past has submitted and has been granted concept plan zoning changes under RPD zone. Current zoning allows for flexible and creative zoning procedures that have allowed the Thornwilde Development to accommodate density, market and layout and product changes over time. As with past approved submissions, the submitted concept change falls in line with the

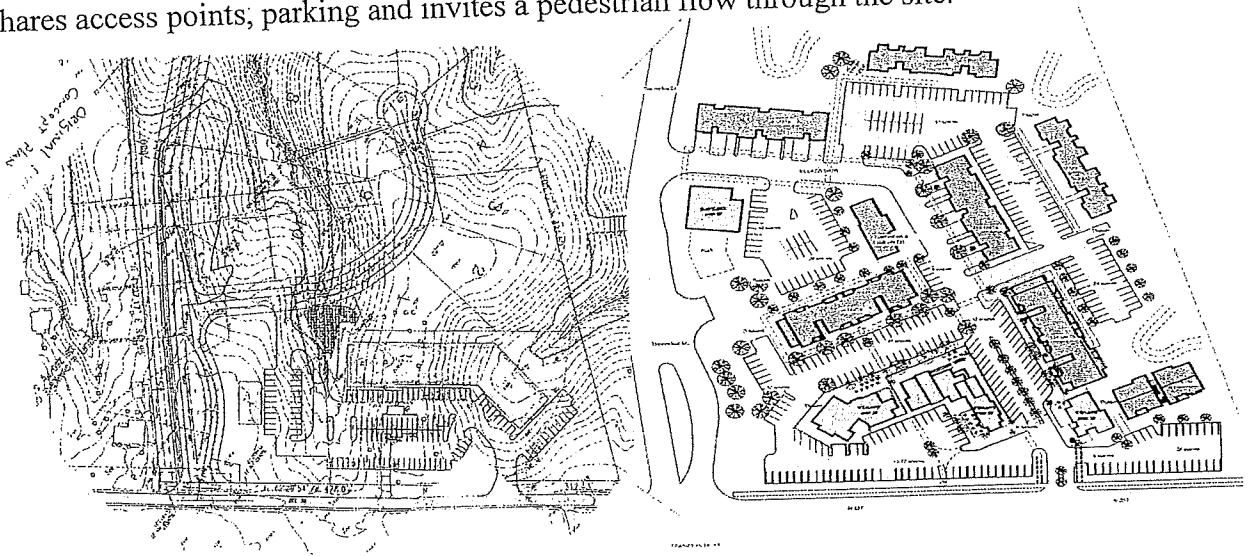
approved Boone County Comprehensive Plan and the overall original intent of the Thornwilde development. Boone County Comprehensive Plan shown below:

Boone County, KY



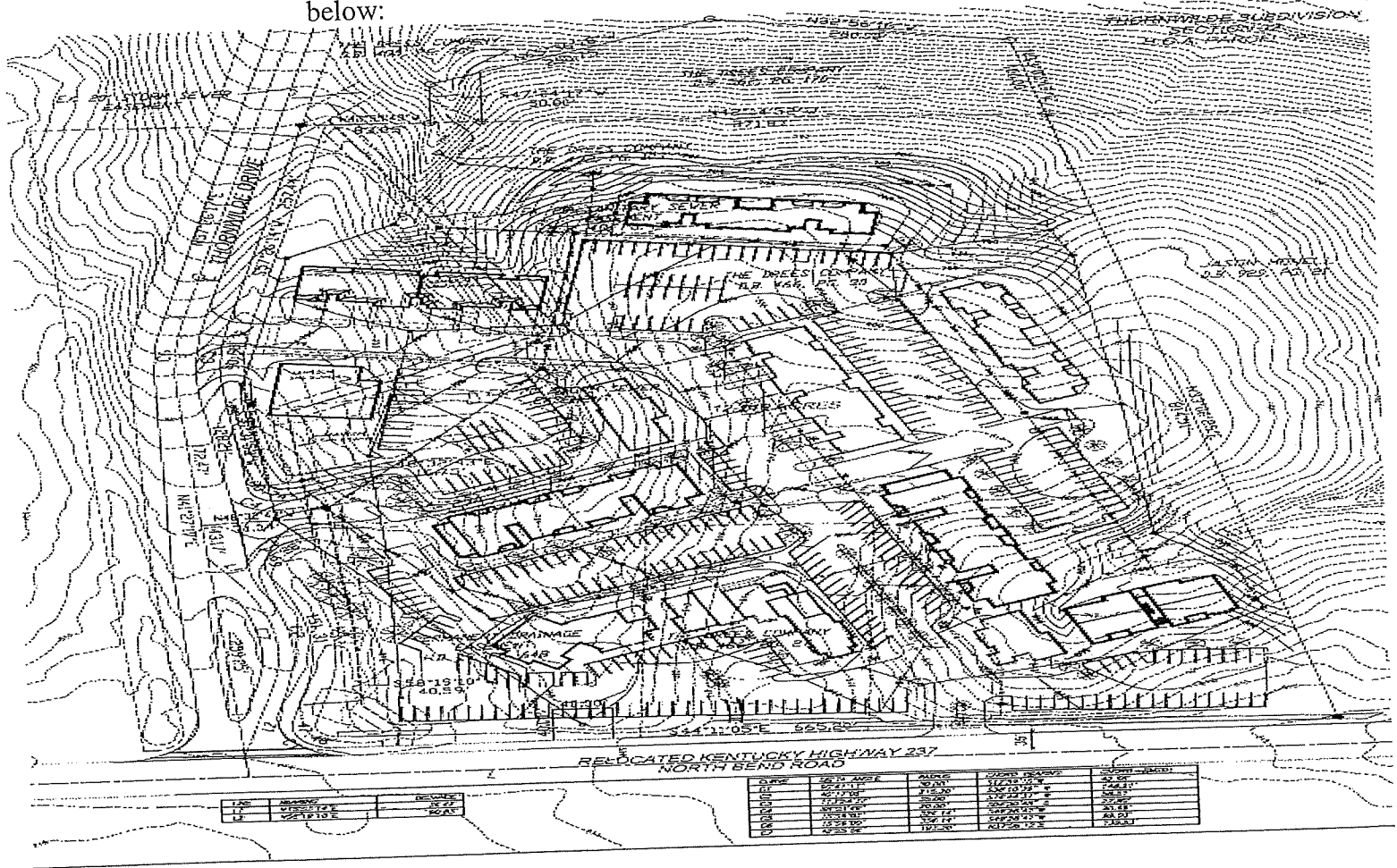
Below describes how the applicable requirements and standards in Article 16 have been satisfied for requesting the concept plan change.

1. The transition from the commercial to single family as the original concept plan shows doesn't allow for a creative way to mix the uses. The new concept plan below integrates the multi-family buildings next to the commercial development in a way to create a Village feel that shares access points, parking and invites a pedestrian flow through the site.

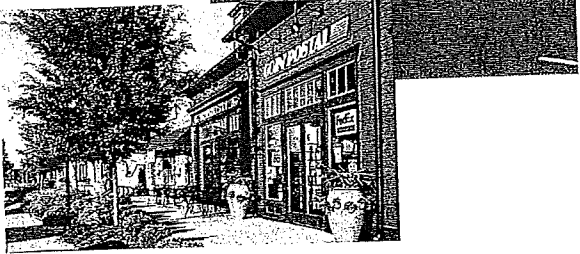
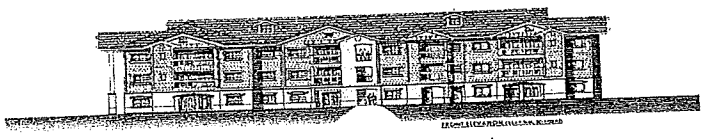
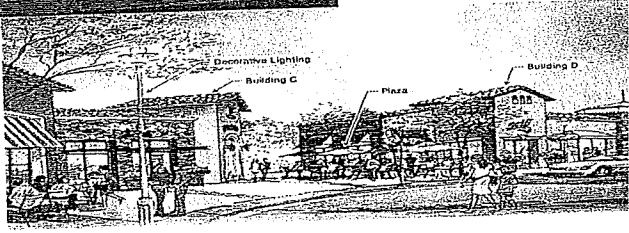
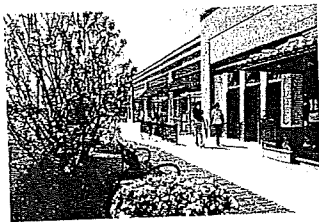
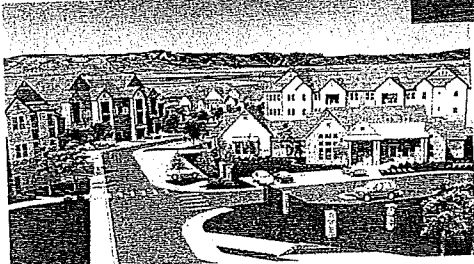
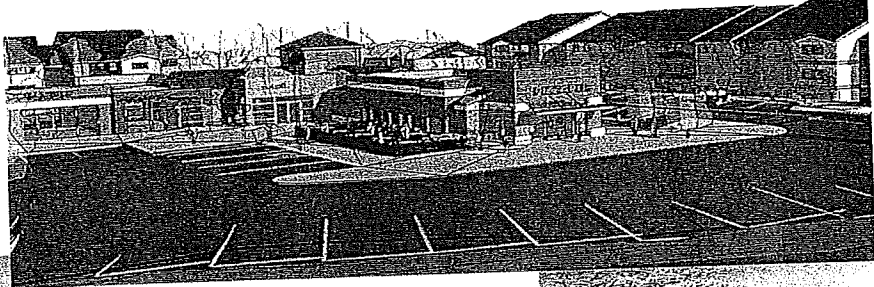
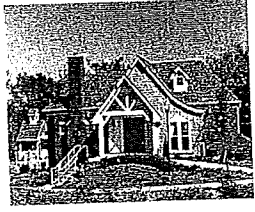
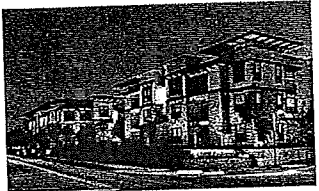
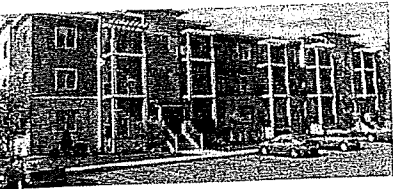


2. Changes to the surrounding area also warrant a change to the original concept plan. In 1994, Rt. 237 was just a two-lane country road. Now, Rt. 237 has three lanes, sidewalks and biking lanes and is considered a major thoroughfare. BCPC recent approval of Toebben Development (which is using the same elements of Residential, Attached/Multifamily and Commercial) plays into the Boone County comprehensive plan as shown above.

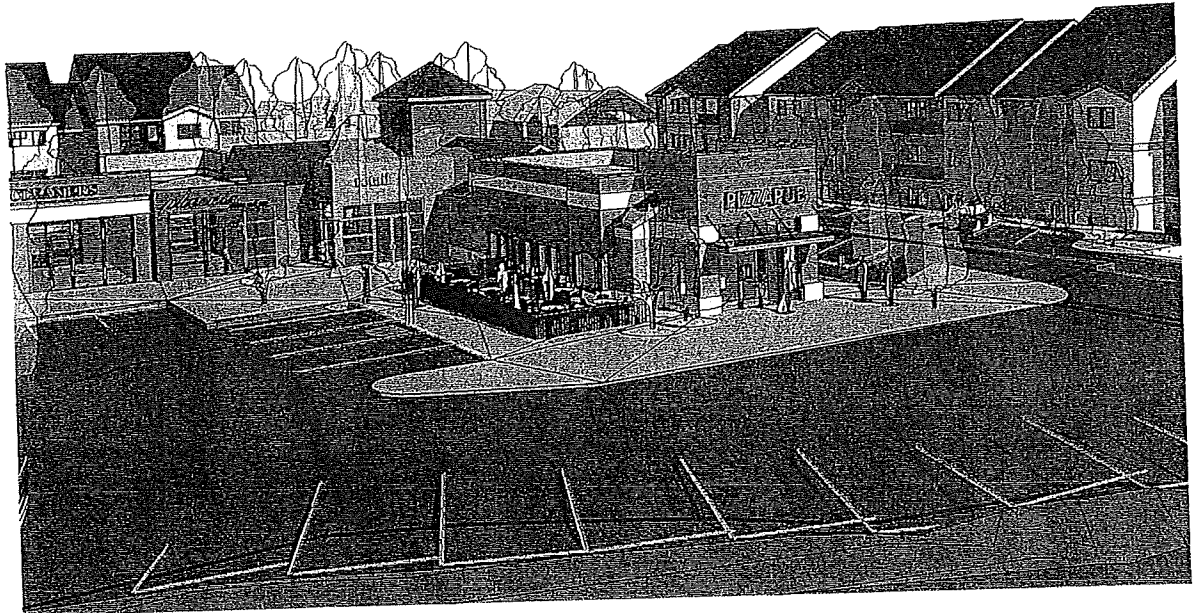
3. The overall consistency is not exceeding the original approved concept plan
4. The Attach/Multi-family components next to the commercial components allow a richer feel and are scaled for a better transition in uses. In short, the economics of the project can be spent on a strip center that looks good from two sides instead of just one, have outdoor pedestrian gathering areas and acts as an amenity for the development and for the county as a whole.
5. Article 16 of the Boone County Zoning Regulation encourages mix uses, walkability, reduced setbacks, pedestrian scale, outdoor pedestrian areas, parking areas to the rear and sides of buildings, decorative streetscapes etc. The submitted concept plan clearly accomplishes this.
6. Market conditions in the housing industry have changed dramatically over the recent recession. Current sale conditions show an upturn in attach/multi-family housing.
7. As a result of the unique characteristics of the current land topography, the change to an attached/multi-family product can better cope with the grades, topo and public improvements in a better cost effective way than single family. See topo/utility plan below:



8. The approved 1994 concept plan already permits these combinations of mixed uses in the RPD zoning.
9. Design elements, color and materials that will be used in the commercial design will be used in the attached/multi-family design. The following collection of pictures shows materials, quality and scale for the type of product that will be used at Thornwilde Village.

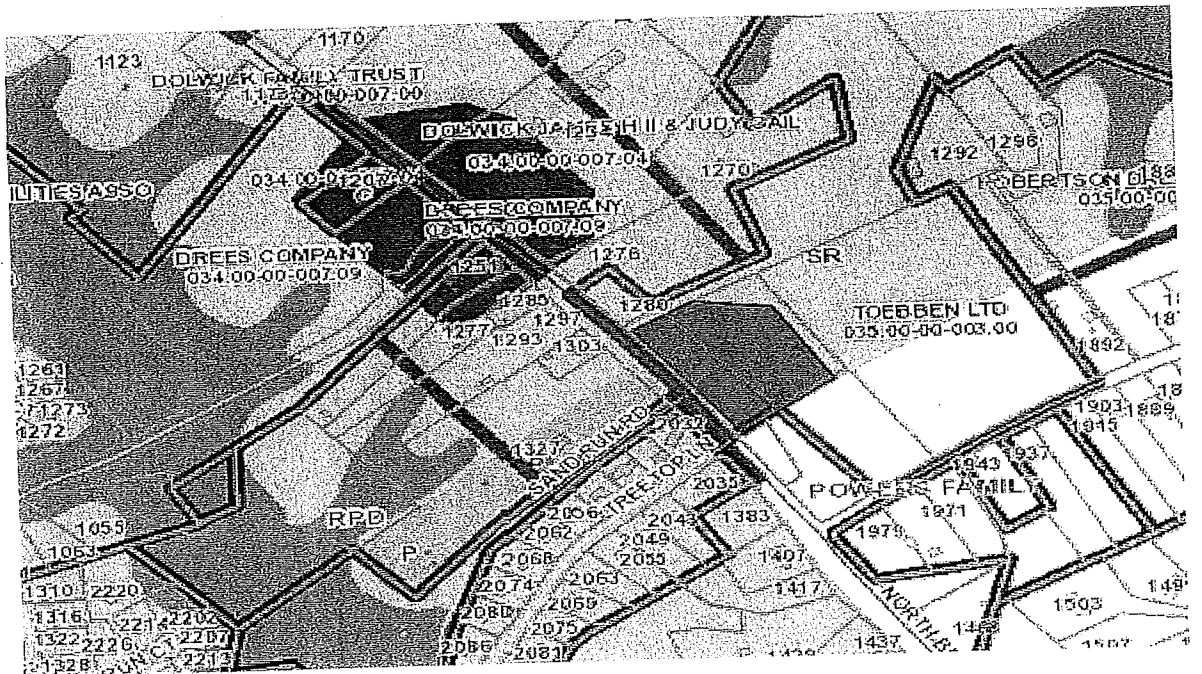


10. Article 16 of the Boone County Zoning Regulations is established to allow an applicant the opportunity to design and construct a planned development. The concept plan revision ties more closely with the elements in the Boone County Comprehensive Plan providing a variety of housing types and transition to what is indicated on the Comprehensive plan and future surrounding developments.
11. As the result of different uses locating next to each other, the attached/multi-family concept allows more people to be closer to the convenience of what the commercial area offers; therefore reducing traffic, number of trips and less dependency of vehicular traffic.
12. Residential Planned Development (RPD) is a zoning district which comprises a significant residential population and may consist of a variety of housing types such as but not limited to single-family detached lots, single-family attached, apartments, townhouses, landominiums, condominiums and duplexes in a planned development setting.
13. RPD districts allow flexibility in comparison to the remainder of the Boone County Zoning Regulations by offering innovative approaches and design. The following below site plan and renderings meet those goals and objectives and therefore consistent to the Boone County Comprehensive Plan

