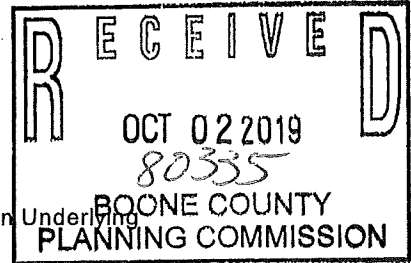


APPLICATION FORM

CONCEPT DEVELOPMENT PLAN
BOONE COUNTY PLANNING COMMISSION

SEVEN (7) COPIES OF SUBMITTED DRAWINGS ARE REQUIRED



(See Boone County Zoning Regulations)

SECTION A (To be completed by applicant)

- 1. Check One: [X] Concept Development Plan (Includes Utilization of an Underlying Zone in a Planned Development)
2. Name of Project Tesla EV Charging Station Installation
3. Location of Project 70 Spiral Drive, Florence, KY, 41042
4. Total Acreage of Site 26.32 = meijer property. Tesla project = 0.09 acres.
5. Current Zoning O2/C-2/PD/CD/HDO
6. Date of Previous Zoning Map Amendment or Approved Concept Development Plan (if applicable) N/A
7. Is the site subject to a specialized Land Use Study... N/A
8. Proposed Uses Addition of parking area, electrical equipment, and EV Charging posts.
9. Proposed Building Intensities N/A - Electrical & Civil work only.
10. Have you submitted a Concept Development Plan? This is the application.
11. Are you also applying for: [X] Conditional Use Permit
12. Name of Applicant(s) GPD Group - Contact: Zachary Whitney
13. Address of Applicant(s) 520 S. Main Street, Akron, OH 44311
14. Name of Property Owner(s) Meijer Stores LTD
15. Address of Property Owner(s) 2929 Walker Ave NW, Grand Rapids, MI 49544
16. Are there any existing buildings on the site? Yes, How many? (1)
17. Deed Book D793, Page No. 603, Group No. 2033B
18. Have you had a pre-application meeting with BCPC Staff? NO
19. Please check the following organizations/agencies which you have discussed the proposed development with in the last several months:

- Boone County Water District
Florence Public Services Dept.
[X] Duke Energy
Sanitation District #1

(over)

**EXHIBIT**

**“A”**

## STAFF REPORT

Request of GPD Group c/o Zachary Whitney (applicant) for Meijer Stores Ltd. (owner) for a Change in Concept Development Plan in a Office Two/Commercial Two/Planned Development/Houston Donaldson Study Corridor Overlay (O-2/C-2/PD/HDO) zone for an approximate 0.09 acre portion of the 26.32 acre lot located at 4990 Houston Road, Florence, Kentucky (Meijer). The request is for a Change in Concept Development Plan to allow an electric vehicle charging station.

November 6, 2019

### PROPOSAL

This application is for a Change in Concept Development Plan to allow an electric vehicle charging station on the west side of the Meijer tract along Spiral Boulevard in Florence. The project site is a 0.09 acre area which faces Spiral Boulevard and is across the street from Crunch Fitness (77 Spiral Boulevard) and Cincinnati Overstock (83 Spiral Boulevard), and to the south of Chili's restaurant (5000 Houston Road). In context of the zoning regulations, an electric vehicle charging station is considered to be a fueling station.

The proposal includes the construction of twelve parking spaces along an existing driveway and the installation of electrical charging related equipment. The spaces themselves are standard parking spaces. Approximate 5.5 foot high charging posts with the Tesla logo on both sides and 6 foot high "Tesla" regulatory style signs (1.5' X 1') are proposed at each space.

An enclosure which contains electrical equipment is proposed to be placed between the new parking spaces and Spiral Boulevard. The plan dimensions for the enclosure are approximately 18 feet by 18.5 feet. The enclosure itself is proposed to be 8 feet high and constructed of vertical Trex members with defined posts and post caps. A standalone electrical transformer is proposed to the south of the parking row and equipment enclosure, and a new freestanding light fixture is proposed to be placed between the equipment enclosure and the new parking spaces.

### ADJACENT LAND USES AND ZONING

The land uses and zoning adjoining the site include the following:

- A. Two restaurants and a multi-tenant commercial building are located to the north/northeast along the south side of Houston Road and between Spiral Boulevard and Meijer Drive (O-2/C-2/PD/HDO).
- B. A multi-tenant retail center with two outlots along Houston Road is located across the street on the west side of Spiral Boulevard (O-2/C-2/PD/HDO).

- C. The remainder of the Meijer store property is located to the east and southeast (O-2/C-2/PD/HDO).
- D. Three multi-story office buildings and a fitness club are located to the south along the south side of Meijer Drive in proximity to the Spiral Boulevard/Meijer Drive intersection (O-2/C-2/PD/HDO).

### SITE CHARACTERISTICS

The site is within a grassed area that is between two access drives into the Meijer site from Spiral Boulevard. An internal driveway runs along the east side of the site. It contains several deciduous trees along Spiral Boulevard and several freestanding light fixtures along the interior driveways. The topography of the grassed area slopes gently downward from the southeast corner to the northwest corner with an approximate 6 foot drop in elevation.

### SITE HISTORY

- 1990** A third Buchanan and NKS Concept Development Plan application was conditionally approved for the overall Houston Lakes development (two prior proposals were unsuccessful). A retail center was approved for the current Meijer lot and three outlot uses (two restaurants and a bank) were approved for the Houston Road frontage area in front of the retail center. This is the currently approved Concept Development for the site subject to the Zoning Administrator determination discussed below. A Change in Concept Development Plan was conditionally approved in 2007 to allow the bank to be replaced with the multi-tenant commercial building which was constructed at the Houston Road/Meijer Drive intersection (applicable approval documents are attached).
- 1992** In response to a request from Meijer representatives, the Zoning Administrator determined that the construction of a Meijer store in lieu of the above mentioned retail center was a minor change to the approved Concept Development Plan and was therefore permitted without the review and approval of a new Concept Development Plan (letter dated 3/10/92 is attached).

### RELATIONSHIP TO COMPREHENSIVE PLAN

The Our Boone County - 2040 Plan's Future Land Use Map designates the site as "Commercial." This designation is described as "retail, corporate and professional office, interchange commercial, indoor commercial recreation, restaurants, services, etc."

The Land Use Element provides the following quotes that relate to the proposal and/or general area.

- A. A substantial portion of this section is located within the 2013 Houston-Donaldson Study area and has excellent development and/or redevelopment opportunity due to its high visibility from and easy access to I-75. Existing parcels should be developed/redeveloped to be aesthetically harmonious with existing surrounding land uses and cleaned up environmentally as much as is needed for new development ("14. Florence Commercial," p. 125).
- B. Regionally-oriented growth should be confined to Mall Road and Houston Road, and should not expand onto U.S. 42, KY 18 or Hopeful Church Road. However, the highway-scaled, automobile oriented appearance of this growth should be minimized through the provision of pedestrian improvements, smaller facilities within outlots in conjunction with larger strip-style centers, multiple, scattered parking areas in lieu of large central parking areas, and street trees along Mall Road ("14. Florence Commercial," p. 126).
- C. Commercial development in this section should remain near the arterial roads or close to the interstate. These consumer service and traffic-oriented developments must be contained to specific areas adjacent to the arterial roads and interstate. The coordination of curb cuts and parking lot connections in the already developed areas must occur in order to maintain a reasonable ease of travel along these roads ("14. Florence Commercial," p. 126).
- D. The visual appearance of commercial areas in general and of Mall Road in particular, should be enhanced through the design of buildings, landscaping, signs, and arrangement of buildings. Attention to aesthetic impacts of proposed developments should be part of all phases of the review process ("14. Florence Commercial," p. 126).

The Land Use Element's Future Land Use Development Guidelines provide the following general comments that relate to the proposal.

- A. Developments in Boone County should begin with an assessment of existing site features to determine positive and useful attributes, as well as features that should or can be preserved. Development design should incorporate the use of these attributes for the benefit of the development and the County as a whole. Development plans should identify such areas, and delineate disturb limits to protect those areas that have been defined ("Utilization of Existing Vegetation and Topography," p. 95).
- B. Developments in Boone County must recognize the potential impacts upon adjoining land uses and incorporate a transition of land uses, building setbacks, and/or landscaping to minimize these impacts. Potential impacts include visual, noise or vibrations, odors, dust, smoke, and light. Buffering to mitigate these impacts should be an integral part of the design of proposed projects; where

appropriate, existing site features should be used in meeting this guideline. Developments should provide buffering along public roadways, to soften the visual impact ("Buffering," p. 95).

- C. Developments in Boone County must include landscaping to accompany the proposed project. Retention of existing healthy vegetation is considered a component of landscaping and is encouraged. This landscaping should be designed to improve the public view of a development, and should be incorporated into parking lots and other vehicle circulation areas, as well as within open spaces and around structures. Landscaping is intended to soften the visual impacts of the development from adjoining properties and roadways.

Developments along major roadways in Boone County must include landscaping between the development and the right-of-way in order to promote the aesthetic appearance from the roads and to facilitate the compatibility of differing land uses ("Landscaping," p. 96).

