

APPLICATION FORM

ZONING MAP AMENDMENT
BOONE COUNTY PLANNING COMMISSION

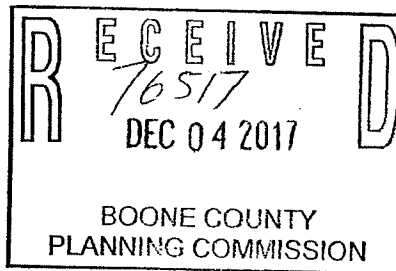
FIVE (5) COPIES OF SUBMITTED DRAWINGS ARE REQUIRED

(See Boone County Zoning Regulations)

SECTION A (To be completed by applicant)

- 1. Name of Project Union, Kentucky, Multi-Tenant Building
2. Location of Project East side of US 42, south of Fowlers Lane, Union, KY 41091
3. Total Acreage of Site 1.71 +/- acres
4. Current Zoning of Site RSE/UNO
5. Proposed Zoning (Classification being requested) C-2/PD/CD
6. Proposed Uses (please specify each use) Restaurant and Retail/Service
7. Names of Applicant(s) Anchor Retail Solutions LLC
8. Address of Applicant(s) 3825 Edwards Road, Suite 630
9. Name of Property Owner(s) Arlinghaus I LLC
10. Address of Property Owner(s) 142 Barnwood Drive
11. Proposed Building Intensities (please specify)
12. Are there any existing buildings on the site? No
13. Deed Book 1099 Page No. 614 Group No. 2047
14. Are you also applying for:
15. Have you submitted a Concept Development Plan? Yes
16. Have you had a pre-application meeting with BCPC Staff? Yes 9/26/17
17. Please check the following organizations/agencies which you have discussed the proposed development within the last several months:

- Boone County Water District
Florence Public Services Dept.
Duke Energy
Sanitation District #1
Cincinnati Bell
Owen Electric Cooperative, Inc.
Boone County Public Works Department
Kentucky Transportation Cabinet
Cincinnati/N. KY International Airport (KY Airport Zoning Commission for height restrictions near the airport)



EXHIBIT

“A”

STAFF REPORT

Request of **Anchor Retail Solutions LLC (applicant)** for **Arlinghaus I LLC (owner)** for a Zoning Map Amendment from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Commercial Two/Planned Development (C-2/PD) for an approximate 1.71 acre site located on the east side of U.S. 42, on the southeast corner of the intersection with Fowlers Lane and to the west of the property at 9075 U.S. 42 (Kroger fuel center), Union, Kentucky. The request is for a zone change to allow a multi-tenant commercial building.

January 3, 2018

REQUEST

The applicant has submitted an application to change the zoning of a 1.7139 acre tract from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Commercial Two/Planned Development (C-2/PD). The subject property is located on the east side of U.S. 42 and to the west of the Kroger fuel center located at 9075 U.S. 42, Union, Kentucky.

The submitted Concept Development Plans show a 7,522 square foot multi-tenant commercial building, with a Panera drive through restaurant (5,019 SF with 120 total seats inside and outside) and a retail tenant space (2,503 SF). Access to the building is proposed from two curb cuts that connect to the Kroger/StoryPoint private driveway network. The parking lot is shown with 83 parking stalls and 28' tall light poles (25' poles with 3' base). A small patio is shown in front of the building. White 3 board fencing is shown along the U.S. 42 frontage.

The submitted building elevation drawings show a one-story building (24' tall to top of highest parapet) that will be constructed with brick veneer, stone veneer, ceramic tile, EIFS, and canvas awnings. Masonry and EIFS percentages have been provided on two of the building elevations. The front patio is shown to be enclosed with vinyl or metal fencing.

PERTINENT HISTORY

1. The subject property was rezoned from Rural Suburban Estates (RSE) to Rural Suburban Estates/Union Town Overlay (RSE/UTO) with the adoption of the 2000 Union Town Plan.
2. In 2014, the City of Union approved a Zoning Map Amendment from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Commercial Two/Planned Development (C-2/PD) for the adjoining 35.25 acre area to the north and east. The approved Concept Development Plan shows the following improvements:
 - A 22.53 acre site with a 136,000 square foot Kroger Marketplace, 8,000 square foot liquor store, fuel center, and 730 parking stalls. Access to site is shown from a private driveway network that connects to U.S. 42 in two locations. The first location is directly opposite Old Union Road and the second is directly opposite Fowlers Lane.
 - A 7.99 acre outlot in the northeastern portion of the site.
 - A 4.73 acre outlot area along U.S. 42 between the proposed access points.

3. On May 19, 2016, Boone County Planning Commission approved a Site Plan application for Lot 1 of Union Marketplace Subdivision. The plan shows a 135,976 square foot Kroger Marketplace, 8,000 square foot liquor store, fuel center, and 730 parking stalls.
4. On September 28, 2016, Boone County Planning Commission received a copy of the approved U.S. 42 road improvements plans for the Kroger Marketplace development. The plans show the following improvements at Kroger's southern access point (opposite Fowlers Lane). A new traffic signal, a 465 right hand deceleration lane with a 100' taper lane, a 300' left hand turn lane, and sidewalk/crosswalk improvements.
5. In 2017, the City of Union approved a Zoning Map Amendment from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Public Facilities/Planned Development (PF/PD) for the adjoining 14.104 acre area to the south. The approved Concept Development Plan shows a 187,017 square foot elderly housing facility with 175 parking spaces. The facility was shown with 120 independent living apartments and 42 enhanced living apartments.
6. In November 2017, a Conveyance Plat was approved by the Planning Commission for the subject 1.7139 acre tract.

SITE CHARACTERISTICS

The 1.7139 acre tract is currently undeveloped and has 265.35' of street frontage on U.S. 42. The site was graded as part of the Kroger Marketplace/Kroger Fuel Center Project in 2016. The approved grading plan shows that topography of the site falls from 864' above sea level at the center of the site to 852' above sea level at the U.S. 42/Kroger driveway intersection. Access to the site is provided from the Kroger Marketplace/Fuel Center private driveway system. A water main, overhead utilities, and public sidewalks exist in the U.S. 42 right-of-way. A sanitary sewer main runs through the southwest portion of the site.

ADJACENT LAND USES AND ZONING

- Northeast: Kroger Marketplace and Fuel Center Lot (C-2/PD/CD)
- Northwest: Kroger Marketplace and Fuel Center Lot (C-2/PD/CD)
- Southeast: Undeveloped Property (Note - StoryPoint Site Plan Has Been Approved) (PF/PD/CD)
- Southwest: U.S. 42, Fowlers Lane, Villas at Fowler Creek (SR-2/UTO), and a Single-Family Residential Dwelling on a 12.93 Acre Tract (UNO & RSE/UTO)

RELATIONSHIP TO COMPREHENSIVE PLAN

The 2010 Boone County Comprehensive Plan's 2035 Land Use Plan designates the site as Rural Lands and High Suburban Density Residential uses. These classifications are defined as follows:

Rural Lands (RL) - "Wooded, agricultural, recreational, or low density residential uses of up to one dwelling unit per two acres for the 25 year land use plan. Residential construction in Rural Lands does not occur in a formal subdivision."

High Suburban Density Residential (HSD) - "Single-family and/or attached housing of up to 8 dwelling units per acre. This classification is typified by townhouse, condominium, and zero-lot line development, and also pertains to existing mobile home parks".

The Land Use Element (Union Area, pp. 168-169) contains the following passages regarding the general area:

- A. This section of Boone County contains the City of Union and a substantial portion of Hathaway Road and U.S. 42. In general, this section should experience considerable residential growth due to Union's current residential nature, growth associated with the City of Florence, and improvements to U.S. 42. For these reasons, the 2000 Union Town Plan was prepared by the Boone County Planning Commission, City of Union, and Boone County Fiscal Court. The plan's recommendations are shown on the 2035 Future Land Use Map and also affect the Boone County Zoning Regulations.
- B. New subdivisions should be developed with connecting roads providing alternative routes for residential traffic and decreasing the impact on major roadways. Road connections should include multi-modal elements such as pedestrian paths and bike lanes, and bus stops; Wetherington Boulevard is an example. Further road connections that prohibit driveway access and street parking should occur between U.S. 42, Camp Ernst Road, Longbranch Road, and Hathaway Road. Hathaway Road needs special attention to protect this important east-west route. Water and sanitary sewer infrastructure should accompany such road connections and extensions as to prevent premature, estate-style residential development, and to prevent frontage subdivisions that result in poor access management. Development proposals shall be carefully evaluated relative to the area-wide road network and any necessary contributions to the network.

The Land Use Element provides the following Future Land Use Development Guidelines that relate to the proposal.

- A. As described in the Goals and Objectives, different development densities can occur in Boone County as long as the development is designed in a proper manner and the infrastructure exists or is planned to support the development. The Goals and Objectives also acknowledge that fluctuations in the economy and housing market should be considered when evaluating the uses and design of developments.

Business, commercial, or industrial parks should be designed and developed to allow future residential or other uses to access through them where the corridor may extend to large developable tracts or may provide a future road connection. A roadway that is suitable for mixed use development should serve side streets for businesses rather than serving individual businesses directly, include an urban curb and gutter design, and provide pedestrian amenities and green space along the right-of-way. In this way, future residential development could be developed beyond the initial development, but be readily marketed.

Sanitary sewer issues have larger impact on where development occurs than do the other types of infrastructure in Boone County. The presence of sanitary sewer has a direct impact on the lot sizes of residential development in particular. Most residential development in outlying areas without access to sewer service typically develops on lots that are larger than an acre. In fact, septic system permits almost always require residential lots of greater than one acre to accommodate leach areas. On the other hand, the presence of public sanitary sewer facilities

have normally resulted in an increased demand for more dense single-family residential development. This development often occurs roughly in the range of three dwelling units per acre - which typifies single family projects in Boone County at this time. There is little development occurring in Boone County between these two general density levels. As pointed out in the Housing Element, this phenomenon often results in different housing and lot characteristics adjacent to each other. In these conditions, proposed residential developments that have smaller lot sizes than surrounding land uses are recommended to utilize the following design mechanisms to address the impacts on existing development.

The proposed development should consider in its design a band of larger lots and setbacks along the affected perimeter of the site adjacent to existing development to provide an adequate transition.

The proposed development should consider a band of development along the existing main roadway in which the houses face the main road to appear rural, and where the lots in this band are larger than those in the balance of the proposed development. Building setbacks and lot frontages should imitate existing setbacks along the road.

Highly visible portions of the proposed development site should consider green space. The style of green space, including tree types, fencing or other similar features should imitate the existing road corridor views. Grandiose subdivision entryways may not be appropriate depending on existing conditions in the corridor (Development Layout, Lot Sizes and Setbacks, pg. 162).

- B. Developments in Boone County must recognize the potential impacts upon adjoining land uses and incorporate a transition of land uses, building setbacks, and/or landscaping to minimize these impacts. Potential impacts include visual, noise or vibrations, odors, dust, smoke, and light. Buffering to mitigate these impacts should be an integral part of the design of proposed projects; existing site features should be used in meeting this guideline. Developments should provide buffering along public roadways, to soften the visual impact. Appropriate wooded areas and stream valleys should remain as open space within developments and between developments. Developments proposed adjacent to planned or established open spaces should provide pedestrian access where appropriate. Natural green space benefits the community as well as encourages developers to create innovative development designs through clustering of buildings and impermeable area. Typically, buffering is required and provided between unlike land uses; however, as the development of Boone County fills in previously rural areas, different lot sizes and designs of residential development sometimes impact each other. Where an appropriate gradation of lot size and setbacks cannot be designed into a proposed residential subdivision development of a significantly higher density than existing adjacent residential uses, deliberate vegetation buffering may need to be incorporated into the design to help protect low density residential areas. Buffer areas should use and supplement existing site features where possible (Buffering, pp. 162-163).
- C. Developments in Boone County must include landscaping to accompany the proposed project. Retention of existing healthy vegetation is considered a component of landscaping and is encouraged. This landscaping should be designed to improve the public view of a development, and should be incorporated into parking lots and other vehicle circulation areas, as well as within open spaces and around structures. Landscaping is intended to minimize the visual impacts of the development from adjoining properties and roadways. The amount of heat absorbed by impervious cover from sun radiation is decreased by landscaping, which reduces energy costs. Landscaping helps purify the air of harmful pollutants, thus reducing health impacts. It also helps reduce the quantity and improve the quality of storm water runoff, including temperature. The use of bioretention islands (water filtering basins) and grass swales should be used where possible in place of raised islands. Native species are often heartier than nursery stock and should be

used wherever possible to minimize pesticides and other high impact forms of maintenance. Developments along major roadways in Boone County must include landscaping between the development and the right-of-way in order to promote the aesthetic appearance from the roads and to facilitate the compatibility of differing land uses (Landscaping, pg. 163).

- D. Developments in Boone County must recognize the potential impacts of stormwater runoff. Developments must design and incorporate adequate provisions for the channelization and control of the rate of stormwater flow on and from the site. One goal of local storm water programs should be to minimize the amount of storm water generated by decreasing the amount of pavement and encouraging green rooftops. Creating less runoff by increasing on-site infiltration is a method of managing runoff.

Control and mitigation practices for erosion associated with developments must be provided. At a minimum, developments must seed and mulch all graded areas and provide siltation controls. Stormwater management and erosion control measures must be concurrent with site work in order to be effective. Stormwater management officials must also consider the cumulative effects of increased development runoff in watersheds (Stormwater Management and Erosion Control, pg. 163).

- E. Developments in Boone County must recognize the potential impacts of associated traffic on adjoining properties and transportation systems. The need exists to protect the capacity of the existing roadway network, and to plan improvements to accommodate new development and travel patterns. Access management provisions include the coordination of curb cuts, adequate corner clearance and site distance for access points, adequate space between access points, shared access points and parking facilities, provisions for access connections to adjoining properties, and dedication of public right-of-way. In subdividing property, arterials or collectors should not be used for direct access to lots, rather a system of local streets should feed into the collector and arterial system. Connections between and within commercial and industrial developments allow for more efficient provision of transit service (Access Management, pg. 163).
- F. Developments in Boone County must be designed, where appropriate, to improve the County's transportation network system of roadways, and functional classifications must be used in the planning for and designing of new developments. Collector roadways should be extended and developed to provide for the safe movement of traffic through and between subdivisions. Development along existing arterials and collector roadways should not have direct driveway access, but be served by a local street. Appropriate road connections should be constructed to provide alternate routes for traffic to and through specific projects and to enhance the capacity of existing streets. The traditional grid system can provide an alternative to typical suburban design. Parallel and frontage roads should be used to minimize impacts of individual sites on collector and arterial roadways. Appropriate pedestrian networks should be incorporated into the design of developments which will generate or experience significant pedestrian use. These networks should not only provide internal paths, but should provide connections to adjoining uses where appropriate. Public open space and recreation sites should be connected to each other by bicycle and pedestrian paths where appropriate. Transit Oriented Development (TOD) is discussed in detail in the Transportation Element, and should be examined as a future direction in Boone County development patterns (Transportation and Pedestrian Network, pp. 163-164).
- G. Developments in Boone County should give consideration to the overall design of the area. Site, landscape, and building design should be a primary concern at the early stages of the development, with an emphasis on the aesthetic impact of the proposed use. The minimal use of signs is encouraged; signage should be adequate to identify a specific development, but should not be used as a means to compete for motorist attention. The objective is to avoid the confusion and/or distraction of motorists, and to avoid the potential negative impacts of signs on

the visual appearance of a development or corridor. Overhead utility lines should be placed underground wherever possible, and junction boxes screened from public view. Neo-traditional residential and commercial development should be encouraged to provide quality development that blends with the natural or historic character of parts of Boone County. This Comprehensive Plan encourages the restoration, renovation and/or adaptive re-use of historically significant structures in Boone County (Design, Signs, and Historic Preservation, pg. 164).

The Population Element contains the following passages which relates to the request:

- A. The Population Element outlines population estimates based on OKI transportation analysis zone "regions." The site is located in the Union region. The population of the Union region is estimated to change to 33,568 in 2020, and to 41,886 in 2030 (OKI Traffic Zone Forecast, pp. 20-21).

The Business Activity Element makes the following statements regarding the general area:

- A. "Many problems with the location and arrangement of commercial uses have emerged, especially along the KY 18 and U.S. 42 corridors. Commercial developments along KY 18 and U.S. 42 west of Mall Road should not be regionally oriented nor should they attract additional traffic, but rather be served by the existing or future population of the area" (Recommended Areas of Commercial Activity, pg. 68).
- B. "The land surrounding the U.S. 42 area, southwest of Florence, will experience additional commercial growth towards the City of Union, but should be done on a smaller scale to mitigate any adverse impacts and to be compatible with surrounding land uses and densities. The U.S. 42 corridor toward Union will have a mixture of commercial, office, institutional, public facility and residential uses. As defined by the 2000 Union Town Plan, commercial development should occur in neighborhood business district form in specified locations instead of strip style commercial. The Union Town Center could be used as an example of planning efforts for other parts of the U.S. 42 corridor, as well as other corridors in Boone County. Ultimately, mixed use town centers should be developed at major crossroads throughout Boone County without commercial development along the roadways between the town centers. Street connectivity is critical in these types of corridors" (Recommended Areas of Commercial Activity, pg. 68).
- C. "Commercial developments that occur in the KY 18 and U.S. 42 corridors should be limited in scale (e.g. building, intensity, use, etc.) and clustered to serve growing residential neighborhoods. Planned Unit Developments should be encouraged so that commercial construction is incorporated within new residential areas and designed properly. These commercial clusters should not function as an extension of Florence and Mall Road, but as distinct units serving the Burlington and Union areas. This principle should apply to the Limaburg area and surrounding area as well. The commercial development in this area should coordinate closely with the projected high and medium density residential development to form a more coherent community and create neighborhood centers rather than just a strip extending out of Florence. An example of this type of design is the Oakbrook Marketplace, which ties into the subdivision while serving a larger trade area" (Recommended Areas of Commercial Activity, pg 68).

The Goals and Objectives include the following pertinent statements:

- A. Proper planning is achieved through accepted land use management tools and coordination with public infrastructure (Overall, Goal).

- B. Development issues shall be viewed in terms of promoting overall quality of life. Mixing of residential and other land uses shall be encouraged within appropriately planned and designed neighborhood developments (Overall, Objective 2).
- C. Future growth shall be accompanied by adequate infrastructure and services. Existing infrastructure and services shall be monitored, maintained, and improved as needed (Overall, Objective 4).
- D. The incremental effects of developments on the environment, including, but not limited to storm water, erosion, woodlands, streams, and views shall be evaluated and addressed with public comment (Environment, Objective 11).
- E. Boone County businesses are developed in appropriate locations and are compatible with their surrounding areas (Business Activity, Goal).
- F. Compact, efficient development patterns shall be encouraged for business districts (e.g. industrial, commercial and office) with appropriately sized and well maintained buffer spaces between the business use and other land usage (Business Activity, Objective).
- G. Commercial uses shall be designed and located to coordinate with the surrounding land uses and shall have safe and effective access and ample parking spaces (Business Activity, Commercial Objective).
- H. The large scale mixing of commercial and non-commercial uses shall occur in planned developments or in areas where consideration has been given to assure compatibility with surrounding land uses. The location of large scale commercial uses shall take into consideration existing or planned infrastructure conditions so as to protect the existing and future population and businesses. Small scale mixing of commercial uses within residential areas shall be carefully assessed and properly placed so as to enhance the neighborhood. Architectural Design Review and consistent sign standards shall be implemented on a corridor by corridor basis, when justified by legitimate land use planning principles (Business Activity, Commercial Objective).
- I. Safe, efficient and environmentally sound public services and facilities shall exist for all development (Public Services and Facilities, Goal).
- J. The local transportation system in Boone County shall be maintained and improved so that overall safety and level of service will be enhanced. Inefficiencies in the system will be identified and addressed. New land development shall be closely coordinated with the local transportation system to assure efficiency and continuity with the regional multi-modal transportation system (Transportation, 2nd Goal).
- K. Proper vehicular and pedestrian access to adjoining property shall be provided and/or retained when a property develops or redevelops (Transportation, 2nd Goal, Objective 3).
- L. Traffic impact analysis should be used as a tool to address impacts to the existing roadway system (Transportation, 3rd Goal, Objective 5).

RELATIONSHIP TO 2010 UNION TOWN PLAN

The 2000 Union Town Plan Land Use Plan Map designations mirror those shown on the 2010 Boone County Comprehensive Plan's Future Land Use Map. The Union Town Land Use Map shows the following land use designations on the subject site:

Town Homes - 6 dwelling units per acre; attached or detached housing; units can not be on top of each other.

Green Areas - non-buildable, uncleared land (except for connector road, pedestrian/bike paths, and recreational building). This area is 100' wide along the US 42 frontage.

FINDINGS NECESSARY FOR MAP AMENDMENT

Before any map amendment is granted, the Planning Commission or the legislative body use the following criteria in granting approval of a zone change or map amendment:

1. The map amendment is in agreement with the adopted comprehensive plan and any specific study designed to further detail the Boone County Comprehensive Plan for the location in question; or
2. The existing zoning classification is inappropriate and that the proposed zoning classification is appropriate; or
3. There have been major changes of an economic, physical, or social nature not anticipated in the adopted comprehensive plan that substantially alter the area's character.

For map amendments involving the Planned Development District (PD), Employment Planned Development (EPD), or Residential Planned Development (RPD), the applicable provisions of Article 15 or Article 16 must be met in addition to the criteria stated above.

PLANNED DEVELOPMENT STANDARDS

The application needs to be evaluated in terms of the Planned Development Standards that are found in Section 1514 of the Boone County Zoning Regulations. A proposed Concept Development Plan shall fulfill the criteria outlined in this section unless a portion of the criteria do not apply or relate, in whole or part, to a specific proposal (see attachments).

STAFF COMMENTS

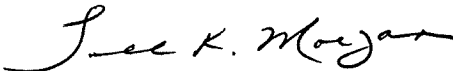
1. Staff would like to note that the Boone County Comprehensive Plan and 2000 Union Town Plan have not been updated since the Kroger Marketplace and StoryPoint projects were approved.
2. Staff would like the applicant to address the following:
 - A. An updated Traffic Impact Study was submitted with the application (see attachments). Staff would like the Study to address the recent road improvements that were made on U.S. 42 and if they are adequate to address the trips that will be generated by the overall development (Kroger Marketplace, Kroger Fuel, Kroger Outlots, StoryPoint, and proposed outlot).
 - B. Is the proposed development following the Union Kroger Marketplace outlot design requirements (see attachments)? If not, please outline the proposed changes.
 - C. Can the building elevations be updated and provide masonry, tile, and EIFS percentages on all four facades?

- D. Can building material samples be provided?
 - E. Will the parapet walls be tall enough to fully screen the roof mounted mechanical equipment?
 - F. The plan shows that drive through signage is being proposed. Does it comply with code?
 - G. The plans shows that shared buffer yards are being proposed with Kroger? Will a shared buffer yard agreement be recorded.
 - H. The plans show the storm water from the site will go to one of Kroger's detention basins. Was the basin sized to accommodate the storm water from the subject site?
4. Staff sent out an agency memo and received comments back from Union Fire Department and Sanitation District No. 1. These comments are attached to the Staff Report.

CONCLUSION

This request needs to be evaluated by the Boone County Planning Commission and City of Union in terms of Articles 3 and 15 of the Boone County Zoning Regulations, the appropriate planning documents noted herein, and the potential impacts on existing and planned uses in the area. The Future Land Use Map will need to be amended if the request is approved.