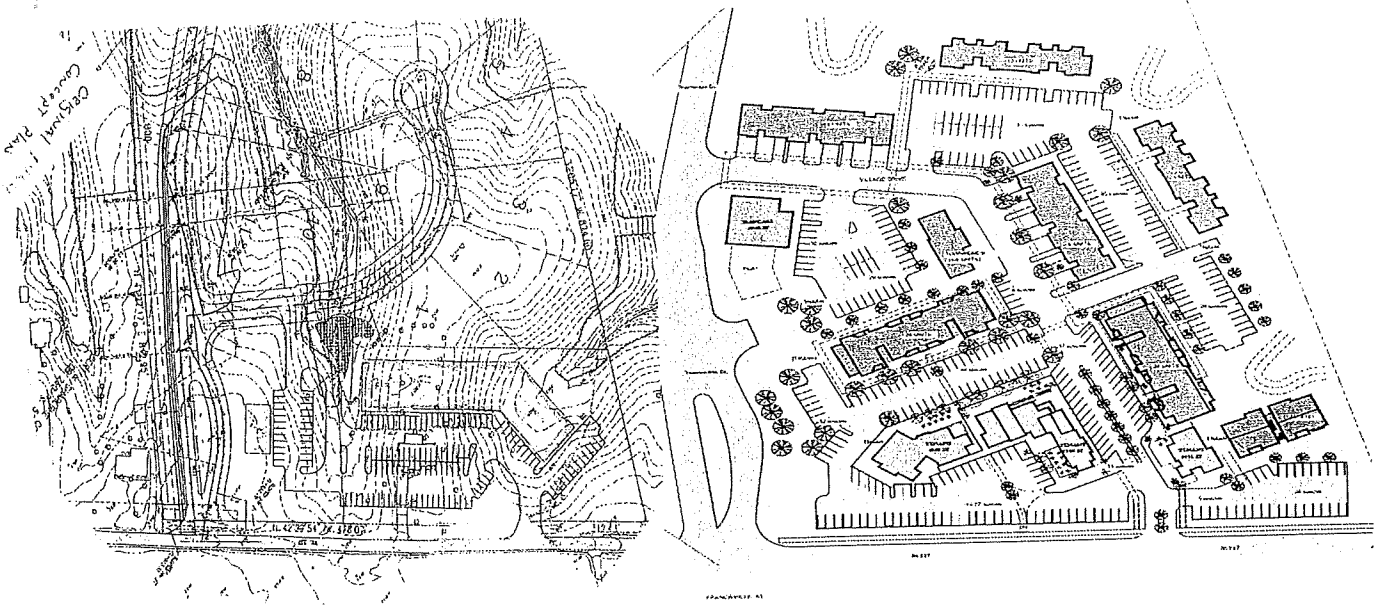


recreational/commercial services to achieve a balance, cohesive and inter-related community.

15. Moving the entrance off of Thornwilde Drive down and away the intersection of Rt. 237 assures a safe and efficient transportation patterns.
16. Under the RPD zone, this change is consistent with allowing for a more efficient and flexible use of land.
17. The concept change does not hinder nor deter development of surrounding undeveloped properties. The addition of attach/multi-family units should actually compliment the surrounding land as designated on the comprehensive plan as commercial (C) and Public (P) use (see attached comprehensive plan). The recently approved zone change on the Toebben property compliments the proposed concept plan revisions.



18. The development has a pedestrian scale and orientation and the entire site is walkable to the adjacent commercial use.
19. The new concept plan satisfies the current demand for a different housing type. In a report released by Harvard Joint Center for Housing Studies noted **"Of those planning to purchase a home and finance it with a mortgage, almost half are unlikely to obtain the mortgage they would like under their current financial circumstances because they either lack a down payment or the means to save for one, do not have the income to support the implied mortgage payment, or are hampered by concerns about their history or related personal financial problems,"**
20. Providing an alternative housing type will also allow individuals and families to stay in the area, these include divorcées that want to stay in the area to be close to family or work, aging populations looking for one level living, grandparents wanting to live by family, young individuals looking to get out of the parents basement, newlyweds looking for their first home or young couples looking for a creative and convenient place to live.
21. The attached/multi-family units relationship with commercial development is a crucial component to making this a success unlike what is shown on the original concept plan.



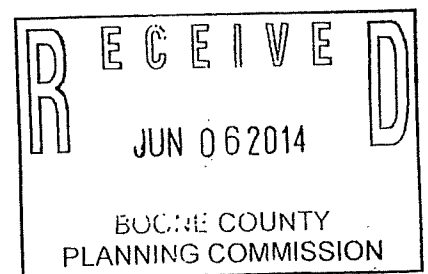
22. The Attached/Multi-Family user is more likely to use and access the conveniences of what the commercial area has to offer and therefore is a better plan than the originally concept.

Thornwilde Village

Flexibility Narrative:

The submission of this concept plan shall have the flexibility to allow the attached/multi-family product to replace the commercial building located by the entrance off of Thornwilde Drive. Adding multiple stories above the storefronts, on part or all of the commercial buildings, can be an option to allow for a variety of uses such as, commercial, professional office and/or multi-family residential.

The concept plan is a preliminary layout and thus the design and site plan must have the flexibility to change as the engineering, market conditions and site characteristics are finalized over time.



SCALE: 1/8" = 1'-0"
 DATE: 10/20/14
 DRAWN BY: J. HUBER
 CHECKED BY: M. WEBER
 PROJECT NO.: THORNEWILDE VILLAGE

CONCEPT DESIGN
THORNEWILDE VILLAGE
 HEBRON, KENTUCKY



Hub + Weber
Architects, PLLC

200 West Fifth Street
 Hebron, KY 40131
 Ph: 859-497-3655
 Fax: 859-485-3243
 hwb@hwb.com

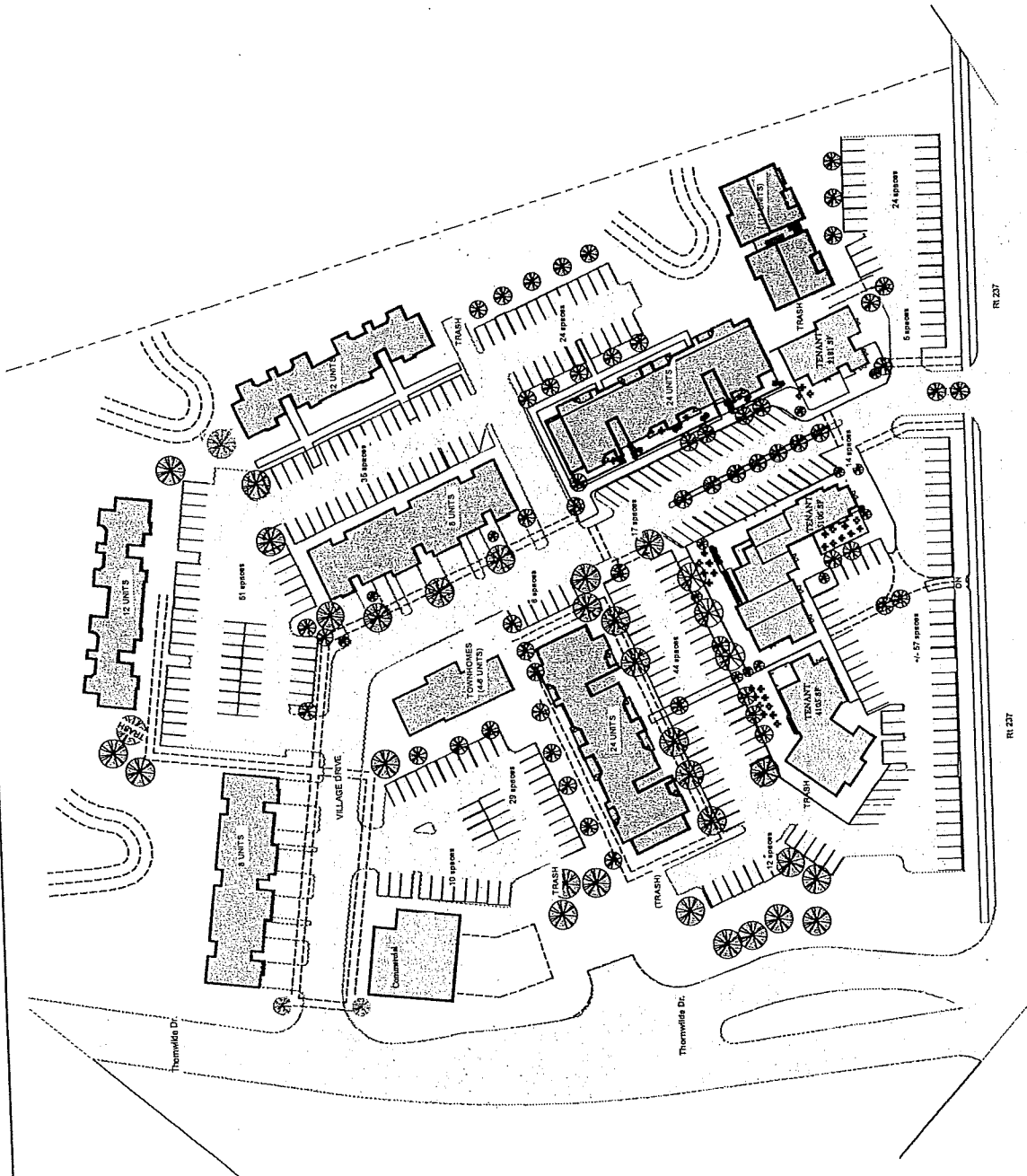
Issue: 3/17/2014
 Revised:

ARCHITECTURAL
 SITE PLAN

C101

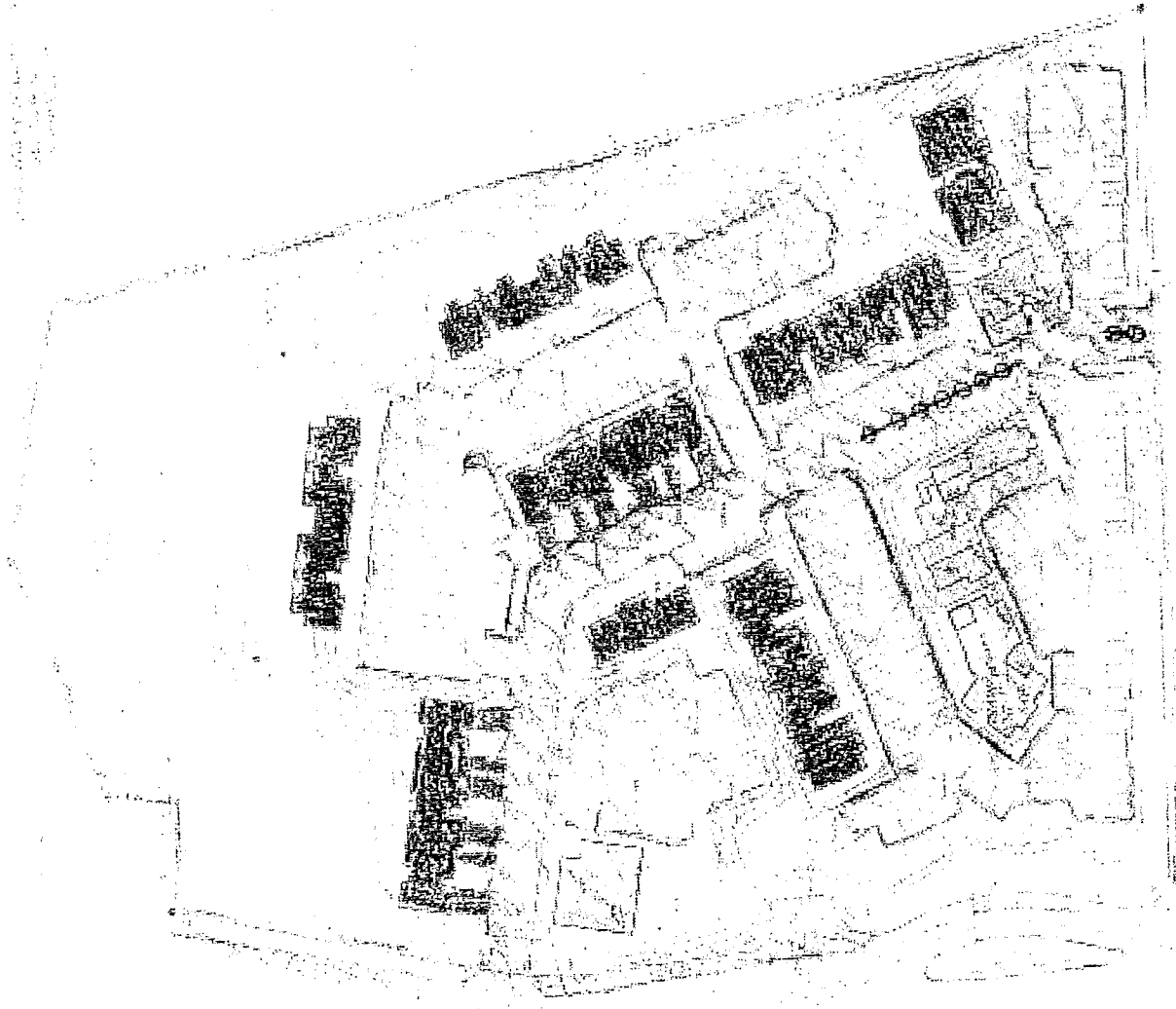
02/20/14

THORNEWILDE IGA OPEN SPACE GREEN BELT



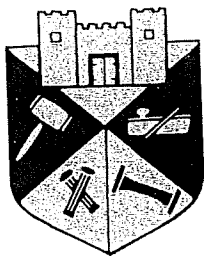
FRANCISVILLE, KY

SITE PLAN
 P. 47
 NORTH



Scale 1" = 20'

Thamesville, N.Y.
1917



TOEBBEN COMPANIES

541 Buttermilk Pike, Suite 104 • Crescent Springs, Kentucky 41017
Phone: (859) 331-1560 • Fax (859) 331-9702
E-mail: toebben@toebben.com • Web Site: www.toebben.com

June 23, 2014

Boone County Planning Commission
2950 Washington Street
Burlington, KY 41005

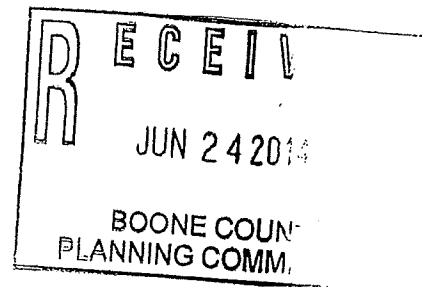
Re: Thornwilde Village

Kevin Wall,

I am writing to support the new concept plan change for Thornwilde Village. I believe this will be an important site for the Hebron community and Mr. Drees has produced a plan which will enhance the area.