- D. Developments in Boone County must recognize the potential impacts of associated traffic on adjoining properties and transportation systems. The need exists to protect the capacity of the existing roadway network and to plan improvements to accommodate new development and travel patterns. Access management provisions include the coordination of curb cuts, adequate corner clearance and sight distance for access points, adequate spacing between access points, shared access points and parking facilities, provisions for access connections to adjoining properties, and dedication of public right-of-way. In subdividing property, arterial or collector streets should not be used for direct access to lots, rather a system of local streets should feed into the collector and arterial street system. Connections between and within commercial and industrial developments allow for more efficient provision of transit service ("Access Management," p. 96).

- E. Developments in Boone County should give consideration to the overall design of the area. Site, landscape, and building design should be a primary concern at the early stages of the development, with an emphasis on the aesthetic impact of the proposed use. The minimal use of signs is encouraged; signage should be adequate to identify a specific development, but should not be used as a means to compete for motorist attention. The objective is to avoid the confusion and/or distraction of motorists, and to avoid the potential negative impacts of signs on the visual appearance of a development or corridor. Utility lines should be placed underground wherever possible, and junction boxes screened from public view ("Design, Signs, and Cultural Resource Preservation," p. 97).

The Demographics Element provides the following statements regarding amenities and consumer needs.

- A. Furthermore, by continuing to monitor the composition of the population, Boone County will be more prepared to provide proper and adequate amenities that encourage them to stay in the county rather than moving elsewhere for their needs to be met. Knowing the makeup of the population will assist in providing the appropriate housing, employment, and retail shopping opportunities to meet their needs at the countywide level as well as within specific areas within the county as development plans and zone change requests are reviewed (“Identify the Needs of the Population,” p. 19).

The Our Boone County - 2040 Plan’s Goals and Objectives include the following pertinent statements.

- A. Boone County shall strive to achieve diversity and balance in land use while protecting natural resources and natural systems (“Overall,” Objective 3).
- B. Promote quality of life amenities to retain current residents and attract new residents in order to have a stable and diversified population (“Demographics,” Goal A, Objective 3).
- C. Provide appropriate services, housing, employment, and shopping opportunities in order to meet the needs of the population in all geographic areas of the County (“Demographics,” Goal A, Objective 4).
- D. Development shall utilize existing topography and vegetation (including mature trees) and preserve the existing character of the land (“Environment,” Objective 7).
- E. Boone County shall promote a vibrant, diverse, and sustainable economy that both recognizes the value of the environment and encourages and incentivizes innovation and prosperity with meaningful employment opportunities (“Economy,” Goal A).
- F. Industries shall be promoted in suitable locations to make the county a vital part of a strong regional economy (“Economy,” Goal A, Objective 2).
- G. Boone County businesses are developed in appropriate locations and are compatible with surrounding land uses (“Economy,” Goal B).
- H. Compact, efficient development patterns shall be encouraged for industrial, commercial, and office uses with appropriately sized and well maintained buffer spaces between the business use and other land uses (“Economy,” Goal B, Objective 1).
- I. Commercial uses shall be designed and located to coordinate with the surrounding land uses and shall have safe access and adequate parking (“Economy,” Goal B,

Objective 3).

- J. Interstate commercial uses, as well as the freight logistics industry, shall be in close proximity to interstate interchanges for maximum convenience and economy to the traveling public, while minimizing traffic congestion except in the City of Florence where overlay districts exist to promote this type of industry ("Economy," Goal B, Objective 4).
- K. Priority shall be given towards maintaining, protecting, and improving the capacity and safety of the existing road system across jurisdictions ("Transportation," Goal A, Objective 1).
- L. Roadway capacity shall be preserved by utilizing access management policies and guidelines ("Transportation," Goal B, Objective 4).

#### RELATIONSHIP TO HOUSTON-DONALDSON STUDY

The subject site is within "Subarea Four" as outlined in the Houston-Donaldson Study. The recommendations for this subarea are summarized as follows:

- Future development and/or redevelopment in Subarea Four should be of commercial or office type land uses and not of light industrial or residential uses, except for the light industrial area on Spiral Drive.
- Existing office and retail structures on Meijer Drive be either completed or occupied.
- That redevelopment in Subarea Four aesthetically fit in with the existing land uses (p. 33).

The following Goals and Objectives are stated for Subarea Four:

- A. **Goal:** Maintain the successful business climate of the Subarea.

**Objectives:**

1. Continue to consistently apply the Design Standards for future development and/or redevelopment.
  2. Continue to support the Commercial and Office uses and zoning classifications in future editions Boone County Zoning Ordinance and future editions of the Boone County Comprehensive Plan and its Future Land Use map.
- B. **Goal:** Enhance the non-motorized system in the South Entrance Subarea.

**Objectives:**

1. Disconnects in the sidewalk network within the Subarea should be filled in.
2. Redevelopments in the Subarea should incorporate new internal sidewalks that connect to the Subarea's sidewalk system.

C. **Goal:** Protect the integrity of the Gunpowder Creek Watershed.

**Objectives:**

1. Use future development and/or redevelopment of properties in the Subarea for opportunities to apply efforts to improve quality and quantity of stormwater runoff (p. 44).

BOONE COUNTY TRANSPORTATION PLAN

The Boone County Transportation Plan includes the following project that is in the general area. Spiral Boulevard adjoining the subject site is part of the overall route between the two destinations stated.

Location: TANK Transit Center on Mall Road to the TANK Transit Center in Covington

Type: Transit

Total Cost: \$5,360,000

Description: Create a high-frequency, enhanced bus transit corridor. Provide specialized branding and operational treatments. Improve bus stop/station design and amenities. Include bicycle and pedestrian facilities for multi-modal safety, mobility and connectivity.

This is a "Tier 1" project that is to be completed between 2020 and 2030.

STAFF COMMENTS

1. COMPREHENSIVE PLAN AND HOUSTON-DONALDSON STUDY

The Planning Commission and Florence City Council need to consider the Comprehensive Plan as it relates to this request (cf., Planned Development Criterion #11 in Section 1514 of the Boone County Zoning Regulations). The Our Boone County - 2040 Plan's Future Land Use Map designates the site as "Commercial." This designation is described as "retail, corporate and professional office, interchange commercial, indoor commercial recreation, restaurants, services, etc."

The Land Use Element states, "A substantial portion of this section is located within the 2013 Houston-Donaldson Study area and has excellent development and/or redevelopment opportunity due to its high visibility from and easy access to I-75. Existing parcels should be developed/redeveloped to be aesthetically harmonious with existing surrounding land uses and cleaned up environmentally as much as is needed for new development" ("14. Florence Commercial," p. 125). The Land Use Element also states that "regionally-oriented growth should be confined to Mall Road and Houston Road, and should not expand onto U.S. 42, KY 18 or Hopeful Church Road. However, the highway-scaled, automobile oriented appearance of this growth should be minimized through the provision of pedestrian improvements, smaller facilities within outlots in conjunction with larger strip-style centers, multiple, scattered parking areas in lieu of large central parking areas, and street trees along Mall Road" ("14. Florence Commercial," p. 126). Both the Demographics Element (p. 19) and the Goals and Objectives ("Demographics," Goal A, Objectives 3 and 4) refer to the provision of amenities and appropriate services in a general sense.

The Houston-Donaldson Study summarizes the recommendations for Subarea Four, which includes the subject site, with the following bullet points.

- Future development and/or redevelopment in Subarea Four should be of commercial or office type land uses and not of light industrial or residential uses, except for the light industrial area on Spiral Drive.
- Existing office and retail structures on Meijer Drive be either completed or occupied.
- That redevelopment in Subarea Four aesthetically fit in with the existing land uses (p. 33).

## 2. PD OVERLAY ZONE CRITERIA

Staff offers the following comments regarding the standards set forth in Section 1514 "Planned Development Criteria" of the Boone County Zoning Regulations. Concept Development Plan proposals in Planned Developments are to be primarily evaluated against these criteria. This section also says that "Concept Development Plan proposals within areas that are subject to a specific land use or corridor study shall be evaluated against the criteria or requirements of such study as well as the criteria in this section" (p. 15.4). As stated above, the site is within the "Houston-Donaldson Study Corridor Overlay District" (HDO) and is subject to the Houston-Donaldson Study.

This section also clarifies that "the Concept Development Plan shall fulfill the following criteria unless a portion of the criteria do not apply or relate, in whole or part, to a specific proposal." It is Staff's conclusion that several of the criteria do not apply or relate since the proposal involves the addition of 12 parking spaces to a facility which already has almost 1,000 spaces, the additional spaces will use

existing driveways and curb cuts, and the additional spaces will be placed along the interior edge of a large landscape area. Additionally, the charging facility will not have a stereotypical "gas station" character with a canopy, sales of convenience goods, etc. (#2 "Compatibility of Uses). The following issues were identified relative to the criteria in Section 1514 and the standards in the Houston-Donaldson Study.