Respectfully submitted,



Todd K. Morgan, AICP
Senior Planner

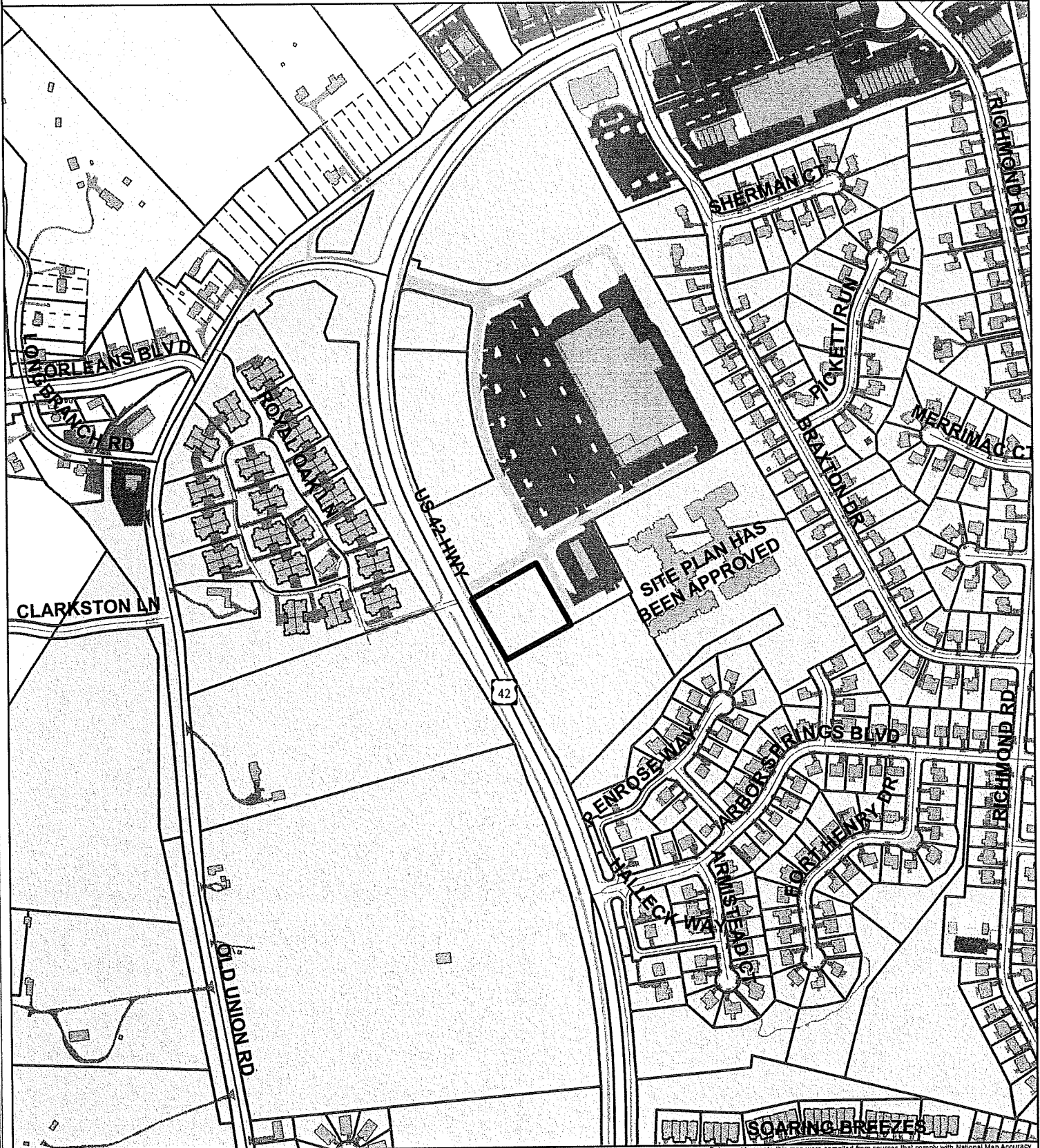
TKM/ss

Attachments:

- *Site Vicinity Map
- *Concept Development Plans
- *Traffic Impact Study Update
- *2016 Aerial Map With Building and Pavement Layers
- *Zoning Map
- *2035 Future Land Use Map
- *2000 Union Town Land Use Plan
- *2014 Approved Concept Plan for Kroger Marketplace and Outlots
- *2014 Kroger Marketplace Outlot Conditions
- *2017 Approved Concept Plan (Landscaping Plan) for StoryPoint
- *Planned Development Criteria
- *Agency Email
- *12/18/17 Email From Chief Michael Morgan
- *12/19/17 Email from Greg Haggard
- *Application

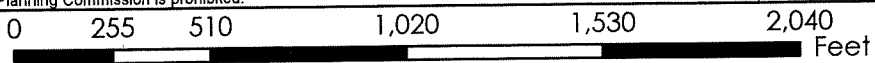
SITE VICINITY MAP

www.boonecountygis.com



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1 inch = 500 feet



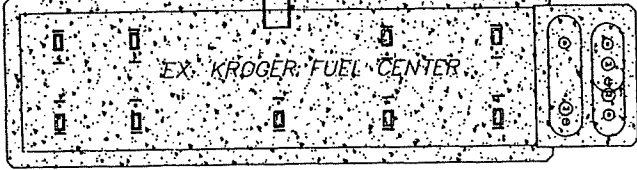
Boone County GIS - Putting Northern Kentucky on the Map



Map Created: 07/01/2013

Map Created: 07/01/2013
ArcMap Document: BooneMap (f)es.mxd

ZONED C-2\PD\CD



EX LOT #1
UNION MARKETPLACE SUBDIVISION
P.C. 6, PG. 104
AREA = 22.5977 ACRES

KROGER LIMITED PARTNERSHIP I
D.B. 106B, PG. 549

259.50' W
S 61'46"48" W

20' PUBLIC WATER MAIN EASEMENT
P.C. 6, PG. 104

EX 5' CONCRETE WALK

ZONED C-2\PD\CD

NEW CURB & PAVEMENT BY OTHERS

ZONED PF/PD/CD

NEW WALK BY OTHERS

ENCLOSED DUMPSTER

ARLINGHAUS I LLC
D.B. 776, PG. 001

RESTAURANT
5,019-SF

RETAIL
2,503 SF

F.F. = 864.0
83 PARKING SPACES
WITH 4 BIKE RACKS

PATIO w/
HANDRAIL

PROPOSED LOT
AREA = 1.7139 ACRES

20' PUBLIC SANITARY SEWER EASEMENT
P.C. 6, PG. 104

PUBLIC WATER MAIN EASEMENT
P.C. 6, PG. 104

NEW SIGN

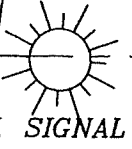
NEW WHITE 3 BOARD HORSE-STYLE FENCE

N 26'51"41" W 265.35'

EX 5' CONCRETE WALK

EX TRAFFIC BOX

EX 6' CONCRETE WALK



EX SIGNAL

U.S. 42

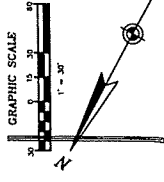


THOMAS GRAHAM ASSOCIATES, INC.
 • Engineers
 • Surveyors
 603 Campbell Road
 Cincinnati, Ohio 45231
 Telephone # 513-241-1100
 Fax # 513-241-1319

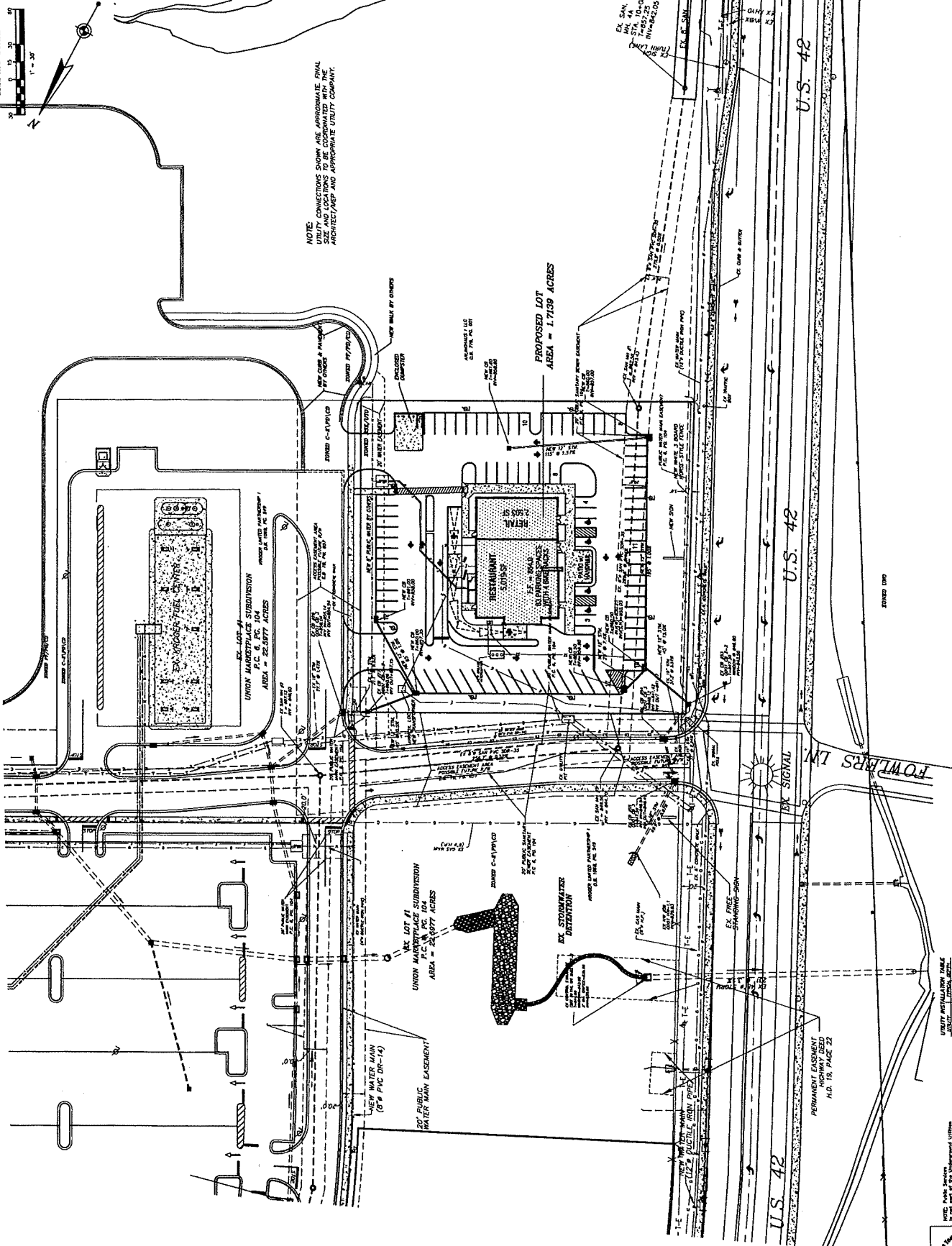
DATE: NOV. 29, 2017
 SCALE: 1" = 30'
 JOB NO.: 7237
 DRAWN BY: J. CASPERSEN
 CHECKED BY: J. CASPERSEN

SITE UTILITY PLAN
 PROPOSED RESTAURANT/RETAIL DEVELOPMENT
 UNION ROAD (U.S. 42)
 UNION KENTUCKY
 BOONE COUNTY

Drawn By: J. CASPERSEN
 Sheet: **C 3.0**
 Job No.: 6075-17



NOTE:
 UTILITY CONNECTIONS SHOWN ARE APPROXIMATE RIVAL
 LOCATIONS AND SHOULD BE VERIFIED BY THE
 APPLICANT AND APPROPRIATE UTILITY COMPANY.

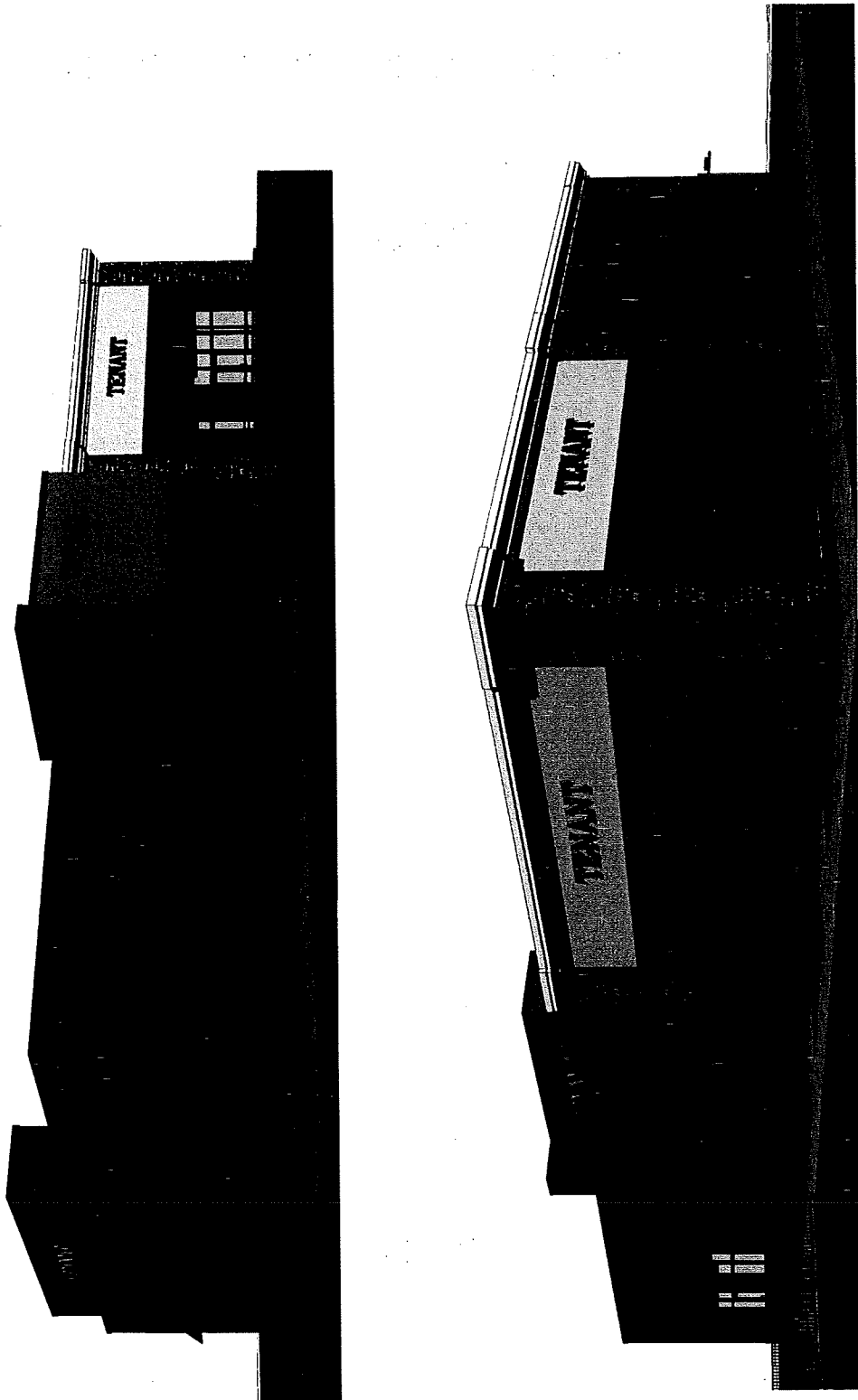


UTILITY INSTALLATION TABLE

UTILITY	DEPTH	INSTALLATION
WATER	36"	AS SHOWN
SEWER	36"	AS SHOWN
GAS	36"	AS SHOWN
ELECTRIC	36"	AS SHOWN
TELEPHONE	36"	AS SHOWN
CABLE TV	36"	AS SHOWN

A CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF ALL EXISTING UTILITIES AND THE CONTRACTOR IS TO ADJUST THE UTILITIES LISTED ABOVE AS NECESSARY.





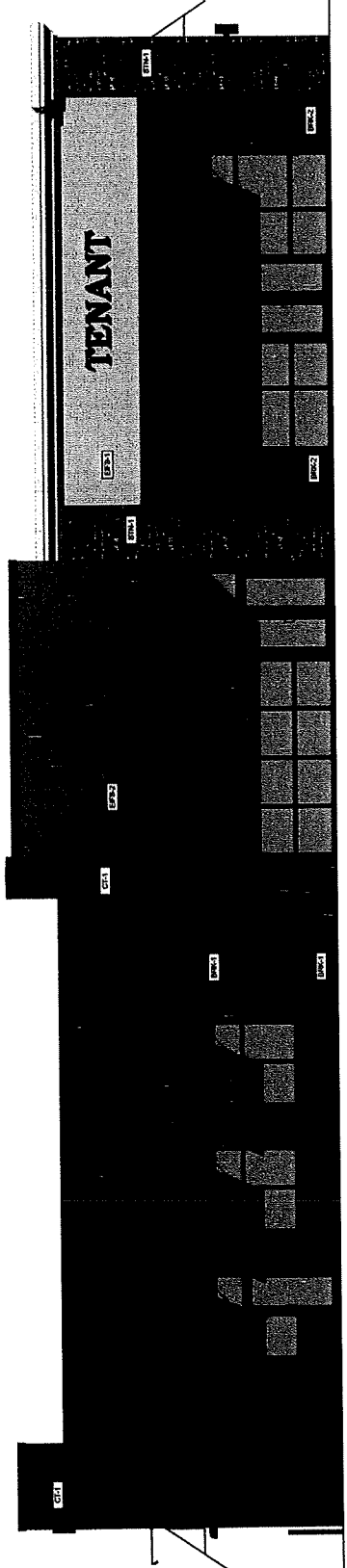
PANERA AND RETAIL SHELL
UNION, KY
12.04.17



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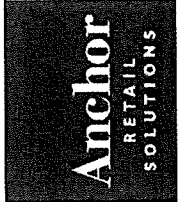
73% MASONRY
27% EIFS

POURING WALL



① COLOR FRONT ELEVATION
12.04.17

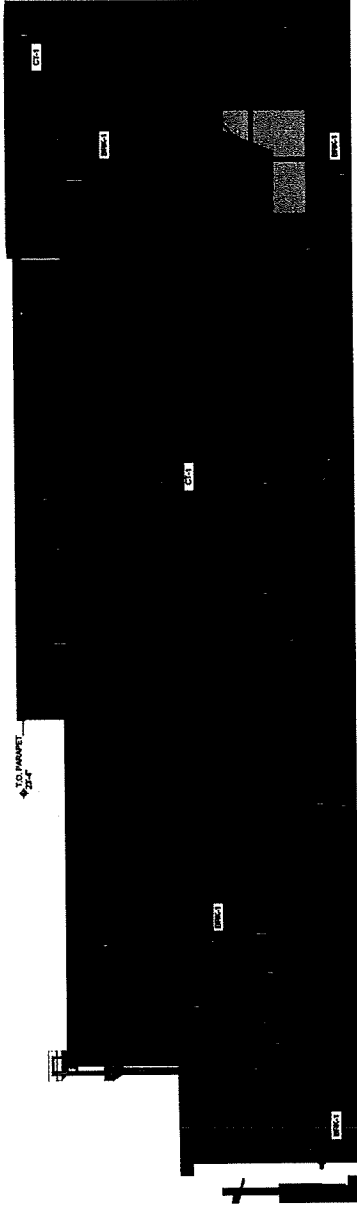
EXTERIOR FINISH SCHEDULE		
MARK	MATERIAL	COLOR
MARK-1	DAVITA JEWELRY	REARVIEW MIRROR
MARK-2	MARSHALLS TILE	BLACK
MARK-3	BRICK	BLACK
MARK-4	BRICK	BLACK
MARK-5	BRICK	BLACK
MARK-6	BRICK	BLACK
MARK-7	BRICK	BLACK
MARK-8	BRICK	BLACK
MARK-9	BRICK	BLACK
MARK-10	BRICK	BLACK
MARK-11	BRICK	BLACK
MARK-12	BRICK	BLACK
MARK-13	BRICK	BLACK
MARK-14	BRICK	BLACK
MARK-15	BRICK	BLACK
MARK-16	BRICK	BLACK
MARK-17	BRICK	BLACK
MARK-18	BRICK	BLACK
MARK-19	BRICK	BLACK
MARK-20	BRICK	BLACK
MARK-21	BRICK	BLACK
MARK-22	BRICK	BLACK
MARK-23	BRICK	BLACK
MARK-24	BRICK	BLACK
MARK-25	BRICK	BLACK
MARK-26	BRICK	BLACK
MARK-27	BRICK	BLACK
MARK-28	BRICK	BLACK
MARK-29	BRICK	BLACK
MARK-30	BRICK	BLACK
MARK-31	BRICK	BLACK
MARK-32	BRICK	BLACK
MARK-33	BRICK	BLACK
MARK-34	BRICK	BLACK
MARK-35	BRICK	BLACK
MARK-36	BRICK	BLACK
MARK-37	BRICK	BLACK
MARK-38	BRICK	BLACK
MARK-39	BRICK	BLACK
MARK-40	BRICK	BLACK
MARK-41	BRICK	BLACK
MARK-42	BRICK	BLACK
MARK-43	BRICK	BLACK
MARK-44	BRICK	BLACK
MARK-45	BRICK	BLACK
MARK-46	BRICK	BLACK
MARK-47	BRICK	BLACK
MARK-48	BRICK	BLACK
MARK-49	BRICK	BLACK
MARK-50	BRICK	BLACK
MARK-51	BRICK	BLACK
MARK-52	BRICK	BLACK
MARK-53	BRICK	BLACK
MARK-54	BRICK	BLACK
MARK-55	BRICK	BLACK
MARK-56	BRICK	BLACK
MARK-57	BRICK	BLACK
MARK-58	BRICK	BLACK
MARK-59	BRICK	BLACK
MARK-60	BRICK	BLACK
MARK-61	BRICK	BLACK
MARK-62	BRICK	BLACK
MARK-63	BRICK	BLACK
MARK-64	BRICK	BLACK
MARK-65	BRICK	BLACK
MARK-66	BRICK	BLACK
MARK-67	BRICK	BLACK
MARK-68	BRICK	BLACK
MARK-69	BRICK	BLACK
MARK-70	BRICK	BLACK
MARK-71	BRICK	BLACK
MARK-72	BRICK	BLACK
MARK-73	BRICK	BLACK
MARK-74	BRICK	BLACK
MARK-75	BRICK	BLACK
MARK-76	BRICK	BLACK
MARK-77	BRICK	BLACK
MARK-78	BRICK	BLACK
MARK-79	BRICK	BLACK
MARK-80	BRICK	BLACK
MARK-81	BRICK	BLACK
MARK-82	BRICK	BLACK
MARK-83	BRICK	BLACK
MARK-84	BRICK	BLACK
MARK-85	BRICK	BLACK
MARK-86	BRICK	BLACK
MARK-87	BRICK	BLACK
MARK-88	BRICK	BLACK
MARK-89	BRICK	BLACK
MARK-90	BRICK	BLACK
MARK-91	BRICK	BLACK
MARK-92	BRICK	BLACK
MARK-93	BRICK	BLACK
MARK-94	BRICK	BLACK
MARK-95	BRICK	BLACK
MARK-96	BRICK	BLACK
MARK-97	BRICK	BLACK
MARK-98	BRICK	BLACK
MARK-99	BRICK	BLACK
MARK-100	BRICK	BLACK