Thank you,

John Toebben
President
Toebben Builders



Bill Toebben, President
Toebben Companies
Toebben Construction Co.
Commercial/Industrial Construction
Design/Build
General Contracting
Construction Management

John Toebben, President
Toebben Builders, Inc.
Residential Construction
Country Squire Estates
Luxury Homes
Squire Valley Landominiums,
Villa Hills, Ky

Landstone Developers, Inc.
Residential Community Developers
Rivers Pointe, Hebron, KY
Red Stone Village, Union, KY

Industrial Park
Enterprise V, Boone County, Ky
Mt. Zion Technology Park,
Boone County, Ky

COMMERCIAL PROPERTIES
Kentucky Properties
High Street Center, Crescent Springs
Mall Road Center, Florence
Toebben Exec. Center, Crescent Springs
Mt. Zion Center, Boone County
Summit Medical Building, Crittenden
Crestville Center, Crescent Springs
Fifth Street Center, Covington
Huff Realty Building, Ft. Mitchell
Grainger, Boone County
Technology Park, Boone County
North Bend Square, Hebron

Ohio Properties
Keystone Plaza, Blue Ash
Milford Shopping Center, Milford
Houston Park Center, Colerain Township
Western Row Center, Deerfield Twp.
Wilmington Comm. Park, Wilmington
Harrison Retail Center, Harrison

Agricultural Property
Toebben TTT Farm, Union, KY
Toebben TTT Tree Farm, Carrollton, Ky

EXHIBIT

“B”

ZONE CHANGE/CONCEPT PLAN COMMITTEE REPORT

TO: Boone County Planning Commission

FROM: Ben Brandstetter, Chairman

DATE: August 6, 2014

RE: Request of **Thornwilde Inc. (applicant)** for **The Drees Company (owner)** for a Change in an Approved Concept Development Plan in a Residential Planned Development (RPD) zone for a 8.648 acre site located on the northwest corner of the North Bend Road/Thornwilde Drive intersection, including the tract at 1207 North Bend Road, Boone County, Kentucky. The request is for a Change in an Approved Concept Development Plan to allow a combination of commercial, attached single family residential, and multi-family residential uses.

REMARKS:

We, the Committee, recommend approval of the above referenced request based on the following findings of fact and with the following conditions.

FINDINGS OF FACT

1. The Committee has concluded that the proposed Change in an Approved Concept Development Plan application is in agreement with the 2010 Boone County Comprehensive Plan due to the following reasons.
 - A. The Comprehensive Plan's Future Land Use Map designates the site as Commercial (approximately 23% of the site), Suburban Residential (approximately 53%), and Developmentally Sensitive (approximately 23%) (figures add to 99% due to rounding). The Developmentally Sensitive designation does not prohibit development, nor does it outline specific contemplated land uses or densities. Rather, the description of this designation includes the phrase "any development on land identified as Developmentally Sensitive must be carefully assessed by the developer and the Planning Commission to determine the ability of the land to support the proposed project," meaning that each site and proposal needs to be evaluated on its own merits.

The Committee has evaluated the proposal and found it, in conjunction with the remainder of the overall approved Thornwilde Subdivision development, to be consistent with the general land use concept outlined on the Future Land Use Map. The residential density for the overall Thornwilde Subdivision development with this proposed change would be 2.1 units per acre, whereas the Concept Development Plan approved in 1994 for the initial phase had a residential density of 2.4 units per acre. Both figures are consistent with the Suburban

Residential designation which permits up to 4 units per acre. The 1994 Concept Development Plan also included a significant number of attached residential units.

The property owner has agreed to a condition which would limit commercial uses on the site to 26,000 gross square feet, which is consistent with the approved 1994 Concept Development Plan. Much of the site is proposed to be maintained as open space, including the west hillside portion which adjoins the Thornwilde HOA parcel, the yard area along the north property line, and the street frontage area along Thornwilde Drive. The open space maintained in the west hillside portion of the site corresponds to the Developmentally Sensitive designation shown on the Future Land Use Map. The overall approach is consistent with Section 1607 "Land Use Standards" (EPD/RPD zones) of the Boone County Zoning Regulations which states "the intensity of planned residential developments, in particular, should be generally consistent with the Future Land Use Map designations for the area. Residential densities, however, shall be concentrated on portions of the site to leave open space on other portions."

- B. The applicable Land Use Element text ("17. North Hebron," pg. 180) does not discuss this specific site, but it does discuss the overall area. It generally recommends Suburban Residential uses, creating "open space between future developments" by maintaining wooded areas on "Developmentally Sensitive and scenic hillsides," and maintaining "stream valleys and ridge bluff edges." As discussed above, the proposal, in conjunction with the remainder of the overall approved Thornwilde Subdivision development, is consistent with the general land use concept outlined on the Future Land Use Map including the Suburban Residential designation. Also as discussed above, the Developmentally Sensitive area in the west hillside portion of the site will be maintained as open space.
- C. Regarding commercial development in the area, the Business Activity Element ("Recommended Areas of Commercial Activity," pg. 69) states "commercial development in Hebron will center around the intersection of new KY 237 and KY 20 with a limited amount of convenience and neighborhood commercial uses north of the I-275 interchange to serve the SouthPark Industrial Park, Park West Industrial Park, and the growing number of residential subdivisions along North Bend Road." The proposed commercial phase consists of smaller scaled buildings which would provide "a limited amount of convenience and neighborhood commercial uses" as sought by the Business Activity Element. Again, the commercial phase would be limited to 26,000 gross square feet, the same amount that was approved on the 1994 Concept Development Plan. Relative to the commercial phase, the proposed Concept Development Plan is largely a "repackaging" of the approved 1994 Concept Development Plan.

- D. The Housing Element anticipated additional development in this area. Housing Element ("North River/I-275 Corridor Area," pg. 83) specifically mentions Thornwilde Subdivision by stating "North Bend Road has been upgraded to better serve development in that corridor, including the continuation of Thornwilde and the new Rivers Pointe Estates." Regarding the residential component of this specific proposal, the Housing Element also states that: "large, left-over parcels in urban service areas are well suited for planned residential development that includes high density housing" ("Population Needs," pg. 79); "throughout Boone County, high density developments should be close to thoroughfares to achieve a gradation of building site dimensions and land uses outlined in the Future Land Use Plan" ("Housing Types," pp. 79 and 80); and "multi-family housing developments should have convenient access to commercial districts or should provide their own supporting commercial uses" ("Housing Types," pp. 79 and 80).
- E. The Committee has concluded that the proposal, with the agreed conditions, is in agreement with the applicable Future Land Use Development Guidelines as described in the Comprehensive Plan, and the Goals and Objectives. The Land Use Element's "Future Land Use Development Guidelines" include several provisions that pertain to this proposal which are quoted in the Staff Report. These pertain to landscaping and buffering, signage, architectural design, minimizing disturbance and retaining existing vegetation, access management and impacts on roadways, and storm water management. These basic tenets are also discussed in the Goals and Objectives.
2. The Committee has concluded that the Concept Development Plan fulfills the applicable requirements of Article 16 "Employment Planned Development (EPD) Residential Planned Development (RPD)" of the Boone County Zoning Regulations, including Section 1607 "Land Use Standards," Section 1608 "Compatibility Assessment Standards," Section 1609 "Design Standards," and Section 1610 "Findings Necessary For A Zoning Map Amendment Or Concept Development Plan In An EPD Or RPD District". Compliance with these sections is discussed in the Staff Report for this request, and the property owner has agreed to the conditions listed below which address these comments. The Concept Development Plan includes the Project Narrative and Flexibility Narrative that were submitted with the original application and are attached to the Staff Report, and the 7/16/14 letter and accompanying attachments from Phil Drees, President, Thornwilde, Inc. ("7/16/14 Thornwilde Village outline," attached) that was provided by the applicant to the Zone Change Committee in response to the issues raised at the 7/2/14 Public Hearing.
3. The issues of sanitary sewer capacity for the North Hebron area and traffic were raised at the 7/2/14 Public Hearing for this request. Regarding sanitary sewer capacity, both the applicant and Committee understand that this project can not be developed until capacity is available. Information from Sanitation District #1 regarding this issue is outlined in the Staff Report. Regarding traffic, the applicant provided a traffic analysis that was attached to the 7/16/14 Thornwilde Village outline which

demonstrated that any additional traffic generated by this development over the approved 1994 Concept Development Plan would be small from an empirical standpoint.

4. The Committee has concluded that the attached conditions are necessary to achieve consistency with the specific goals, objectives, and policies of the 2010 Boone County Comprehensive Plan and the applicable requirements of Article 16 "Employment Planned Development (EPD) Residential Planned Development (RPD)" of the Boone County Zoning Regulations. The Committee has also concluded that the attached conditions are necessary to mitigate any foreseeable community impacts that may be created by the development. The property owner has signed a letter demonstrating agreement with these conditions.

CONDITIONS

1. The development shall follow the commitments and representations outlined in the 7/16/14 letter and accompanying attachments from Phil Drees, President, Thornwilde, Inc. ("7/16/14 Thornwilde Village outline," attached), except when clarified or superseded by the conditions below.
2. The commercial buildings shall be limited to a maximum of 26,000 gross square feet of floor area and the residential portion of the development shall be limited to a maximum of 120 units. The commercial building shown by the Thornwilde Drive access point may be substituted with an 8 unit townhouse building, which would increase the maximum amount of permitted residential units to 128. If this substitution is opted, the maximum amount of permissible commercial space shall be reduced to 21,675 gross square feet.
3. The "retail sale of motor fuels" and "the rental of trucks and trailers (only permitted to be displayed in the side or rear of the property)" (C-1 zone Accessory Uses #7 and #9) shall not be permitted in the development.
4. Condition #4 from the 3/16/94 Committee Report for the original Thornwilde Subdivision Concept Development Plan approval still applies to this development. This condition states:

Every effort shall be made to retain as many existing trees as possible on the site. Therefore, grading limits shall be established during the Improvement or Grading Plan stage for each phase of the development. These grading limits will be used to identify tree cover that will be retained and enable measures to be taken to protect these trees during construction.

The intent with the current proposal is to maintain as many existing trees as possible that are outside of the active development area. The grading limits shall be

established through the Site Plan procedure if the Grading Plan or Improvement Plan procedures do not apply.

5. The open space areas and streetscape along the main street/drive between North Bend Road and Thornwilde Drive (labeled as "Village Drive" in the Concept Development Plan materials) shall include the types of amenities and improvements represented in "Exhibit D Streetscape Ideas" that is attached to the 7/16/14 Thornwilde Village outline. Specific examples are listed in the 7/16/14 Thornwilde Village outline. Compliance with this condition shall be determined by the Zoning Administrator, who shall review development applications for consistency with the thematic and qualitative commitments and representations in Exhibit D.

Additionally, entry improvements (such as columns, decorative pavement, light fixtures, landscaping, etc.) shall be provided at the intersections with North Bend Road and Thornwilde Drive. The entry improvements shall visually correlate to the existing brick Thornwilde Subdivision entry monument and the streetscape improvements.

6. The main street/drive between North Bend Road and Thornwilde Drive (labeled as "Village Drive" in the Concept Development Plan materials) shall have curbing and sidewalks on both sides. Box curbs and an integral curb/walk shall be used along the east section of this street/drive.
7. Buffer Yard C shall be followed along the north property line. If a commercial building abuts the north property line, then berming shall be provided in the adjoining buffer area (as stipulated in the 7/16/14 Thornwilde Village outline), and the remaining area along the north property line will follow Buffer Yard C options.
8. The two opposing offset intersections in the approximate center of the development, along the main street/drive between North Bend Road and Thornwilde Drive (labeled as "Village Drive" in the Concept Development Plan materials), shall either be realigned into a four way intersection or discernibly offset into two separate intersections. The two opposing offset intersections are located at the two 24 unit residential buildings, 8 unit residential building, and commercial building
9. The qualitative or cosmetic aspects of on-site signage, including materials, colors, and design details, shall visually correlate to the opted architectural theme and streetscape improvements.

A copy of the Public Hearing minutes accompanies the findings and recommendation serving as a summary of the evidence and testimony presented by the proponents and opponents of this request. Attached is the signature page for the Zone Change Committee Vote.



Thornwilde, Inc.

510 Graves Ave Suite 206
Erlanger, Ky 41018

Office (859) 814-8186
Fax (859) 360-1761

Thornwilde Village

July 16, 2014

Boone County Planning Commission
2950 Washington St, Rm 317
P.O. Box 958
Burlington, KY 41005

Re: Thornwilde Subdivision – RPD Concept Plan Revision

Dear Boone County Committee Members:

We would like to request a review and recommendation of approval to the Concept Plan Revision with the following clarifications from staff report and the public meeting.

1. Commercial area
 - a. Clarify that the uses would conform to the original approval of C-1 uses
 - b. Buildings will be limited to 1 to 3 stories
 - c. The existing berming and Thornwilde entrance sign on South property line will remain, satisfies the requirements set out in the original approval and if a commercial building abuts the North Property line then six foot berming will be provided as stipulated in the original approval.
2. Overall site
 - a. Overall acreage of the site is approximately 11 – 12 acres. The 8.648 acres mention in the development narrative was the total area less the approximate 4 acre commercial portion.
 - b. Thornwilde subdivision has provided and surpassed the necessary open space requirement. In addition the land directly west of the site has been dedicated as open space that protects the developmentally sensitive area along the creek.
 - c. Viox engineers has estimated approximately 6.4 acres is green or open area surface (see Exhibit A)
 - d. A future connection point to adjoining land the North can be achieved as shown on the attached exhibit B
 - e. Sidewalks will be provided on both sides of Village Drive
 - f. “Village Drive” name will changed to a name determined later
 - g. The existing stub intersection known as Sedgewicke Way will be removed and curb line restored.
3. Residential Area

- a. The North property line would follow Buffer Yaru C in Article 36 on the zoning regulation with credit given to existing trees that can be retained.
 - b. Majority of the units would consist of two bedroom-two baths. The average size is anticipated to be 1,200 square foot units.
 - i. Two bedroom-one bath units range from 925-1050 square feet
 - ii. Two bedroom-two bath units range from 1,000-1250 square feet
 - iii. Three bedroom-two bath units range from 1,750-2,150 square feet
4. Traffic Analysis
- a. A preliminary traffic analysis has been conducted by Jack Pflum; traffic engineer, on peak hour generation (see exhibit C). Conclusion, The increase in traffic volumes associated with the proposed change to the overall approved Concept Development Plan is modest.
 - b. As stipulated by Kentucky Transportation Cabinet (KTC) the entrance off of Rt 237 will require a permit and a more detail traffic study will be completed at that time.
5. Preliminary Building Design or Architecture
- a. Okay with a condition similar to the Rivers Point Estates development
 - i. "The design related aspects of the Attached/Multi-Family residential and Commercial buildings shall generally follow the text, photos and graphic exhibits in the concept development narrative. Design elements, colors and materials that will be used in the commercial design will be used in the attached/multi-family design. Compliance with this condition shall be determined by the Zoning Administrator, who shall review individual proposals in the development for consistency with the thematic and qualitative commitments and representations in the Concept Development exhibit."
 - b. Streetscape Details
 - i. The demarcation of the site already exists in the large curved "Thornwilde" sign located at the corner of site.
 - ii. A combination of landscaping, street trees, crosswalks, decorative lighting, and street furniture will be used along Village drive. In addition, at or near the commercial center bike racks, seat walls, planters, trellis, benches, sculptures or other streetscape items that create a sense of place will be used. Example shown on Exhibit D
 - c. Signage
 - i. Signage for Commercial One (C-1) District section 3413 would be followed.
 - d. Utilities
 - i. Utilities are planned to be underground
 - ii. Storm water management will be designed to handle the necessary requirements during the site plan review procedure.