- A. No landscape plan was submitted (#2 "Compatibility of Uses" and #6 "Landscaping"). Several deciduous trees exist on the opposite side of the overall landscape area where the charging station is proposed (mostly in or along the Meijer Drive right-of-way line), but none are in proximity to the charging station itself. Criterion 6 "Landscaping" states, "Substantial landscaping shall be provided in a planned development with emphasis given to street scape areas, buffer zones, and the provision of significant landscaping (in terms of size of landscape areas, and quantity and quality of landscape materials) within the developed portions of the site" (p. 15.3-4). The Comprehensive Plan's Future Land Use Development Guidelines state, "Developments should provide buffering along public roadways, to soften the visual impact" (p. 95) and "landscaping is intended to soften the visual impacts of the development from adjoining properties and roadways" (p. 96).

When considering that the proposal includes an equipment enclosure and multiple charging posts and signs, Staff recommends that landscaping from Buffer Yard A (5 small deciduous trees OR 3 large deciduous, medium deciduous, and/or evergreen trees AND 30 small shrubs OR 15 large shrubs per 100 linear feet) with the large shrub option be provided across the front of the charging station including the parking spaces facing Meijer Drive (approximately 112' wide). Additionally, a screen of large shrubs is recommended around the freestanding transformer.

- B. An 8 foot high Trex fence is proposed to enclose the mechanical equipment associated with the charging station (#2 "Compatibility of Uses" and #7 "Architecture"). In relation to buildings, Criterion 7 "Architecture" states, "the predominant building materials shall have a solid appearance, such as could be achieved with brick, stone, and architectural grade cast concrete products designed to replicate natural materials" (p. 15.4). Relative to mechanical equipment screening, the Houston-Donaldson Study states, "Ground mounted equipment shall be screened either with landscaping or enclosed with an enclosure structure which uses the same materials, colors, and design detailing as the principal building" (p. 57). Based on the high visibility location along Spiral Boulevard, Staff recommends that the equipment be enclosed with a structure which meets the Houston-Donaldson Study standards and correlates to the existing building (versus the Trex fence), and that landscaping be used for aesthetic enhancement (versus opaque screening) as recommended under "A" above.

- C. The charging station is proposed to be placed within an existing open space area (#3 "Open Space"), but the amount of space involved is negligible in context of the overall Meijer site. The Houston-Donaldson Study requires 22 percent of the overall site to "be devoted to landscaping as permanent green space." Approximately 27.9 percent of the Meijer lot is currently green space (approximately 8.27 acres).
- D. "Tesla" graphics are shown on the proposed charging posts and regulatory style signs are proposed for each charging space (#9 "Signage"). The charging posts are approximately 5.5 feet high. The regulatory style signs are 1.5 square feet in area (1' X 1.5') and 6 feet high. These signs are shown on Sheet C-6. Due to the incidental nature of the charging posts and signs, Staff has no comments provided the landscaping recommended under "A" above is installed.
- E. The Comprehensive Plan is discussed above (#11 "Conformance with Comprehensive Plan").

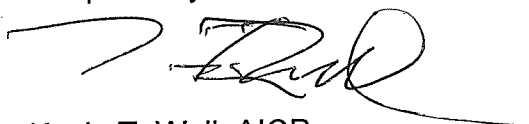
### 3. OUTSIDE AGENCY COMMENTS

Staff has requested comments on the proposal from the Florence City Administration, Florence Fire/EMS Department, and Public Services Department. Such comments have not been received as of this writing. Any written comments received from these agencies will be forwarded to the Zone Change Committee for review.

### CONCLUSION

The Boone County Planning Commission and the Florence City Council need to evaluate the application in terms of the Our Boone County - 2040 Plan (the adopted Comprehensive Plan), 2013 Houston-Donaldson Study, Article 15 "Planned Development District" of the Boone County Zoning Regulations, and the potential impacts on the existing and planned uses in the area. The Future Land Use Map will not need to be amended if this request is approved.

Respectfully submitted,



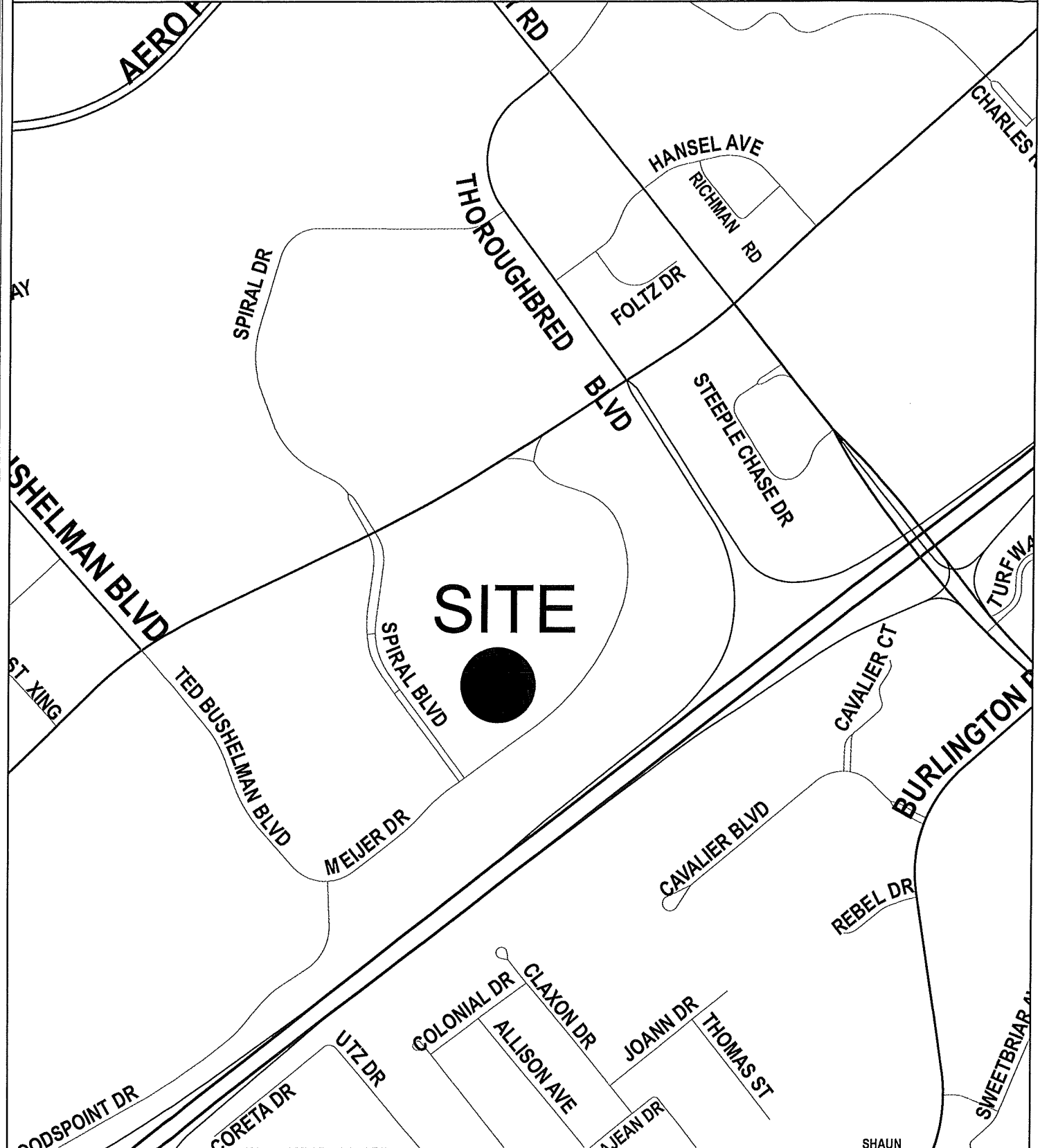
Kevin T. Wall, AICP  
Director, Zoning Services

Attachments:

- Location map
- Air photo map w/ zoning
- 2035 Future Land Use map
- 1990 Concept Development Plan
- 9/5/90 GBBN/Buchanan/N.K. Management Committee Report
- 11/2/90 letter from Anne McBride to Bruce Janken, City Coordinator, regarding conditions
- 11/16/90 letter from Dale Wilson to Mayor Roger Rolfes regarding conditions
- 3/10/92 Zoning Administrator decision letter from Kevin Costello to C. Francis Barrett and Anne McBride
- Application materials including concept development plan

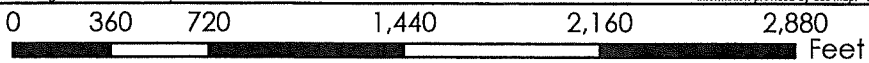
# Location

www.boonecountygis.com



**SITE**

1 inch = 706 feet



Copyright 2019 Boone County GIS. Reproduction by any means without the express permission of the Boone County Planning Commission is prohibited.

Data used to create this map were compiled from sources that comply with National Map Accuracy Standards. Boone County GIS extends no warranty with respect to the accuracy or content of the information provided by this map. This map should be used for general planning purposes only.