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UNION, KY
12.04.17



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1. TO FINISH
ELEVATION B

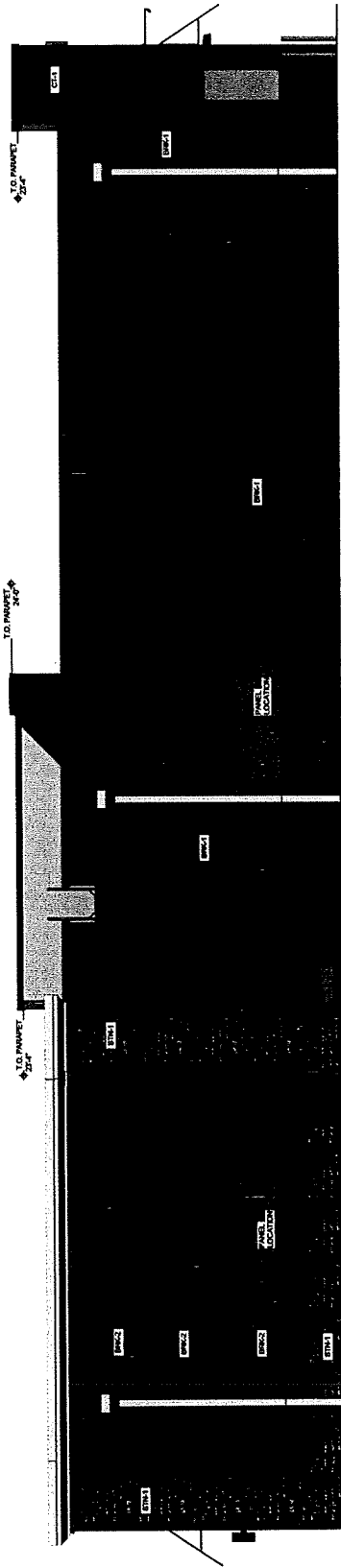
MARK	MATERIAL	MANUFACTURER	EXTERIOR FINISH SCHEDULE	COLOR	REMARKS
FR-01	CONCRETE	CONCRETE	CONCRETE	CONCRETE	CONCRETE
FR-02	BRICK	BRICK	BRICK	BRICK	BRICK
FR-03	WOOD	WOOD	WOOD	WOOD	WOOD
FR-04	GLASS	GLASS	GLASS	GLASS	GLASS
FR-05	PAINT	PAINT	PAINT	PAINT	PAINT
FR-06	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-07	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-08	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-09	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING
FR-10	INSULATION	INSULATION	INSULATION	INSULATION	INSULATION
FR-11	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION
FR-12	CLADDING	CLADDING	CLADDING	CLADDING	CLADDING
FR-13	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-14	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-15	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-16	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING
FR-17	INSULATION	INSULATION	INSULATION	INSULATION	INSULATION
FR-18	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION
FR-19	CLADDING	CLADDING	CLADDING	CLADDING	CLADDING
FR-20	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-21	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-22	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-23	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING
FR-24	INSULATION	INSULATION	INSULATION	INSULATION	INSULATION
FR-25	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION
FR-26	CLADDING	CLADDING	CLADDING	CLADDING	CLADDING
FR-27	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-28	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-29	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-30	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING
FR-31	INSULATION	INSULATION	INSULATION	INSULATION	INSULATION
FR-32	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION
FR-33	CLADDING	CLADDING	CLADDING	CLADDING	CLADDING
FR-34	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-35	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-36	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-37	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING
FR-38	INSULATION	INSULATION	INSULATION	INSULATION	INSULATION
FR-39	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION
FR-40	CLADDING	CLADDING	CLADDING	CLADDING	CLADDING
FR-41	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-42	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-43	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-44	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING
FR-45	INSULATION	INSULATION	INSULATION	INSULATION	INSULATION
FR-46	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION
FR-47	CLADDING	CLADDING	CLADDING	CLADDING	CLADDING
FR-48	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-49	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-50	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-51	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING
FR-52	INSULATION	INSULATION	INSULATION	INSULATION	INSULATION
FR-53	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION
FR-54	CLADDING	CLADDING	CLADDING	CLADDING	CLADDING
FR-55	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-56	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-57	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-58	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING
FR-59	INSULATION	INSULATION	INSULATION	INSULATION	INSULATION
FR-60	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION
FR-61	CLADDING	CLADDING	CLADDING	CLADDING	CLADDING
FR-62	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-63	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-64	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-65	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING
FR-66	INSULATION	INSULATION	INSULATION	INSULATION	INSULATION
FR-67	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION
FR-68	CLADDING	CLADDING	CLADDING	CLADDING	CLADDING
FR-69	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-70	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-71	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-72	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING
FR-73	INSULATION	INSULATION	INSULATION	INSULATION	INSULATION
FR-74	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION
FR-75	CLADDING	CLADDING	CLADDING	CLADDING	CLADDING
FR-76	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-77	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-78	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-79	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING
FR-80	INSULATION	INSULATION	INSULATION	INSULATION	INSULATION
FR-81	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION
FR-82	CLADDING	CLADDING	CLADDING	CLADDING	CLADDING
FR-83	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-84	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-85	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-86	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING
FR-87	INSULATION	INSULATION	INSULATION	INSULATION	INSULATION
FR-88	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION
FR-89	CLADDING	CLADDING	CLADDING	CLADDING	CLADDING
FR-90	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-91	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-92	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-93	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING
FR-94	INSULATION	INSULATION	INSULATION	INSULATION	INSULATION
FR-95	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION	FOUNDATION
FR-96	CLADDING	CLADDING	CLADDING	CLADDING	CLADDING
FR-97	ROOFING	ROOFING	ROOFING	ROOFING	ROOFING
FR-98	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL	MECHANICAL
FR-99	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL	ELECTRICAL
FR-100	PLUMBING	PLUMBING	PLUMBING	PLUMBING	PLUMBING



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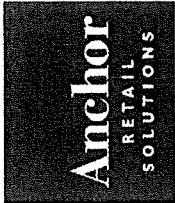
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1 - COLOR RENDERING
12.04.17

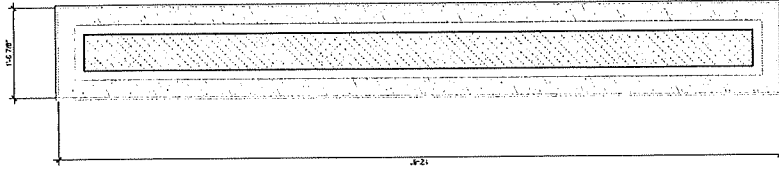
MARK	MATERIAL	MANUFACTURER	COLOR
BRK1	BRICK	BRICK	BRICK
BRK2	BRICK	BRICK	BRICK
BRK3	BRICK	BRICK	BRICK
BRK4	BRICK	BRICK	BRICK
BRK5	BRICK	BRICK	BRICK
BRK6	BRICK	BRICK	BRICK
BRK7	BRICK	BRICK	BRICK
BRK8	BRICK	BRICK	BRICK
BRK9	BRICK	BRICK	BRICK
BRK10	BRICK	BRICK	BRICK
BRK11	BRICK	BRICK	BRICK
BRK12	BRICK	BRICK	BRICK
BRK13	BRICK	BRICK	BRICK
BRK14	BRICK	BRICK	BRICK
BRK15	BRICK	BRICK	BRICK
BRK16	BRICK	BRICK	BRICK
BRK17	BRICK	BRICK	BRICK
BRK18	BRICK	BRICK	BRICK
BRK19	BRICK	BRICK	BRICK
BRK20	BRICK	BRICK	BRICK
BRK21	BRICK	BRICK	BRICK
BRK22	BRICK	BRICK	BRICK
BRK23	BRICK	BRICK	BRICK
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BRK25	BRICK	BRICK	BRICK
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BRK27	BRICK	BRICK	BRICK
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BRK50	BRICK	BRICK	BRICK



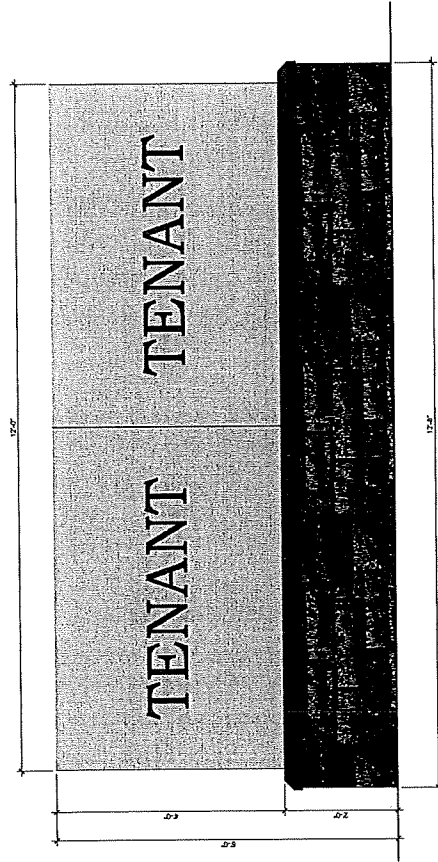
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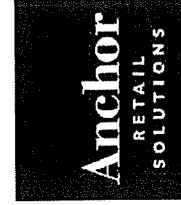


① MONUMENT SIGN
142.78"
421"



② MONUMENT SIGN
124"
122"

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UNION, KY
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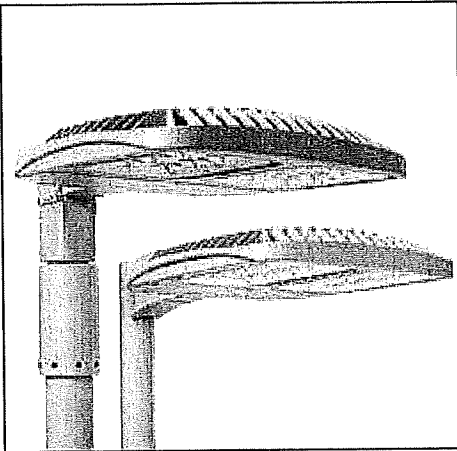


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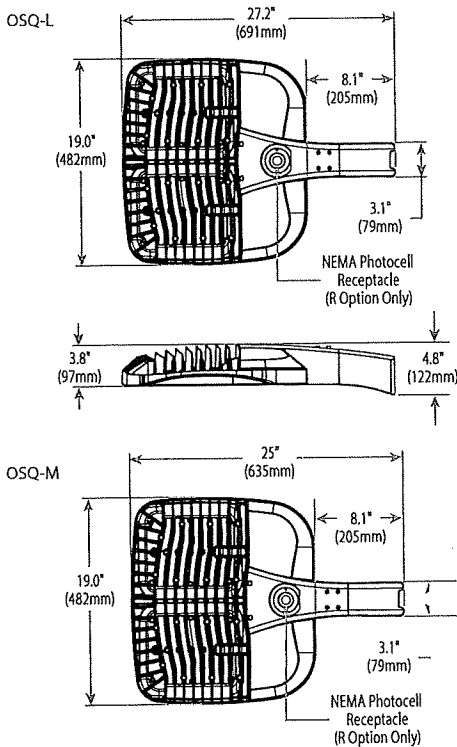
WLS LIGHTING SYSTEMS

OSQ SERIES LED AREA

SPECIFICATIONS



DIMENSIONS



Made in the U.S.A. of U.S. and imported parts.
Meets Buy American requirements within the ARRA.

CONSTRUCTION & MATERIALS

- Slim, low profile design minimizes wind load requirements
- Luminaire housing is rugged die cast aluminum with an integral, weathertight LED driver compartment and high performance heat sink
- Convenient interlocking mounting method on direct arm mount. Mounting adaptor is rugged die cast aluminum and mounts to 3-6" (76-152mm) square or round pole, secured by two 5/16-18 UNC bolts spaced on 2" (51mm) centers
- Mounting for the adjustable arm mount adaptor is rugged die cast aluminum and mounts to 2" (51mm) IP, 2.375" (60mm) O.D. tenon
- Adjustable arm mount can be adjusted 180° in 2.5° increments
- Designed for uplight and downlight applications
- Exclusive Colorfast DeltaGuard® finish features an E-Coat epoxy primer with an ultra-durable powder topcoat, providing excellent resistance to corrosion, ultraviolet degradation and abrasion. Silver, bronze, black, and white are available
- Weight 26.5 lbs. (12kg) for the OSQ-M and 28.5lbs. (13kg) for the OSQ-L

ELECTRICAL SYSTEM

- Input Voltage: 120-277V or 347-480V, 50/60Hz, Class 1 drivers
- Power Factor: > 0.9 at full load
- Total Harmonic Distortion: < 20% at full load
- Integral 10kV surge suppression protection standard
- To address inrush current, slow blow fuse or type C/D breaker should be used
- 10V Source Current: 0.15mA

REGULATORY & VOLUNTARY QUALIFICATIONS

- cULus Listed
- Suitable for wet locations
- Enclosure rated IP66 per IEC 60529 when ordered without R option
- Consult factory for CE Certified products
- Certified to ANSI C136.31-2001, 3G bridge and overpass vibration standards
- 10kV surge suppression protection tested in accordance with IEEE/ANSI C62.41.2
- Meets FCC Part 15, Subpart B, Class A standards for conducted and radiated emissions
- Luminaire and finish endurance tested to withstand 5,000 hours of elevated ambient salt fog conditions as defined in ASTM Standard B 117
- Meets Buy American requirements within ARRA
- DLC qualified when ordered with asymmetric optics with 40K or 57K. Please refer to www.designlights.org/QPL for most current information
- RoHS compliant. Consult factory for additional details

WLS-OSQ-M Electrical Data*							
Input Power Designator	System Watts 120-480V	Total Current (A)					
		120V	208V	240V	277V	347V	480V
B	86	0.73	0.43	0.37	0.32	0.25	0.19
K	130	1.09	0.65	0.56	0.49	0.38	0.28

* Electrical data at 25°C (77°F). Actual wattage may differ by +/- 10% when operating between 120-480V +/- 10%

WLS-OSQ-L Electrical Data*							
Input Power Designator	System Watts 120-480V	Total Current (A)					
		120V	208V	240V	277V	347V	480V
T	166	1.41	0.83	0.72	0.62	0.49	0.35
U	215	1.83	1.08	0.93	0.81	0.62	0.45

* Electrical data at 25°C (77°F). Actual wattage may differ by +/- 10% when operating between 120-480V +/- 10%

Approved By: _____ Project Name: _____

Location: _____ Date: _____

1919 Windsor Place • Fort Worth, TX 76110 • 800.633.8711 • Fax: 817.735.4824 • www.wslighting.com

WLS LIGHTING SYSTEMS

Consider the Impact!

OSQ SERIES

LED AREA

ORDERING INFORMATION SELECT APPROPRIATE CHOICE FROM EACH COLUMN TO FORMULATE ORDER CODE. Refer to example below.

SERIES	VERSION	MOUNT	DISTRIBUTION	INPUT POWER	COLOR TEMP	LINE VOLTAGE	LUMINAIRE FINISH	OPTIONS
OSQ-M - Medium	A	NM - No Mount	2ME - Type II Medium 3ME - Type III Medium 4ME - Type IV Medium 5ME - Type V Medium 5SH- Type V Short WSN - Wide Sign 15D - 15 Flood 25D - 25 Flood 40D - 40 Flood 60D - 60 Flood	B - 86W K - 130W	30K - 3000K 40K - 4000K 57K - 5700K	UL - Universal 120 - 277V UH - Universal 347 - 480V	BZ - Bronze BK - Black SV - Silver WH - White CC - Custom Color	BLSMF - Backlight Shielding for Medium BLSMR - Backlight Shielding for Medium with Rotated Optics BLSLF - Backlight Shielding for Large BLSLR - Backlight Shielding for Large with Rotated Optics DIM - 0-10V Dimming F - Fuse ML - Multi-Level PML - Programmable Multi-Level, 20-40' Mounting Height PML2 - Programmable Multi-Level, 10-30' Mounting Height R - NEMA Photocell Receptacle ¹ Q9 - Field Adjustable Output RL - Rotate Optic Left RR - Rotate Optic Right XA-SENSREM - Hand held Remote NO - No Options
OSQ-L - Large			2ME - Type II Medium 3ME - Type III Medium 4ME - Type IV Medium 5ME - Type V Medium 5SH- Type V Short WSN - Wide Sign 15D - 15 Flood 25D - 25 Flood 40D - 40 Flood 60D - 60 Flood	T - 166W U - 215W				

OSQ-M A DM 3M K 57K UL BZ NO
(EXAMPLE ORDER)

ORDER:
WLS-OSQ-

FOOTNOTES:

1 - Not available with UP option, for use with downlight only.

Tenons and Brackets ¹ (must specify color)

Square Internal Mount Vertical Tenons (Steel) - Mounts to 3-6" (76-152mm) square aluminum or steel poles PB-1A* - Single PB-2A* - 180° Twin PB-3A* - 180° Triple PB-4A*(90) - 90° Quad PB-4A*(180) - 180° Quad	Round External Mount Vertical Tenons (Steel) - Mounts to 2.375" (60mm) O.D. round aluminum or steel poles or tenons PB-2R2.375 - Twin PB-3R2.375 - Triple PB-4R2.375 - Quad
Square Internal Mount Horizontal Tenons (Aluminum) - Mounts to 4" (102mm) square aluminum or steel poles PD-2A4(90) - 90° Twin PD-2A4(180) - 180° Twin PD-3A4(90) - 90° Triple PD-4A4(90) - 90° Quad	Round External Mount Horizontal Tenons (Aluminum) - Mounts to 2.375" (60mm) O.D. round aluminum or steel poles or tenons - Mounts to square pole with PB-1A* tenon PT-1 - Single (Vertical) PT-2(90) - 90° Twin PT-2(180) - 180° Twin PT-3(90) - 90° Triple PT-4(90) - 90° Quad
Wall Mount Brackets - Mounts to wall or roof WM-2 - Horizontal for OSQ-AA mount WM-4 - L-Shape for OSQ-AA mount WM-DM - Plate for OSQ-DA mount	Mid-Pole Bracket - Mounts to square pole PW-1A3** - Single PW-2A3** - Double Ground Mount Post - For ground mounted flood luminaires PGM-1 - for OSQ-AA mount

¹ Refer to the Bracket and Tenons spec sheet for more details

Mount (Luminaire must be ordered separately)

OSQ-	Color Options: SV Silver BK Black	BZ Bronze WH White
OSQ-AA Adjustable Arm OSQ-DA Direct Arm		

Approved By: _____ Project Name: _____

Location: _____ Date: _____

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WLS LIGHTING SYSTEMS

Consider the Impact!

MEMORANDUM

DATE: October 25, 2017
SUBJECT: Traffic Impact Study Update
PREPARED BY: Michael Hafner, P.E., PTOE (TEC Engineering, Inc.)
PREPARED FOR: Mr. Brad Copp

This analysis has been performed to estimate the trip generated by a proposed outlot development adjacent to the Kroger site on US 42 in Union, Kentucky. The site has a proposed restaurant and retail use that are 5,019 ft² and 2,500 ft², respectively. Generated trip have been developed for two separate scenarios; scenario 1 consists of trip generation of the proposed site assuming no interaction with adjacent developments, and scenario 2 consists of trip generation of the proposed site as if it were part of the larger Kroger site development. The results of these trip generation scenarios are summarized below.

I. Trip Generation (No interaction with adjacent sites)

The trips generated by the proposed outlot site are illustrated below. The *Institute of Transportation Engineers (ITE) Trip Generation Manual* was used to generate trips for the site. This manual is the most widely accepted publication for projecting traffic volumes specifically related to how a site is used. **Table 1** illustrates the generated trips seen on an independent site.

Table 1: Generated Trips – Scenario 1 Standalone Outlot

Traffic Generated by Development									
Site	Land Use Type (site)	ITE Code	Unit of Measure	Peak Hour of Adjacent Street Traffic					
				AM			PM		
				Total	Enter	Exit	Total	Enter	Exit
1	Restaurant (sit-down)	932	5,019 ft ²	54	30	24	49	30	20
	Specialty Retail Center	826	2,500 ft ²	0	0	0	27	12	15
Total Trips				54	30	24	77	42	35

The ITE Trip Generation Manual does not include AM peak hour (7-9am) trip rates for the Specialty Retail Center. These uses are typically not open during the AM peak period and therefore no trips are assumed to be generated by this use in the AM peak hour. Retail and restaurant-oriented developed are often located adjacent to busy streets, in this case US-42. These sites attract a portion of their trips from traffic passing the site on the way from an origin to an ultimate destination. These trips do not add new traffic to the adjacent roadway network but constitute a "pass-by" trip. The pass-by trips for the site were determined from values provided in the *ITE Trip Generation Handbook*. Final Pass-by trips values and new primary trip values are provided in the following table.

Table 2: Pass-by Reductions

Site	Land Use	Pass by trips						Primary (Non Pass by) Trips			
		AM			PM			AM		PM	
		Pass-by Rate	Enter	Exit	Pass-by Rate	Enter	Exit	Enter	Exit	Enter	Exit
1	Restaurant (sit-down)	0%	0	0	43%	13	9	30	24	17	11
	Specialty Retail Center	0%	0	0	0%	0	0	0	0	12	15
	Total Trips		0	0		13	9	30	24	29	27

The scenario 1 results indicate that the proposed outlot site will generate 54 trips in the AM (54 new trips, 0 pass-by trips) and 77 trips in the PM (56 new trips, 21 pass-by trips).

II. Trip Generation (outlot assumed as part of larger development)

Table 3 depicts the generated trips for the entire development area including the proposed outlot (site 1), the Kroger site (site 2), and the senior housing site (site 3). The proposed site is now looked at within the larger context of the full development.

Table 3: Generated Trips – Scenario 2 Outlot as a part of larger development

Traffic Generated by Development									
Site	Land Use Type (site)	ITE Code	Unit of Measure	Peak Hour of Adjacent Street Traffic					
				AM			PM		
				Total	Enter	Exit	Total	Enter	Exit
1	Restaurant (sit-down)	932	5,019 ft ²	54	30	24	49	30	20
	Specialty Retail Center	826	2,500 ft ²	0	0	0	27	12	15
2	Medical-Dental Office	720	6,000 ft ²	14	11	3	23	6	17
	Specialty Retail Center	826	12,000 ft ²	0	0	0	50	22	28
	Restaurant (fast-food)	934	6,000 ft ²	273	139	134	196	102	94
	Specialty Retail Center	826	6,000 ft ²	0	0	0	36	16	20
	Drive-in Bank	912	6,000 ft ²	74	41	33	154	77	77
	Restaurant (fast-food)	934	6,000 ft ²	273	139	134	196	102	94
	Restaurant (fast-food)	934	5,000 ft ²	227	116	111	163	85	78
	Supermarket	850	144,000 ft ²	490	304	186	1020	520	500
	Gasoline/service Station	944	18 Pumps	219	112	107	250	125	125
	3	Residential	252	162 DUs	32	11	21	41	22
	Total Trips			1656	903	753	2206	1119	1087

Since the development is a multi-use development, some of the trips generated by the site can be classified as internal trips. Multi-use developments are defined as follows:

A multi-use development is typically a single real-estate project that consists of two or more ITE land use classifications between which trips can be made without using the off-site road system. Multi-use developments are commonly found ranging in size from 100,000 square feet to 2 million square feet.

A key characteristic of a multi-use development is that trips among various land uses can be made on site and these internal trips are not made on the major street system. These trips are made by walking or vehicles entirely on internal pathways or internal roadways without using streets external to the site.

The ITE Internal Trip Generation Tool was used to estimate the internal trips for this development. The results are illustrated in Table 4.

Table 4: Internal Distributions

Site	Land Use	AM						PM					
		Entering Trip			Exiting Trips			Entering Trips			Exiting Trips		
		Int.	Ext.	Total	Int.	Ext.	Total	Int.	Ext.	Total	Int.	Ext.	Total
1	Restaurant (sit-down)	2	28	30	2	22	24	8	22	30	10	10	20
	Specialty Retail Center	0	0	0	0	0	0	3	9	12	3	13	16
2	Medical-Dental Office	2	9	11	2	1	3	5	1	6	4	13	17
	Specialty Retail Center	0	0	0	0	0	0	6	16	22	5	23	28
	Restaurant (fast-food)	13	126	139	11	123	134	32	70	102	40	54	94
	Specialty Retail Center	0	0	0	0	0	0	4	12	16	3	17	20
	Drive-in Bank	0	41	41	0	33	33	0	77	77	0	77	77
	Restaurant (fast-food)	13	126	139	11	123	134	32	70	102	40	54	94
	Restaurant (fast-food)	10	106	116	9	102	111	26	59	85	33	45	78
	Supermarket	30	274	304	33	153	186	115	405	520	94	406	500
	Gasoline/service Station	0	112	113	0	107	107	0	125	125	0	125	125
3	Residential	1	10	11	4	17	21	14	8	22	13	6	21
	Totals	71	832	903	71	682	753	245	874	1119	245	842	1087

The pass-by and primary trips (non pass-by trips) for the development can be seen in *Table 5*.