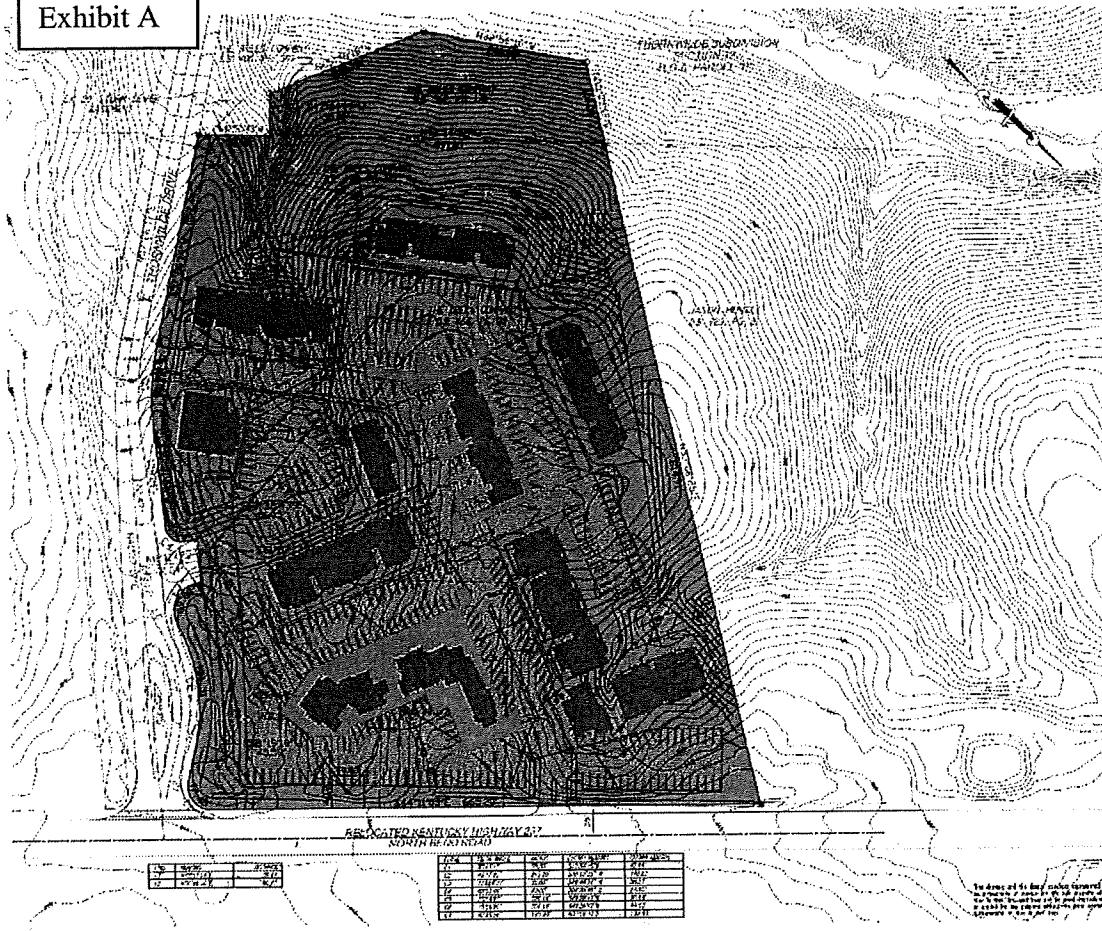
Based on the submitted Thornwilde Subdivision concept plan and the clarification and issues addressed, it is our opinion that the rezoning and plan revision are in agreement with the previously adopted Boone County Comprehensive Plan and is consistent with the RPD approved zoning. We respectfully request such a finding and recommendation from the Planning Commission.

Thank you for your time and consideration of this request.

Sincerely,


Philip Drees
President
Thornwilde Inc.

Exhibit A



SECRET

CONCEPT PLAN
THORNWILDE VILLAGE
HERRON, BOONE COUNTY, KENTUCKY

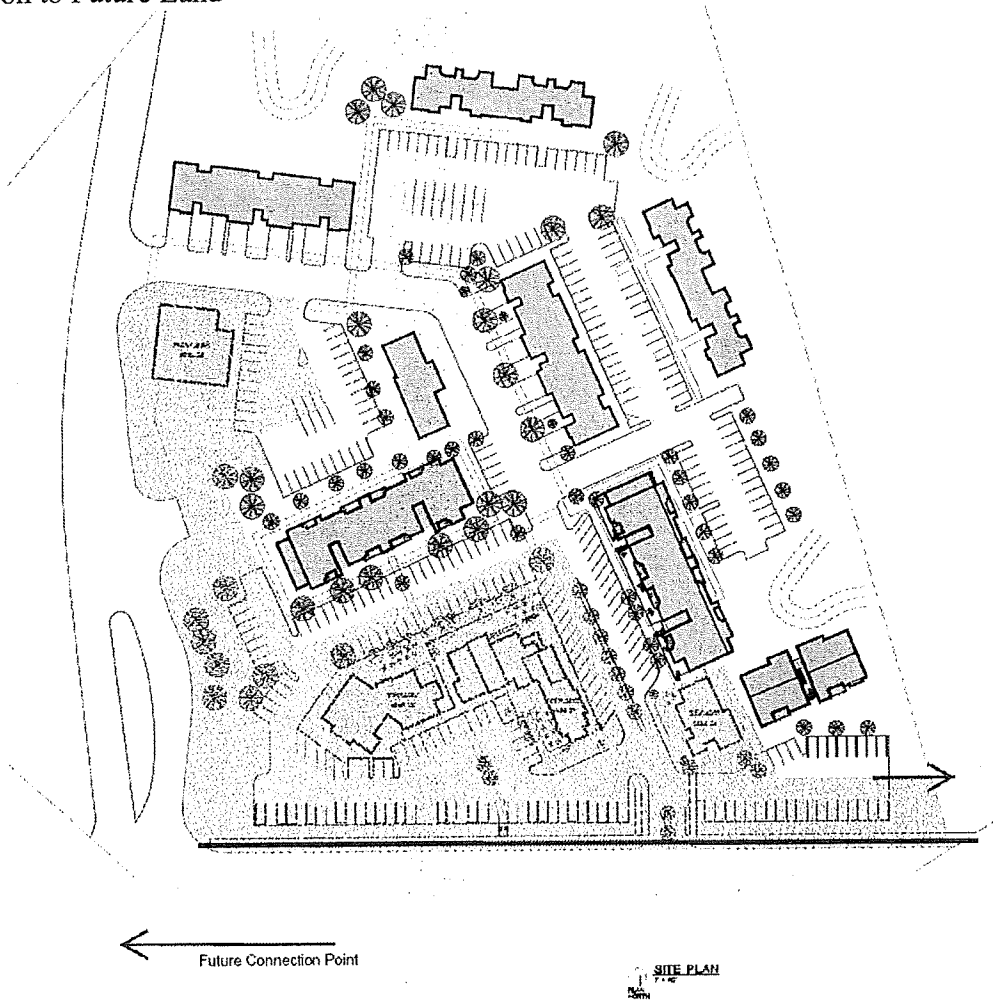

VIOX & VIOX

DATE	DESCRIPTION
10/1/00	PRELIMINARY
10/1/00	FINAL

PAGE 1

The area of this map is reserved for the use of the State of Kentucky and is not to be used for any other purpose without the express approval of the State of Kentucky.

Exhibit B
Connection to Future Land



CONCEPT PLAN	
THORNEWILD VILLAGE	
PRELIMINARY PLAN	
	
Hub + Weber Architects, PLLC	
10000 1st Avenue Suite 200 Thornewild, VA 22081 (703) 441-1111	
Scale:	1" = 20'
Revised:	11/2014
ARCHITECTURAL SITE PLAN	
C101	
11/14	



John E. "Jack" Pflum, P.E. Consulting Engineering

7541 Hosbrook Road, Cincinnati, OH 45243

Email: jackpflum@cinci.rr.com

Telephone: 513.919.7814

MEMORANDUM REPORT

TRIP GENERATION STUDY

A COMPARISON OF ALTERNATE DEVELOPMENT PLANS FOR THORNWILDE SUBDIVISION BOONE COUNTY, KENTUCKY

TO: Phillip Drees, President, Thornwilde, Inc.
FROM: Jack Pflum, PE, JEP Consulting Engineering
SUBJECT: Trip Generation Analysis
CC: Michelle Bollman, PE, Viox & Viox

DATE: July 14, 2014

Purpose

The purpose of this Memorandum Report is to provide an analysis of the trip generation volume of different land use development plans for the area generally known as Thornwilde Subdivision.

Description of Analysis

The extent of development and proposed reconfiguring of the Approved Concept Development Plan is fully described in the Boone County Planning Commission Staff Report, dated July 2, 2014. It is assumed the reader of this Memorandum Report is fully familiar with the contents of the Staff Report. Additional background information was provided by Mr. Phillip Drees.



The four development scenarios used for comparison are:

Scenario A – Approved for development to date by the original Concept Development Plan. Includes 287 Single Family Detached (SFD homes), 240 Single Family Attached (SFA) homes, and 26,000 sq ft flex space commercial use.

Scenario B – Includes a proposed change in the Approved Concept Development Plan for an approximately 11 acre parcel. The approved land use included 11 SFD homes and 26,000 sq ft flex space commercial use. The proposed land use change will substitute 120 multifamily homes and 16,000 sq ft of flex commercial space for this parcel.

Scenario C – Proposed land use change for the 11 acre parcel. The previously approved land use would be replaced with 120 multifamily homes and 16,000 sq ft of flex space commercial use.

Scenario D – The original Thornwilde Subdivision area (Scenario A) has been expanded with an approved expansion of additional residential units. The approved total is 673 single family detached homes and 354 single family attached homes.

Trip Generation

Trip* generation estimates are calculated by reference to the Institute of Transportation Trip Generation Manual, Eighth Edition. Average daily and peak hour rates were used to estimate the total trips generated.

*NOTE: A trip is a one way direction of travel. For example, travel from home to work and back to home accounts for two trips.

1. Single Family Detached Homes (SFD) – Land Use Code #210 (Single Family Detached)
2. Single Family Attached Homes (SFA) – Land Use # 230 (Condominium/Townhouse)
3. Multifamily Homes – Land Use Code #221 (Low Rise Apartments)
4. Flex Space Commercial – Land Use Code #814 (Specialty Retail)

Table 1, 2, 3, and 4 below show the trips generated (rounded to the nearest 5) for each of the above Scenarios.



Table 1 – Scenario A Trips

LAND USE (UNITS)	24 HOUR	AM PEAK HOUR	PM PEAK HOUR
SFD (287)	2745	215	290
SFA (240)	1395	105	125
COMM (26,000)	1150	70	180
TOTAL	5290	390	595

Table 2 – Scenario B Trips

LAND USE (UNITS)	24 HOUR	AM PEAK HOUR	PM PEAK HOUR
SFD (276)	2640	205	280
SFA (240)	1395	105	125
MULTI FAMILY (120)	790	55	70
COMM (16,000)	710	45	110
TOTAL	5535	410	585

Table 3 – Scenario C Trips

LAND USE (UNITS)	24 HOUR	AM PEAK HOUR	PM PEAK HOUR
MULTIFAMILY (120)	790	55	70
COMM (16,000)	710	45	110
TOTAL	1500	100	180

Table 4 – Scenario D Trips

LAND USE (UNITS)	24 HOUR	AM PEAK	PM PEAK
SFD (673)	6440	505	680
SFA (354)	2055	155	185
COMM (26,000)	1150	70	180
TOTAL	9845	730	1045

The above calculations are based on average values for the land use units.



Total trips for each of the above four Scenarios are summarized in Table 5.

Table 5 – Total Trips for Each Scenario

SCENARIO	24 HOUR	AM PEAK HOUR	PM PEAK HOUR
A	5290	390	595
B	5535	410	585
C	1500	100	180
D	9845	730	1045

Comparison of Trip Generation Traffic Volumes

Comparison of Scenario A and Scenario B in Table 5 shows that the proposed land use change for the 11 acre site increases the 24 hour trips by 245, increases the AM peak hour trips by 20, and decreases the PM peak hour trips by 10.

Comparison of Scenario A (approved Concept Development Plan) with Scenario B (includes the change of land use for the 11 acre parcel) shows that 24 hour trips will increase by 4.6%, AM peak hour trips will increase by 5.0%, and PM peak hour trips will decrease by 1.0%.

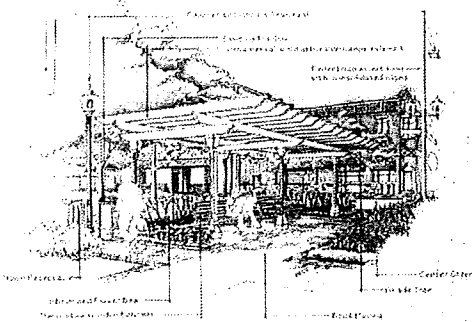
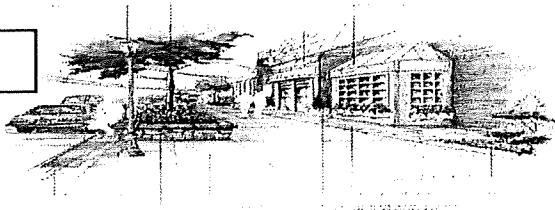
The net additional traffic generated by Scenario B has a modest impact on overall traffic volumes for the expanded Thornwilde Subdivision area as described in Scenario D. Table 5 shows that 24 hour trips will increase by 245 (2.5%), AM peak hour trips will increase by 20 (2.7%), and PM peak hour trips will decrease by 10 (-1.0%).

Conclusion

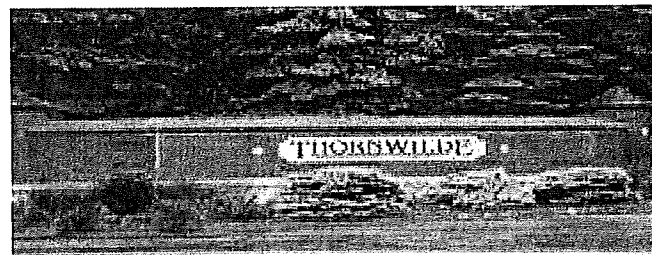
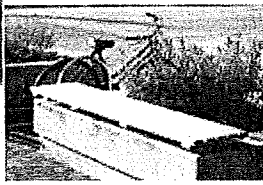
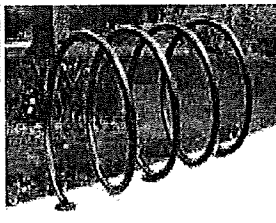
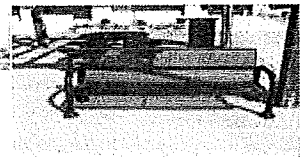
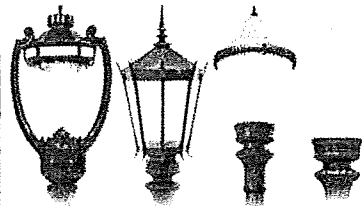
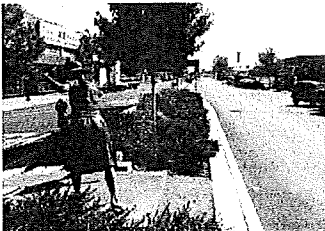
Several sources were investigated for the purpose of obtaining traffic count volume on KY 237 in the vicinity of Thornwilde Dr. There was no credible current count data available.

The increase in traffic volumes associated with the proposed change to the overall approved Concept Development Plan is modest.

Exhibit D



Streetscape Ideas



BOONE COUNTY PLANNING COMMISSION
BOONE COUNTY ADMINISTRATION BUILDING
BOONE COUNTY FISCAL COURTROOM
PUBLIC HEARING
JULY 2, 2014
7:30 P.M.

COMMISSION MEMBERS PRESENT:

Mr. Ben Brandstetter
Mr. Greg Breetz
Mr. Mike Ford, Vice Chairman
Mrs. Janet Kegley
Mr. Jim Longano
Mr. Don McMillian
Ms. Lisa Reeves
Mr. Charlie Rolfsen, Chairman
Mr. Bob Schwenke
Mr. Steve Turner

COMMISSION MEMBERS NOT PRESENT:

Mr. Randy Bessler
Mr. Kim Bunger
Mr. Mark Hicks
Mr. Charlie Reynolds
Ms. Susan Schultz

LEGAL COUNSEL PRESENT:

Mr. Dale T. Wilson

STAFF MEMBERS PRESENT:

Mr. Kevin P. Costello, AICP, Executive Director
Mr. Kevin T. Wall, AICP, Director, Zoning Services

Chairman Rolfsen called the Public Hearing to order at 7:33 P.M. and introduced the first item on the Agenda:

CHANGE IN APPROVED CONCEPT DEVELOPMENT PLAN - Kevin Wall, Staff

1. Request of **Thornwilde Inc. (applicant)** for **The Drees Company (owner)** for a Change in an Approved Concept Development Plan in a Residential Planned Development (RPD) zone for a 8.648 acre site located on the northwest corner of the North Bend Road/Thornwilde Drive intersection, including the tract at 1207 North Bend Road, Boone County, Kentucky. The request is for a Change in an Approved Concept Development Plan to allow a combination of commercial, attached single family residential, and multi-family residential uses.

Staff member, Kevin T. Wall, presented the Staff Report, which included a Powerpoint presentation (see Staff Report). He explained that the request is a Change in an Approved Concept Development Plan. He noted there have been a number of changes in Thornwilde Subdivision over the last several years. The original zone change goes back to 1994. The original zone change included 287 detached single family dwelling units, 240 attached single family dwelling units and approximately 26,000 square feet of commercial space along North Bend Road on 217 acres. The current application is to reconfigure the site into a village concept with approximately 16,000 square feet of commercial space and 120 attached single family dwelling units and/or multi-family dwelling units. The site includes a wooded hillside and has a house located on the property.