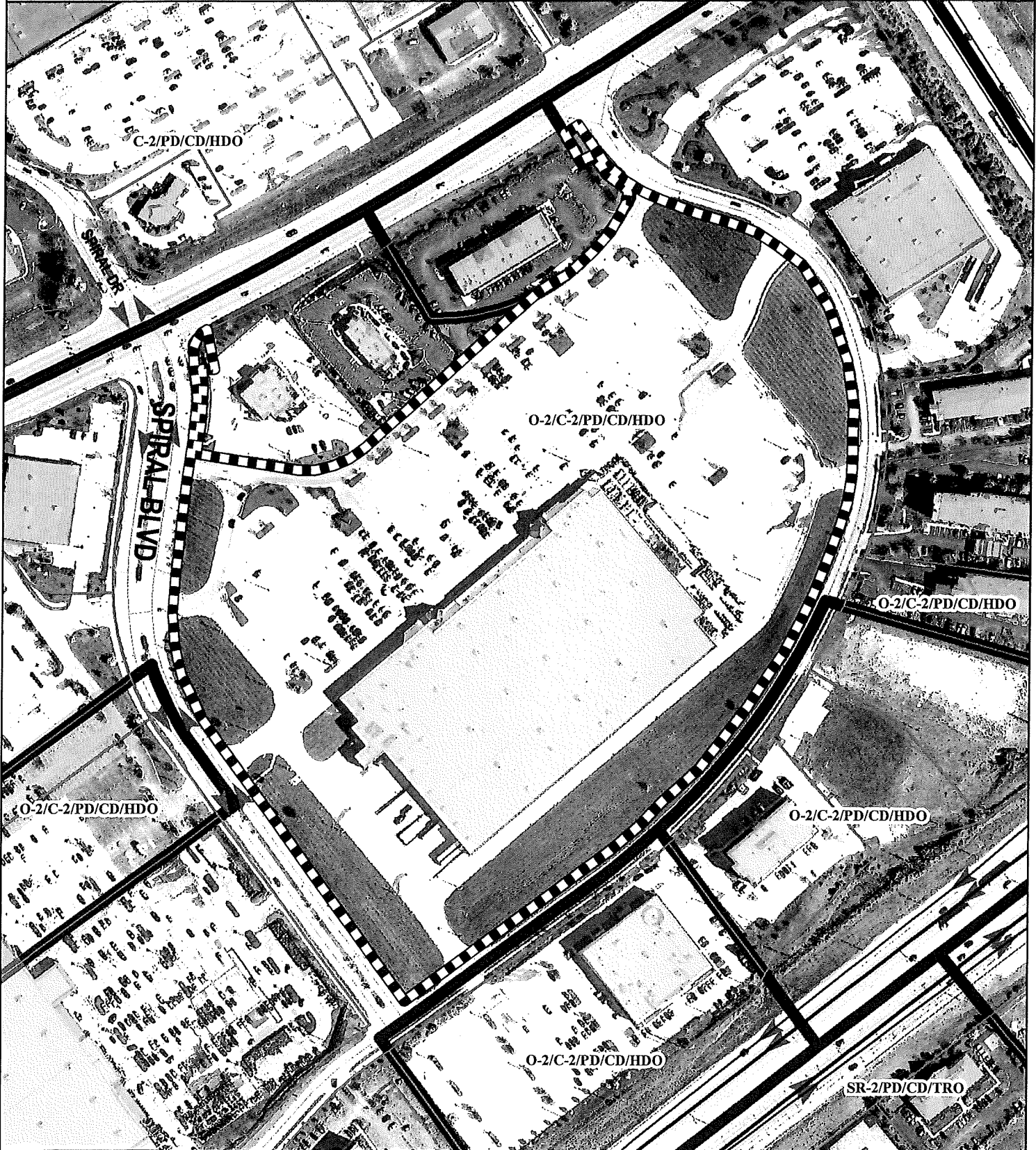


**Boone County GIS - Putting Northern Kentucky on the Map**



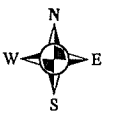
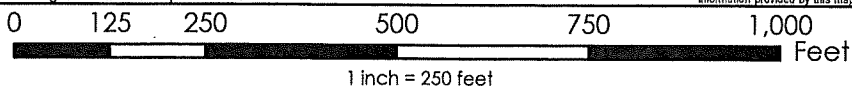
# Zoning

www.boonecountygis.com



Copyright 2019 Boone County GIS. Reproduction by any means without the express permission of the Boone County Planning Commission is prohibited.

Data used to create this map were compiled from sources that comply with National Map Accuracy Standards. Boone County GIS extends no warranty with respect to the accuracy or content of the information provided by this map. This map should be used for general planning purposes only.



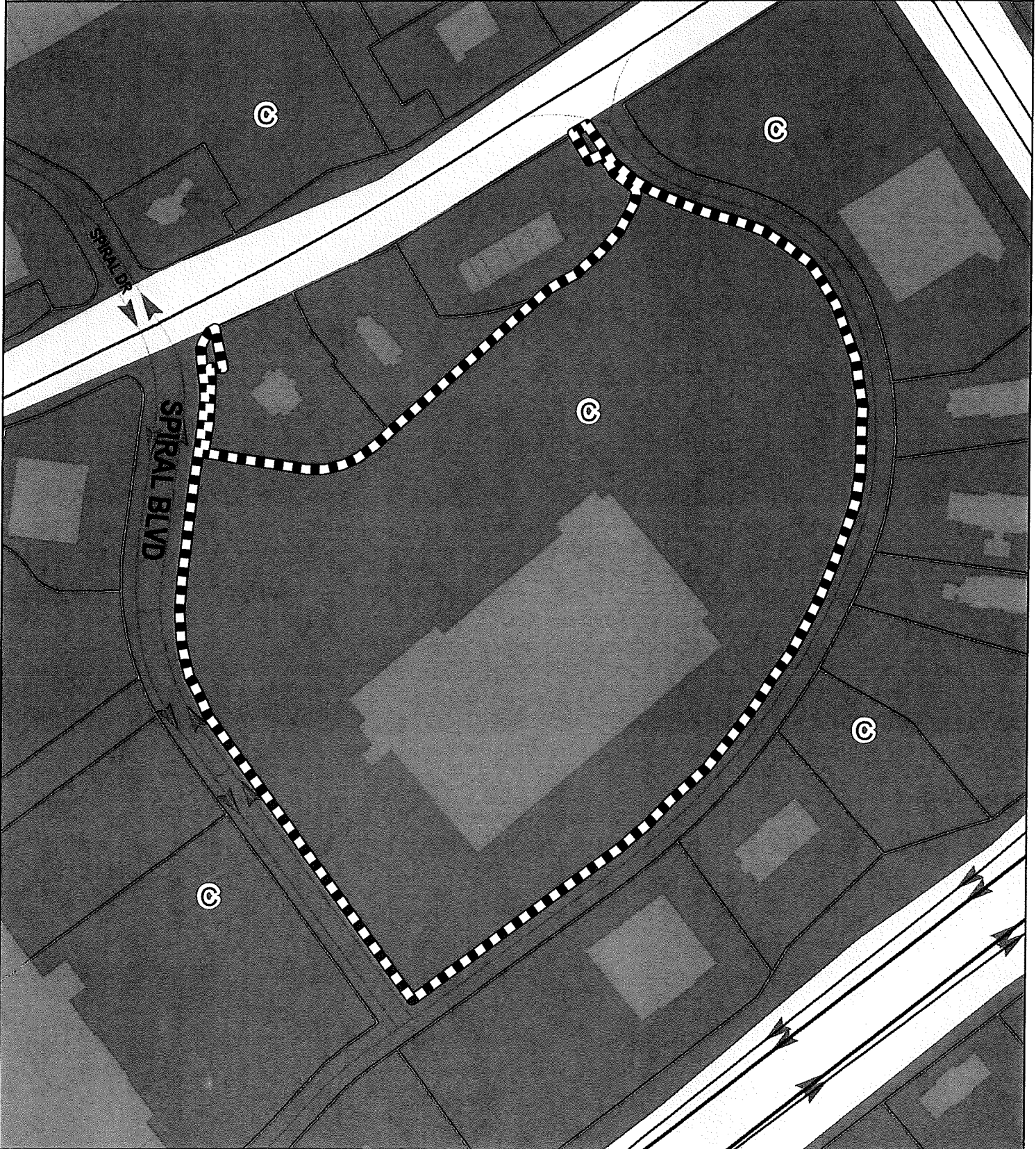
**Boone County GIS - Putting Northern Kentucky on the Map**

Map Created: xx/xx/2019

Map File: F:\GIS\MapDocs\2019\1119  
ArcMap Document: \*.mxd

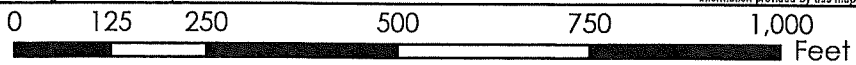
# Future Land Use

www.boonecountygis.com



Copyright 2019 Boone County GIS. Reproduction by any means without the express permission of the Boone County Planning Commission is prohibited.

Data used to create this map were compiled from sources that comply with National Map Accuracy Standards. Boone County GIS extends no warranty with respect to the accuracy or content of the information provided by this map. This map should be used for general planning purposes only.