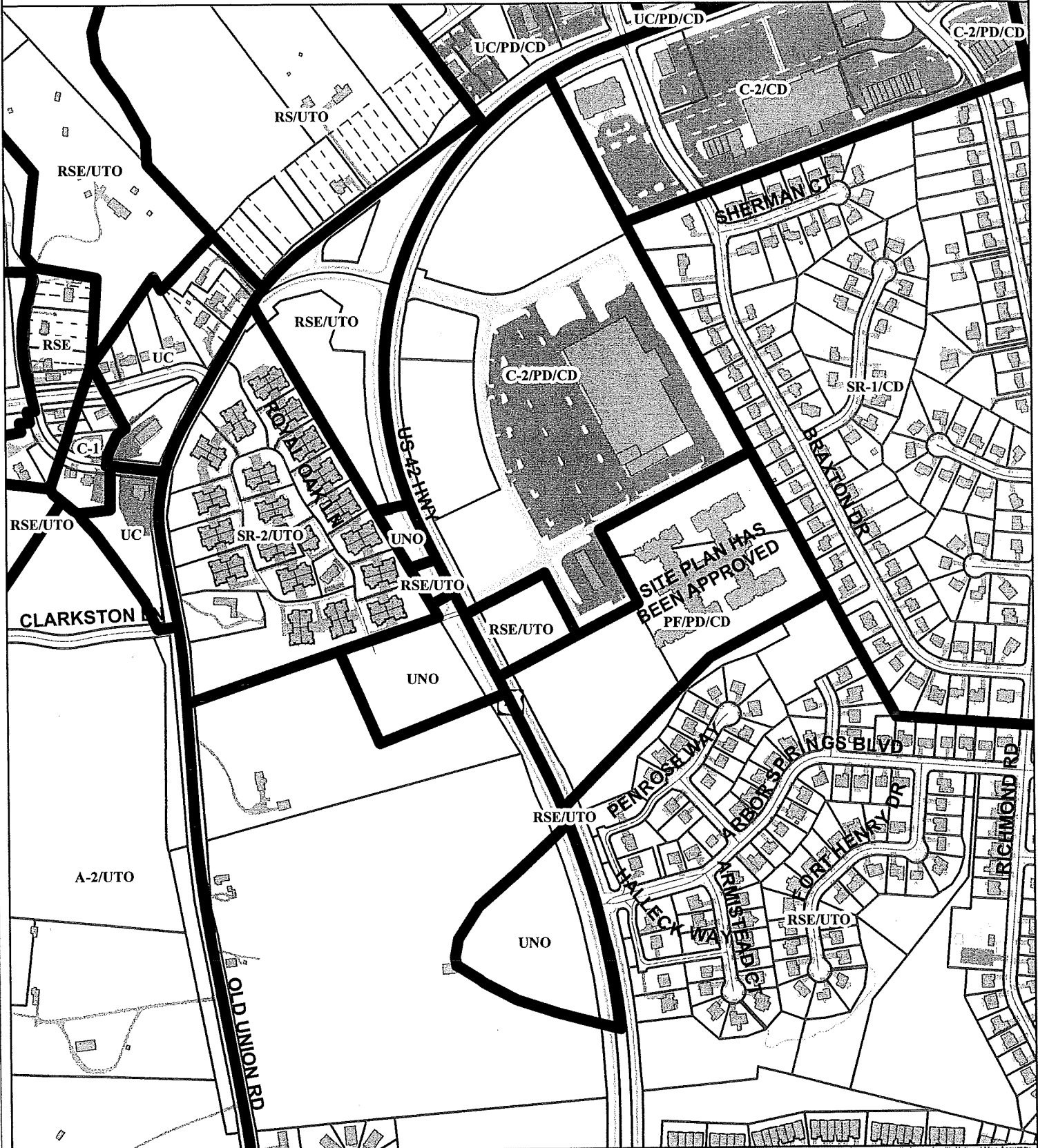
Table 5: Pass-by and Primary Trips

Site	Land Use	Pass-by Trip %		Total Trips				Pass-by trips				Primary (Non-Pass by) Trips			
				AM		PM		AM		PM		AM		PM	
		AM	PM	Enter	Exit	Enter	Exit	Enter	Exit	Enter	Exit	Enter	Exit	Enter	Exit
1	Restaurant (sit-down)	0%	43%	28	22	22	10	0	0	9	4	27	22	12	6
	Specialty Retail Center	0%	0%	0	0	9	13	0	0	0	0	0	0	9	13
2	Medical-Dental Office	0%	0%	9	1	1	13	0	0	0	0	9	1	1	13
	Specialty Retail Center	0%	0%	0	0	16	23	0	0	0	0	0	0	16	23
	Restaurant (fast-food)	49%	50%	126	123	70	54	62	60	35	27	64	63	35	27
	Specialty Retail Center	0%	0%	0	0	12	17	0	0	0	0	0	0	12	17
	Drive-in Bank	0%	47%	41	33	77	77	0	0	36	36	41	33	41	41
	Restaurant (fast-food)	49%	50%	126	123	70	54	62	60	35	27	64	63	35	27
	Restaurant (fast-food)	49%	50%	106	102	59	45	52	50	29	22	54	52	29	22
	Supermarket	0%	36%	274	153	405	406	0	0	146	146	274	153	259	260
	Gasoline/service Station	58%	42%	112	107	125	125	65	62	53	53	47	45	73	73
	3	Residential	0%	0%	10	17	8	6	0	0	0	0	10	17	8
Totals				832	682	874	842	241	233	343	315	591	449	531	527

The scenario 2 results indicate that the proposed outlot site will generate 54 trips in the AM (50 new trips, 0 pass-by trips, and 4 internal trips) and 77 trips in the PM (40 new trips, 13 pass-by trips, and 24 internal trips).

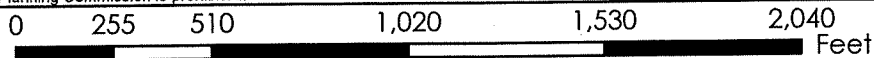
ZONING MAP

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1 inch = 500 feet

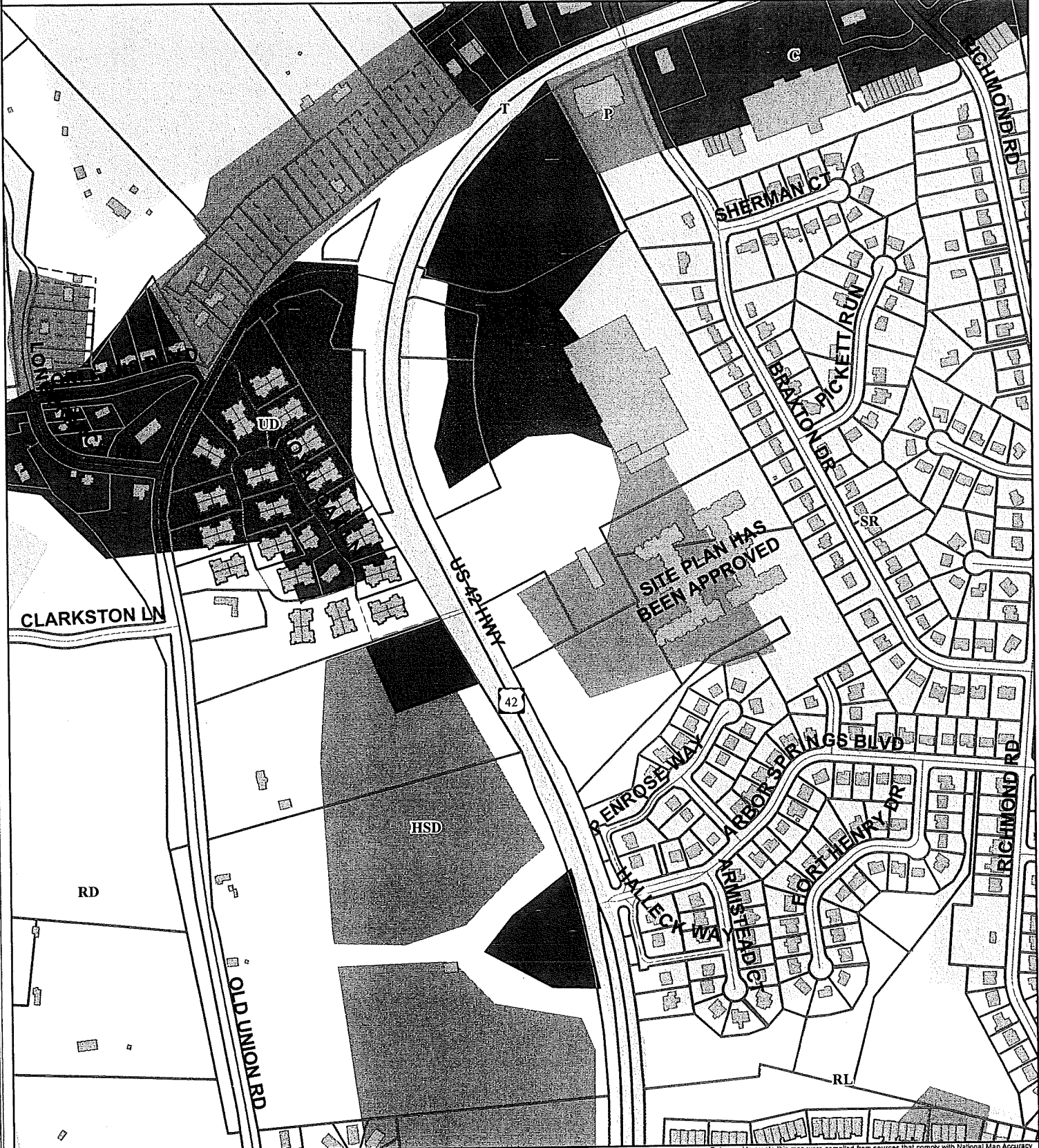


Boone County GIS - Putting Northern Kentucky on the Map



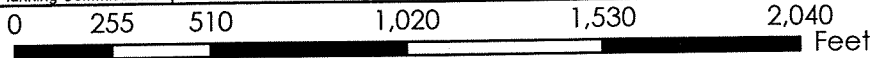
2035 FUTURE LAND USE MAP

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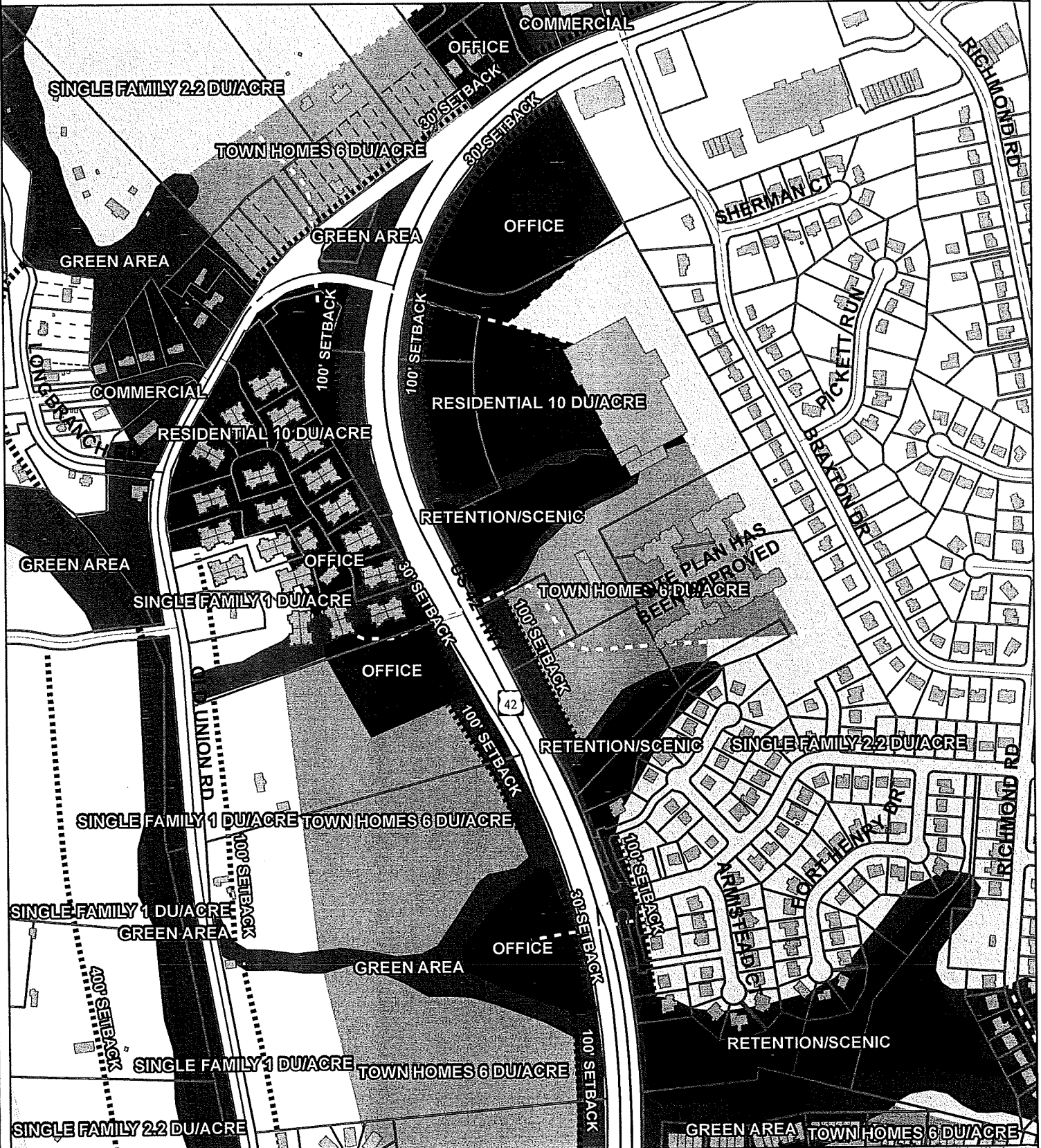
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Map Created: 07/01/2013
ArcMap Document: BooneMap (file).mxd

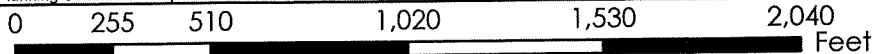
2000 UNION TOWN LAND USE PLAN

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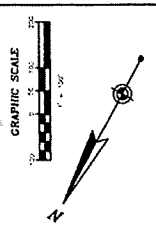
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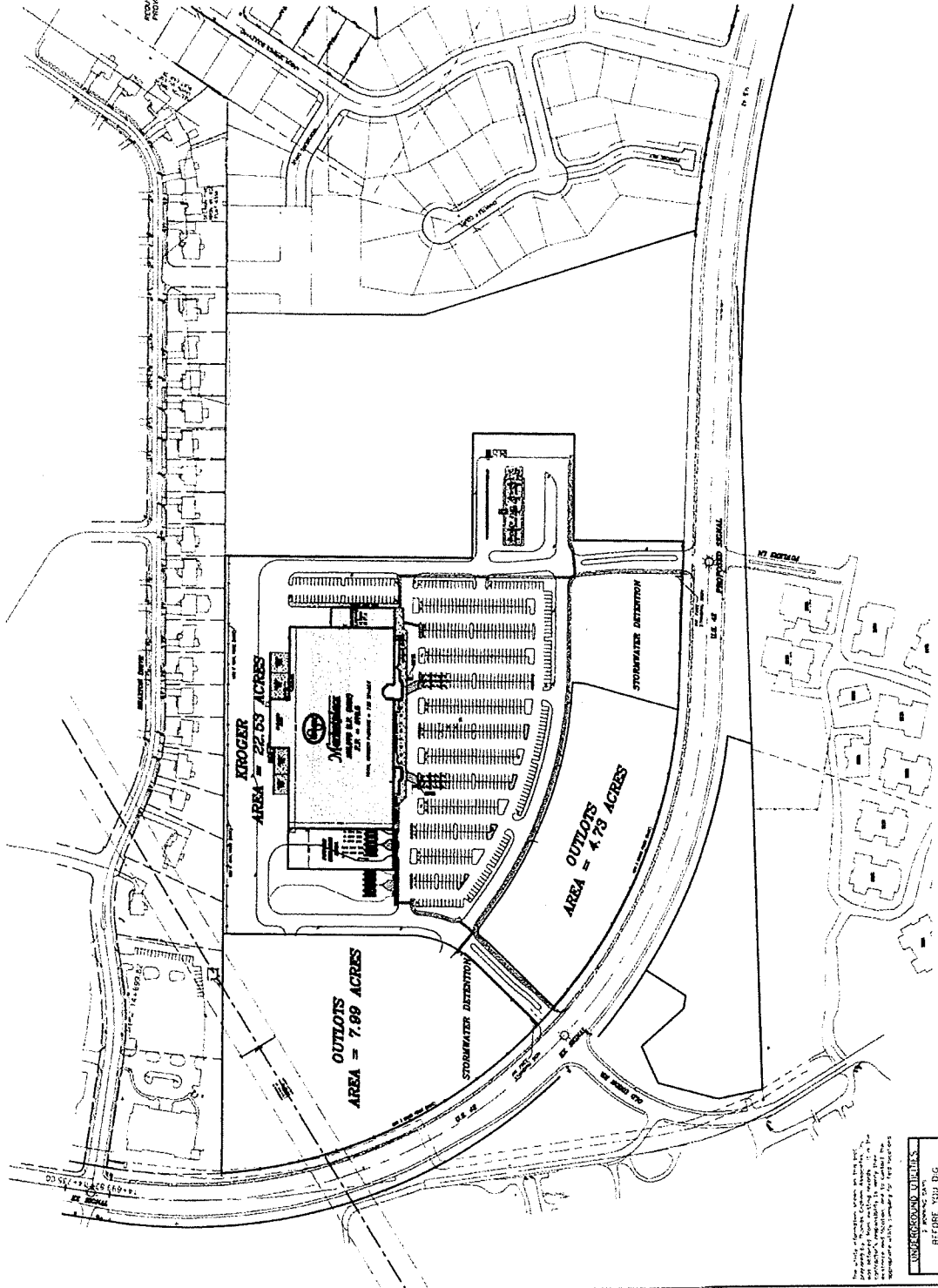
Boone County GIS - Putting Northern Kentucky on the Map

SHOWN AT 12/4/13 PUBLIC HEARING
136,000 SQUARE FOOT KROGER MARKETPLACE
8,000 KROGER LIQUOR STORE
730 PARKING STALLS
TWO OUTLOT AREAS



KROGER TRACT
PARKING REQUIREMENTS
 KROGER MARKETPLACE - 8 SPACES
 KROGER LIQUOR STORE - 80 SPACES
 STORE EXPANSION (26,333 SF) - 80 SPACES
 100 CARS (8 SPACES) - 8 SPACES
 OUTLOT STALLS (42 SPACES) - 21 SPACES
 TOTAL PARKING REQUIRED - 196 SPACES
 TOTAL SPACES PROVIDED - 200 SPACES
 PARKING SPACES PROVIDED - 200 SPACES
 BAY SPACES PROVIDED - 47

INTERIOR VMA ANALYSIS
 TOTAL INTERIOR VMA - 240,794 SF
 TOTAL INTERIOR LANDSCAPE VMA - 20,874 SF
 TOTAL INTERIOR VMA TO BE PLANTED - 261,668 SF
 PROPOSED PERCENTAGE OF TOTAL VMA TO BE PLANTED - 2.8%



CONSTRUCTION NOTES
 1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS PLUMBING CODE AND THE MISSOURI PLUMBING CODE.
 2. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS ELECTRICAL CODE AND THE MISSOURI ELECTRICAL CODE.
 3. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS MECHANICAL CODE AND THE MISSOURI MECHANICAL CODE.
 4. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS FIRE CODE AND THE MISSOURI FIRE CODE.
 5. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS SANITATION CODE AND THE MISSOURI SANITATION CODE.
 6. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS WATER CODE AND THE MISSOURI WATER CODE.
 7. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS AIR CODE AND THE MISSOURI AIR CODE.
 8. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS ENVIRONMENTAL CODE AND THE MISSOURI ENVIRONMENTAL CODE.
 9. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS ZONING CODE AND THE MISSOURI ZONING CODE.
 10. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS PLANNING CODE AND THE MISSOURI PLANNING CODE.
 11. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS HISTORIC PRESERVATION CODE AND THE MISSOURI HISTORIC PRESERVATION CODE.
 12. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS PUBLIC WORKS CODE AND THE MISSOURI PUBLIC WORKS CODE.
 13. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS PUBLIC SAFETY CODE AND THE MISSOURI PUBLIC SAFETY CODE.
 14. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS PUBLIC UTILITIES CODE AND THE MISSOURI PUBLIC UTILITIES CODE.
 15. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS PUBLIC HEALTH CODE AND THE MISSOURI PUBLIC HEALTH CODE.
 16. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS PUBLIC WELFARE CODE AND THE MISSOURI PUBLIC WELFARE CODE.
 17. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS PUBLIC EDUCATION CODE AND THE MISSOURI PUBLIC EDUCATION CODE.
 18. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS PUBLIC RECREATION CODE AND THE MISSOURI PUBLIC RECREATION CODE.
 19. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS PUBLIC CULTURE CODE AND THE MISSOURI PUBLIC CULTURE CODE.
 20. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF ST. LOUIS PUBLIC ARTS CODE AND THE MISSOURI PUBLIC ARTS CODE.

- G. The Kroger drive through pharmacy and banking lanes can be reduced from ten feet (10') in width to nine feet (9') in width.
 - H. Kroger Marketplace and Kroger Fuel Center is permitted to display merchandise (landscaping materials, Christmas trees, firewood, produce, pumpkins, grills, car accessories, food and drinks, etc.) in the areas marked on the Concept Development Plan without the issuance of Temporary Commercial Display Permits.
 - I. The outlot buildings shall not follow the Commercial Two (C-2) building setback requirements. See the attached Outlot design standards for the specific requirements.
- 4. The Concept Development Plan shows that Kroger Marketplace will have an outdoor seating area. The fencing around the seating area shall comply with the submitted plan and fencing detail.
 - 5. The footcandle measurements on the Kroger lot shall comply with the submitted photometric plan. Light poles and fixtures shall not exceed 41.5' in height and all lighting shall be directed downwards and inwards towards the subject site.
 - 6. The following sidewalks shall be provided in the development:
 - A. The sidewalks shown on the 1/15/14 Zone Change Committee exhibit.
 - B. Pedestrian connectivity shall be required for all outlot buildings (4.73 acre and 7.99 acre). Sidewalk connections shall be provided between main building entrances and the public sidewalk system on US 42 or the sidewalk system along the development driveways.
 - C. Sidewalk connections between all outlot buildings.
 - 7. Boone County Library shall determine if a pedestrian connection is required between the 7.99 acre outlot area and the library (Scheben Branch). The discussion between the owner/developer and the library shall occur before the first outlot develops in the 7.99 acre area. The final decision by the library shall be furnished to the Boone County Planning Commission in writing. The owner/developer shall pay for all improvements located on the outlot property.
 - 8. The Outlot areas shown on the submitted Concept Development Plan shall be developed according to the "Outlot Uses and Standards" attachment.

*Union Kroger Marketplace Outlot Conditions
from City of Union Ordinance No. 2014-004*

**Kroger Marketplace
Union, KY
Outlot Uses and Standards**

Principally Permitted Uses:

1. Eating and drinking establishments including alcoholic beverages, and drive-through, franchise style fast food establishments. With the condition that no more than four (4) eating and drinking establishments are permitted in the outlot areas;
2. Hardware stores;
3. Grocery stores and supermarkets;
4. Stores with retail sales of meat, fish, seafood, dairy, and poultry products;
5. Fruit and vegetable stores; bakeries, candy, nut and confectionery stores;
6. Liquor, beverage, pharmacy, drug and proprietary stores;
7. Banking services, savings and loan associations, credit unions, and other credit services, including drive-through facilities;
8. Insurance carriers and agents;
9. Real estate and related services;
10. Accounting, auditing and bookkeeping services;
11. Postal services and packaging services provided the use is essential for pick-up and delivery convenience and not storage or transfer activities more appropriate to an employment district;
12. Physician, dental, optical goods and services;
13. Veterinary services and pet grooming services;
14. Beauty and barber services and tanning salons;
15. Nursery and day care centers;
16. Laundering, dry cleaning and dyeing services;
17. Alteration, and garment repair and custom tailoring;
18. Shoe sales and repair, shoe shining and hat cleaning services;
19. Department stores, family clothing, shoe stores, specialty clothing or boutiques and other apparel retail trade;
20. Jewelry stores;
21. Household appliances and electronic sales and repair;
22. Art, craft and hobby supplies and products, gifts and novelties;
23. Antiques and used merchandise;
24. Books, stationery, newspapers, greeting cards, magazines, and related media;
25. Florists excluding greenhouses;
26. Sporting goods including bicycles;
27. Draperies, curtains, upholstery and floor coverings;
28. Paint, glass, and wallpaper stores;
29. Photo finishing services;
30. Funeral homes and crematoriums excluding cemeteries or mausoleums;
31. Furniture, home furnishings including specialty and floor coverings;
32. Specialized upholstery and furniture repair or refinishing services;
33. Apparel stores;
34. Household appliances, china, glassware and metalware;
35. Legal services, engineering, architectural, education and professional services;
36. Title abstracting services, holding and investment services;
37. Advertising services including direct mail;
38. Business and management consulting services, credit services;

39. Employment services;
40. Consumer and mercantile credit reporting, adjustment and collection services;
41. Travel arranging, transportation ticket and public event or promotional booking agencies;
42. Radio and television broadcasting studios excluding transmitting stations and towers;
43. Art, music and dancing schools, libraries and museums;
44. Medical and dental laboratory services;
45. Medical clinics - out - patient services;
46. Welfare and charitable services;
47. Business associations and professional membership organizations including civic, social and fraternal organizations;
48. Art and craft galleries and similar exhibit space;
49. Aquariums, botanical gardens and other natural exhibitions;
50. Real estate management services and builders offices excluding any outside storage;
51. Photographic, stenographic, and other duplicating and mailing services;
52. Research, development and testing services of an office nature;
53. Business colleges or schools;
54. Retail sale of office supplies and equipment;
55. Generic professional offices;
56. Video rental and sales;
57. Storm water management facilities designed in accordance with Article 3 of the Boone County Subdivision Regulations (site plan review required);
58. Pet stores excluding kennels;
59. Toy and game stores;
60. Telephone sales and service; and
61. Recreation centers, gymnasiums, clubs, and athletic uses and recreational facilities provided that no single use exceeds 12,000 square feet.