Mr. Wall referred to the Comprehensive Plan's Future Land Use Map. It suggests Suburban Density Residential (SD), single-family housing up to 4 units per acre on 50% of the site, Developmentally Sensitive (DS) on 25% of the site backing up to the Thornwilde HOA area and Commercial (C) for the remaining 25% of the site. Mr. Wall explained that the original approval not only included commercial space but also 11 detached single-family residences. This area has a separate access point off Thornwilde Drive. The proposed residential use is part of the integrated village approach of an additional 111 units compared with the original approval. Mr. Wall explained that the applicant submitted a narrative requesting some flexibility in terms of switching out some of the commercial to residential and adding second or third floors to the commercial for additional residential dwelling units. The original overall density for Thornwilde Subdivision would increase from 1.9 units per acre to 2.1 units per acre based upon 1,037 dwelling units on 545 acres. The original density in 1994 for the 217 acres was 2.4 units per acre. The density of the site in question is just shy of 14 units per acre. However, the overall density is being reduced compared to 20 years ago. The basic plan includes townhouses with individual driveways and garages. Village Drive is planned to be a looped road from North Bend Road to Thornwilde Drive. Mr. Wall showed a drawing of the planned open space and building architectural styles. He showed photographs of the site and adjacent properties.

In terms of Staff comments, Mr. Wall referred to the Staff Report and various references to the Comprehensive Plan on pages 5-14 and summary of the issues on pages 15-18. A key issue of the proposed project is a lack of sanitary sewer capacity. SD1 has submitted written comments about this issue in the Staff Report. Improvements are possible but there is no definite funding source. Another key issue is meeting the RPD zoning standards in regard to overall design and amenities - section 1607, 1608 and 1609 of the zoning regulations. Access to the site from North Bend Road is by permit only. The overall recurring theme in the RPD standards is open space. More details are needed about the location and planned amount of open space. Are there going to be any recreational facilities for the proposed project as noted in section 1609?

How much of the existing vegetation will be retained? Will there be any improvements - paths, shelters, etc? What about buffering along the northern property line? It is zoned residential and has a house located on the property. Mr. Wall suggested an urban design road with streetscape features for the proposed Village Drive. It would include sidewalks and street trees on both sides. It is also recommended that the intersection in the middle of the development be four-way instead of being off-set. The applicant submitted a collage of architectural styles but has not selected a preferred style like the neighboring project. There was a condition that tied down the building architecture in that project. The applicant did not address the proposed signage for the project as required in Article 16. Mr. Wall suggested the signage requirements outlined in the Commercial One (C-1) zoning district and the Small Community (SC) Overlay District zoning district. Mr. Wall included examples of signage from the Toebben development in the Staff Report. Mr. Wall offered some suggestions for screening the proposed development along the northern property line. This included retaining some of the existing vegetation and planting new features on a berm. Mr. Wall referred to a condition from the original application that limited the commercial uses to C-1 uses. The applicant has not addressed this issue in the new application. There is no specific building height mentioned in the commercial area. It could be multi-story with residential on top - maybe 2 or 3 stories high. Mr. Wall also noted that the 4 acre tract located north of the project site could develop in the future. Should there be a driveway, street, or emergency connection? He further questioned the peak hour traffic projections and requested more information such as the peak hour of 90 trips a.m. or p.m.? What are the turning movements? Are there any level of service changes? Are improvements necessary? Mr. Wall also asked the applicant to verify whether all the utilities will be underground? What is the acreage of the site? Is it +/- 8 acres or is it 8.60 acres? The tax records indicate 12 acres. The for sale sign says 11 acres. It is important to obtain the correct figure because it affects the project's density. The name Village Drive cannot be used since it is already taken for emergency response purposes. Mr. Wall concluded that since the proposal was at the Concept Development Plan stage, the project will still need to meet all of the applicable requirements of Site Plan and Subdivision Review.

At this time, Chairman Rolfsen asked if the applicant was ready to make a presentation?

Mr. Phil Drees, applicant, said he has lived in the Hebron area for 15 years and actually in the subdivision for 7 years. He has been a developer since 1998. He explained that he does smaller projects not large ones. Mr. Drees referred to a Powerpoint presentation. He explained that the request is a Concept Development Plan change and not a Zone Change. Mr. Drees noted that 4 acres of commercial use has already been approved for the site. Thornwilde Subdivision was originally approved in 1994 at 2.5 units per acre. He showed a copy of the original Thornwilde plan. The site in question had about 8 acres of residential use with single family detached lots. There is a built-in buffer from the site to the remaining portion of the subdivision. In 2003, Thornwilde Subdivision grew with additional acreage and attached units were relocated to the rear of the subdivision. Thornwilde Drive connects to Williams Road. The total number of units for the subdivision was 364 attached units and 673 single-family units on 545 acres for an overall density of 2.02 units per acre. The RPD zone allows up to 4 units per acre. Today, Thornwilde includes a new school. When this happened, 208 attached units were eliminated. Some single-family units were added. The current density is now 1.05 units per acre. More open space was added along with a trail and a second community facility. With his proposed project, the overall density will be 2.06 units per acre. He also presented a side by side Concept Plan comparison. Initially, there were 11 single-family lots in the rear of a large retail strip center. The

new concept involves a better design and feel for a residential neighborhood. The new design is more exciting and inviting. He referred to a project in West Chester called The Point. They create space for people to gather. The design was a L-shape, which resulted in creating a courtyard and space around a group of buildings and not just one large building. In addition, they brought the buildings closer to the street. They also have a residential appearance. Mr. Drees explained the basis for recommending approval. The proposed change still keeps Thornwilde Subdivision below the overall planned densities as approved in 1992. It meets the Goals and Objectives of the Boone County Comprehensive Plan. It offers a better and creative transition of uses following the adopted Boone County Comprehensive Plan. The addition of the Thornwilde Village mix will compliment the surrounding land as designated on the Comprehensive Plan. It satisfies Article 16 of the zoning regulations. The Concept Plan change does not hinder nor deter development of surrounding undeveloped properties. The RPD zoning allows for flexibility in site layout, approaches and design. It also already allows and permits combination of uses. The changes in economic, social, and infrastructure to the surrounding area makes the proposed project a better plan than originally proposed. It provides a range of housing types and KY 237 improvements have been completed. There are 3 lanes in front of the site with bike lanes. The proposed project and plan accomplishes a pedestrian scale community that people can live in close proximity to a mix of uses. The characteristics in the land topography and necessary utilities are being addressed more efficiently and economically. Mr. Drees stated that he felt the neighborhood impact is less than what Thornwilde was originally designed and approved. The density is lower and there was a reduction in the number of attached units in the back. There is less traffic due to the decrease in the number of units. There is a minimal or no impact on schools with the typical demographic that resides in the planned units. The site is separate from the remainder of Thornwilde Subdivision. The residents that will live in the proposed project will not use or belong to the HOA facilities. The original Concept Plan created a disconnect between uses. The new concept embraces the commercial and residential components into an inviting, gathering place type of setting. The attached/multi-family developments should have convenient access to thoroughfares and commercial districts. The proposed development has a pedestrian scale and orientation that makes it walkable. The widening of KY 237 was designed to handle growth in Hebron.

Mr. Drees also stated that he surveyed the surrounding community. There were over 415 responses and 97% of the people who responded lived within 5 minutes of the site. The residents were very clear on what they did not want to see at the site. They didn't want to see a typical, strip center but wanted something more inviting. Mr. Drees showed some examples of what the current Concept Plan would allow. The survey indicated that 91% of the people chose surroundings that included a lifestyle gathering place center surrounded by high density residential. The survey also indicated that 75% of the respondents would ride their bikes to Thornwilde Village and 59% responded that attached multi-family should surround the retail center.

Mr. Mike Schoettelkotte, stated that the Drees Company owns the subject property and has a contract to sell the property to Phil Drees. Over the last 20 years, there have been a lot of changes in Thornwilde Subdivision. The size of the community doubled. Thornwilde was the primer for the Hebron area. The Drees Company worked to obtain the sanitary sewer for the area. It was the first planned development for the area. The commercial was planned so many years ago. Drees didn't know that the commercial would happen. It was their best guess of where to put the commercial. The big question was on the remaining portion of the site. What do they do? It was too early for additional attached or multi-family housing. That type of housing

was placed in the back portion of the subdivision because it would take awhile. It has been 20 years and Drees still hasn't built the attached housing yet. A lot has been completed in Thornwilde Subdivision - clubhouse, 2 pools, a lot of open space, walkways and a school. Sewer capacity is an issue. There is an on-going effort to address it. The sanitary sewer issue will resolve itself. Mr. Schoettelkotte stated that Mr. Drees has taken the concept of the commercial and residential to another step. It fits in the area. On the original Concept Development Plan, there wasn't much open space in the 12 acre site. However overall, there was a substantial amount in the subdivision which meet the RPD requirements. The Drees Company is very supportive of the project.

Chairman Rolfsen asked if there was anyone in the audience who wanted to speak in favor or against the request?

Mr. Richard Catron, 2421 Winter Woods, stated that the sanitary sewer pumping station next to the site smells badly at various times a year. Several people have moved because of this situation. It is brought up at each HOA meeting. It is a major issue that needs to be addressed now. If one leaves Thornwilde Subdivision between 7:00 a.m., it takes you 20 minutes to get to I-275. If you leave at 10:00 a.m., it takes only 2 minutes. Traffic backs up because of the roundabouts and traffic signals. With Amazon located near I-275, it was impossible to travel through the area. The proposed project will bring more people to the area. There are strip retail centers already failing. There are already vacant apartments in the area. It is not a profitable business. People will not visit the proposed commercial area because it doesn't have the same amenities like Crestview Town Center.

Ms. Laura Renke, 1238 Crosspointe, stated that there are only 2 lanes between the roundabouts. There is no good flow. The green space or open space is owned by the Thornwilde HOA. It is not part of the proposed development. The survey that was conducted was limited to only 2 options. There was not a third option of not liking any of it. It seemed that the applicant is unsure of a lot of his proposal. Why change a concept until you are sure of what you want to do? If you change the concept and he is unsure about it, who knows what we will get?

Mr. Mark Schuller, 1325 Coastal Court in Union, Kentucky. He explained that he lives in Triple Crown and he has a strip commercial development near his subdivision. There is a sidewalk that allows residents to walk to it. It is constantly busy with people, golf carts and bikes. It is nothing like what the applicant has built in West Chester. The atmosphere is significantly less and it creates a culture and atmosphere like Dillard's at Crestview Hills. It takes a visionary to make it happen. He stated that he would like something like the West Chester development at the end of his subdivision where there is more gathering spaces.

Mr. Jason Monell, 1173 North Bend Road, states that his property will be immediately impacted by the proposed development. To require no green space is appalling. The development is being crammed right up to their backside. There will need to be a traffic signal due to the amount of traffic in the area. This will destroy the road project. The project will not only impact the Thornwilde residents but everyone along North Bend Road.

Ms. Sherry Herald, 1409 Stoney Hollow Court, explained that all of the marketing material about Thornwilde given by the Drees Company showed that the site would be a single-family area. There was no mention of townhomes or commercial. She inquired about the curb cut off

Thornwilde Drive. Drees officials responded that it was a pull off for the concrete trucks. The area was going to be patio homes. She expressed a concern about traffic patterns and the impact on Thornwilde Drive especially morning traffic. What will be the effects of the development on the creek and trails? Ms. Herald stated she is opposed to apartments because the subdivision is made up of single-family residences. The school impact should really be looked at in greater detail. The proposed development will have an impact on the Thornwilde HOA primarily trespassing on the trails, roads and pools. It can't be a gated community because of the school. The existing commercial could be divided up now. The survey did not include elderly people and it did not verify whether people lived in the subdivision. She also asked about others who could not attend the Public Hearing. She asked the Planning Commission to deny the request. Mr. Costello responded that the Planning Commission followed the State public notification requirements posting a sign on the site, notifying the adjoining property owners and publishing a legal ad in the *Boone County Recorder*.

Ms. Laura Renke, stated that she is on the Thornwilde HOA Board. The board put the Public Hearing notice on their website. The HOA did not do a mailing.

Mr. Bart Porter, 206 Robson Lane, stated that he liked the new concept because it is more aesthetically pleasing than a typical box strip center.

Mr. Jon Salsbury, 1233 Thistle Down Drive, noted that he loved the neighborhood because of the design - lakes, trails and trees. He stated that there were 2 signs posted advertising the survey. He actually took a picture of the sign and posted it on the Thornwilde Facebook group page. There were 73 comments. There was a fair amount of visibility. The survey was designed not to respond in a negative way. The North Bend Road/Thornwilde intersection is busy and painful to pull out at peak times. A traffic signal should be installed. Mr. Salsbury said that he likes the original approved design or concept. The impact on the school will be a negative due to the increase in number of residents. The mixed use marketplace is a good thing particularly the gathering space. Comments on Facebook were supportive of this notion. He did not hear from anyone who supported the mixed housing.

Ms. Trina Webb, 2533 Southwind Court, explained that Thornwilde is a community of single-family homes. They may have planned additional multi-family homes. Having a single-family residential street built off Thornwilde Drive is perfectly acceptable. There is no need to have a big box store on the site. If Mr. Drees has the ability to make it aesthetically pleasing with the village concept, he can do it. Perhaps a Starbucks could be located in the commercial area. The survey didn't give you an option of any input or options particularly the multi-family buildings. She stated that she preferred the original plan.

Mr Jimmy Howell, 1736 Coachtrail Drive, stated that he thought the new plan was much more pleasing than the original plan. He preferred to drive down the road and see the new design. To see something similar to what is near the interstate would not be pleasing. The proposed project is a good idea.

Mr. Jim Dolwick, 1234 North Bend Road, stated that since it has already been approved that it is difficult to change horses in mid-stream. There will be 109 families and over 1,000 homes from the Toebben development. There is a traffic problem now and it will only get worse.

Ms. Shaun Fitzgerald, 2455 Posey Court, stated she is not opposed to commercial development if the existing properties along North Bend Road were doing okay. There are more strip centers empty now than they are full. In 3 years, she has seen 6 businesses close. It is a high turnover rate. If these buildings aren't full, who will drive 2 miles down the road to shop? Empty buildings cause problems - transient people and crime. Ms. Fitzgerald stated she is opposed to apartments but is open to condominiums and patio homes like in Triple Crown. No one anticipated apartments on this site with people paying rent. Toyota is leaving the area and there will be a lot of vacant homes. The schools are crowded.

Mr. Rob Speicher, 1524 Blooming Springs, stated that Drees has a sign near the entrance of the subdivision showing the value of the houses generally mid \$200,000 - \$350,000. The sweet spot is \$300,000 - \$350,000 range. He is not opposed to an upscale development. One should consider the value of the surrounding area in designing the Village project.

Mr. Jeff Shroeder, 1408 Stoney Hollow, stated that as the community was built, it evolved into a single-family environment with costly homes. It seems disingenuous to make a change from single-family expensive homes to lower higher density units. It was not part of the original plan. There is also a lack of details in the submitted plan.

At this time, Chairman Rolfsen asked if the Planning Commission members had any questions or comments?

Chairman Rolfsen asked about the acreage figure. Mr. Drees responded that it is about 11 acres which includes the 4 acres of commercial. Mr. Wall stated that the overall density does not change. Chairman Rolfsen asked what is the estimated number of people living in the development? Mr. Drees replied 120 units. He stated that he didn't know exactly the number of 1 bedroom, 2 bedroom, or 3 bedroom units. He wanted flexibility. It could be all for sale units. Chairman Rolfsen asked does the HOA allow house rentals? An unidentified person in the audience responded no.

Mr. Longano asked what is the plan for offering amenities for the project other than the gathering area? Mr. Drees responded that it is the strip center area. The multi-family area is subsidizing the commercial area. He also stated that since he is upgrading the commercial area, he needs a higher density to support the cost. It usually takes at least 250 units to support a community pool. The proposed project is only 120 units. He assumed that the Thornwilde residents did not want the proposed project to be part of the HOA. He would pay dues if they would allow it. The Point at West Chester has a trellis, patio and fountain.

Mr. Schwenke questioned the status of the sewage. Is just one area or does it involve others like Toeppen? Mr. Schoettelkotte replied that the sewer issue affects all new development in Thornwilde and other communities in the area. The Sand Run lift station has been up sized a number of times and there were plans to replace it and new force main. SD1 has no money to make the big improvement so development can continue. There are a certain number of homes that have received capacity reservation to have sewage flow in to the Sand Run lift station but it is limited. On an interim basis, SD1 will be making some temporary improvements with the installation of a holding tank to allow the capacity that has been reserved to be used. But beyond what has already been approved, new communities will not be able to develop until the major improvement is made. The proposed development would not be able to tie into the

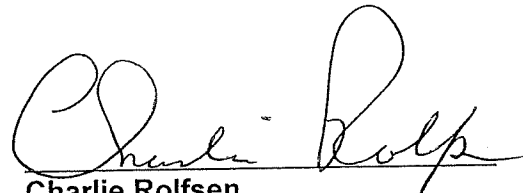
current system because of the capacity issue until the major improvement is made. The cost of it is between \$7 - 9 million. A small part of Toebben's development can occur now. Mr. Schoettelkotte noted that the holding tank will address the odor issue by preventing an overflow. Sewage will be held back and then pump back to the plant off hours. Mr. Schwenke asked about the location of water retention on the project site? Mr. Drees showed 2 possible locations of retention/detention ponds. The commercial area may have an underground system.