1 inch = 250 feet



**Boone County GIS - Putting Northern Kentucky on the Map**

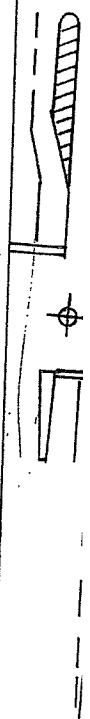
Map Created: xzjxx/2019

Map File: C:\Users\GIS\Desktop\Map 1121  
ArcMap Document: \*.mxd



# L E G E N D

PARCEL	USE	ACREAGE	BLDG. SQ. FT.	PARKING	BUILDING FLOOR PLATE	PHASE
A	OFFICE	7.00	33,000	190	3 @ 6,600 EA.	2
B	OFFICE	31.50	510,000	2,100	6 ST. @ 85,000	4
C	0-2/C-2	2.60	20,000	100	.....	3
D	0-2/C-2	2.30	23,500	75	.....	3
E	0-2/C-2	2.75	27,000	110	.....	3
F	0-2/C-2	3.00	32,000	130	.....	3
G	HOTEL	3.25	72,000	130	3 ST. @ 24,000	3
H	HOTEL	2.25	48,000	115	3 ST. @ 16,000	3
I	RETAIL	6.00	60,000	330	.....	3
J	RESTAURANT	1.90	6,300	65	.....	2
K	RESTAURANT	1.40	3,850	60	.....	2
L	RESTAURANT	1.60	5,300	65	.....	2
M	BANK	1.20	3,650	30	.....	2
N	RETAIL	27.50	276,000	1,500	.....	1
R/W	.....	5.75	.....	.....	.....	1
TOTAL	.....	100.00	1,120,000	5,000	602,400	.....



## COMMITTEE REPORT

#1

TO: Boone County Planning Commission

FROM: Fred Burch, Chairman

DATE: September 5, 1990

RE: Request of GBBN Architects (applicant) for Buchanan Development Corporation and N.K. Management (owners) for the Utilization of an Underlying Zone in Planned Development on a 101 acre site located at the southwest corner of Houston Road and the I-75/Turfway interchange, Florence, Kentucky. The site is currently zoned Office Two/Commercial Two/Planned Development (O-2/C-2/PD). Houston Lakes is the commercial and office development being proposed.

### REMARKS:

We, the Committee, recommend approval of the Houston Lakes request based upon the following Findings of Fact and with the following Conditions:

#### Findings of Fact

1. Even though the request does not agree with the specific office/commercial mixture and the commercial density recommendations of the Houston-Donaldson Study, the Committee believes that the Houston Lakes request generally meets the intent and Goals and Objectives of the Boone County Comprehensive Plan and the Houston-Donaldson Study. More specifically:

The applicant has indicated that the type of retail proposed in the Houston Lakes development is not regional in character. The size and type of retail uses are proposed to be consistent with existing retail uses in the Turfway Business Park, and draw customers from areas immediately surrounding the Houston Road area, not from the entire Northern Kentucky Area.

A major focus of the Houston Lakes development is the proposed corporate office section "B". This portion of the development is proposed to be a high-visibility office use that will help set the character of the Houston Road corridor in accordance with the Houston-Donaldson Study. With this corporate office center and with the proposed office campus use, the Committee believes that the Houston Lakes project is appropriate for the site. Without one or the other of these two important parts of the project, the Committee does not believe that the request is appropriate for the site. The Committee is founding its recommendation for approval of this request on the applicant's assurances that these portions of the submitted Concept Development Plan will be developed. These two portions of the development are critical in establishing the proper development character along both I-75 and Houston Road.

Specific references to the Boone County Comprehensive Plan and Houston-Donaldson Study are made in the 7/25/90 Staff Report.

2. With the conditions contained in this report and agreed to by the applicant, the request meets the general objectives of the Houston-Donaldson Study and Article 15 Planned Development of the Boone County Zoning Regulations, for creative development design that includes sufficient green space and appropriate appearance from the public view. Even though many of the uses are not actually proposed as strict office uses, there are enough assurances in the applicant's submitted materials and in the conditions that the overall development will be consistent with the employment district character foreseen for this important corridor area of Boone County. These assurances include the arrangement of outlots along Houston Road, building and parking setback standards, landscaping and signage standards, building design, and additional public review of specific uses.
3. The developer and applicant have agreed to make significant road improvements adjacent to the site that are recommended by the Houston-Donaldson Study, and to phase the retail portion of the development according to the status of the Houston Extension. The developer and property owner have agreed to work with other area developers, owners, the Planning Commission, and the City of Florence to help remedy the existing and forecasted traffic congestion around the Turfway Interchange. These actions are in accordance with the Houston-Donaldson Study which was designed to correlate Study Area development with infrastructure improvements.

#### CONDITIONS

The applicant is being asked to agree to include these items as part of the Concept Development Plan in order to clarify the plan as presented at the 7/25/90 Public Hearing. Further, these conditions are intended to clarify the suitable uses and development for the presented plan.

1. Since the submitted traffic analysis and facts presented at the 7/25/90 Public Hearing indicate that the Houston Road Extension to Donaldson Highway is necessary for the area road system to have a near-satisfactory level of service, the applicant and developer agree to not to take occupancy of the retail center, section "N", of the development until construction of the Houston Extension to Donaldson Highway is substantially underway.
2. The four out-parcels "C" through "F" shall undergo additional review when specific uses are proposed by the developer. This additional review shall include Technical Committee evaluation, a recommendation of that Committee to the full Planning Commission, and a Planning Commission recommendation to the City of Florence. These recommendations must determine if the proposed uses are consistent with the submitted Concept Development Plan and traffic generation figures, and present an appropriate appearance from I-75 that is consistent with the Houston-Donaldson Study.

3. The outlots "C" through "F" shall include a minimum building setback of 50 feet from the I-75 right of way and from the Spiral Boulevard right-of-way, as proposed in the submitted restrictive covenants. These setback areas shall include berming and landscaping, and the setback area along I-75 shall include no parking, circulation or other paved areas. The proposed buildings, if retail, shall be consistent in architectural design to be compatible with an office environment. No mechanical equipment, whether on the roof or ground, shall be visible from I-75 or Spiral Boulevard.
4. The proposed retail shown as "I", shall be reconfigured to address comments at the 7/25/90 Public Hearing, and as shown to the Committee. The building nearest Houston Road shall be rotated approximately 90 degrees as shown on the detail drawing submitted to the Committee by GBBN Architects. As indicated by the applicant, the retail buildings in Section "I" shall have store frontages on all sides. All roof equipment shall be screened from public view, and parking areas shall include berms and plantings between the proposed pavement and all adjacent right-of-ways. A water retention or significant landscaping feature shall be located adjacent to the Houston Road and Turfway Interchange ramps to be consistent in appearance with the Turfway Business Park.
5. Since the proposed corporate office complex is essential for the character of the development in terms of the Houston-Donaldson Study Area, the developer shall contract a professional, marketing consultant to develop strategies of attracting potential corporate users to the site. Marketing efforts shall be undertaken on a national basis and coordinated with the Planning Commission and City of Florence. Documentation of marketing efforts shall be provided. The office development shall be a minimum of 5 stories in height.
6. The proposed retail center shall have a continuous facade that is similar in concept to that employed at the Turfway Business Park. The rear and side walls of the retail center shall be constructed of the similar materials as the facade. Roof parapets shall be utilized on all sides of the buildings, as necessary, to screen all roof equipment from public view. Cross section drawings may be required as part of Architectural Design Review. As indicated by the applicant, the rear of the retail center will be screened by berms, evergreen plantings, and service court walls constructed of the finished building materials. The relative amounts of paved area and green space behind the retail center shall be generally consistent with that shown on the Concept Development Plan. The proposed retail center shall be generally consistent with the submitted Concept Development Plan in terms of number and size of users.
7. The architecture of the proposed hotels shall compliment the appearance of adjacent outlot uses.
8. Amenities, such as the pavement treatment at intersections, retention lakes, and entry features, shall be consistent with the submitted Concept

Development Plan. As indicated by the applicant, the development of the 101 acre site shall contain approximately 22 percent green space. Each phase of development submitted for review shall include calculations of green space area. All parking areas shall contain landscaping that is consistent with the Turfway Business Park. These standards include the following:

- a) Provide planting islands between every 10 to 15 spaces to avoid long rows of parked cars. The size should be a minimum of nine feet wide to allow for an adequate planting area. Each of these planting islands should provide at least one shade tree having a clear trunk height of at least six feet. For code required parking, 1 tree and 3 shrubs are to be utilized per fifteen spaces.
  - b) Provide eight-foot wide landscape strips for every four rows of parking, or large planting islands at the ends of parking rows.
  - c) In addition, canopy tree plantings along the public streets of the development shall accompany the proposed berms and 50 foot landscape setbacks submitted as part of the application.
9. Development of the proposed outlots along Houston Road shall be consistent in design with the submitted Concept Development Plan in terms of diagonal building orientation to Houston Road, access off of secondary site drives, and amount of green space.
  10. All signage within the development shall be consistent with the submitted standards, and meet the objectives of the Houston-Donaldson Study Special Sign Regulations.
  11. The developer agrees to reconstruct portions of Woodspoint Drive east of the proposed site access if sight distance limitations are determined to create potential safety hazards, by an independent traffic engineer acceptable to the Planning Commission.
  12. The Spiral Drive entrance will contain dual left turn lanes on south bound Houston Road. The improvements to the Woodspoint Drive and Houston Road intersection will include two south bound through lanes on Houston Road to include a taper back to one travel lane south of Woodspoint Drive. All improvements to public roads described in this report shall be made by the developer or owner of the site during the first phase of development, except to the extent that signalization of Spiral Drive shall be shared with the adjacent property owner as previously agreed. The construction of Spiral Drive and Spiral Boulevard to form the "ring road" shall also accompany the first phase of development. The northernmost access to the site shall be designed as an exclusive right-in and right-out drive. The design of this intersection shall include a ramp-like right turn lane to physically eliminate left turns into the site from Houston Road. The developer shall provide a separate right turn lane from Houston Road onto the southbound I-75 entrance ramp.