Accessory Uses:

Accessory uses and structures customarily incidental and subordinate to any of the permitted uses including:

1. Signage;
2. Parking;
3. Delivery areas with no outside storage or unpacking;
4. Temporary buildings incidental to construction;
5. Automatic teller machines attached to the principal structure;
6. Accessory uses for office facilities. With the condition that the accessory use can be retail sales only. The accessory use shall not be a restaurant.

Area/Use Restrictions:

The northeast portion of the 7.99 acre outlot area shall be reserved for development as either a minimum of two office outlots or a minimum of 12,000 square feet of office building area.

Design Requirements:

1. Setbacks:

Principally Permitted Structures:

- A. Front Yard Minimum Setback: Ten (10) feet from the right-of-way line.
 - B. Rear Yard Minimum Setback: Twenty (20) feet from the property line.
 - C. Side Yard Minimum Setbacks: Five (5) feet from the property line.
2. Height:
 - A. The maximum height of any commercial use is one story.
 - B. The maximum height of any office use is fifty (50) feet.
 3. Impervious Surface: No more than eighty percent (80%) of the site may be covered with impervious surfaces. The remaining twenty percent (20%) shall be landscaped or kept in its natural state.
 4. Maximum Building Intensity: 15,000 square feet of building per acre of land.
 5. Utilities: All utilities within the development, except for street lights, shall be located underground.
 6. Building Orientation: All structures shall front toward existing US 42. Any drive-through windows or automatic teller machines must be located on the side or rear of buildings away from US 42.
 7. Fencing on US 42: A white three board horse style fence will be constructed along the frontage of US 42 as part of the initial phase of development.
 8. Architecture: The following architectural standards shall be reviewed by Planning Commission Staff when Major Site Plan applications are submitted for each outlot building. Any appeal of the architectural standards shall be heard by the Boone County Planning Commission's Technical Design Review Committee.
 - A. Materials: All sides of the principle structure that are visible from all roads except for rear accessed alleys shall constructed of traditional materials including: stone, brick, architectural concrete masonry units with integral color (painted blocks not acceptable), wood or glass; or synthesized materials that appear as such. Rear and side elevations shall have the same architectural treatment as the front. The amount of EIFS on any given building facade shall correlate to the percentage of EIFS shown on the front facade of the Kroger Marketplace building. Exposed concrete foundations shall be finished with brick or stone or a material of the appearance of such. Concrete foundations can be exposed at a minimum of two feet but screened with landscaping.
 - B. Garage/Loading/Unloading Doors: Garage or loading/unloading doors shall not face US 42.
 - C. Entrance: All building shall have their main entrance on the primary street (US 42).
 - D. Building Length: No outlot building shall have a continuous unbroken facade that is greater than 150' unless it broken up with architectural treatments or projections (changes in color shall not be considered an architectural treatment).

- E. Roof Equipment - Roof mounted mechanical equipment on all buildings shall be screened from public rights-of-way and from all adjoining residential properties.
9. Waste & Refuse: Trash Areas shall be located to the side or rear of the principal structure. All trash areas shall be completely enclosed within a structure to minimize their visual impact from public streets, internal circulation areas, and adjoining properties. The sides and rear of the enclosure shall be constructed with masonry materials that match the principal building on site. Solid gates shall be used be constructed with wood, vinyl, or metal materials.
10. Parking & Landscaping: A maximum of one row of parking (drive aisle with parking on both sides) shall be permitted between US 42 and the principal structure. All parking areas and drives and pedestrian walkways shall be paved with an impervious surface such as asphalt, concrete or interlocking pavers. All landscaping provided on the outlots shall conform to the Boone County Zoning Regulations. Landscaped and parking areas shall be properly maintained thereafter by the owner in a well-kept condition. Employee, owner or tenant parking will not be permitted on private or public dedicated streets and each property owner shall be responsible to provide such parking facilities on their own property. Parking requirements shall be provided consistent with Article 33 or as approved by the City of Union.
11. Signage: Outlot building mounted signage shall conform to the provisions of Article 34. In addition, each outlot shall be permitted one monument style sign with a stone base. Each sign may contain 48 square feet of sign area per side at a maximum height of 6 feet. Thirty percent (30%) of the sign area may be used as a manual changeable display area. Signs shall be located outside of any vehicular sight triangle and be at least 5 feet from any property line. Signs may be illuminated and must be located within a landscaped bed.
12. Lighting: All lighting shall comply with Section 3316 and Figure 33.5 of the Boone County Zoning Regulations. Fixtures mounted on light poles shall not exceed 30' in height and shall be oriented downwards and inwards towards the subject site.
13. Outside Sales and Storage: Shall comply with Sections 3154 and 3510 of the Boone County Zoning Regulations.
14. Maintenance: The owner or lessees of any portion of the property must at all times keep the premises, structures, all other improvements and appurtenances thereon in a safe, clean, and well-manicured condition and comply in all respects with all governmental health and police requirements and will contain and remove at their own expense any trash or rubbish of any character whatsoever which may accumulate on said lots. Grass shall be cut on all lots by the owner or lessee of such lots.

that is consistent and compatible with existing or planned infrastructure;

3. A development pattern which preserves and utilizes natural topography and geologic features, scenic vistas, trees and other vegetation, and prevents the disruption of natural drainage patterns;
4. More efficient use and development of land than is generally achieved through conventional regulations resulting in substantial savings through shorter utilities, streets, and other infrastructure;
5. A development pattern consistent with the adopted Comprehensive Plan and any other appropriate land use studies;
6. A signage package that emphasizes consistency and the minimal use of signs.

SECTION 1510

Provisions Governing Planned Developments

Because of the special characteristics of planned developments, special provisions governing the development of land for this purpose are required. Whenever there is a conflict or difference between the provisions of this article and those of other articles of this order, the provisions of this article shall prevail. Subjects not covered by this article shall be governed by the respective provisions found elsewhere in this order.

SECTION 1511

Permitted Uses

Uses listed as principally permitted, accessory or conditional uses in the underlying zone are allowed within a Planned Development district as long as the requirements of this article have been met, including approval by the appropriate legislative body or Fiscal Court of a Concept Development Plan. Further, any other uses not listed in the underlying zone are permitted if found to be compatible and of benefit to the community and meet the requirements of this article, including approval for any such use or uses through a Concept Development by the appropriate legislative body or Fiscal Court. Uses proposed for a Planned Development district shall not adversely affect adjacent property, and/or the public health, safety, and general welfare and/or the provisions of the adopted Comprehensive Plan. The review and decision making process under this article identify adverse impacts and address how they may be minimized, if possible, to satisfy the requirements of this article. In no event shall this article on its face or as applied prohibit all use of property zoned with a Planned Development overlay district.

It is also provided that the amount of land devoted to each use in a Planned Development shall be determined through the Concept Development Plan review and approval process. The appropriate legislative body or Fiscal Court shall have final decision making authority and the Planning Commission is a recommending body pursuant to the provisions of this article.

SECTION 1512

Intensity

The intensity of use in a Planned Development may exceed by no more than fifty (50) percent the maximum intensity permitted in the underlying zone district.

SECTION 1513

Minimum Size

The minimum size or area required for a Planned Development overlay shall be no less than five (5) acres.

SECTION 1514

Planned Development Criteria

Concept Development Plan proposals in a Planned Development shall be primarily evaluated against the criteria listed below. The Concept Development Plan shall fulfill the following criteria unless a portion of the criteria do

not apply or relate, in whole or part, to a specific proposal. The examples listed in this section are for illustrative purposes. Specific solutions used to fulfill these criteria are contingent upon the size, scale, site conditions, design, uses and impacts of a proposed development.

1. **Mixed Use Development and Pedestrian Orientation:** Planned Developments shall generally have a mixed use orientation (combination of differing types of residential, commercial, public or civic, and/or industrial uses) both within the development itself and relative to the relationship between the proposed planned development and adjacent sites. The additional intensity allowed in a Planned Development by Section 1500 shall only be permitted when a true mixed use and/or an amenity - oriented development with community and recreation facilities as described in this standard is provided.

In general, planned developments shall have a pedestrian orientation, where it is possible to live, work, shop, and play in the same immediate vicinity without a required dependence on the automobile. This may be accomplished through the use of comprehensive pedestrian circulation networks including multi-purpose paths and walks along main routes and open spaces such as stream corridors, between major destinations within the development and adjoining areas, secondary walk connections to the multi-purpose paths, the creation of a designed pedestrian environment including street trees in addition to other required landscaping, decorative street lights and other street furniture, and seating areas, and the use of integral curb walks where appropriate along streets. Disruptions in major paths due to street and drive intersections shall be minimal. Additionally in commercial areas, pedestrian orientation can be accomplished by placing buildings in close proximity to the street with parking areas to the side and rear of buildings, mixing uses within the same multi-story buildings, building entrances directly facing streets with reduced setbacks, architectural design which employs display windows, projecting signs, and awnings at street level, and designed outdoor seating and gathering spaces at the street level. Each development proposal must demonstrate in detail how the project will be made walkable throughout.

2. **Compatibility of Uses:** Measures shall be taken to assure compatibility of land uses within a planned development itself and adjacent sites. Such measures may include the provisions of buffer zones, common open space areas and landscape features, transitional land uses, or a mixed-use development in which no specific type of land use is dominate. When applicable, the design methods recommended in the "Development Layout, Lot Sizes, and Setbacks" section of the Comprehensive Plan's Land Use Element shall be employed. Compatibility measures/mitigation measures shall exceed the usual minimum standards of this order when needed to address impacts of the proposed development.
3. **Open Space:** Useable open space(s), in an amount over and above setback and buffer yard areas and open areas required by the underlying zone, shall be provided. These spaces may be provided in the form of parks, plazas, arcades, commons, trails, sports courts or other athletic and recreational areas, outdoor areas for the display of sculptural elements, etc. Land reservations for community facilities may be considered in lieu of useable open space. The use of single loaded streets to provide multi-purpose paths, park areas, or to protect stream corridors, may be proposed for this purpose. Open Space areas are encouraged to have street frontage and visibility. Any site proposed to be publically dedicated or donated for park or open space purposes shall comply with the appropriate legislative body's requirements for acceptance of such dedications or donations.
4. **Multi-Modal Transportation System:** Planned developments shall incorporate multi-modal transportation elements through the development, depending on the foreseeable needs of future residents and users of the site, and the relationship of the project site to the community at large. Such multi-modal elements may include provisions for mass transit stops or stations, car pooling lots, pedestrian and bicycle paths and lanes, bicycle parking areas, etc. Multi-modal facilities are encouraged to be combined with the pedestrian systems and open spaces described in Sections 1 and 3.
5. **Preservation of Existing Site Features:** Existing topography, significant tree cover, tree lines along property lines, cemeteries, and water courses and water bodies shall be largely preserved and incorporated into the project design, where appropriate and consistent with the remainder of this article. The retention of such features may also fulfill portions of the requirements in Section 3 "Open Space" and Section 6 "Landscaping."
6. **Landscaping:** Substantial landscaping shall be provided in a planned development with emphasis given

to street scape areas, buffer zones, and the provision of significant landscaping (in terms of size of landscape areas, and quantity and quality of landscape materials) within the developed portions of the site. The use of landscape design guidelines is required for multi-phased projects. The retention of existing healthy, substantial trees should occur wherever possible. Properly designed street tree plantings may be permitted to fulfill some landscaping requirements as part of an overall amenity package.

7. **Architecture:** Sites which are subject to architectural requirements through adopted overlay districts or land use studies shall follow said requirements. For all other sites, a consistent architectural theme shall be provided in planned developments. The theme shall largely use traditional, regionally influenced architectural forms and elements and shall allow variations within it. Traditional styles such as Georgian, Federal, I-House, Cape Cod, Craftsman, Tudor, Queen Anne, Italianate, early 20th century commercial structures, and local farm structures may be used as models. Contemporary or transitional styles of comparable quality may also be considered. The architectural theme shall also relate to existing structures on the project site and adjacent sites, especially if such existing structures are historic.

For attached or multi-family residential developments and commercial or office developments, the predominant building materials shall have a solid appearance, such as could be achieved with brick, stone, and architectural grade cast concrete products designed to replicate natural materials. Roof designs shall have a finished appearance through the use of three dimensional pitched roof forms with architectural grade roofing and/or the use of defined parapets with cornice lines. Long building facades and roof planes shall be interrupted through the use of three dimensional jogs in the building footprint and secondary roof forms such as hips, dormers, and gables. Such buildings shall include architectural detailing for cosmetic enhancement, largely use natural colors, and use a consistent design treatment on all facades. The use of architectural guidelines or building prototypes is required for all multi-phased projects.

Developments should be mixed-use in character with multi-level buildings where commercial services are proposed. Walkability must be planned for when locating commercial and residential uses in proximity to each other. Office and residential uses are strongly advocated above commercial uses in business districts to decrease dependence on the automobile.

8. **Historic and Prehistoric Features:** Historic and prehistoric features on the project site shall be retained, utilized, and incorporated into the overall project design if physically and economically feasible.
9. **Signage:** A consistent signage theme shall be provided within a planned development. Building mounted signs shall be the predominate signage on the project site. Freestanding signs shall be monument style and of a limited size and height. Signage shall visually correlate to the planned architectural theme by the use of consistent design details, materials, and colors. The use of signage design guidelines is required for multi-phased projects.
10. **Transportation Connections and Entry Points:** The provision of transportation connections (street connections, pedestrian paths, multi-purpose trails, sidewalks, and bicycle facilities) shall be provided in all planned development unless physically unfeasible or unsafe. This shall include connections to adjoining properties and developments, and inter-connectivity within the development itself, and contain minimal use of cul-de-sacs or other dead-end types of streets only when necessary. Transportation connections shall account for the County's adopted Transportation Plan and any adopted bikeway and/or pedestrian plans. In addition, the various entry points (streets, paths, etc.) into a planned development shall be marked or otherwise defined through the use of landscaping, low-key signage on retaining walls, architectural or sculptural elements, archways, markers, etc. Any structures used to demarcate entry points shall visually correlate to the planned architectural theme by the use of consistent design details, materials, and colors.
11. **Conformance with Comprehensive Plan:** All planned developments shall conform to the provisions of the adopted Comprehensive Plan and take into account the limitations of existing or planned infrastructure.

Further, Concept Development Plan proposals within areas that are subject to a specific land use or corridor study shall be evaluated against the criteria or requirements of such study as well as the criteria in this section.

A Planned Development and its uses, buildings or structures shall be minimally subject to the supplemental performance and development standards of this order, unless superseded by any special requirements, conditions, variances or other particulars imposed by the Planning Commission during the concept or preliminary

application and hearing phases described in this article. Such special conditions may include provisions governing, common open space, lands or facilities, disposition of open land, infrastructure provisions including any physical design and/or any other requirement found to be necessary, appropriate or desirable for the purposes of this district.

Such conditions shall be made a part of the terms under which the development is approved. Any violation of such conditions shall be deemed a violation of this order.

SECTION 1515

Procedure

After review and recommendation by the Planning Commission and upon approval by the legislative body or Fiscal Court, a planned development zoning district or classification may be applied to any other existing district in this order. The zoning of property with a Planned Development overlay district and an underlying zone can occur without approval of a Concept Development Plan, however, a Concept Development Plan must be approved before a Planned Development overlay district can be utilized. Upon approval of a Concept Development Plan, the Official Zoning Map shall be annotated for the land area involved so that the district name includes the notation, "CD", which gives notice that a development for that property has been approved pursuant to this article. Planned Development districts shall be approved by the legislative body or Fiscal Court. Planned Development districts may originate from the Planning Commission, appropriate legislative unit or a private property owner. Applications for a Concept Development Plan review and approval shall originate from the property owner.

For development plans on sites within the Houston-Donaldson Study Area, the recommended Implementation procedure in that study shall be followed. This may result in a development not following the normal Planned Development process if those development plans are consistent with the recommendations of the Houston-Donaldson Study. This shall be determined through a Long Range/Comprehensive Planning Committee (or equivalent committee) recommendation to the full Planning Commission at the next regular Business Meeting. Decision as to the review process to be followed shall be by simple majority vote.

SECTION 1516

Pre-Application Meeting

If a property owner proposes a Planned Development district, he or she shall meet with an appropriate staff person of the Planning Commission prior to the submission of a Concept Development Plan. The purpose of the meeting will be to discuss early and informally the purpose and effect of this order and the criteria and standards which may apply, and to familiarize the developer with the detailed objectives of the Comprehensive Plan and its elements, this order and this article.

SECTION 1517

Concept Development Plan and Utilization of an Underlying Zone in a Planned Development

The Planning Commission shall require that a Concept Development Plan be submitted for the development or redevelopment of property located in a Planned Development zone. The Concept Development Plan may be filed to the Planning Commission by the owner(s), owner by contract (option) or lessee of property for which the planned development is proposed. If the Concept Development Plan involves a zone change, then the proposal is subject to the criteria in Section 308 as well as the provisions of this article. If the Concept Development Plan does not involve a zone change, or only involves uses that are permitted in the underlying zone (also called a "Utilization of an Underlying Zone Within a Planned Development"), then the proposal is subject only to the provisions of this article. The contents of the Concept Development Plan submittal shall include the items listed in Section 303.

SECTION 1518

Public Hearing by Planning Commission

Upon receipt of an application for approval of a Concept Development Plan or a Zoning Map Amendment involving the Planned Development Overlay District, the Planning Commission shall hold at least one public

Todd Morgan

From: Todd Morgan
Sent: Wednesday, December 13, 2017 4:31 PM
To: 'Linzy.Brefeld@ky.gov'; kfeldhaus@fuse.net; 'Greg Haggard'; 'm.morgan@unionky911.org'; David Plummer (davidp@cityofunionky.org)
Subject: Anchor Retail Solutions LLC/Arlinghaus I LLC - Zoning Map Amendment Request
Attachments: ANCHOR RETAIL-EX CONDITIONS.pdf; ANCHOR REATIL- PROPOSED LAYOUT.pdf; ANCHOR REATIL- GRADING.pdf; ANCHOR RETAIL-UTILITY.pdf; ANCHOR RETAIL-ELEVATIONS.pdf

I wanted to make you aware of the following Zoning Map Amendment application:

Request of **Anchor Retail Solutions LLC (applicant)** for **Arlinghaus I LLC (owner)** for a Zoning Map Amendment from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Commercial Two/Planned Development (C-2/PD) for an approximate 1.71 acre site located on the east side of US 42, on the southeast corner of the intersection with Fowlers Lane and to the west of the property at 9075 US 42 (Kroger fuel center), Union, Kentucky. The request is for a zone change to allow a multi-tenant commercial building.

I have attached copies of the Concept Development Plans that were submitted with the request. The public hearing has been scheduled for January 3, 2018, at 7:30 P.M.

Please review the request relative to your agency's policies and service abilities and provide any comments in writing to me by December 27, 2017. Please provide a written response, even if your agency has no comments on the proposal. Comments can be e-mailed to tmorgan@boonecountyky.org or faxed to (859) 334-2264. Your feedback is greatly appreciated.

Sincerely,

Todd K. Morgan, AICP
Senior Planner, Zoning Services
Boone County Planning Commission
(859) 334-2196

Todd Morgan

From: Morgan, Michael <m.morgan@unionky911.org>
Sent: Monday, December 18, 2017 10:21 AM
To: Todd Morgan
Subject: RE: Anchor Retail Solutions LLC/Arlinghaus I LLC - Zoning Map Amendment Request

Todd,

Everything looks okay for us.

Michael

Michael Morgan, Fire Chief
Union Fire Protection District
9611 U.S. Hwy. 42
Union, KY 41091
859.384.3342 Ext. 102 Office
859.620.0675 Cell

From: Todd Morgan [<mailto:TMorgan@boonecountky.org>]
Sent: Wednesday, December 13, 2017 4:31 PM
To: 'Linzy.Brefeld@ky.gov' <Linzy.Brefeld@ky.gov>; kfeldhaus@fuse.net; 'Greg Haggard' <GHAGGARD@sd1.org>; Morgan, Michael <m.morgan@unionky911.org>; David Plummer (davidp@cityofunionky.org) <davidp@cityofunionky.org>
Subject: Anchor Retail Solutions LLC/Arlinghaus I LLC - Zoning Map Amendment Request

I wanted to make you aware of the following Zoning Map Amendment application:

Request of **Anchor Retail Solutions LLC (applicant)** for **Arlinghaus I LLC (owner)** for a Zoning Map Amendment from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Commercial Two/Planned Development (C-2/PD) for an approximate 1.71 acre site located on the east side of US 42, on the southeast corner of the intersection with Fowlers Lane and to the west of the property at 9075 US 42 (Kroger fuel center), Union, Kentucky. The request is for a zone change to allow a multi-tenant commercial building.

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Sincerely,

Todd K. Morgan, AICP
Senior Planner, Zoning Services
Boone County Planning Commission

Todd Morgan

From: Haggard, Greg <ghaggard@sd1.org>
Sent: Tuesday, December 19, 2017 4:36 PM
To: Todd Morgan
Cc: Greg Stacy
Subject: Re: Anchor Retail Solutions LLC/Arlinghaus I LLC - Zoning Map Amendment Request

Todd,

Sanitary sewer is available to serve this site as depicted on the plan. Sanitary sewer capacity has been reserved. Please let me know if you have any questions.

Best regards,

On Wed, Dec 13, 2017 at 4:30 PM, Todd Morgan <TMorgan@boonecountyky.org> wrote:

I wanted to make you aware of the following Zoning Map Amendment application:

Request of **Anchor Retail Solutions LLC (applicant)** for **Arlinghaus I LLC (owner)** for a Zoning Map Amendment from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Commercial Two/Planned Development (C-2/PD) for an approximate 1.71 acre site located on the east side of US 42, on the southeast corner of the intersection with Fowlers Lane and to the west of the property at 9075 US 42 (Kroger fuel center), Union, Kentucky. The request is for a zone change to allow a multi-tenant commercial building.

I have attached copies of the Concept Development Plans that were submitted with the request. The public hearing has been scheduled for January 3, 2018, at 7:30 P.M.

Please review the request relative to your agency's policies and service abilities and provide any comments in writing to me by December 27, 2017. Please provide a written response, even if your agency has no comments on the proposal. Comments can be e-mailed to tmorgan@boonecountyky.org or faxed to (859) 334-2264. Your feedback is greatly appreciated.