Mrs. Kegley asked the applicant to investigate the size of the units in terms of number of bedrooms and impact on the schools. She indicated that she was unaware if the school district had a concern.

Chairman Rolfsen asked the Staff about the type of traffic study being requested. Mr. Wall replied that the Staff Report suggested some items to look at even if the applicant doesn't know exactly the type of unit he will build. What is the basis for the peak hour figure? What is the change in level of service? This will address the need for any improvements like turning lanes and a traffic signal. Chairman Rolfsen asked the applicant to validate the 90 trip figure. Mr. Brandstetter asked if Mr. Drees would have this information available at the Committee Meeting? Mr. Drees responded that he could have an average figure. Mr. Brandstetter told the applicant to discuss the study with the Staff. Different land uses have different trip generation.


There being no further questions or comments, Chairman Rolfsen announced that the Committee Meeting for this item will be on July 16, 2014 at 5:00 P.M. in this room. This item will be on the Agenda for the Business Meeting on August 6, 2014 at 7:00 P.M. Chairman Rolfsen closed the Public Hearing at 9:02 P.M.

APPROVED:



Charlie Rolfsen
Chairman

Attest:



Kevin P. Costello, AICP
Executive Director

ZONE CHANGE/CONCEPT PLAN COMMITTEE VOTE

TO: Boone County Planning Commission

FROM: Ben Brandstetter, Chairman


DATE: July 16, 2014

RE: CHANGE IN APPROVED CONCEPT DEVELOPMENT PLAN - Ben Brandstetter, Chairman, Kevin Wall, Staff

1. Request of **Thornwilde Inc. (applicant)** for **The Drees Company (owner)** for a Change in an Approved Concept Development Plan in a Residential Planned Development (RPD) zone for a 8.648 acre site located on the northwest corner of the North Bend Road/Thornwilde Drive intersection, including the tract at 1207 North Bend Road, Boone County, Kentucky. The request is for a Change in an Approved Concept Development Plan to allow a combination of commercial, attached single family residential, and multi-family residential uses.

REMARKS:

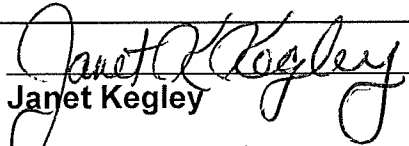
We, the Committee Members were present at the Committee Meeting and voted on the above request or else were absent from voting. Further, based upon the vote, the Committee directs the Staff to draft the findings of fact and conditions if deemed necessary in order to complete the Committee Report.



Ben Brandstetter, Chairman
For Project Absent _____
Against Project _____
Abstain _____ Deferred _____

Greg Breetz
For Project _____ Absent _____
Against Project _____
Abstain _____ Deferred _____

Kim Bunger
For Project _____ Absent _____
Against Project _____
Abstain _____ Deferred _____




Janet Kegley
For Project Absent _____
Against Project _____
Abstain _____ Deferred _____

Susan Schultz
For Project _____ Absent _____
Against Project _____
Abstain _____ Deferred _____

Charlie Reynolds (Alternate)
For Project _____ Absent _____
Against Project _____
Abstain _____ Deferred _____

Mark Hicks (Alternate)
For Project _____ Absent _____
Against Project _____
Abstain _____ Deferred _____

Jim Longano (Alternate)
For Project _____ Absent _____
Against Project _____
Abstain _____ Deferred _____


Randy Bessler
For Project
~~X Randy Bessler~~

TOTAL: _____ DEFERRED _____ FOR PROJECT _____ ABSENT _____
_____ AGAINST PROJECT _____ ABSTAIN _____

SUPPORTING INFORMATION



VIOX & VIOX

Civil Engineers, Surveyors, and Landscape Architects

466 Erlanger Road
Erlanger, Kentucky 41018

Tel: 859.727.3293

Fax: 859.727.8452

www.vioxinc.com

May 27, 2014

DESCRIPTION OF 8.648 ACRES

Located in Boone County, Kentucky, lying on the south side of North Bend Road (Kentucky Highway 237) and on the west side of Thornwilde Drive, being part of the same property recorded in Deed Book 466, page 27, Deed Book 466, page 30, Deed Book 562, page 170, Deed Book 490, page 309 and Deed Book 492, page 103 and is more particularly described as follows:

Unless otherwise stated, any monument referred to herein as an iron pin (set) is a ½ inch diameter rebar eighteen inches in length with a plastic cap stamped "PLS 3357". All bearings referred to herein are based upon Thornwilde Subdivision, Section 32.

Beginning at an iron pin (set) at the common corner of The Drees Company (Deed Book 466, page 30) and H.O.A Parcel "P" of Thornwilde Subdivision, Section 32 in the westerly right-of-way line of Thornwilde Drive, 30.00 feet as measured perpendicular to centerline; thence with said common line North 42°55'43" West 83.85 feet to an iron pin (existing); thence South 47°04'17" West 50.00 feet to an iron pin (existing); thence North 65°36'18" West 200.75 feet to an iron pin (existing); thence North 32°56'16" West 200.00 feet to an iron pin (existing); thence North 33°05'17" East 100.00 feet to an iron pin (existing) at the common corner of The Drees Company, H.O.A. Parcel "P" and Jason Monell (Deed Book 929, page 21); thence with a common line of The Drees Company and Monell North 33°06'25" East 760.08 feet to a point; thence leaving said common line South 42°26'50" East 139.09 feet to a point; thence South 23°26'19" West 111.95 feet to a point; thence South 66°42'29" East 88.61 feet to a point; thence South 23°05'07" West 167.68 feet to a point; thence South 66°38'27" East 253.27 feet to a point; thence South 23°37'12" West 161.04 feet to a point; thence North 66°40'32" West 43.85 feet to a point; thence South 25°01'45" West 29.49 feet to a point; thence North 66°40'32" West 68.15 feet to a point; thence South 24°42'12" West 76.73 feet to a point; thence South 54°17'31" West 26.69 feet to a point; thence South 35°42'29" East 161.83 feet to a point in the right-of-way line of Thornwilde Drive; thence with a said right-of-way line and a curve turning to the right with an arc length of 31.70 feet, a radius of 326.14 feet, a chord bearing of South 54°39'12" West and a chord length of 31.69 feet to a point; thence South 57°26'14" West 257.44 feet to the point of beginning containing 8.648 acres.

This description is based on a survey made by Gregory A. Larison, PLS 3357, for Viox & Viox, Inc. June 3, 2008. The purpose of this description is for preliminary planning purposes as required by the Boone County Planning Commission and does not meet the standards as outlined in 201 KAR 18:150 and is not to be used for property transfer.



Thornwilde, Inc.

510 Graves Ave Suite 206
Erlanger, Ky 41018

Office (859) 814-8186
Fax (859) 360-1761

Thornwilde Village

May 29, 2014

Boone County Planning Commission
2950 Washington St, Rm 317
P.O. Box 958
Burlington, KY 41005

Re: Thornwilde Subdivision – RPD Concept Plan Revision

Dear Boone County Commissioners:

We would like to request a review and recommendation of approval to the Concept Plan Revision of the Thornwilde subdivision RPD zone. This request is for an approximately 8-acre area located near the corner of Rt 237 and Thornwilde Drive - locally referred to as Francisville City limits.

Based on the submitted Thornwilde Subdivision concept plan, it is our opinion that the rezoning and plan revision are in agreement with the previously adopted Boone County Comprehensive Plan and is consistent with the RPD approved zoning. We respectfully request such a finding and recommendation from the Planning Commission.

Thank you for your time and consideration of this request.

Sincerely,

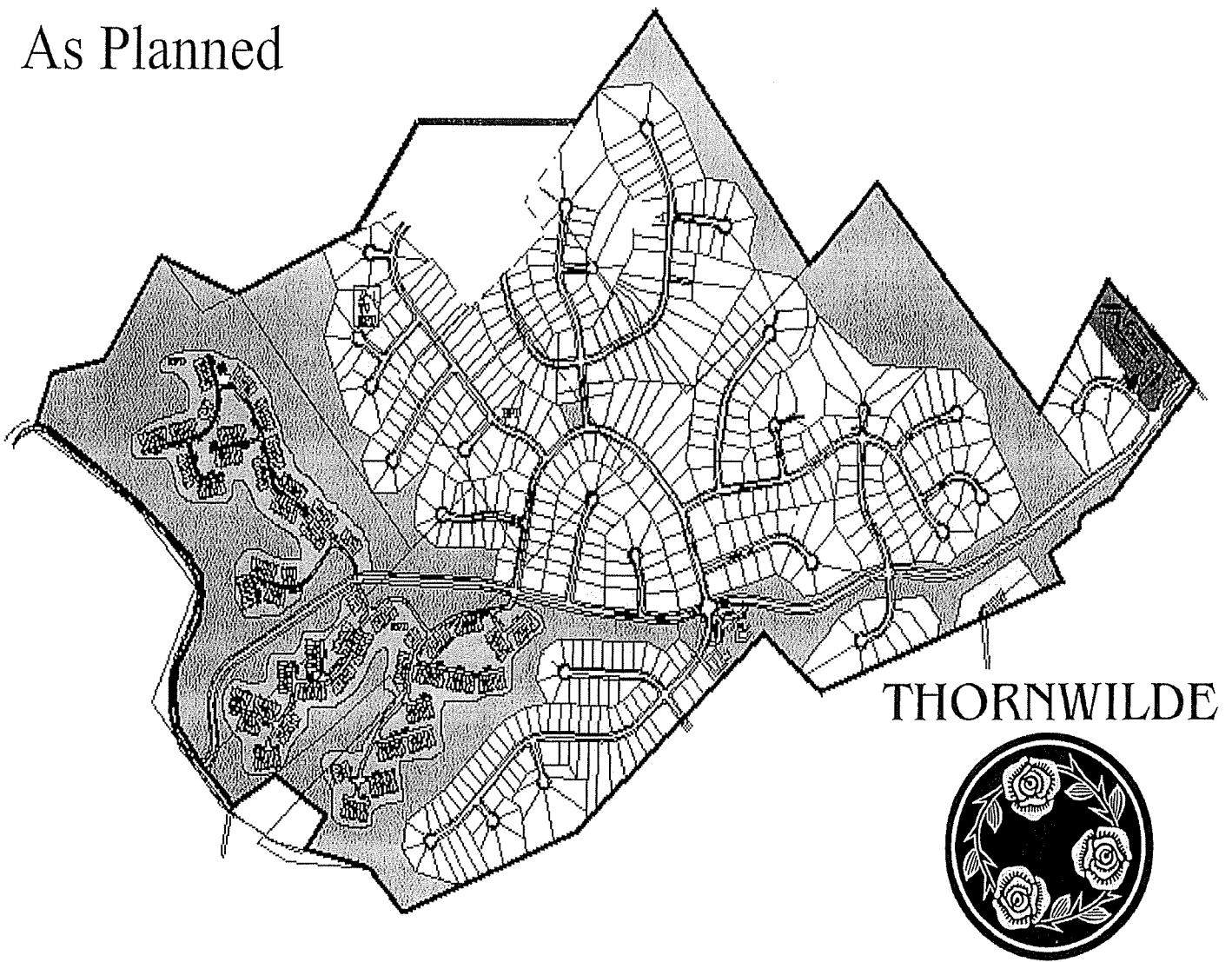
Philip Drees
President
Thornwilde Inc.

Thornwilde Village
Concept Plan Change
Francisville, KY

Thornwilde History: In 1994, Boone County approved Thornwilde Subdivision as a RPD zone. The original Thornwilde concept plan showed a total of 527 units on 217 acres, of which approximately half was going to be attached units (condo, townhomes or a higher density type product). The original concept plan also showed a 4-acre commercial area located at the corner of Thornwilde Dr and Rt 237. The density planned for Thornwilde was 2.5 units per acre with a wide variety of open space and community facilities.

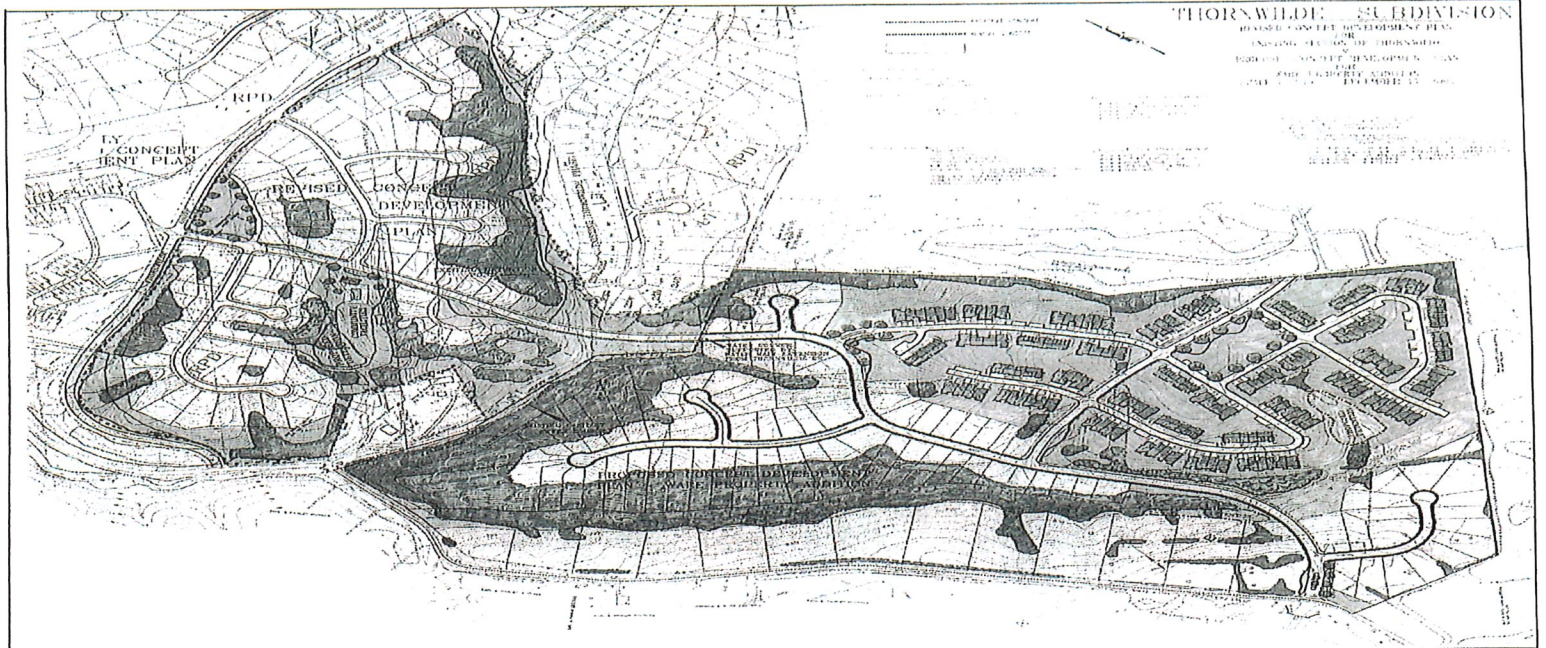
The following diagram shows the original Thornwilde layout.