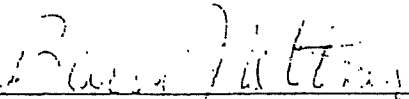
13. Spiral Drive will be connected to Woodspoint Drive along the I-75 right-of-way to allow for traffic to flow between this site and future development on the World of Sports site. All driveway entrances on Spiral Drive and Boulevard shall be properly aligned.
14. The developer agrees to work with the City of Florence, Boone County, and area property owners and developers in arriving at a solution to the existing and forecasted traffic congestion at the Turfway Interchange.
15. The main access drive shall contain three exit lanes. The developer agrees to dedicate additional right-of-way to Woodspoint Drive to allow for a future additional exit lane onto Houston Road. The developer and owner agree to participate financially with the Turfway Business Park in installing the proposed traffic signal at Spiral Drive, and to install the proposed light at Woodspoint Drive.
16. Consistent with the statements made by the applicant at the 7/25/90 Public Hearing that the section "B" office is proposed for Phase IV in four to eight years, the developer agrees to actively pursue and market development of the proposed corporate office section "B" for a period of a minimum of four years from today's date.

A copy of the Public Hearing minutes accompanies the findings and recommendation serving as a summary of the evidence and testimony presented by the proponents and opponents of this request.

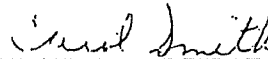


Fred Burch, Chairman

Rector Jones



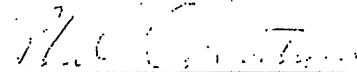
Barry Neltner



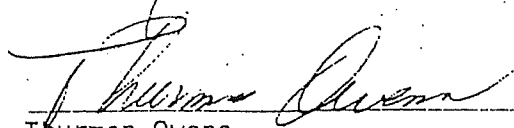
Carol Smith



Larry Barnett



Phil Damstrom



Thurman Owens

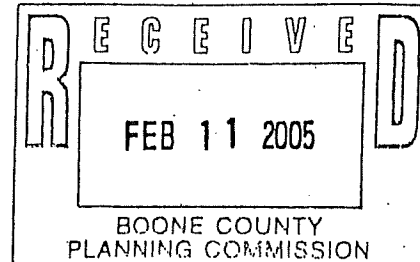
**BARRETT  
&  
WEBER**400 ATLAS BUILDING  
524 WALNUT STREET  
CINCINNATI, OHIO 45202(513) 721-2120  
FAX (513) 721-2139C. FRANCIS BARRETT  
H. PATRICK WEBER  
M. MICHELE FLEMING  
PETER E. KOENIG  
JANET L. BELL

\*\*\*

Non-lawyer Professional  
ANNE F. McBRIDE, A.I.C.P.  
Professional Land Planner

A Legal Professional Association

November 2, 1990

HAND DELIVEREDMr. Bruce W. Janken  
City Coordinator  
7931 U.S. Highway 42  
P.O. Box 457  
Florence, KY 41042Re: Buchanan Development Corporation  
Houston Lakes

Dear Bruce:

I appreciate your taking the time to meet with me on October 24, 1990 to review the status of the City Council's consideration of the Houston Lakes Development proposed by Buchanan Development Corporation, which is to be located on the east side of Houston Road and the west side of I-75, south of the I-75/Turfway interchange in the City of Florence. The mixed-use development will provide a tremendous opportunity to create a corporate office center with support services such as hotels and restaurants while providing additional retail alternatives for the residents of Florence.

At our meeting, we reviewed the Conditions for Development which are part of the approval recommendation from the Boone County Planning Commission. I understand that there were five items which the Council felt required additional review and/or explanation, as follows:

Condition #1: The Council would like a clarification as to the understanding of the construction of the Houston Extension to Donaldson Highway being "substantially underway" prior to issuance of a Certificate of Occupancy for the retail center, Section "N".

Condition #5: The Council would like a summary as to how the corporate office complex will be marketed.

## BARRETT &amp; WEBER

A Legal Professional Association

Mr. Bruce W. Janken

November 2, 1990

Page Two

Condition #6: The Council would like a clarification as to the facade treatment for the rear and side elevations of the retail center having "similar" materials as the front elevation.

Condition #14: The Council would like to know what role Buchanan Development Corporation will play in arriving at a solution to existing and forecasted traffic congestion at the Turfway Interchange.

Condition #16: The Council would like to better understand the logic behind the marketing of the corporate office center for a period of "four years."

Having had the opportunity to review these items with representatives of Buchanan Development Corporation, we believe the following information may be of use to the Council in reviewing the conditions pertaining to this matter:

Condition #1: During their initial review of the proposed development, the Boone County Planning Commission had concerns that the retail center might be operational prior to the extension of Houston Road to Donaldson Highway. Boone County was concerned that construction of the Houston Road extension might not become a reality - even if the plans were approved by the Kentucky Transportation officials and funding allocated by the state. Therefore, it was felt that, if construction started, the extension would become a reality. The additional condition of construction being "substantially underway" was then agreed upon. Thus, to alleviate any possible concern, Buchanan Development waived the ability to obtain a Certificate of Occupancy for the retail center until construction of the Houston extension was substantially underway. The major concern of Boone County has now been addressed, since the funding for the project is available, as well as being irrevocably committed to the Houston Extension. Additionally, given the construction timing of the two projects, it appears likely that the Houston Extension will be nearly complete by the time the retail center is ready to apply for a Certificate of Occupancy.

## BARRETT &amp; WEBER

A Legal Professional Association

Mr. Bruce W. Janken  
November 2, 1990  
Page Three

Condition #5: Successful marketing of the corporate office center, to be included in the Houston Lakes development, will take a partnership of public and private efforts. Buchanan Development will assert itself as the developer of the corporate office center, but the City of Florence must play a major role in the marketing of the Houston Lakes Corporate Office Center for it to be a success. Buchanan Development envisions the creation of a small committee of City representatives who would be available to review and revise the marketing efforts of Buchanan. Additionally, Buchanan would ask that the City of Florence be active in promoting the site to potential corporations or developers that contact the City in search of a new location. Without the endorsement of the City of Florence, marketing efforts for the Houston Lakes Corporate Office Center will be greatly hindered in attracting the type of corporations needed to assure the project's success.

Buchanan Development has retained the services of Shamrock Public Relations and Advertising to assist in marketing the corporate office center. Shamrock Public Relations and Advertising is preparing a proposed advertisement (draft copy attached) describing the corporate office site and is researching what trade publications are best suited to attracting a corporate office use. This research has included a review of the editorial status and readership of a variety of real estate publications plus recommendations for advertising placement. Specifically, Buchanan Development is currently considering placement of advertising in the following publications: Real Estate Review, Real Estate Finance Journal, National Real Estate Investor and Commercial Property News. These publications target corporate real estate executives, developers, investors, brokers, and mortgage bankers and lenders. Additionally, the publications Area Development Sites and Planning and Business Facilities target top level executives and

**BARRETT & WEBER**

A Legal Professional Association

Mr. Bruce W. Janken  
November 2, 1990  
Page Four

facility planners responsible for selecting site locations for a variety of corporations. The majority of these publications are monthly or quarterly and notations have been made regarding special publications such as the "Site Seekers Guide" published annually by Business Facilities. Additionally, Buchanan Development will work with both the Cincinnati Business Courier and the Cincinnati Business Record to insure participation in appropriate real estate issues.

Buchanan Development is committed to actively marketing the corporate office center and is connected with a number of local and national organizations which will be used to network the site to potential corporate offices. In addition to the Northern Kentucky and Greater Cincinnati Chamber of Commerce organizations, Buchanan will also be utilizing the efforts of the Tri-County Economic Development Corporation (Tri-ED) and the Kentucky Department of Economic Development. As a member of the National Association of Industrial and Office Parks (NAIOP), Buchanan will be using their publications, directory, conventions and staff to aggressively market the site. Buchanan Development will also work with various real estate brokers and agents in promoting the Houston Lakes Corporate Office Center as a premiere office site in the tri-state area.

Condition #6: Buchanan Development Corporation is committed to having the rear and side elevations of the proposed retail center constructed with building materials that are equal to or better than those that are used on the facade or front elevation of the center. This will provide a uniform appearance of high quality for the entire building.