Sincerely,

Todd K. Morgan, AICP

APPLICATION FORM

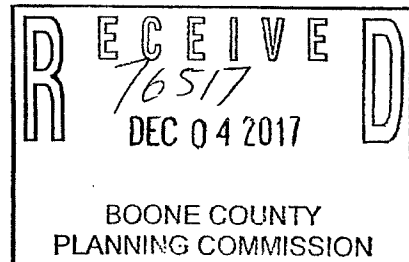
ZONING MAP AMENDMENT
BOONE COUNTY PLANNING COMMISSION

FIVE (5) COPIES OF SUBMITTED DRAWINGS ARE REQUIRED
(See Boone County Zoning Regulations)

SECTION A (To be completed by applicant)

- 1. Name of Project Union, Kentucky, Multi-Tenant Building
2. Location of Project East side of US 42, south of Fowlers Lane, Union, KY 41091
3. Total Acreage of Site 1.71 +/- acres
4. Current Zoning of Site "RSE/UNO"
5. Proposed Zoning (Classification being requested) "C-2/PD/CD"
6. Proposed Uses (please specify each use) Restaurant and Retail/Service
7. Names of Applicant(s) Anchor Retail Solutions LLC
8. Address of Applicant(s) 3825 Edwards Road, Suite 630
9. Name of Property Owner(s) Arlinghaus I LLC
10. Address of Property Owner(s) 142 Barnwood Drive
11. Proposed Building Intensities (please specify)
12. Are there any existing buildings on the site? No
13. Deed Book 1099 Page No. 614 Group No. 2047
14. Are you also applying for:
15. Have you submitted a Concept Development Plan? Yes
16. Have you had a pre-application meeting with BCPC Staff? Yes 9/26/17
17. Please check the following organizations/agencies which you have discussed the proposed development within the last several months:

- Boone County Water District
Florence Public Services Dept.
Duke Energy
Sanitation District #1
Cincinnati Bell
Owen Electric Cooperative, Inc.
Boone County Public Works Department
Kentucky Transportation Cabinet
Cincinnati/N. KY International Airport (KY Airport Zoning Commission for height restrictions near the airport)



EXHIBIT

“B”

ZONE CHANGE/CONCEPT PLAN COMMITTEE REPORT

TO: Boone County Planning Commission

FROM: Kim Bunger, Chairman

DATE: February 7, 2018

RE: Request of **Anchor Retail Solutions LLC (applicant)** for **Arlinghaus I LLC (owner)** for a Zoning Map Amendment from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Commercial Two/Planned Development (C-2/PD) for an approximate 1.71 acre site located on the east side of U.S. 42, on the southeast corner of the intersection with Fowlers Lane and to the west of the property at 9075 U.S. 42 (Kroger fuel center), Union, Kentucky. The request is for a zone change to allow a multi-tenant commercial building.

REMARKS:

We, the Committee, recommend approval of the above referenced request based on the following findings of fact and with the following conditions:

FINDINGS OF FACT

1. The Committee concluded the existing zoning classification of Rural Suburban Estates/Union Town Overlay (RSE/UTO) is inappropriate and the proposed zoning classification of Commercial Two/Planned Development (C-2/PD) is appropriate for the following reasons:
 - A. In 2014, the City of Union approved a Zoning Map Amendment from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Commercial Two/Planned Development (C-2/PD) for the adjoining 35.25 acre area to the north and east. The approved Concept Development Plan shows the following improvements:
 - A 22.53 acre site with a 136,000 square foot Kroger Marketplace, 8,000 square foot liquor store, fuel center, and 730 parking stalls. Access to site is shown from a private driveway network that connects to U.S. 42 in two locations. The first location is directly opposite Old Union Road and the second is directly opposite Fowlers Lane.
 - A 7.99 acre outlot in the northeastern portion of the site.
 - A 4.73 acre outlot area along U.S. 42 between the proposed access points.
 - B. On May 19, 2016, Boone County Planning Commission approved a Site Plan application for Lot 1 of Union Marketplace Subdivision (adjoining property to north and east). The plan shows a 135,976 square foot Kroger Marketplace, 8,000 square foot liquor store, fuel center, and 730 parking stalls. This development has since been constructed and is open for business.
 - C. In 2017, the City of Union approved a Zoning Map Amendment from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Public Facilities/Planned Development (PF/PD) for the adjoining 14.104 acre area to the south. The approved Concept Development Plan shows a 187,017 square foot elderly housing facility (StoryPoint) with 175 parking spaces. The facility will contain 120 independent living apartments and 42 enhanced living apartments once it's constructed.

- D. Access to the site is proposed from the Kroger/StoryPoint private driveway system.
 - E. The proximity of the subject site to U.S. 42.
2. The request is in conformance with the Planned Development (PD) Standards found in Section 1514 of the Boone County Zoning Regulations. The submitted plans show the development will have a pedestrian orientation, proper architecture, significant landscaping, and low level monument signage. The development will also have a mixed use orientation when considering the Planned Developments on the adjoining sites.
 3. The Committee has concluded that the attached conditions are necessary to achieve consistency with the specific goals, objectives, and policies of the 2010 Boone County Comprehensive Plan. The Committee has also concluded that the attached conditions are necessary to mitigate any foreseeable community impacts that may be created by the development. The property owner has signed a letter demonstrating agreement with the conditions.

CONDITIONS

1. The approval is based on the following plans and materials:
 - A. The submitted site layout and landscaping plans (received 12/4/17).
 - B. The revised building elevation drawings (received 1/17/18).
 - C. The building material sample board (received 1/17/18).
 - D. The monument sign drawing (received 1/17/18).
 - E. The drive through signage and canopy elevations (received 1/17/18)

Minor changes to the plans or building materials can be approved by the Zoning Administrator.
2. All roof mounted mechanical equipment shall be screened from public right-of-ways and adjoining properties by parapet walls.
3. The sides and rear of the dumpster enclosure shall be constructed with brick that matches the building on site. Solid gates shall be used and be constructed with wood, vinyl, or metal materials.
4. All lighting shall comply with Section 3316 and Figure 33.6 of the Boone County Zoning Regulations. Fixtures mounted on light poles shall not exceed 30' in height and shall be oriented downwards and inwards toward the subject site.
5. The following Planned Development Waivers are being approved with the request:
 - A. Two drive through signs (preview board and menu board) shall be permitted next to the drive through lane (see the site layout plan and the drive through signage drawings for the approved locations and specifications).
 - B. A formal buffer yard agreement shall not be required between Kroger and the future property owner of the subject site. The subject site shall be landscaped per the landscaping plan that was received on 12/4/17.

A copy of the Public Hearing minutes accompanies the findings and recommendation serving as a summary of the evidence and testimony presented by the proponents and opponents of this request. Attached is the signature page for the Zone Change/Concept Development Plan Committee Vote.

ZONE CHANGE/CONCEPT PLAN COMMITTEE VOTE

TO: Boone County Planning Commission

FROM: Kim Bunger, Chairman

DATE: January 17, 2018

ZONING MAP AMENDMENT - Kim Bunger, Chairman, Todd K. Morgan, Staff

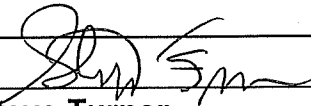
1. Request of **Anchor Retail Solutions, LLC (applicant)** for **Arlinghaus I, LLC (owner)** for a Zoning Map Amendment from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Commercial Two/Planned Development (C-2/PD) for an approximate 1.71 acre site located on the east side of US 42, on the southeast corner of the intersection with Fowlers Lane and to the west of the property at 9075 US 42 (Kroger fuel center), Union, Kentucky. The request is for a zone change to allow a multi-tenant commercial building.

REMARKS:

We, the Committee Members were present at the Committee Meeting and voted on the above request or else were absent from voting. Further, based upon the vote, the Committee directs the Staff to draft the findings of fact and conditions if deemed necessary in order to complete the Committee Report.

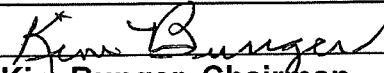
Greg Breetz

For Project ___ Absent ___
Against Project ___
Abstain ___ Deferred ___



Steve Turner

For Project Absent ___
Against Project ___
Abstain ___ Deferred ___

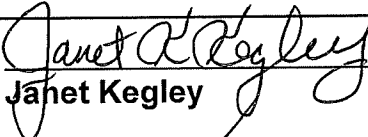


Kim Bunger, Chairman

For Project Absent ___
Against Project ___
Abstain ___ Deferred ___

Randy Bessler (Alternate)

For Project ___ Absent ___
Against Project ___



Janet Kegley

For Project ___ Absent ___
Against Project ___
Abstain ___ Deferred ___

Lisa Heilman, (Alternate)

For Project ___ Absent ___
Against Project ___
Abstain ___ Deferred ___

Kim Patton

For Project ___ Absent ___
Against Project ___
Abstain ___ Deferred ___

Mark Hicks (Alternate)

For Project ___ Absent ___
Against Project ___
Abstain ___ Deferred ___

Richard Lunnemann (Alternate)

For Project ___ Absent ___
Against Project ___
Abstain ___ Deferred ___

TOTAL: ___ DEFERRED 3 FOR PROJECT ___ ABSENT
___ AGAINST PROJECT ___ ABSTAIN

**BOONE COUNTY PLANNING COMMISSION
BOONE COUNTY ADMINISTRATION BUILDING
BOONE COUNTY FISCAL COURTROOM
PUBLIC HEARINGS
JANUARY 3, 2018
7:30 P.M.**

COMMISSION MEMBERS PRESENT:

Mr. Randy Bessler
Mr. Greg Breetz
Mrs. Lori Heilman
Mr. Mark Hicks
Mrs. Janet Kegley
Mr. Rick Lunnemann
Mr. Don McMillian
Ms. Jamie Nieves
Mr. Kim Patton
Mr. Charlie Rolfsen, Chairman
Mr. Bob Schwenke
Mr. Brad Shipe
Mr. Steve Turner, Temporary Presiding Officer
Mr. Kenny Vaught

COMMISSION MEMBERS NOT PRESENT:

Mr. Kim Bunger, Secretary/Treasurer

LEGAL COUNSEL PRESENT:

Mr. Dale T. Wilson

STAFF MEMBERS PRESENT:

Mr. Kevin P. Costello, AICP, Executive Director
Mr. Todd K. Morgan, AICP, Senior Planner
Mr. Robert A. Jonas, AICP, Director, Planning Services

Chairman Rolfsen called the Public Hearing to order at 7:35 P.M. and introduced the first item on the Agenda:

ZONING MAP AMENDMENT - Todd K. Morgan, Staff

1. Request of **Anchor Retail Solutions, LLC (applicant)** for **Arlinghaus I, LLC (owner)** for a Zoning Map Amendment from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Commercial Two/Planned Development (C-2/PD) for an approximate 1.71 acre site located on the east side of US 42, on the southeast corner of the intersection with Fowlers Lane and to the west of the property at 9075 US 42 (Kroger fuel center), Union, Kentucky. The request is for a zone change to allow a multi-tenant commercial building.

Staff Member, Todd Morgan, presented the Staff Report, which included a PowerPoint presentation (see Staff Report). The 1.71 acre site is located near the Kroger Fuel Center and the proposed StoryPoint project. The request involves a Zone Change from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Commercial Two Planned Development (C-2/PD) in order to allow 7,522 square foot retail center with 2 tenants. The proposed center will include a Panera restaurant with a drive-through. There will also be another tenant in the center. The Panera tenant space is 5,019 square feet and the speculative tenant space is 2,503 square feet. There will be 83 parking spaces on site. Access to the site is from the Kroger/StoryPoint private driveway. Two curb cuts will serve the site. White board fencing will be installed along US 42 in front of the retail center. Mr. Morgan showed building elevations of the proposed retail center. The building will be brick, stone, EIFS and ceramic tile. He presented the four elevations (sides) of the proposed building and a landscaping plan. The applicant is proposing a shared buffer with Kroger along the eastern and northern property line. The number of plants meet the code requirements. Mr. Morgan also showed a drawing of the proposed free-standing sign with two tenant panels. He also reviewed the site history of the Kroger and StoryPoint sites as outlined on pages 1-2 in the Staff Report. References to the Boone County Comprehensive Plan are made on pages 2-7 of the Staff Report. He also mentioned the site is part of the Union Town Plan area. The Union Town Plan also recommended a green area, a 100 foot building setback and town home development. The Comprehensive Plan and the Union Town Plan have not been updated since the Kroger Zone Change and the StoryPoint Zone Change applications were approved. Mr. Morgan showed photos of the site and adjoining properties.

In terms of Staff Comments, Mr. Morgan referred to page 8 of the Staff Report that outlines the findings necessary for approving or denying a Zoning Map Amendment and the Planned Development (PD) standards.

Staff would like the applicant to address the following:

- A. An updated Traffic Impact Study was submitted with the application (see attachments). Staff would like the Study to address the recent road improvements that were made on U.S. 42 and if they are adequate to address the trips that will be generated by the overall development (Kroger Marketplace, Kroger Fuel, Kroger Outlots, StoryPoint, and proposed outlot).
- B. Is the proposed development following the Union Kroger Marketplace outlot design requirements (see attachments)? If not, what are the proposed changes?

- C. Can the building elevations be updated and provide masonry, tile, and EIFS percentages on all four facades?
- D. Can building material samples be provided?
- E. Will the parapet walls be tall enough to fully screen the roof mounted mechanical equipment?
- F. The plan shows that drive through signage is being proposed. Does it comply with code?
- G. The plans shows that shared buffer yards are being proposed with Kroger? Will a shared buffer yard agreement be recorded?
- H. The plans show the storm water from the site will go to one of Kroger's detention basins. Was the basin sized to accommodate the storm water from the subject site?

Finally, Staff sent out an agency memo and received comments back from Union Fire Department and Sanitation District No. 1. These comments are attached to the Staff Report.

At this time, Chairman Rolfsen asked if the applicant was present and wanted to proceed with their presentation? Mr. Greg Dale, McBride Dale & Clarion, noted that he was representing the applicant. He stated that the existing zoning of residential is not appropriate given the changes in the area since the adoption of the current Comprehensive Plan. With regard to the Staff Comments, he offered to address them now or at the Committee Meeting. He expressed no concerns or objections. He will provide the information at the Committee Meeting.

At this time Chairman Rolfsen asked if there was anyone in the audience who wanted to speak in favor or against the request? Mr. Carl Dieso, 9154 Armistead Court, stated that he was a new resident in Boone County and Union. He expressed a concern about traffic. In his opinion, retail strip style development is getting closer to the residential portion of the community. As you enter Union, you will see an entire strip center with 7 outlots and the proposed project. He urged the Board to think about what it will look like and how it will interact with the community and residents. He noted that he wasn't aware of the development of the adjoining vacant lot.

In response to Mr. Dieso's comments, Mr. Morgan replied that in 2014, the City of Union overturned the Planning Commission's recommendation to deny the Kroger request. Further, the Planning Commission and the City of Union approved the StoryPoint senior housing project in 2017 adjacent to the subject site with conditions.

Chairman Rolfsen asked if the other tenant for the proposed retail center was known? Mr. Morgan replied no. It could be a commercial or office use. Chairman Rolfsen stated that traffic was a major concern for the Kroger site along with the Union Town Plan and that is why the Planning Commission unanimously denied the zone change several years ago.

Ms. Noreen Morgan, resident of Fowlers Creek subdivision, inquired about the color of the building. Is it really going to be green and yellow? Mr. Morgan explained that the Planning Commission can impose building color conditions on the zone change request in order to meet the PD standards and be compatible with the adjoining buildings.

Chairman Rolfsen asked how the proposed Panera restaurant relates in size to the Panera store on Houston Road? Mr. Brian Copp from Anchor Retail Solutions stated it will be smaller. The building is a prototype that was just built in West Chester Township. It is not bright yellow. That building has been completed. Chairman Rolfsen asked if the applicant could take some photographs of the West Chester Township restaurant? Mr. Copp said they intend to comply with the requirements from the other outparcels.

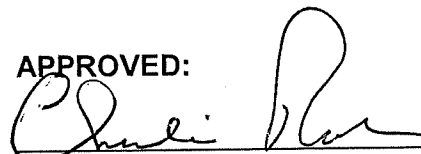
Chairman Rolfsen asked if the proposed restaurant had a double drive-through? Mr. Copp replied only one. Chairman Rolfsen stated that the Houston Road restaurant is a nightmare in terms of parking. People park at Watson's. He mentioned that it is critical that Panera get an agreement with Kroger. Mr. Morgan stated that the proposed parking meets the zoning requirements.

Mr. Patton asked the applicant if the handicapped sidewalk is going across 4 lanes of traffic? Maybe it could be moved to the side and the traffic island be moved down?

Mr. Hicks asked what developable property is left? Mr. Morgan responded that the vacant lots to the east will be the completion of Arbor Springs Subdivision. Vicksburg Drive would be extended into a cul-de-sac with residential lots. Mr. Bob Schroder agreed. Mr. Rick Lunnemann asked how to define masonry? Mr. Copp replied that it would be brick, stone and ceramic tile.

Seeing no further questions or comments, Chairman Rolfsen announced that the Committee Meeting for this item will be on January 17, 2018 at 5:00 P.M. in the Fiscal Courtroom. This item will be on the Agenda for the Business Meeting on February 7, 2018 at 7:00 P.M. Chairman Rolfsen closed the Public Hearing at 7:59 P.M.

APPROVED:



Charlie Rolfsen
Chairman

Attest:



Kevin P. Costello, AICP
Executive Director

SUPPORTING INFORMATION

Description: Proposed 1.7139 Acres Parcel to be rezoned from RSE/UTO to C-2/PD

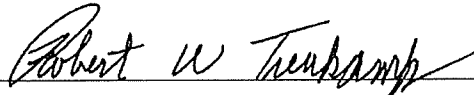
Situated in City of Union, Boone County, Kentucky and being more particularly described as follows:

Beginning at the southwest corner of Lot #1, Union Marketplace Subdivision as recorded in P.C. 6, Page 104, said point also being in the easterly Right-of-Way line of U.S. 42; thence departing the said east Right-of-Way line of U.S. 42, North $61^{\circ} 46' 48''$ East, 278.30 feet; thence South $28^{\circ} 13' 12''$ East, 265.27 feet; thence along a new division line, South $61^{\circ} 46' 48''$ West, 284.59 feet to the said east Right-of-Way line of U.S. 42; thence along the said east Right-of-Way line of U.S. 42, North $26^{\circ} 51' 41''$ West, 265.35 feet to the point of beginning.

The above described parcel contains 1.7139 Acres of land and is subject to all easements and restrictions of record.

Bearings based on the Record Plat for Section Sixteen, Arbor Springs at Plantation Pointe dated February, 2012.

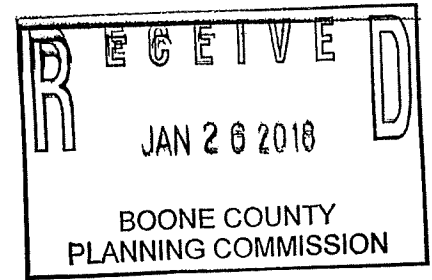
This description was prepared by Robert W. Trenkamp, Registered Land Surveyor in Kentucky #2524 and is based on a survey made by Thomas Graham Associates, Inc. dated October 6, 2017.



Robert W. Trenkamp, Registered Land Surveyor #2524 in Kentucky

MEMORANDUM

DATE: January 25, 2018
SUBJECT: Traffic Analysis for Outlot in Union KY
PREPARED BY: Michael Hafner, P.E., PTOE (TEC Engineering, Inc.)
PREPARED FOR: Mr. Brad Copp



This analysis has been performed to determine the traffic impact of the proposed outlot development adjacent to the Kroger site on US 42 in Union, Kentucky. The site has a proposed restaurant and retail use that are 5,019 ft² and 2,500 ft², respectively.

I. Existing Conditions

Traffic data was obtained from previous studies completed for the Kroger development (TEC Engineering) and for the StoryPoint senior living facility adjacent to the Kroger development (the Kleingers Group). This study uses the 2025 Build traffic from the StoryPoint analysis and adds the proposed outlot trips to determine the impact of the proposed development. The intersection of US 42 and Fowlers Lane is analyzed as a signalized intersection within a coordinated signal system. The speed limit of US 42 is 55 mph within the vicinity of the proposed development.

II. Trip Generation

The trips generated by the proposed outlot site are illustrated below. The *Institute of Transportation Engineers (ITE) Trip Generation Manual* was used to generate trips for the site. This manual is the most widely accepted publication for projecting traffic volumes specifically related to how a site is used. **Table 1** illustrates the generated trips.

Table 1: Generated Trips – Kroger Outlot

Traffic Generated by Development									
Site	Land Use Type (site)	ITE Code	Unit of Measure	Peak Hour of Adjacent Street Traffic					
				AM			PM		
				Total	Enter	Exit	Total	Enter	Exit
1	Restaurant (sit-down)	932	5,019 ft ²	54	30	24	49	30	20
	Specialty Retail Center	826	2,500 ft ²	0	0	0	27	12	15
Total Trips				54	30	24	77	42	35

The ITE Trip Generation Manual does not include AM peak hour (7-9am) trip rates for the Specialty Retail Center. These uses are typically not open during the AM peak period and therefore no trips are assumed to be generated by this use in the AM peak hour. Retail and restaurant-oriented developed are often located adjacent to busy streets, in this case US-42. These sites attract a portion of their trips from traffic passing the site on the way from an origin to an ultimate destination. These trips do not add new traffic to the adjacent roadway network but constitute a “pass-by” trip. The pass-by trips for the

site were determined from values provided in the *ITE Trip Generation Handbook*. Final Pass-by trips values and new primary trip values are provided in the following table.

Table 2: Pass-by Reductions

Site	Land Use	Pass by trips						Primary (Non Pass by) Trips			
		AM			PM			AM		PM	
		Pass-by Rate	Enter	Exit	Pass-by Rate	Enter	Exit	Enter	Exit	Enter	Exit
1	Restaurant (sit-down)	0%	0	0	43%	13	9	30	24	17	11
	Specialty Retail Center	0%	0	0	0%	0	0	0	0	12	15
	Total Trips		0	0		13	9	30	24	29	27

The proposed outlot site will generate 54 trips in the AM (54 new trips, 0 pass-by trips) and 77 trips in the PM (56 new trips, 21 pass-by trips). The site generated trips were distributed onto US 42 with 70% of the trips traveling to/from the north and 30% of the trips traveling to/from the south. This 70/30 split matches the percentages used in the original Kroger study, as well as the StoryPoint analysis.

The traffic volumes generated by the outlot development were added to the 2025 Build with StoryPoint development to evaluate any impact the outlot development may have. The 2025 AM and PM build volume figures are presented as an attachment.

III. Capacity Analysis

The Build traffic volumes were analyzed using Synchro 9 software at the intersection of US 42 & Fowlers Lane. Information in the original Kroger development Traffic Impact Study indicated that US 42 & Fowlers Lane will be part of a coordinated signal system with a cycle length of 120 seconds in the AM peak hour and 140 seconds in the PM peak hour. These parameters were used to evaluate the build traffic with the proposed outlot development. Additionally, TEC completed Synchro analysis for the 2025 Build condition of the StoryPoint development with the same parameters for comparison purposes. The results of the analysis are provided in **Table 3** below. Capacity analysis results are presented as an attachment.

Table 3: Capacity Analysis Comparison

US 42 & Fowlers Lane		Eastbound			Westbound			Northbound			Southbound			Overall
		L	TH	R	L	TH	R	L	TH	R	L	TH	R	
2025 Build Kroger + StoryPoint	AM	D (50.7s)	E (55.4s)	E (61.6s)	A (3.2s)	A (3.0s)	A (2.8s)	A (4.2s)	A (2.9s)				A (9.4s)	
	PM	E (56.8s)	E (64.5s)	E (60.4s)	A (5.2s)	A (3.7s)	A (3.4s)	A (5.3s)	A (4.5s)	A (4.4s)			A (9.8s)	
2025 Build Kroger + StoryPoint + Outlot	AM	D (49.3s)	D (54.1s)	E (60.7s)	A (3.7s)	A (3.3s)	A (3.2s)	A (5.1s)	A (3.3s)				B (10.3s)	
	PM	D (54.8s)	E (63.6s)	E (59.5s)	A (6.0s)	A (4.3s)	A (4.0s)	A (6.7s)	A (5.1s)	A (5.1s)			B (11.2)	

The results of the capacity analysis indicate the outlot is expected to have very little impact on the operation of the intersection.

IV. Storage Requirement Calculations

Turn lane storage calculations were performed for the 2025 Build scenario traffic volumes according to the KYTC Highway Design Manual using the *KYTC Turn Lane Warrant Calculation Spreadsheet*. Results of the storage lane length calculations were compared to the lengths calculated in the original Kroger development TIS and to the StoryPoint development TIS. A comparison of the calculated turn lane lengths for the intersection of US 42 & Fowlers Lane is provided in Table 4 below. Storage lane length calculations are presented as an attachment.