As Planned

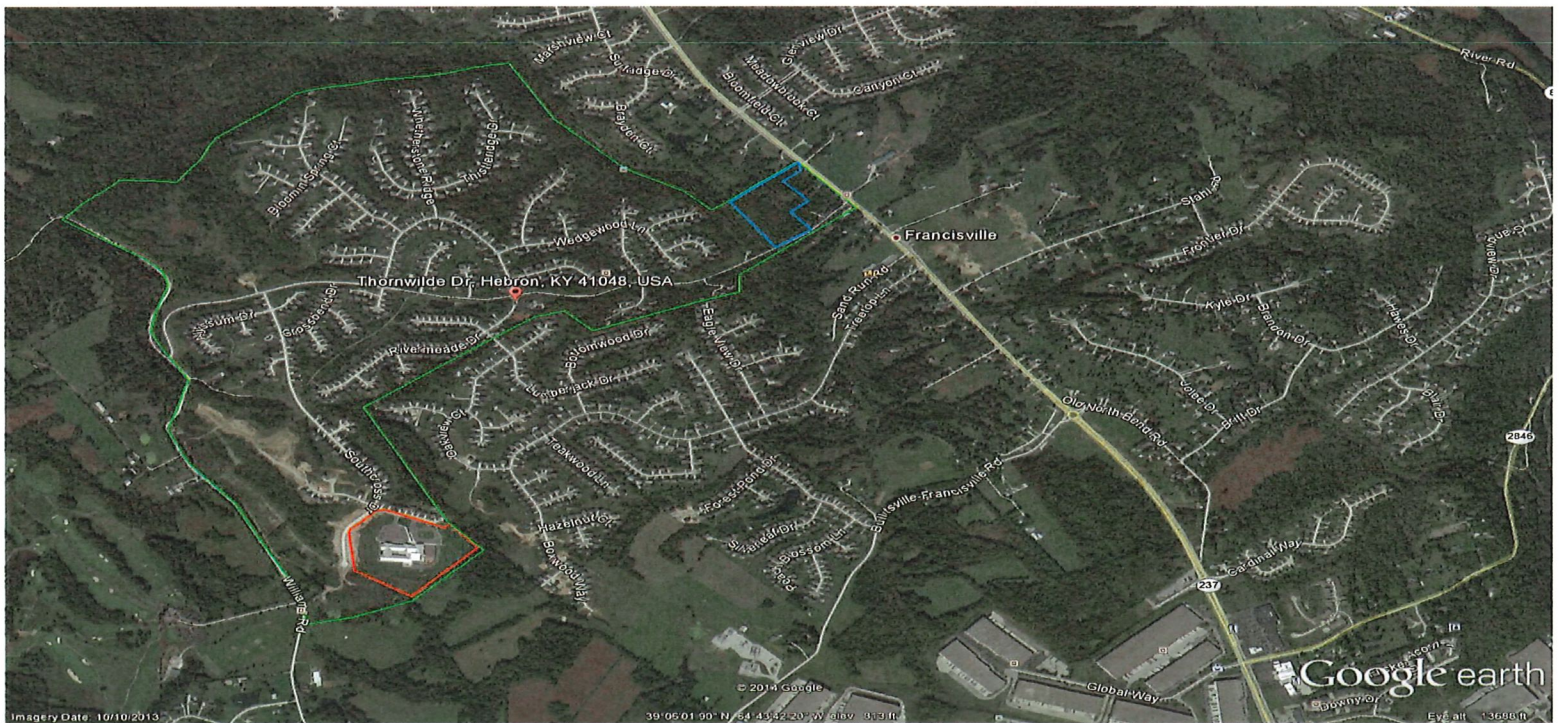


The flexibility of the RPD zone has allowed the Thornwilde concept plan to change over time to meet the needs of Boone County residents. The last concept plan revision approved in 2003 shows

a total of 1,037 homes on 545 acres (364 Attached units and 673 single family houses). Included in this revision was a new community facility/pavilion and pool, a continuation of the open space, pocket parks and a trail walkway system. The added amenities continued and enhanced the overall Thornwilde experience. Below is a diagram showing the approved 2003 concept plan revision.

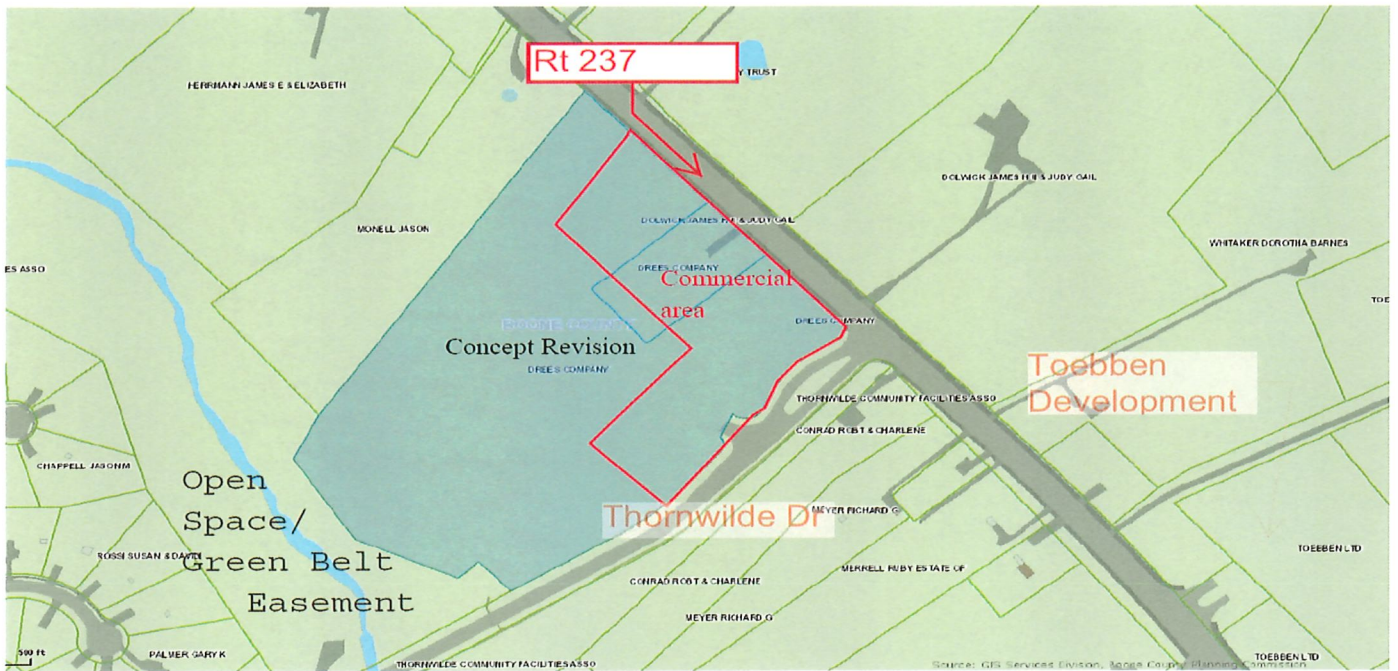


Today - The current plan remains very similar to the original approved submission. The change now includes a majority of the attached product as shown in the Southeast corner of the Thornwilde subdivision (above Diagram) has been replaced by the Thornwilde Elementary School as shown on the current diagram below.

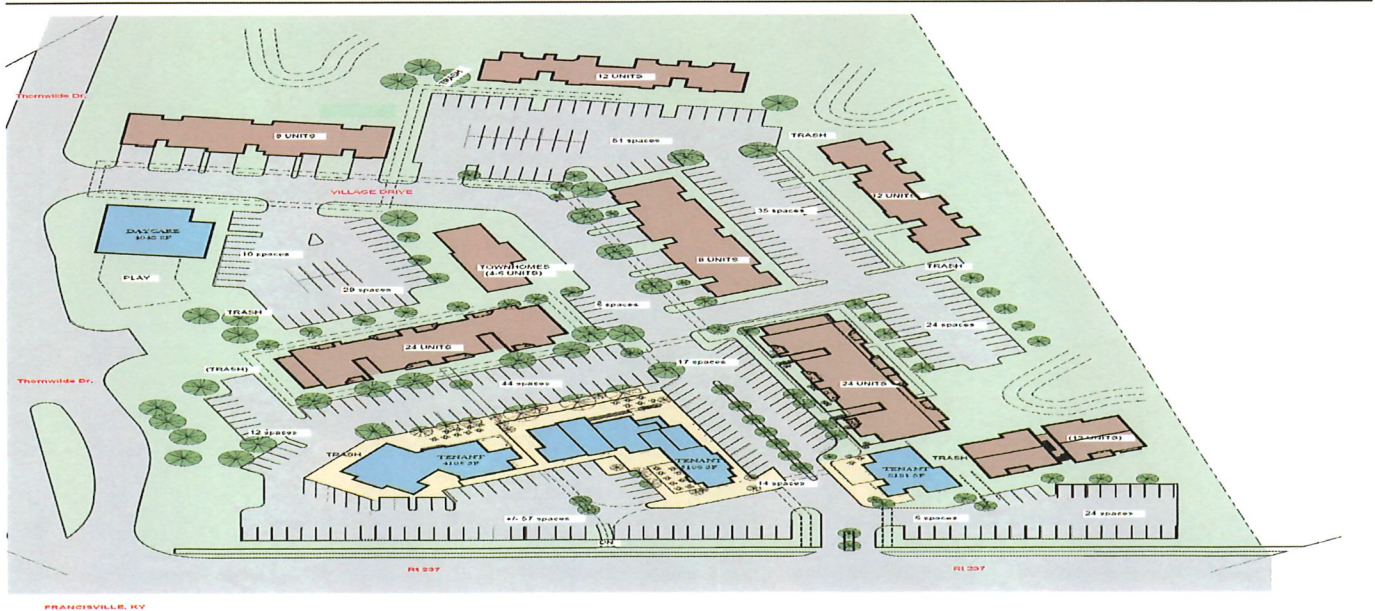


Thornwilde Subdivision Outline
 Thornwilde Elementary School
 Concept Plan Revision

1. Proposal: Request approval of a concept-plan revision of approximately 8 acres, from a RPD concept plan showing detached single family lots to a RPD concept plan showing Single-Family Attached and/or Multi-Family Residential.
2. Location: Located near the corner of Rt 237 and Thornwilde Drive and locally referred to as Francisville. The site is bordered by future commercial development at Thornwilde Dr. and Rt. 237– which was already approved by Boone County in 1994. Rt. 237 has been recently upgraded from a two-lane country road to a major collector road - three lanes wide with bicycle lanes and sidewalks on both sides of the road in anticipation of the Boone County growth. On the North side, the property is bordered by one house on four acres. The West property line abuts the Thornwilde open space/green belt and the South property line abuts Thornwilde Dr. (see plan below)



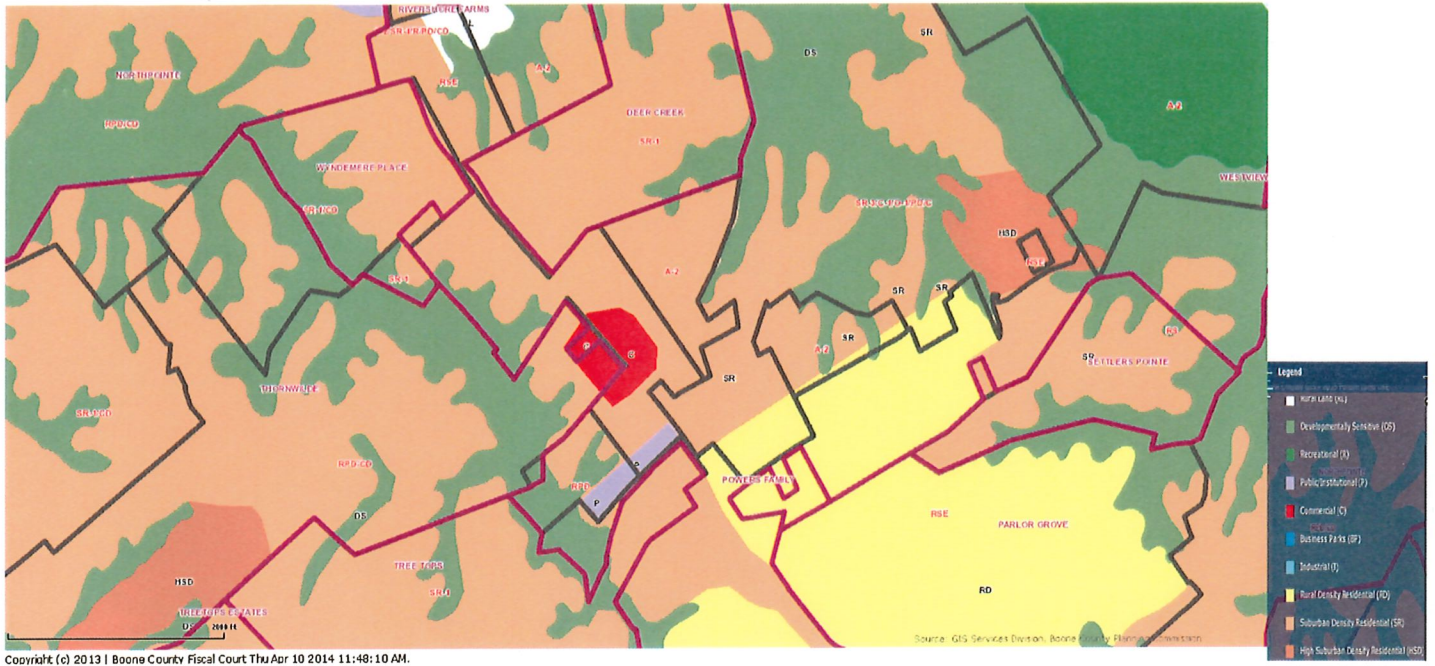
3. Land Use: The proposed plan revisions changes from Single-Family detached to Attached single family and/or Multi-Family units. The propose change will directly relate to the already approved commercial area in a way that will create a mix-use development to be known as “Thornwilde Village”. (see plan below)



4. Density/Intensity of Use: The original approved Thornwilde Development plan showed a density of 2.5 units/ac even though the Boone County RPD and SR-1 zone allows up to 4 units/ac. Currently, Thornwilde is at a density of 1.85 units/ac - 979 units/528 acres - nets out the new school location changes. The new plan of approximately 8 acres will replace 11 single family lots with approximately 120 attached units. This will change Thornwilde's density to 2 units/ac still well below the original concept plan of 2.5 units/Ac.
5. Building locations: The location of all attached product is shown on the submitted concept plan. Setbacks vary and parking fields are minimized along "Village Drive". Emphasis of building placement close to the commercial area is important to creating a walkable, pedestrian-friendly neighborhood center.
6. Buffering: The original approved concept plan didn't show much buffering between the commercial and single-family. It had two distinct entrance points for each use which caused a disconnect between uses. The concept revision invites the different uses into a "Village" setting therefore buffering between the commercial and attached product is not necessary. Natural buffering is provided along the West and North property line as topo and grades will necessitate the buildings to step down with grades. Also, the West property line is buffered by the already existing open space and the creek line separates all existing Thornwilde single-family homes by a considerable distance.
7. Vehicle and Pedestrian Circulation: Village Drive allows free flow of ingress and egress from the site while providing walkable access from the Thornwilde public right-of-ways and bike access off of Rt 237. Village Drive will strive to be a public dedicated street; however depending on width and setback requirements it may have to be private. The current access point on Thornwilde Drive will move south to allow a safer distance from the curved Thornwilde entrance and intersection of Rt 237.
8. Sanitary and Water Service: Public sanitary connection will tie into the Sand Run pump station once capacity issues have been address by the Sanitation District. Extension of the water distribution system will tie into the existing system serving the area.
9. Soil Erosion Control: Best management practices will be incorporated into the site improvement plans at time of submission.
10. School Impact: Minimal impact of additional school age children is expected as most units will be occupied by empty-nesters, single adults and young couples with no children.
11. Thornwilde HOA Impact: Since the site is distinctly separate from the remainder of the subdivision, the residents living at Thornwilde Village will not be a part of the HOA; therefore, no impact on any existing facilities.
12. Traffic Impact: The proposed development is estimated that approximately 90 trips during peak traffic hours. Since the site is directly off of Rt 237 it is anticipated that none of this traffic will impact the majority of the streets in Thornwilde, including Thornwilde Drive because it will be easier to enter and exit the site off the RT.237 entrance than to navigate an extra turn if using the Thornwilde entrance.
13. Comprehensive Plan and RPD zone: The Thornwilde Development in the past has submitted and has been granted concept plan zoning changes under RPD zone. Current zoning allows for flexible and creative zoning procedures that have allowed the Thornwilde Development to accommodate density, market and layout and product changes over time. As with past approved submissions, the submitted concept change falls in line with the