Condition #14: The traffic conditions in the area can only be resolved by a joint commitment from all concerned: Boone County, the City of Florence, the State of Kentucky, and all the property owners and developers in the vicinity of the Turfway Interchange. These parties have already recognized this situation, have joined together, and have met for this

BARRETT & WEBER  
A Legal Professional Association

Mr. Bruce W. Janken  
November 2, 1990  
Page Five

purpose. Buchanan Development is committed to being and remaining an integral part of this process and is willing and anxious to be a part of the Committee being formed to address the issue of existing and forecasted traffic in this area. Additionally, Buchanan has employed the services of the traffic consulting firm of Pflum, Klausmeier and Gehrung. Buchanan will use this consulting firm to analyze the issues raised by the Committee and to identify workable solutions for implementation.

Condition #16: Buchanan Development Corporation is committed to a serious marketing effort for the portion of the Houston Lakes development designated for a corporate office center. Initially, Buchanan felt this marketing effort could be accomplished well within two years; however, the Boone County Planning Commission felt that a four year effort, although a considerable investment on the part of Buchanan, was more desirable. Also, the economy has changed, and this four year period will span significant changes in the physical state of the area as well as the development economy. Although it may be difficult to obtain financing for development in today's market, it is extremely unlikely that this situation will continue for more than a year or two. Any time period beyond this would dilute the needed intensity of effort. In other words, the longer the period, the greater the chance of losing the momentum and enthusiasm that are essential to success.

The subject property and vicinity should continue to grow and flourish during this period but Buchanan will provide the proposed infrastructure improvements as a part of the initial phase of the development. Additional roadway improvements, such as the Houston Road extension, will also be constructed during this time frame, providing an improved traffic flow in and around the Houston Lakes development. In addition to these improvements, corporations visiting the area

## BARRETT &amp; WEBER

A Legal Professional Association

Mr. Bruce W. Janken  
November 2, 1990  
Page Six

will be greeted by the construction of on-site support services (hotels, restaurants, retail businesses and financial institutions) which are desperately needed as a part of a successful corporate office center. Four years of extensive marketing of the corporate office center represent an unprecedented financial commitment on the part of Buchanan Development Corporation.

We hope that the foregoing is satisfactory and responsive to the issues raised by the City Council at their recent meeting. It is my understanding that since November 6, 1990 is an election day, the Planning and Zoning Committee may meet on either November 5 or 8 to review this information. It may then be possible to have a first reading on the proposed Ordinance at the November 13, 1990 meeting and action by Council at the November 27, 1990 meeting.

Please advise if any additional information is needed. I would appreciate your notifying me of the next meeting date for the Planning and Zoning Committee, so that we might be able to attend to answer any questions which may arise. Thank you for your cooperation and professional courtesy in reviewing this matter.

Sincerely,

BARRETT & WEBER



Anne F. McBride, AICP  
Professional Land Planner

AFM/cm  
AFM-8  
Enclosures

cc: Buchanan Development Corporation  
Robert E. Gramann, GBBN  
C. Francis Barrett, Esq.

## ROUSE, SKEES, WILSON &amp; DILLON

ATTORNEYS AND COUNSELORS AT LAW

7000 TANNERS LANE

P. O. Box 750

FLORENCE, KENTUCKY 41042-0750

H. DOUGLAS ROUSE  
HUGH O. SKEES  
DALE T. WILSON  
LARRY H. DILLON

TELEPHONE:  
(606) 371-7407  
(606) 371-7075  
TELECOPIER  
(606) 371-9072

November 16, 1990

Hon. Roger W. Rolfes, Mayor  
City of Florence, Kentucky  
Florence Municipal Building  
7431 U.S. Highway 42  
Florence, Kentucky 41042

Dear Mayor:

As directed, Hugh Skees and I met with Mr. Francis Barrett, attorney for Buchanan Development Corporation. During that meeting, we put together language based on Council's comments and ideas regarding Condition No. 1.

A copy of this language is enclosed. On review, if you have any questions, please call.

Sincerely yours,

  
DALE T. WILSON

DTW/krp  
Enclosure  
cc:

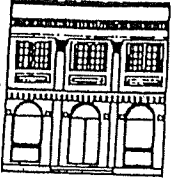
Mr. William Bailey  
Mr. William Hudson  
Dr. Ford Metzger  
Mr. James Collins  
Ms. Evelyn Kalb  
Mr. Byron Mohr  
Mr. Francis Barrett, Esq.

CONDITION NO. 1 AS MODIFIED TO ADDRESS THE CONCERNS OF THE FLORENCE CITY COUNCIL.

Since the City Council of Florence wants reasonable assurance that the extension of Houston Road all the way to Donaldson Highway will be completed and open, the applicant and developer agree not to take occupancy of the retail center, Section "N" of the development, until construction of the Houston extension to Donaldson Highway is substantially complete and there is reasonable assurance that the extension will be completed and open. In order to provide this assurance and to state clearly and definitively what must occur, the applicant and developer agree that all of the following must occur prior to occupancy of Section "N": the plans for the Houston Road extension have been prepared and approved; the funds have been allocated; all parcels of real estate necessary for the extension to Donaldson Highway have been acquired; the contract has been let; and the contractor has drawn 55% of the principal amount of the contract. The aforementioned figure refers to the construction of the extension of Houston Road from its present point of termination to Donaldson Highway. The foregoing provides reasonable and substantial assurance that the extension will be complete and open. In the event the extension is not complete and open at the time of occupancy of Section "N", the applicant and developer shall be responsible for the additional cost of police personnel necessary to assist with traffic as a result of such occupancy.

KPC

BOONE COUNTY PLANNING COMMISSION



2995 Washington Street Burlington, KY 41005  
606-334-2196 FAX 606-334-2264

March 10, 1992

Mr. C. Francis Barrett  
and  
Ms. Anne F. McBride, AICP  
Barrett & Weber  
400 Atlas Building  
524 Walnut Street  
Cincinnati, OH 45202-3114

RE: Proposed Meijer Store  
Houston Lakes, Florence, KY

Dear Mr. Barrett and Ms. McBride:

I have examined the contents of your letter dated February 25, 1992 and the plan showing a community retail center with a garden store and a convenience/gasoline service center facility. I have also reviewed the official record and application for approval of the Houston Lakes Concept Development Plan. Finally, I have met with our legal counsel, Mr. Dale Wilson, and with Mr. Roger Rolfes, City Coordinator of the City of Florence, in order to review the Boone County Planning Commission's approval documents and the City's approval documents. This letter is written based upon your official request and not based on other representatives from companies considering the development of the subject property.

In your letter, you have asked me for a determination whether the submitted information is consistent with the approved Houston Lakes Concept Development Plan and zoning conditions. Since the Houston Lakes project is complex in terms of overall size, mixture of land uses and zoning conditions, this letter will be divided into two sections: analysis and conclusion. The analysis of all the submitted information appears below along with my conclusion.

ANALYSIS OF SEPTEMBER 5, 1990 BOONE COUNTY  
PLANNING COMMISSION COMMITTEE REPORT

Condition 1 - Since the Houston Road extension is open to the public, this condition has been satisfied.

Mr. C. Francis Barrett and Ms. Anne F. McBride, AICP  
March 10, 1992  
Page 2

Condition 2 - This condition involves the outparcels along I-75 and does not pertain to Meijer, Inc., but rather with NS Group, Inc.

Condition 3 - This condition again refers to the outparcels along I-75 and does not pertain to Meijer, Inc., but rather with NS Group, Inc.

Condition 4 - This condition deals with Section I, or the proposed building located closest to the intersection of Houston Road and the I-75 southbound entrance ramp. It does not pertain to Meijer, Inc., but rather with NS Group, Inc.

Condition 5 - The Boone County Planning Commission would like to obtain additional written information from NS Group, Inc. to monitor the Company's marketing strategy for office development. For example, what is the name of the real estate broker for the office development? We would also appreciate the submission of any printed material used in marketing the office portion of the development (e.g. brochures, ads in trade magazines, etc.). This information would be helpful in assuring compliance with this zoning condition.

Condition 6 - Generally, the architecture of the proposed Meijer store shall be consistent with the existing architecture in Turfway Business Park and the standards in the Design Review procedure outlined in the Houston-Donaldson Study. This condition may require that Meijer, Inc. change the architecture of its "prototype" store in order to comply with local zoning regulations.

The proposed 206,311 square foot Meijer store with a 24,482 square foot open air garden center includes the same type of users as approved on the Houston Lakes Concept Development Plan, but only under one roof. For example, the typical Meijer store sells furniture items, home improvement items, office supply items and apparel items. It can also be classified as a department store. The proposed Meijer store does not exceed the maximum square footage allowed, or 276,000 square feet and the maximum number of retail uses permitted (6).

The proposed outlot, a 1,000 square foot convenience store with gasoline pumps, is substantially different than the approved three outlot uses (e.g. two restaurants and a bank). Specific outlot uses were identified on the approved Houston Lakes Concept Development Plan and these uses "blended in" with the professional nature of the office use and complemented the retail center. The Boone County Planning Commission and the City of Florence limited the number of and type of uses in the outlot area. Equally important was the fact that both the Boone County Planning Commission and the City of Florence promoted the need for heavy landscaping around the original

Mr. C. Francis Barrett and Ms. Anne F. McBride, AICP  
March 10, 1992  
Page 3

approved uses and for a water feature as an amenity to the park. All of this is evidenced in the July 25, 1990 Public Hearing minutes, the representations by GBBN, and by the June 20, 1990 application for zoning action. Furthermore, the area labeled as "future parking" infringes on the planned retention feature and landscaping features.