Table 4: Storage Lane Length Comparison

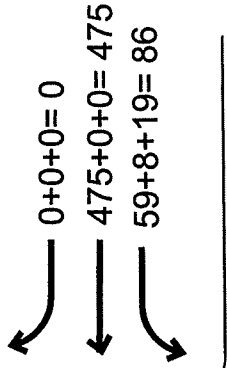
Intersection	Movement	Calculated Length Minimum/Desirable		
		Kroger Only	Kroger + StoryPoint	Kroger + StoryPoint + Outlot
US 42 & Fowlers Ln	NBR	445'/520'	445'/520'	470'/545'
	SBL	295'/345'	320'/345'	345'/395'

According to the storage lane calculations, the storage requirements are very similar to those calculated in the previous studies. The northbound right calculation is within 25' (approximately 1 car) of the calculations in the original Kroger study. The southbound left is within 50' (approximately 2 cars) of the calculations in the original Kroger study. A two-way left turn lane is provided along US 42 north of this intersection so if the southbound left turn lane were ever to reach capacity, there is additional storage available within the two-way left turn lane.

V. Conclusions

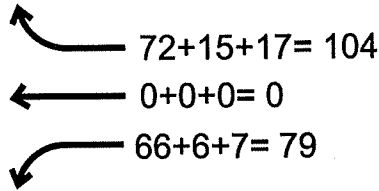
The analysis presented above indicates the proposed outlot is expected to have very little impact on the operation of the intersection of US 42 & Fowlers Lane. As a result, no roadway improvements are recommended as a part of this development.

AM

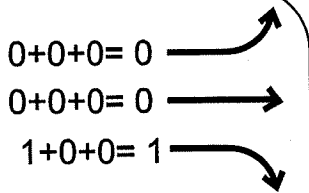


FOWLERS LANE

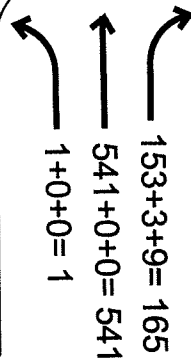
US-42



PROPOSED KROGER DRIVE



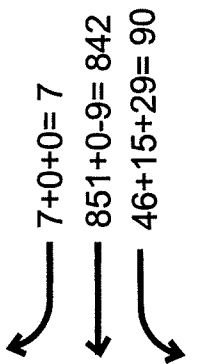
FOWLERS LANE



- LEGEND:**
- A: 2025 KROGER BUILD TRIPS
 - B: 2025 SENIOR HOUSING FACILITY GENERATED TRIPS
 - C: 2025 OUTLOT GENERATED TRIPS
 - D: 2025 BUILD VOLUMES

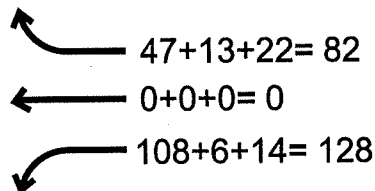
A+B+C=D

PM

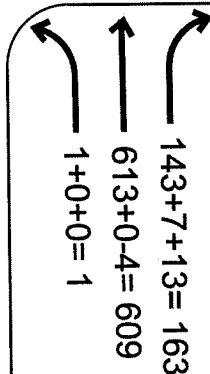
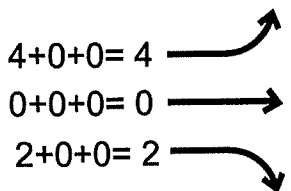


FOWLERS LANE

US-42















PROPOSED KROGER DRIVE



HCM 2010 Signalized Intersection Summary
6: US 42 & Fowlers Ln

01/25/2018

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔		↖	↗		↖	↑↑	↗	↖	↗↔	
Traffic Volume (veh/h)	0	0	1	72	0	87	1	541	156	67	475	0
Future Volume (veh/h)	0	0	1	72	0	87	1	541	156	67	475	0
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1900	1863	1900	1863	1863	1900	1863	1863	1863	1863	1863	1900
Adj Flow Rate, veh/h	0	0	1	78	0	95	1	588	170	73	516	0
Adj No. of Lanes	0	1	0	1	1	0	1	2	1	1	2	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	0	0	129	174	0	129	737	2837	1269	597	2837	0
Arrive On Green	0.00	0.00	0.08	0.08	0.00	0.08	0.80	0.80	0.80	0.80	0.80	0.00
Sat Flow, veh/h	0	0	1583	1410	0	1583	881	3539	1583	704	3632	0
Grp Volume(v), veh/h	0	0	1	78	0	95	1	588	170	73	516	0
Grp Sat Flow(s), veh/h/ln	0	0	1583	1410	0	1583	881	1770	1583	704	1770	0
Q Serve(g_s), s	0.0	0.0	0.1	6.5	0.0	7.0	0.0	4.7	2.9	3.3	4.1	0.0
Cycle Q Clear(g_c), s	0.0	0.0	0.1	6.5	0.0	7.0	4.1	4.7	2.9	8.0	4.1	0.0
Prop In Lane	0.00		1.00	1.00		1.00	1.00		1.00	1.00		0.00
Lane Grp Cap(c), veh/h	0	0	129	174	0	129	737	2837	1269	597	2837	0
V/C Ratio(X)	0.00	0.00	0.01	0.45	0.00	0.73	0.00	0.21	0.13	0.12	0.18	0.00
Avail Cap(c_a), veh/h	0	0	548	547	0	548	737	2837	1269	597	2837	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	0.00	1.00	0.88	0.88	0.88	1.00	1.00	0.00
Uniform Delay (d), s/veh	0.0	0.0	50.6	53.6	0.0	53.8	3.2	2.8	2.6	3.8	2.8	0.0
Incr Delay (d2), s/veh	0.0	0.0	0.0	1.8	0.0	7.8	0.0	0.1	0.2	0.4	0.1	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	0.0	2.6	0.0	3.4	0.0	2.3	1.3	0.7	2.0	0.0
LnGrp Delay(d),s/veh	0.0	0.0	50.7	55.4	0.0	61.6	3.2	3.0	2.8	4.2	2.9	0.0
LnGrp LOS			D	E		E	A	A	A	A	A	
Approach Vol, veh/h		1			173			759			589	
Approach Delay, s/veh		50.7			58.8			2.9			3.1	
Approach LOS		D			E			A			A	
Timer	1	2	3	4	5	6	7	8				
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		103.7		16.3		103.7		16.3				
Change Period (Y+Rc), s		7.5		6.5		7.5		6.5				
Max Green Setting (Gmax), s		64.5		41.5		64.5		41.5				
Max Q Clear Time (g_c+l1), s		6.7		2.1		10.0		9.0				
Green Ext Time (p_c), s		9.7		0.8		9.6		0.8				
Intersection Summary												
HCM 2010 Ctrl Delay			9.4									
HCM 2010 LOS			A									













HCM 2010 Signalized Intersection Summary
 6: US 42 & Fowlers Ln

01/24/2018

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	0	1	79	0	104	1	541	165	88	475	0
Future Volume (veh/h)	0	0	1	79	0	104	1	541	165	88	475	0
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1900	1863	1900	1863	1863	1900	1863	1863	1863	1863	1863	1900
Adj Flow Rate, veh/h	0	0	1	86	0	113	1	588	179	96	516	0
Adj No. of Lanes	0	1	0	1	1	0	1	2	1	1	2	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	0	0	149	192	0	149	724	2793	1250	582	2793	0
Arrive On Green	0.00	0.00	0.09	0.09	0.00	0.09	0.79	0.79	0.79	0.79	0.79	0.00
Sat Flow, veh/h	0	0	1583	1410	0	1583	881	3539	1583	698	3632	0
Grp Volume(v), veh/h	0	0	1	86	0	113	1	588	179	96	516	0
Grp Sat Flow(s),veh/h/ln	0	0	1583	1410	0	1583	881	1770	1583	698	1770	0
Q Serve(g_s), s	0.0	0.0	0.1	7.1	0.0	8.4	0.0	5.0	3.2	4.8	4.3	0.0
Cycle Q Clear(g_c), s	0.0	0.0	0.1	7.1	0.0	8.4	4.3	5.0	3.2	9.9	4.3	0.0
Prop In Lane	0.00		1.00	1.00		1.00	1.00		1.00	1.00		0.00
Lane Grp Cap(c), veh/h	0	0	149	192	0	149	724	2793	1250	582	2793	0
V/C Ratio(X)	0.00	0.00	0.01	0.45	0.00	0.76	0.00	0.21	0.14	0.17	0.18	0.00
Avail Cap(c_a), veh/h	0	0	548	547	0	548	724	2793	1250	582	2793	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	0.00	1.00	0.88	0.88	0.88	1.00	1.00	0.00
Uniform Delay (d), s/veh	0.0	0.0	49.3	52.5	0.0	53.0	3.7	3.2	3.0	4.4	3.1	0.0
Incr Delay (d2), s/veh	0.0	0.0	0.0	1.6	0.0	7.7	0.0	0.2	0.2	0.6	0.1	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	0.0	2.8	0.0	4.0	0.0	2.5	1.5	1.0	2.1	0.0
LnGrp Delay(d),s/veh	0.0	0.0	49.3	54.1	0.0	60.7	3.7	3.3	3.2	5.1	3.3	0.0
LnGrp LOS			D	D		E	A	A	A	A	A	
Approach Vol, veh/h		1			199			768			612	
Approach Delay, s/veh		49.3			57.9			3.3			3.5	
Approach LOS		D			E			A			A	
Timer	1	2	3	4	5	6	7	8				
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		102.2		17.8		102.2		17.8				
Change Period (Y+Rc), s		7.5		6.5		7.5		6.5				
Max Green Setting (Gmax), s		64.5		41.5		64.5		41.5				
Max Q Clear Time (g_c+l1), s		7.0		2.1		11.9		10.4				
Green Ext Time (p_c), s		10.1		1.0		10.0		0.9				
Intersection Summary												
HCM 2010 Ctrl Delay			10.3									
HCM 2010 LOS			B									

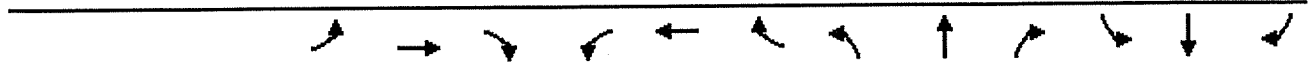
HCM 2010 Signalized Intersection Summary
6: US 42 & Fowlers Ln

01/25/2018

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕		↙	↘		↙	↕	↘	↙	↕	↘
Traffic Volume (veh/h)	4	0	2	114	0	60	1	613	150	61	851	7
Future Volume (veh/h)	4	0	2	114	0	60	1	613	150	61	851	7
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1900	1863	1900	1863	1863	1900	1863	1863	1863	1863	1863	1900
Adj Flow Rate, veh/h	4	0	2	124	0	65	1	666	163	66	925	8
Adj No. of Lanes	0	1	0	1	1	0	1	2	1	1	2	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	103	8	34	201	0	162	486	2823	1263	546	2868	25
Arrive On Green	0.10	0.00	0.10	0.10	0.00	0.10	0.80	0.80	0.80	0.80	0.80	0.80
Sat Flow, veh/h	586	82	334	1409	0	1583	598	3539	1583	659	3596	31
Grp Volume(v), veh/h	6	0	0	124	0	65	1	666	163	66	455	478
Grp Sat Flow(s), veh/h/ln	1001	0	0	1409	0	1583	598	1770	1583	659	1770	1857
Q Serve(g_s), s	0.0	0.0	0.0	6.2	0.0	5.4	0.1	6.6	3.3	3.9	9.8	9.8
Cycle Q Clear(g_c), s	5.4	0.0	0.0	11.6	0.0	5.4	9.9	6.6	3.3	10.5	9.8	9.8
Prop In Lane	0.67		0.33	1.00		1.00	1.00		1.00	1.00		0.02
Lane Grp Cap(c), veh/h	145	0	0	201	0	162	486	2823	1263	546	1411	1481
V/C Ratio(X)	0.04	0.00	0.00	0.62	0.00	0.40	0.00	0.24	0.13	0.12	0.32	0.32
Avail Cap(c_a), veh/h	438	0	0	494	0	492	486	2823	1263	546	1411	1481
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(l)	1.00	0.00	0.00	1.00	0.00	1.00	0.89	0.89	0.89	1.00	1.00	1.00
Uniform Delay (d), s/veh	56.7	0.0	0.0	61.5	0.0	58.8	5.2	3.5	3.2	4.8	3.9	3.9
Incr Delay (d2), s/veh	0.1	0.0	0.0	3.1	0.0	1.6	0.0	0.2	0.2	0.5	0.6	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.2	0.0	0.0	4.9	0.0	2.4	0.0	3.2	1.5	0.8	4.9	5.1
LnGrp Delay(d),s/veh	56.8	0.0	0.0	64.5	0.0	60.4	5.2	3.7	3.4	5.3	4.5	4.4
LnGrp LOS	E			E		E	A	A	A	A	A	A
Approach Vol, veh/h		6			189			830			999	
Approach Delay, s/veh		56.8			63.1			3.6			4.5	
Approach LOS		E			E			A			A	
Timer	1	2	3	4	5	6	7	8				
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		119.2		20.8		119.2		20.8				
Change Period (Y+Rc), s		7.5		6.5		7.5		6.5				
Max Green Setting (Gmax), s		82.5		43.5		82.5		43.5				
Max Q Clear Time (g_c+I1), s		11.9		7.4		12.5		13.6				
Green Ext Time (p_c), s		15.4		0.8		15.4		0.8				
Intersection Summary												
HCM 2010 Ctrl Delay				9.8								
HCM 2010 LOS				A								

HCM 2010 Signalized Intersection Summary
6: US 42 & Fowlers Ln

01/24/2018



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔		↖	↗		↖	↑↑	↗	↖	↑↑	
Traffic Volume (veh/h)	4	0	2	128	0	82	1	609	163	90	842	7
Future Volume (veh/h)	4	0	2	128	0	82	1	609	163	90	842	7
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00	1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1900	1863	1900	1863	1863	1900	1863	1863	1863	1863	1863	1900
Adj Flow Rate, veh/h	4	0	2	139	0	89	1	662	177	98	915	8
Adj No. of Lanes	0	1	0	1	1	0	1	2	1	1	2	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	104	8	35	217	0	188	478	2766	1237	529	2810	25
Arrive On Green	0.12	0.00	0.12	0.12	0.00	0.12	0.78	0.78	0.78	0.78	0.78	0.78
Sat Flow, veh/h	514	71	292	1409	0	1583	603	3539	1583	653	3595	31
Grp Volume(v), veh/h	6	0	0	139	0	89	1	662	177	98	450	473
Grp Sat Flow(s), veh/h/ln	877	0	0	1409	0	1583	603	1770	1583	653	1770	1857
Q Serve(g_s), s	0.0	0.0	0.0	6.3	0.0	7.4	0.1	7.0	3.8	6.6	10.4	10.4
Cycle Q Clear(g_c), s	7.4	0.0	0.0	13.6	0.0	7.4	10.5	7.0	3.8	13.7	10.4	10.4
Prop In Lane	0.67		0.33	1.00		1.00	1.00		1.00	1.00		0.02
Lane Grp Cap(c), veh/h	147	0	0	217	0	188	478	2766	1237	529	1383	1451
V/C Ratio(X)	0.04	0.00	0.00	0.64	0.00	0.47	0.00	0.24	0.14	0.19	0.33	0.33
Avail Cap(c_a), veh/h	413	0	0	488	0	492	478	2766	1237	529	1383	1451
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	1.00	0.89	0.89	0.89	1.00	1.00	1.00
Uniform Delay (d), s/veh	54.7	0.0	0.0	60.4	0.0	57.6	6.0	4.1	3.8	5.9	4.5	4.5
Incr Delay (d2), s/veh	0.1	0.0	0.0	3.1	0.0	1.9	0.0	0.2	0.2	0.8	0.6	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.2	0.0	0.0	5.5	0.0	3.3	0.0	3.5	1.7	1.3	5.2	5.5
LnGrp Delay(d),s/veh	54.8	0.0	0.0	63.6	0.0	59.5	6.0	4.3	4.0	6.7	5.1	5.1
LnGrp LOS	D			E		E	A	A	A	A	A	A

Approach Vol, veh/h		6			228			840			1021	
Approach Delay, s/veh		54.8			62.0			4.2			5.2	
Approach LOS		D			E			A			A	

Timer	1	2	3	4	5	6	7	8
Assigned Phs		2		4		6		8
Phs Duration (G+Y+Rc), s		116.9		23.1		116.9		23.1
Change Period (Y+Rc), s		7.5		6.5		7.5		6.5
Max Green Setting (Gmax), s		82.5		43.5		82.5		43.5
Max Q Clear Time (g_c+l1), s		12.5		9.4		15.7		15.6
Green Ext Time (p_c), s		16.0		1.0		15.9		1.0

Intersection Summary	
HCM 2010 Ctrl Delay	11.2
HCM 2010 LOS	B

Turn Lane Length SBL

Input Fields

Turn Volume	<u>90</u>	Calculated Turn Lane Length (ft)	
Speed Limit	<u>55</u>	Desirable	395
Cycle Length	<u>140</u>	Minimum	345
<i>(Enter 0 for Uncontrolled, 60 for Stop Controlled)</i>			
Approach Percent Grade (G)	0		
Is this a Rural Arterial (Y or N)	N		

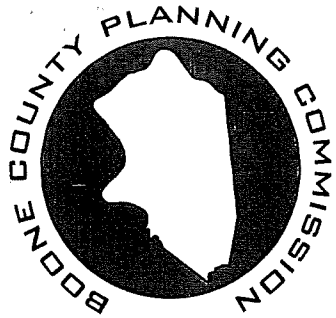
Note: This spreadsheet is intended to supplement the guidance provided in the Auxiliary Turn Lane policy outlined in the KYTC Highway Design Manual. This policy should be fully reviewed and understood prior to using this application.

Turn Lane Length NBR

Input Fields

Turn Volume	<u>163</u>	Calculated Turn Lane Length (ft)	
Speed Limit	<u>55</u>	Desirable	545
Cycle Length	<u>140</u>	Minimum	470
<i>(Enter 0 for Uncontrolled, 60 for Stop Controlled)</i>			
Approach Percent Grade (G)	0		
Is this a Rural Arterial (Y or N)	N		

Note: This spreadsheet is intended to supplement the guidance provided in the Auxiliary Turn Lane policy outlined in the KYTC Highway Design Manual. This policy should be fully reviewed and understood prior to using this application.

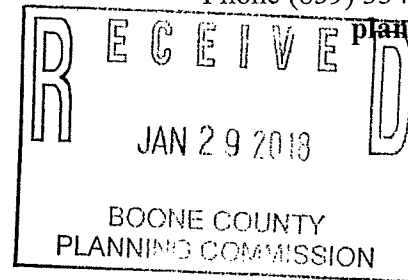


BOONE COUNTY PLANNING COMMISSION

www.boonecountyky.org/pc
www.boonecountygis.com

Boone County Administration Building
2950 Washington Street, Room 317
P.O. Box 958
Burlington, KY 41005

Phone (859) 334-2196; Fax (859) 334-2264
plancom@boonecountyky.org



January 22, 2018

Anchor Retail Solutions
c/o - Mr. Brad Copp
3825 Edwards Road, Suite 630
Cincinnati, OH 45209

RE: Request of **Anchor Retail Solutions LLC (applicant)** for **Arlinghaus I LLC (owner)** for a Zoning Map Amendment from Rural Suburban Estates/Union Town Overlay (RSE/UTO) to Commercial Two/Planned Development (C-2/PD) for an approximate 1.71 acre site located on the east side of U.S. 42, on the southeast corner of the intersection with Fowlers Lane and to the west of the property at 9075 U.S. 42 (Kroger fuel center), Union, Kentucky. The request is for a zone change to allow a multi-tenant commercial building.

Dear Mr. Copp:

The following represents the conditions of approval for the above referenced application as agreed to by the applicant's team and Boone County Planning Commission's Zone Change/Concept Development Plan Committee. Please have the property owner sign the appropriate line on the last page of this letter if you are in agreement with the conditions. Please return this letter to the Boone County Planning Commission office by February 5, 2018.

CONDITIONS

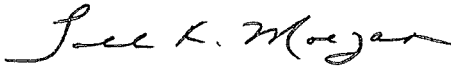
1. The approval is based on the following plans and materials:
 - A. The submitted site layout and landscaping plans (received 12/4/17).
 - B. The revised building elevation drawings (received 1/17/18).
 - C. The building material sample board (received 1/17/18).
 - D. The monument sign drawing (received 1/17/18).
 - E. The drive through signage and canopy elevations (received 1/17/18)

Minor changes to the plans or building materials can be approved by the Zoning Administrator.
2. All roof mounted mechanical equipment shall be screened from public right-of-ways and adjoining properties by parapet walls.
3. The sides and rear of the dumpster enclosure shall be constructed with brick that matches the building on site. Solid gates shall be used and be constructed with wood, vinyl, or metal materials.

Mr. Brad Copp
January 22, 2018
Page 2

4. All lighting shall comply with Section 3316 and Figure 33.6 of the Boone County Zoning Regulations. Fixtures mounted on light poles shall not exceed 30' in height and shall be oriented downwards and inwards toward the subject site.
5. The following Planned Development Waivers are being approved with the request:
 - A. Two drive through signs (preview board and menu board) shall be permitted next to the drive through lane (see the site layout plan and the drive through signage drawings for the approved locations and specifications).
 - B. A formal buffer yard agreement shall not be required between Kroger and the future property owner of the subject site. The subject site shall be landscaped per the landscaping plan that was received on 12/4/17.

Sincerely,



Todd K. Morgan, AICP
Senior Planner, Zoning Services

TKM/ss

AGREEMENT

I, Bob Schroder, do hereby agree to the listed condition of approval for the Zoning Map Amendment application.



Mr. Bob Schroder
Arlinghaus I LLC

1-26-18
Date

**CITY OF UNION, KENTUCKY
ORDINANCE NO. 2018-03**

AN ORDINANCE OF THE CITY OF UNION, KENTUCKY APPROVING, WITH CONDITIONS, A REQUEST OF ANCHOR RETAIL SOLUTIONS, LLC for ARLINGHAUS 1, LLC FOR A ZONING MAP AMENDMENT, SUCH ZONING MAP AMENDMENT TO THE BOONE COUNTY ZONING MAP BEING A ZONE CHANGE FROM RURAL SUBURBAN ESTATES/UNION TOWN OVERLAY (RSE/UTO) ZONE TO COMMERCIAL TWO/PLANNED DEVELOPMENT (C-2/PD) FOR AN APPROXIMATE 1.71 ACRE SITE LOCATED ON THE EAST SIDE OF U.S. 42, ON THE SOUTHEAST CORNER OF THE INTERSECTION WITH FOWLERS LANE AND TO THE WEST OF THE PROPERTY AT 9075 U.S. 42 (KROGER FUEL CENTER), UNION, KENTUCKY

WHEREAS, the City of Union, Kentucky is a member of the county-wide planning unit, having a county-wide planning commission know as the Boone County Planning Commission; and

WHEREAS, the Boone County Planning Commission received a request for a Zoning Map Amendment to the Boone County Zoning Map and such Zoning Map Amendment being a zone change for a for an approximate 1.71 acre site located on the east side of U.S. 42, on the southeast corner of the intersection with Fowlers Lane and to the west of the property at 9075 U.S. 42 (Kroger Fuel Center), Union, Kentucky, and;

WHEREAS, the Boone County Planning Commission as the planning unit for the City of Union, Kentucky was requested to and has conducted a Public Hearing serving as a due process trial-type hearing and made findings recommending approval, with conditions, for the Zoning Map Amendment; and

WHEREAS, the Boone County Planning Commission by Resolution No. R-18-004-A recommended approval of the rezoning described above, with conditions; and

WHEREAS, the recommendation of the Boone County Planning Commission is based upon certain findings and conditions attached to its Resolution, all of which have been reviewed by the City Commission for the City of Union, Kentucky; and

WHEREAS, the Union City Commission desires to affirm and approve the recommendation of the Boone County Planning Commission pursuant to K.R.S. 100.211 within ninety (90) days of the

Planning Commission's final action and grant the application of the Zoning Map Amendment, with conditions.