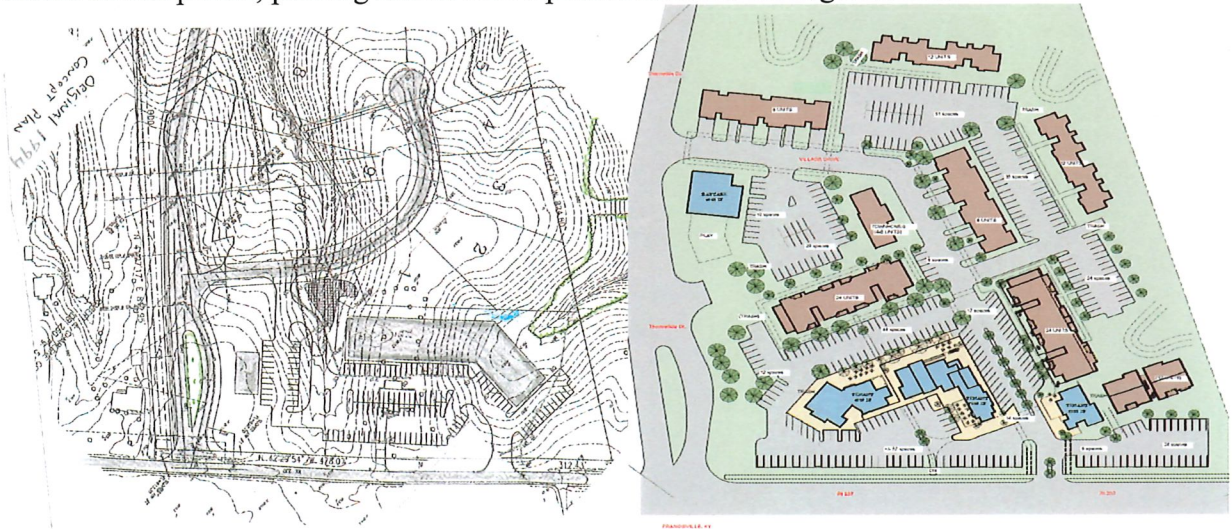
approved Boone County Comprehensive Plan and the overall original intent of the Thornwilde development. Boone County Comprehensive Plan shown below:

Boone County, KY



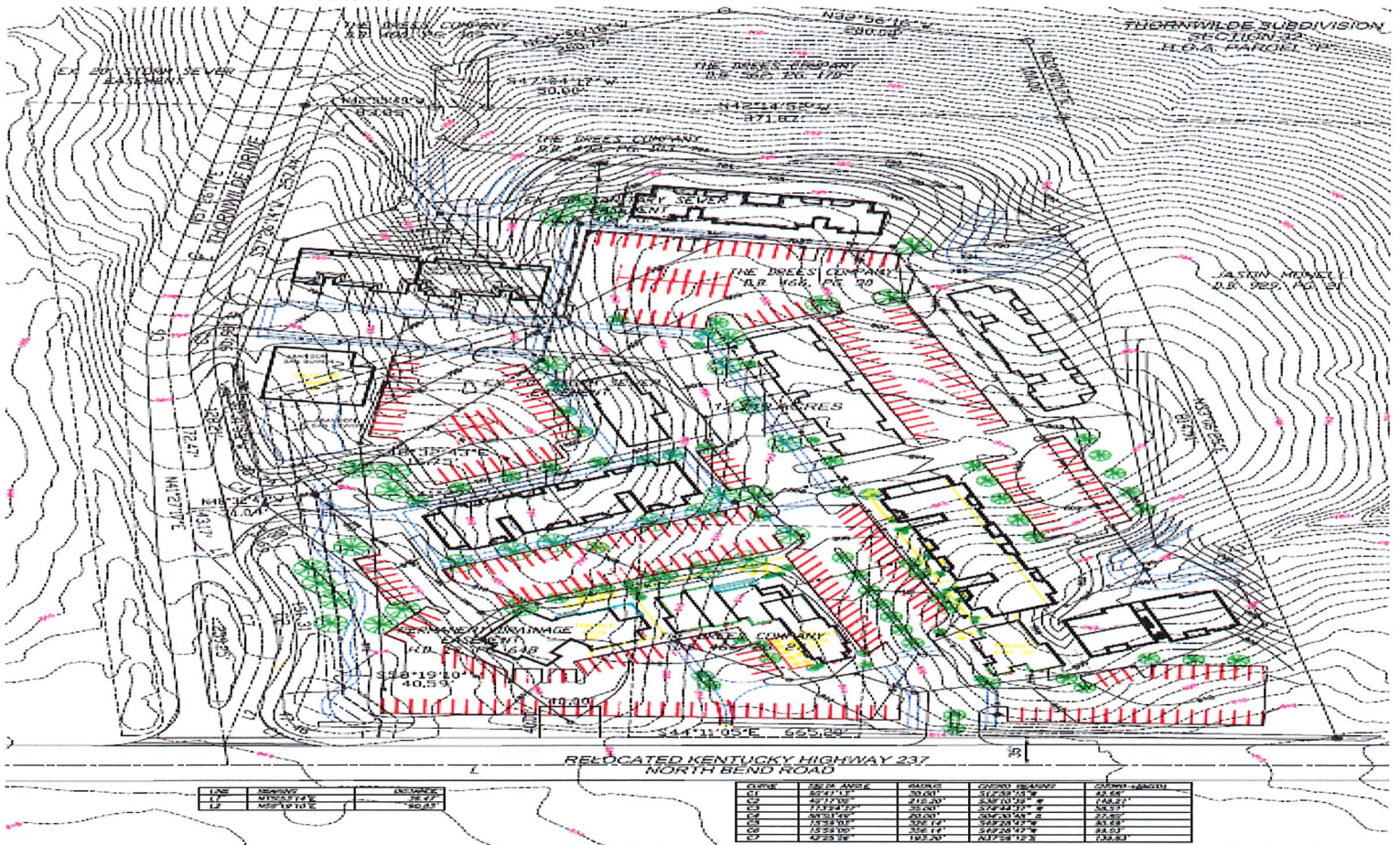
Below describes how the applicable requirements and standards in Article 16 have been satisfied for requesting the concept plan change.

1. The transition from the commercial to single family as the original concept plan shows doesn't allow for a creative way to mix the uses. The new concept plan below integrates the multi-family buildings next to the commercial development in a way to create a Village feel that shares access points, parking and invites a pedestrian flow through the site.

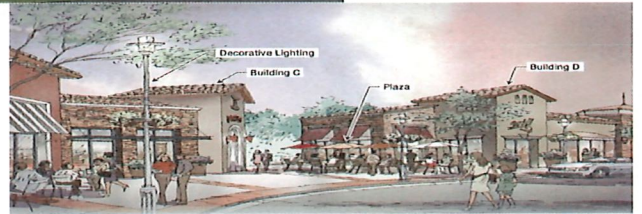
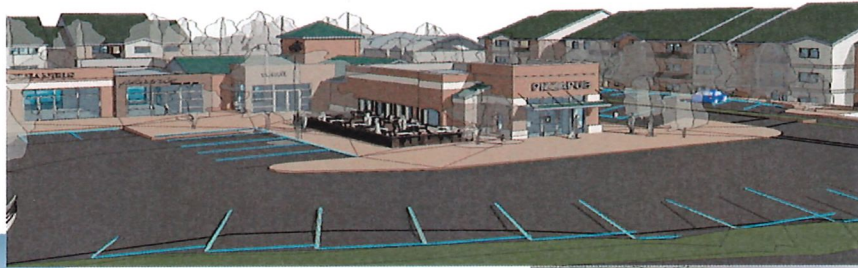


2. Changes to the surrounding area also warrant a change to the original concept plan. In 1994, Rt. 237 was just a two-lane country road. Now, Rt. 237 has three lanes, sidewalks and biking lanes and is considered a major thoroughfare. BCPC recent approval of Toebben Development (which is using the same elements of Residential, Attached/Multifamily and Commercial) plays into the Boone County comprehensive plan as shown above.

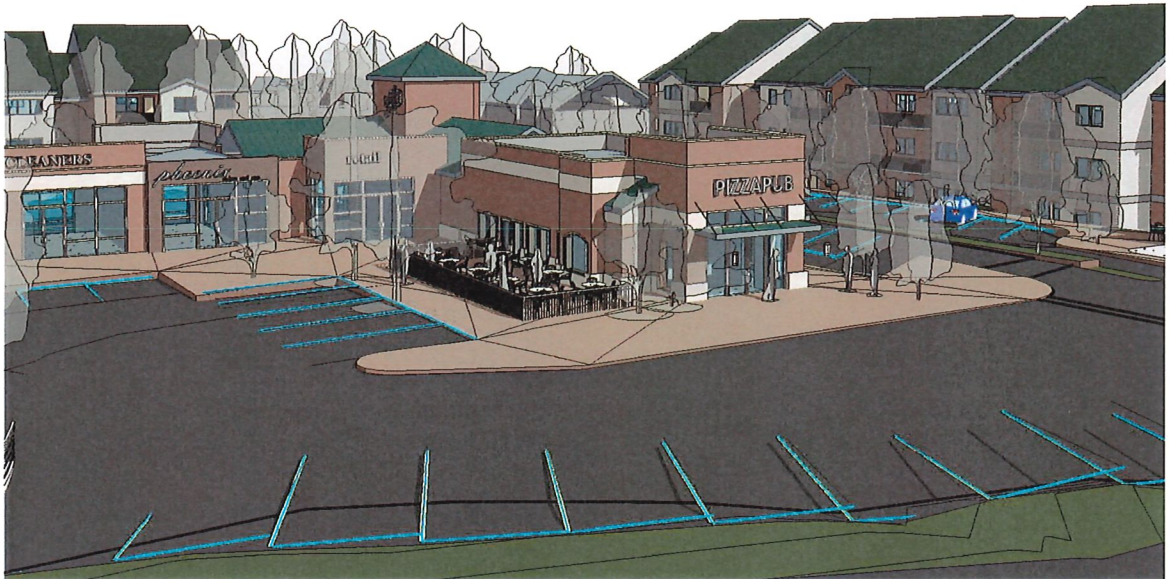
3. The overall density is not exceeding the original approved concept plan
4. The Attach/Multi-family components next to the commercial components allow a richer feel and are scaled for a better transition in uses. In short, the economics of the project can be spent on a strip center that looks good from two sides instead of just one, have outdoor pedestrian gathering areas and acts as an amenity for the development and for the county as a whole.
5. Article 16 of the Boone County Zoning Regulation encourages mix uses, walkability, reduced setbacks, pedestrian scale, outdoor pedestrian areas, parking areas to the rear and sides of buildings, decorative streetscapes etc. The submitted concept plan clearly accomplishes this.
6. Market conditions in the housing industry have changed dramatically over the recent recession. Current sale conditions show an upturn in attach/multi-family housing.
7. As a result of the unique characteristics of the current land topography, the change to an attached/multi-family product can better cope with the grades, topo and public improvements in better cost effective way than single family. See topo/utility plan below:



8. The approved 1994 concept plan already permits these combinations of mixed uses in the RPD zoning.
9. Design elements, color and materials that will be used in the commercial design will be used in the attached/multi-family design. The following collection of pictures shows materials, quality and scale for the type of product that will be used at Thornwilde Village.



10. Article 16 of the Boone County Zoning Regulations is established to allow an applicant the opportunity to design and construct a planned development. The concept plan revision ties more closely with the elements in the Boone County Comprehensive Plan providing a variety of housing types and transition to what is indicated on the Comprehensive plan and future surrounding developments.
11. As the result of different uses locating next to each other, the attached/multi-family concept allows more people to be closer to the convenience of what the commercial area offers; therefore reducing traffic, number of trips and less dependency of vehicular traffic.
12. Residential Planned Development (RPD) is a zoning district which comprises a significant residential population and may consist of a variety of housing types such as but not limited to single-family detached lots, single-family attached, apartments, townhouses, landominiums, condominiums and duplexes in a planned development setting.
13. RPD districts allow flexibility in comparison to the remainder of the Boone County Zoning Regulations by offering innovative approaches and design. The following below site plan and renderings meet those goals and objectives and therefore consistent to the Boone County Comprehensive Plan



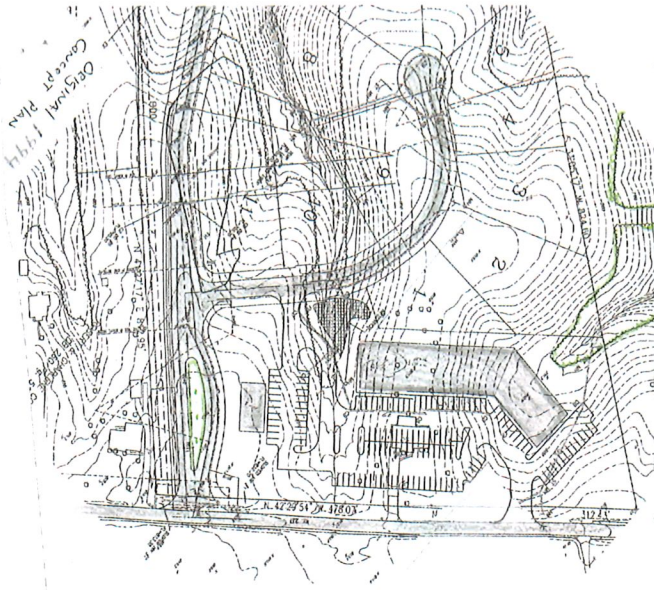


THORNWILDE HOA OPEN SPACE GREEN BELT



FRANCISVILLE, KY

14. With the approval of the new concept plan, the original Thornwilde concept plan stays true to providing a variety of housing types, employment opportunities and



22. The Attached/Multi-Family user is more likely to use and access the conveniences of what the commercial area has to offer and therefore is a better plan than the originally concept.

ORDINANCE 14-16

AN ORDINANCE RELATING TO THE BOONE COUNTY FISCAL COURT APPROVING, WITH CONDITIONS, A REQUEST OF THORNWILDE INC. (APPLICANT) FOR THE DREES COMPANY (OWNER) FOR A CHANGE IN AN APPROVED CONCEPT DEVELOPMENT PLAN IN A RESIDENTIAL PLANNED DEVELOPMENT (RPD) ZONE FOR A 8.648 ACRE TRACT GENERALLY LOCATED ON THE NORTHWEST CORNER OF THE NORTH BEND ROAD/THORNWILDE DRIVE INTERSECTION, INCLUDING THE TRACT AT 1207 NORTH BEND ROAD, BOONE COUNTY, BURLINGTON, KENTUCKY.

WHEREAS, the Boone County Planning Commission received a request for a Change in an Approved Concept Development Plan in a Residential Planned Development (RPD) zone for a 8.648 acre tract generally located on the northwest corner of the North Bend Road/Thornwilde Drive intersection, including the tract at 1207 North Bend Road, Boone County, Kentucky, which is more particularly described below; and,

WHEREAS, the Boone County Planning Commission as the planning unit for the unincorporated areas of Boone County, Kentucky, was requested to and has conducted a Public Hearing serving as a due process trial-type hearing and made findings recommending approval, with conditions, for the Change in an Approved Concept Development Plan.

NOW, THEREFORE, BE IT ORDAINED BY THE FISCAL COURT OF COUNTY OF BOONE, COMMONWEALTH OF KENTUCKY:

SECTION I

That the request for a Change in an Approved Concept Development Plan for the real estate which is more particularly described below shall be and is hereby approved, with conditions, in a Residential Planned Development (RPD) zone for a

8.648 acre tract generally located on the northwest corner of the North Bend Road/Thornwilde Drive intersection, including the tract at 1207 North Bend Road, Boone County, Kentucky. The real estate which is the subject of this request for a Change in an Approved Concept Development Plan in a Residential Planned Development (RPD) zone is more particularly described in DEED BOOK 466, PAGE NOS. 27 & 30, (as supplied by the applicant) as recorded in the Boone County Clerk's office.

SECTION II

That as a basis for the approval for a Change in an Approved Concept Development Plan in a Residential Planned Development (RPD) zone for a 8.648 acre tract generally located on the northwest corner of the North Bend Road/Thornwilde Drive intersection, including the tract at 1207 North Bend Road, Boone County, Kentucky, are the findings of fact of the Boone County Planning Commission as set forth in its minutes and official records for this request shall be and are hereby incorporated by reference as if fully set out in this Ordinance and marked as "Exhibit A."

The Committee recommended approval for this request based on the findings of fact and conditions as set forth in the Committee Report and marked as "Exhibit B."

SECTION III

That this Ordinance shall take effect and be in full force when passed, published and recorded according to law.

Introduced and given First Reading on the 16th day of September, 2014.

Adopted by the Fiscal Court of Boone County after Second Reading at a regular meeting on the 21st day of October, 2014 and signed by the County Judge/Executive as evidence of his approval, attested under seal by the County Fiscal Court Clerk and declared to be in full force and effect.

Gary W. Moore
GARY W. MOORE
BOONE COUNTY JUDGE EXECUTIVE

ATTEST:

Daphne Kornblum
DAPHNE KORNBLUM
FISCAL COURT CLERK

ANNEX D
Connection to Future Land



ARTS PLAN

APPROVED
 Staff _____
 Date 8/6/14
 Boone County
 Planning Commission
 *with additions

 <p>Paul + Weber Architects, PLLC 1000 Peachtree Street, NE Atlanta, GA 30309 404.525.8800 paulandweber.com</p>	<p>ARCHITECTURAL SITE PLAN</p> <p>C101</p>
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Scale 1" = 100'
 Thornwilde Village
 Concept Site Plan