NOW THEREFORE, BE IT ORDAINED BY THE CITY OF UNION, KENTUCKY as follows:

SECTION I

That the request for a change in the Zoning Map Amendment being a Zone Change for an approximate 1.71 acre site located on the east side of U.S. 42, on the southeast corner of the intersection with Fowlers Lane and to the west of the property at 9075 U.S. 42 (Kroger Fuel Center), Union, Kentucky, shall be and is hereby approved, with conditions, as set forth in the Boone County Planning Commission's Resolution R-18-004-A, which includes, but is not limited to, their Findings of Fact and Description of the real estate which is the subject of this Ordinance and is attached hereto and marked Exhibit "A" and incorporated herein by reference.

SECTION II

If approval for Zoning Map Amendment shall be held invalid, in whole or in part, by any court of proper jurisdiction, such invalidity shall not affect the validity of any of the other zoning regulations, zoning map, or comprehensive plan provisions as they are severable from this Ordinance and they are intended to have effect regardless of any invalidity relating to this particular Ordinance.

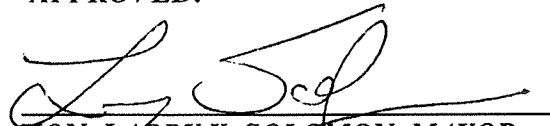
SECTION III

Publication of this Ordinance is hereby authorized to be by summary publication in accordance with Kentucky Law.

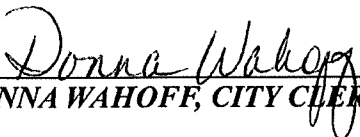
PASSED AND APPROVED ON FIRST READING this the 5th day of March, 2018.

PASSED AND APPROVED ON SECOND READING this 2nd day of April, 2018.

APPROVED:

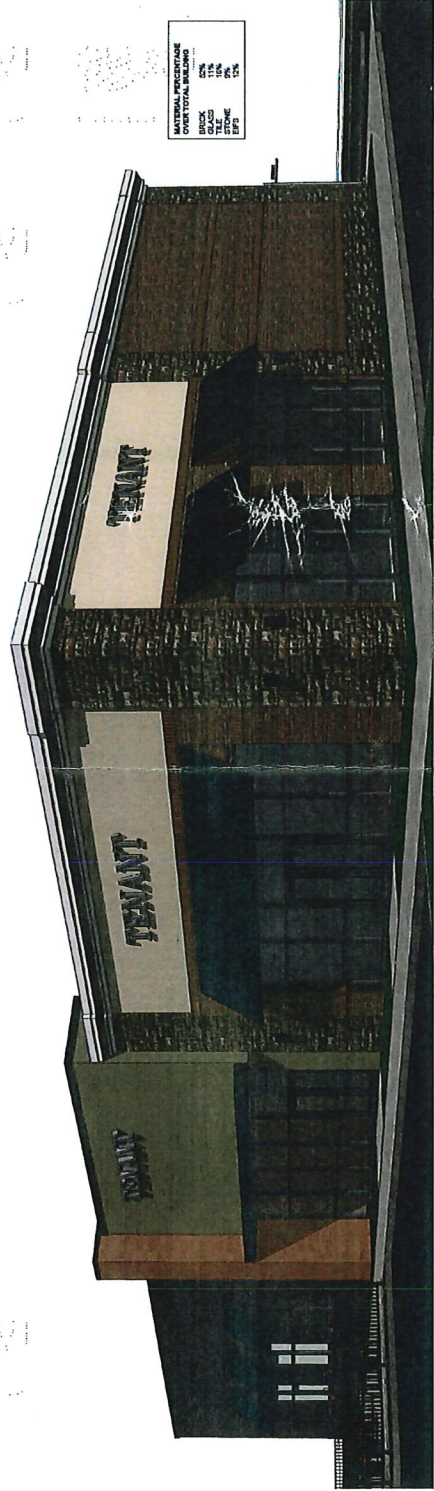

HON. LARRY K. SOLOMON, MAYOR

ATTEST:


DONNA WAHOFF, CITY CLERK



RECEIVED
 JAN 17 2018
 BOONE COUNTY
 PLANNING COMMISSION

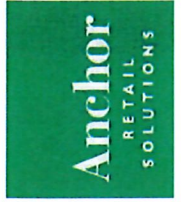


MATERIAL PERCENTAGE	
CONCRETE	15%
GLASS	15%
STEEL	15%
BRICK	15%
WOOD	15%
OTHER	15%
TOTAL	100%

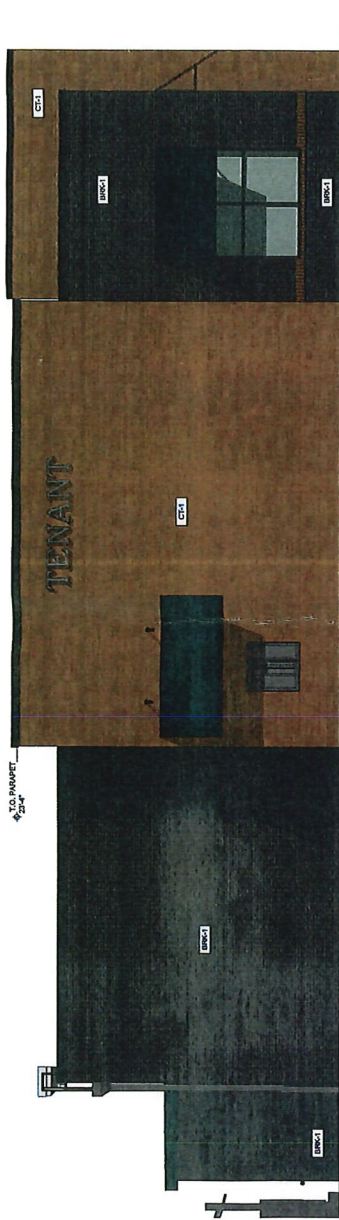
PANERA AND RETAIL SHELL
 UNION, KY
 01.15.18



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 Phone: 615-244-8170 Fax: 615-244-8141 www.mjmach.com



BRICK	46%
GLASS	4%
TILE	50%
STONE	NA
EIFS	NA



① COLOR - SIDE ELEVATION B
1/4" = 1'-0"

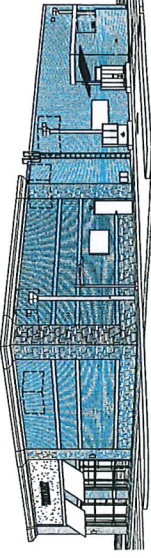
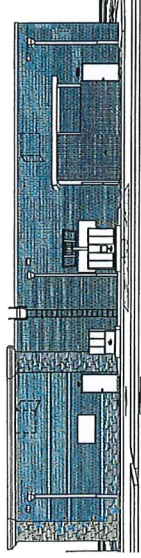
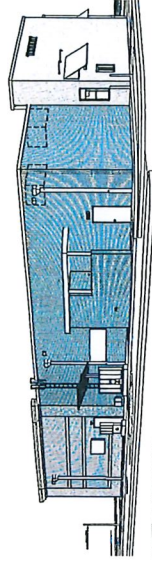
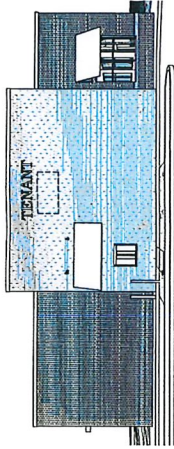
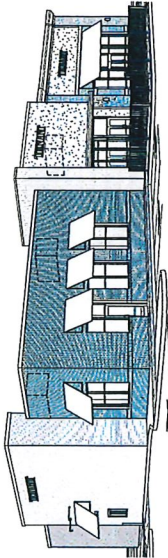
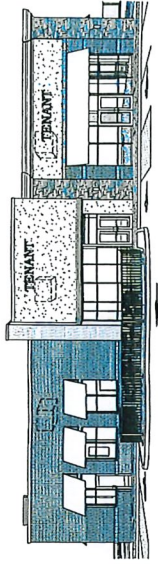
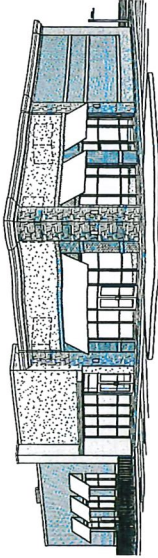
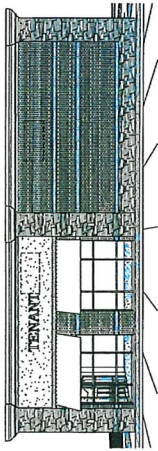
MARK	MATERIAL	MANUFACTURER	COLOR	FINISH
BRICK	CLAY BRICK	UNIVERSAL	BLACK	GLAZED
BRICK	CLAY BRICK	UNIVERSAL	BLACK	GLAZED
BRICK	CLAY BRICK	UNIVERSAL	BLACK	GLAZED
EIFS	EIFS	EIFS	EIFS	EIFS
GLASS	GLASS	GLASS	GLASS	GLASS
STONE	STONE	STONE	STONE	STONE
TILE	TILE	TILE	TILE	TILE



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01.15.18





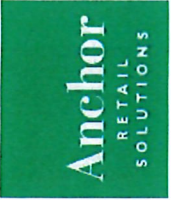
NOTE:

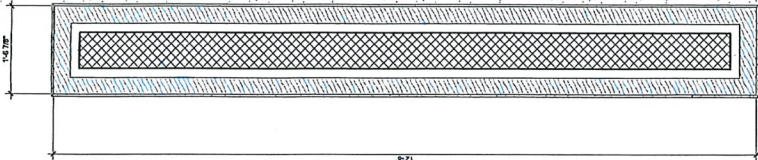
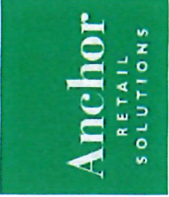
- ALL PERSPECTIVES TAKEN FROM PROPERTY LINE WITH EYE LEVEL HEIGHT AT 5'-6"
- RTU'S DENOTE BY DASHED LINE



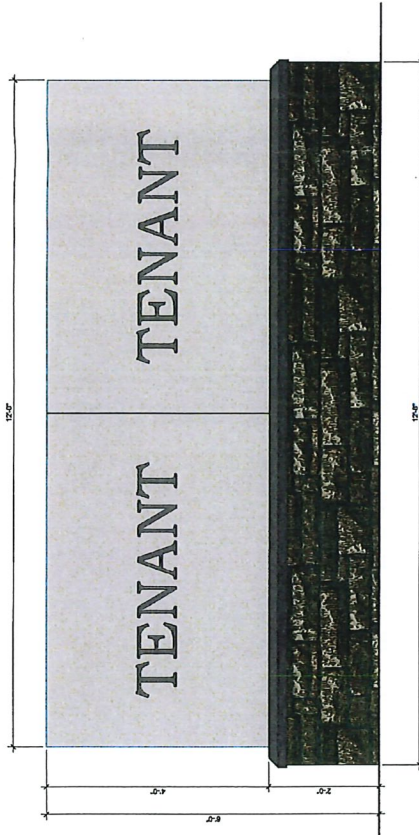
MJM ARCHITECTS 712 4th Avenue South Nashville, TN 37210
 Phone: 615-244-8170 Fax: 615-244-8141 www.mjmach.com

PANERA AND RETAIL SHELL
 SIGHTLINE STUDY
 UNION, KY
 01.15.18





① MONUMENT SIGN
1" = 1'-0"



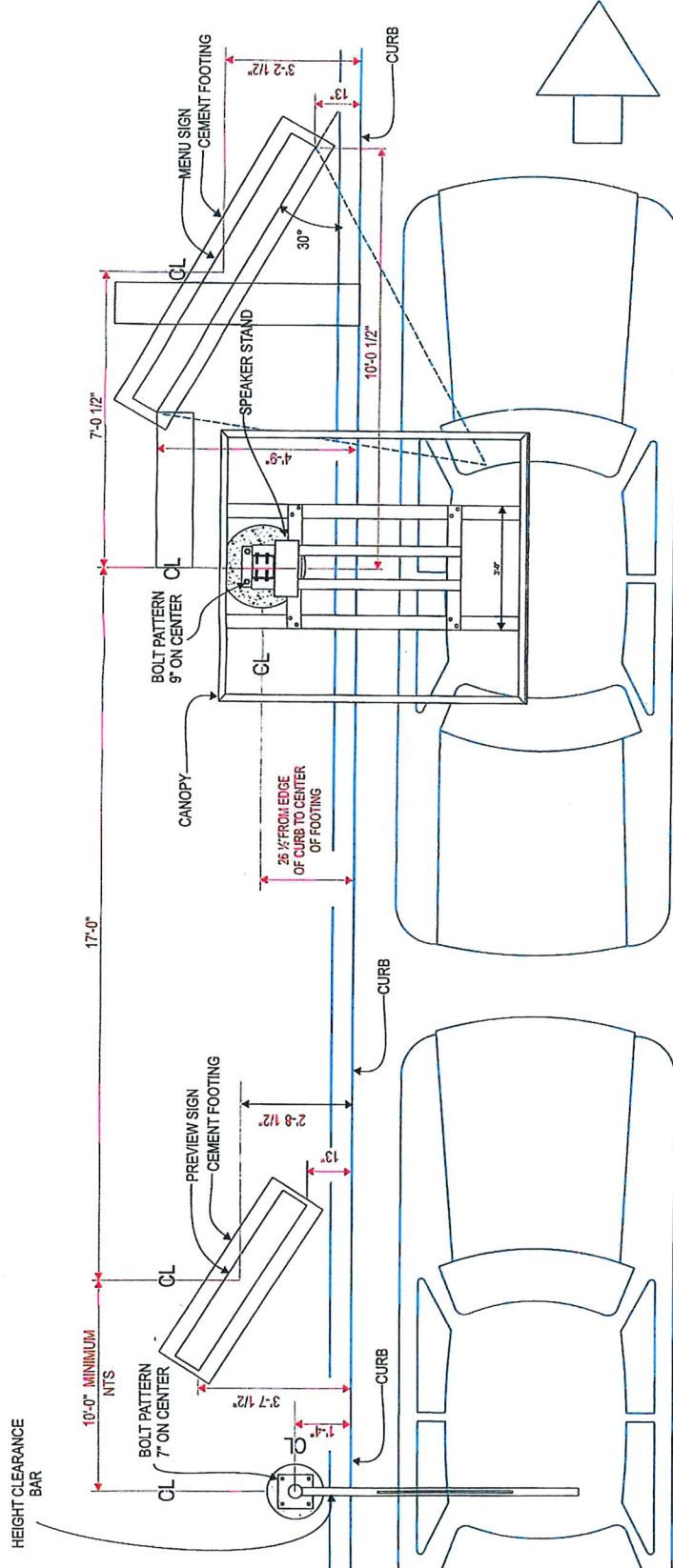
② MONUMENT SIGN
1" = 1'-0"

PANERA AND RETAIL SHELL
UNION, KY
01.15.18



MJM ARCHITECTS 712 4th Avenue South Nashville, TN 37210
Phone: 615-244-8170 Fax: 615-244-8141 www.mjmarsh.com

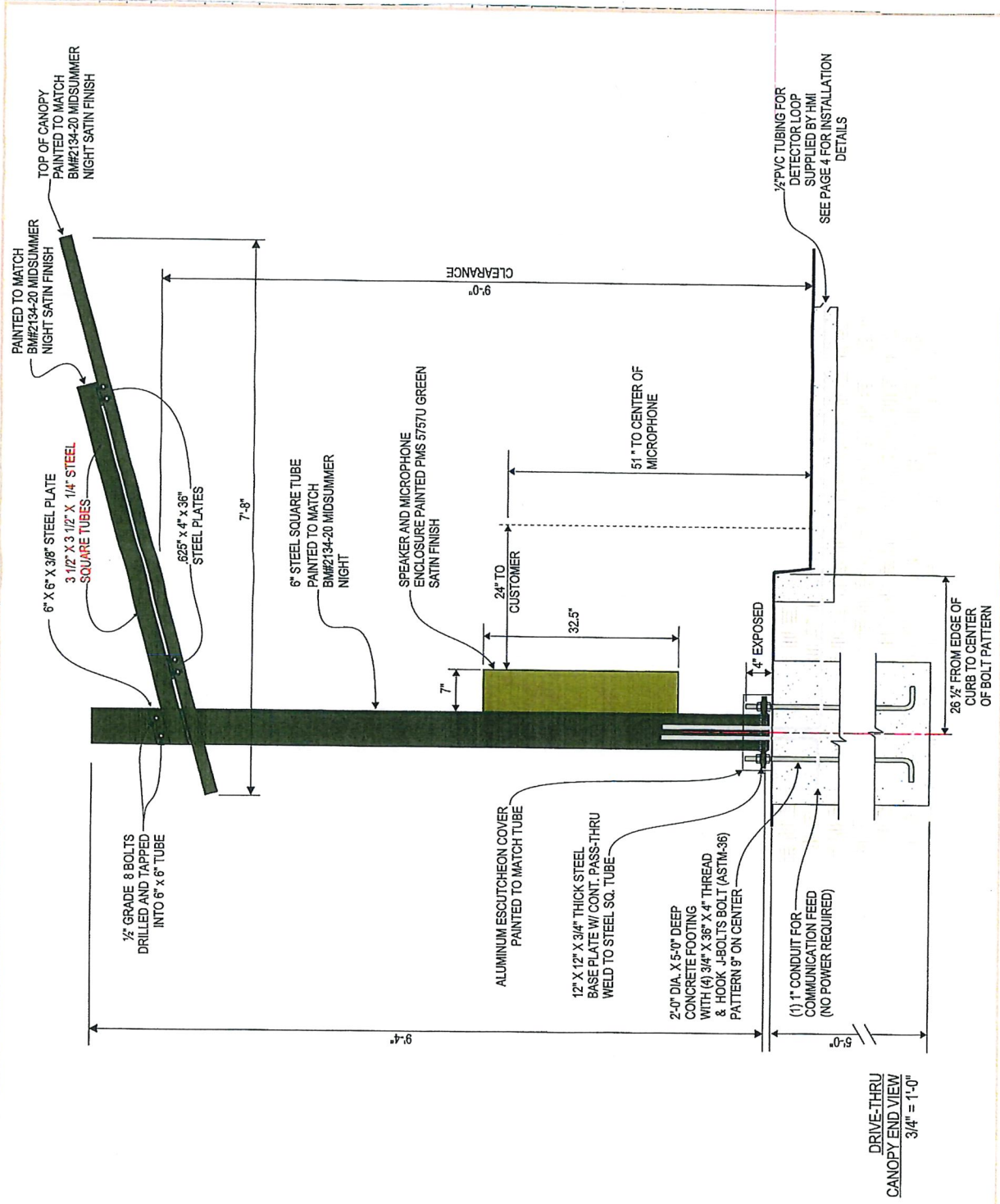
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 JAN 17 2018
 BOONE COUNTY
 PLANNING COMMISSION



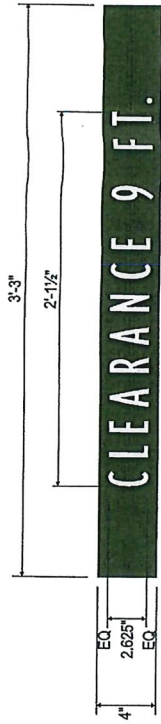
DRIVE-THRU CANOPY, CLEARANCE BAR, SPEAKER STAND & MENU SIGN
 PLAN VIEW DETAIL - 3/8" = 1'-0"



1 PREVIEW BOARD ELEVATION
SCALE - 1" = 1'-0"



DRIVE-THRU
 CANOPY END VIEW
 3/4" = 1'-0"

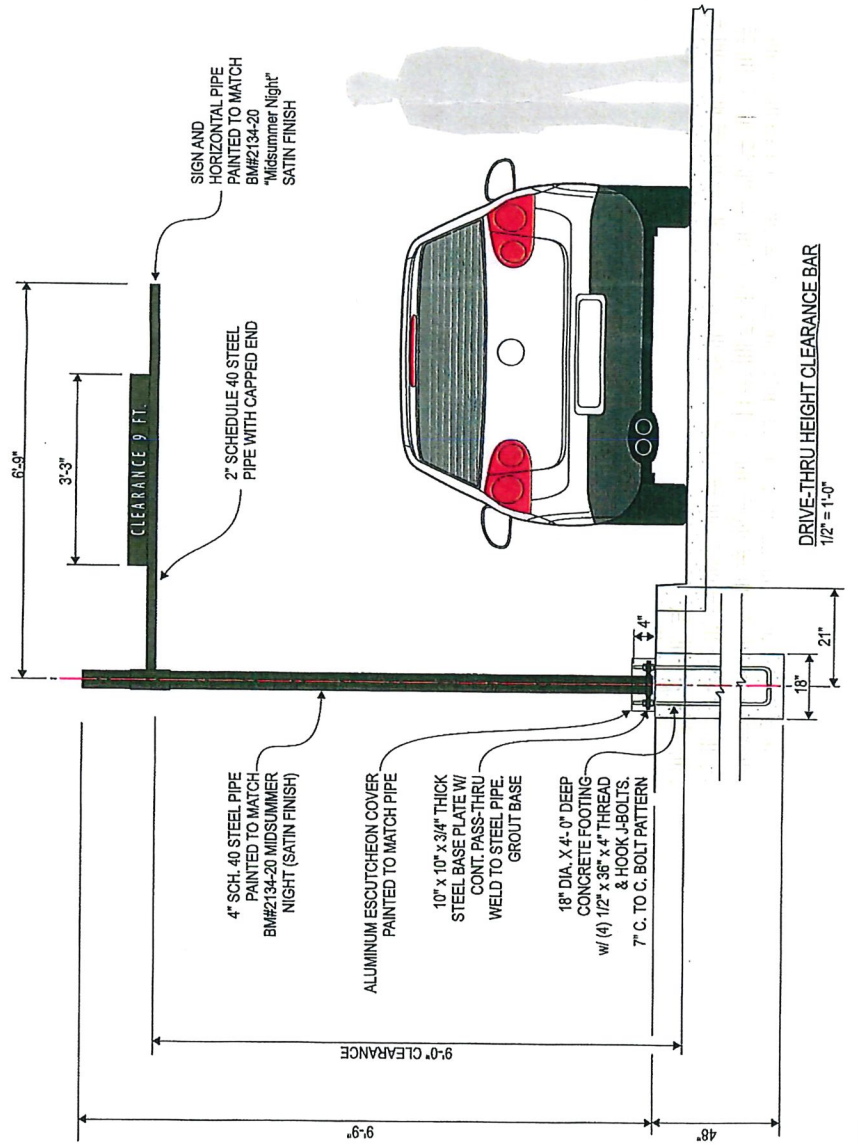


TYPE FONT: TRIPLEX CONDENSED REGULAR, 20% LETTER SPACING

CLEARANCE BAR SIGN

1/2" = 1'-0"

BRAKE FORMED .125 ALUMINUM PANEL
TEK SCREWED TO TOP OF STEEL PIPE





EIFS-1
DRYWIT EIFS TO MATCH
BENJAMIN MOORE
#HC-39 PUTNAM IVORY



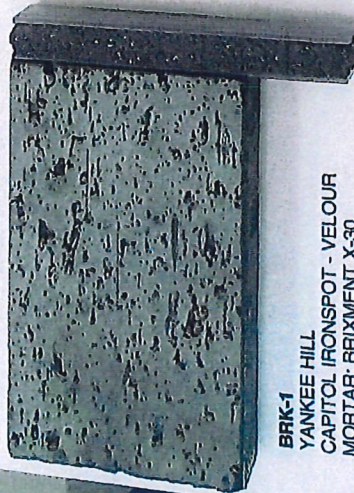
EIFS-2
DRYWIT EIFS TO MATCH
BENJAMIN MOORE
#985 INDIAN RIVER



EIFS-3
DRYWIT
113 - CHINA WHITE



CT-1
CERAMIC TILE - 6x48 W/ 1-3 STAGGER
ARIOSTEA, HIGH-TECH WOODS
ROVERE IMPERO
GROUT: MAPEL #42 MOCHA



BRK-1
YANKEE HILL
CAPITOL IRONSPOT - VELOUR
MORTAR: BRIXMENT X-30



BRK-2
SIOUX CITY BRICK
BADLANDS - VELOUR
MORTAR: HOLCIM SANTEE SALMON



STN-1
RANDOM ASHLAR STONE VENEER
ROCKCAST
STONE CREEK

**PANERA AND RETAIL SHELL
MATERIAL BOARD
UNION, KY**



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