Condition 7 - This condition involves the hotel uses and does not pertain to Meijer, Inc., but rather with NS Group, Inc.

Condition 8 - This condition describes detailed pavement treatment features, amenities such as lakes and entry features, and landscaping features. Generally, it appears that what Meijer proposes is consistent with this condition with the exception of the elimination of the retention feature in the outlot area. These features will be verified at subsequent technical reviews such as Subdivision Review, Site Plan Review and Design Review.

Condition 9 - The proposed use and orientation of use by Meijer, Inc. in the outlot area (lots J, K and M) is not consistent with this condition as a convenience store with fuel pumps was not an approved use and the proposed building is not orientated diagonally.

Condition 10 - The proposed signs by Meijer are not consistent with the adopted Houston-Donaldson Study. For your use, I have enclosed a copy of the signage regulations from the existing text of the Houston-Donaldson Study and the proposed text. The existing text limits the number, size and height of free-standing and monument signs. An option may be to have NS Group, Inc. with Meijer, Inc. apply for a Special Sign District package, a process similar to the Zoning Map Amendment process. For your information, two properties in the Houston Lakes project area have a special sign district (Commonwealth Park and Turfway Commercial Park).

Condition 11 - This condition involving the upgrading of Woodspoint Drive may involve NS Group, Inc. exclusively or both NS Group, Inc. and Meijer, Inc.

Condition 12 - This condition related to road improvements will be implemented by Meijer, Inc. in the first phase of development of the Houston Lakes project.

Condition 13 - The condition related to the connection between Woodspoint Drive and Spiral Drive and who will be responsible for it is uncertain according to your letter.

Condition 14 - Meijer, Inc. and NS Group, Inc. are legally obligated to work with area government officials in order to arrive at a

solution to the traffic congestion problems at the I-75/Turfway Road Interchange.

Condition 15 - This condition describes the design of Spiral Boulevard, the dedication of additional right-of-way along Woodspoint Drive and the financial contribution for traffic signals on Houston Road. Both NS Group, Inc. and Meijer, Inc. will be responsible for this condition.

Condition 16 - This condition discusses marketing the high-rise office use for a period of 4-8 years. This condition does not pertain to Meijer, Inc., but rather with NS Group, Inc.

#### CONCLUSION/DETERMINATION OF PROPOSED USE

In reviewing the material associated with Houston Lakes Concept Development Plan, I have determined that the proposed 206,311 square foot Meijer store with a 24,482 square foot open air garden center is a permitted use and is generally consistent with the approved Concept Development Plan and specific zoning conditions. Thus, Meijer, Inc. can proceed with submitting an application for Subdivision Review, Site Plan Review and Design Review for the proposed Meijer store with an open air garden center. I have also determined that the proposed 1,000 square foot convenience store with fuel pumps is not an approved use based on the above analysis on page 2, and therefore a Public Hearing would have to be held by the Boone County Planning Commission for the review of a Change in Concept Development Plan application by Meijer, Inc. As an alternative, Meijer, Inc. or NS Group, Inc. could still develop the outlot area according to the approved restaurant and bank uses.

In addition, the submitted signage package for the proposed Meijer store is not consistent with the existing Houston-Donaldson Study. The signage options appear below.

- 1) Amend Meijer signage package to meet the current Houston-Donaldson Study requirements.
- 2) Follow the signage "trade-offs and exceptions" procedure as outlined in the current Houston-Donaldson Study.
- 3) Convince the Boone County Planning Commission and the City of Florence to amend the current signage regulations in the Houston-Donaldson Study as it impacts the Houston Lakes site. The text and zoning map of the Houston-Donaldson Study is currently being re-examined as part of the 1992 update.

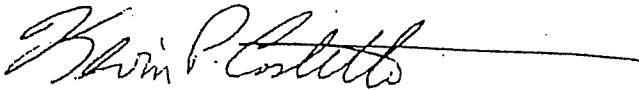
Mr. C. Francis Barrett and Ms. Anne F. McBride, AICP  
March 10, 1992  
Page 5

- 4) Submit a signage package to the Boone County Planning Commission and the City of Florence for an application of a Special Sign District for the Houston Lakes project (see Sections 1950 and 3440 of the Boone County Zoning Regulations).

In conclusion, the above analysis and determination should give your client a clearer direction in developing a Meijer store on the Houston Lakes site.

If you have any questions concerning this letter, you may contact me at your convenience at 334-2196.

Sincerely,



Kevin P. Costello, AICP  
Assistant Director / Zoning Administrator

KPC/vlm

Enclosure

cc: William D. Fromm, AICP, Director  
David A. Gehegan, Sr. Planner  
Roger Rolfes, City Coordinator, City of Florence  
Ronald Noel, NS Group, Inc.

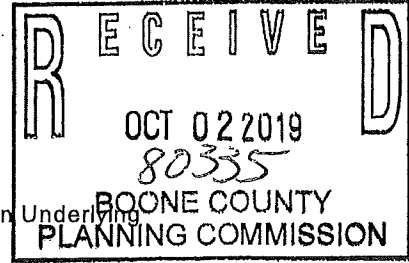
APPLICATION FORM

CONCEPT DEVELOPMENT PLAN
BOONE COUNTY PLANNING COMMISSION

SEVEN (7) COPIES OF SUBMITTED DRAWINGS ARE REQUIRED

(See Boone County Zoning Regulations)

SECTION A (To be completed by applicant)



- 1. Check One:
[X] Concept Development Plan (Includes Utilization of an Underlying Zone in a Planned Development)
Change in an Approved Concept Development Plan
Long Range Planning Committee Review (As stated in the Houston-Donaldson Study)
Long Range Planning Committee Review (As stated in the Union Town Plan)
Zone Change Committee Review of previously approved Concept Development Plan Conditions or Zoning Map Amendment Conditions
Zoning Administrator Review (As stated in the Central Florence Strategic Plan, An Update of the Parkway Corridor Study)
Zoning Administrator Review (As stated in the Mall Road District Study)

- 2. Name of Project Tesla EV Charging Station Installation
3. Location of Project 70 Spiral Drive, Florence, KY, 41042
4. Total Acreage of Site 26.32 = meijer property. Tesla project = 0.09 acres.
5. Current Zoning O2/C-2/PD/CD/HDO
6. Date of Previous Zoning Map Amendment or Approved Concept Development Plan (if applicable) N/A

7. Is the site subject to a specialized Land Use Study approved by the Boone County Planning Commission? (If so, indicate the name of the Study) N/A

- 8. Proposed Uses (please specify each use) Addition of parking area, electrical equipment, and EV Charging posts.
9. Proposed Building Intensities (please specify) N/A - Electrical & Civil work only.

10. Have you submitted a Concept Development Plan? This is the application.

- 11. Are you also applying for:
[X] Conditional Use Permit
Dimensional Variance

12. Name of Applicant(s) GPD Group - Contact: Zachary Whitney
Phone Number 206-204-7397 Fax No. E-Mail zwhitney@gpdgroup.com

13. Address of Applicant(s) 520 S. Main Street
Akron OH 44311
City State Zip

14. Name of Property Owner(s) Meijer Stores LTD
Phone Number Fax No. E-Mail

15. Address of Property Owner(s) 2929 Walker Ave NW
Grand Rapids MI 49544
City State Zip

16. Are there any existing buildings on the site? Yes
How many? (1)

17. Deed Book D793 Page No. 603 Group No.

18. Have you had a pre-application meeting with BCPC Staff? NO

19. Please check the following organizations/agencies which you have discussed the proposed development with in the last several months:

- Boone County Water District
Florence Public Services Dept.
[X] Duke Energy
Sanitation District #1

- Cincinnati Bell
- Owen Electric Cooperative, Inc.
- Boone County Public Works Department
- Kentucky Transportation Cabinet
- Boone County Building Department
- Northern Kentucky Health District
- USDA NRCS/Boone County Conservation District
- KY Division of Water
- Local School District
- Local Fire District
- Other: \_\_\_\_\_

20. Concept Development Plan Jurisdiction/Location
- Unincorporated Boone County  Walton
  - Florence  Union

21. Waiver of 60 Day Time Requirement by Originator for Final Planning Commission Action

Check if Applicable

In accordance with the provisions in KRS 100.211, the applicant(s) and property owner(s) or originators above, hereby waive the 60 day time limit for the Boone County Planning Commission to take final action on my (our) Zoning Map Amendment and/or Concept Development Plan\* application. This time limit waiver is considered effective immediately upon receipt by the Boone County Planning Commission and expires on \_\_\_\_\_.

22. ORIGINAL Property Owner's Signature By: MELJER STORES LIMITED PARTNERSHIP, By: Meijer Group, Inc., Its General Partner  
 (Faxed, Photocopied or Scanned Signatures will NOT be Accepted)

23. ORIGINAL Applicant's Signature [Signature]  
 (Faxed, Photocopied or Scanned Signatures will NOT be Accepted)

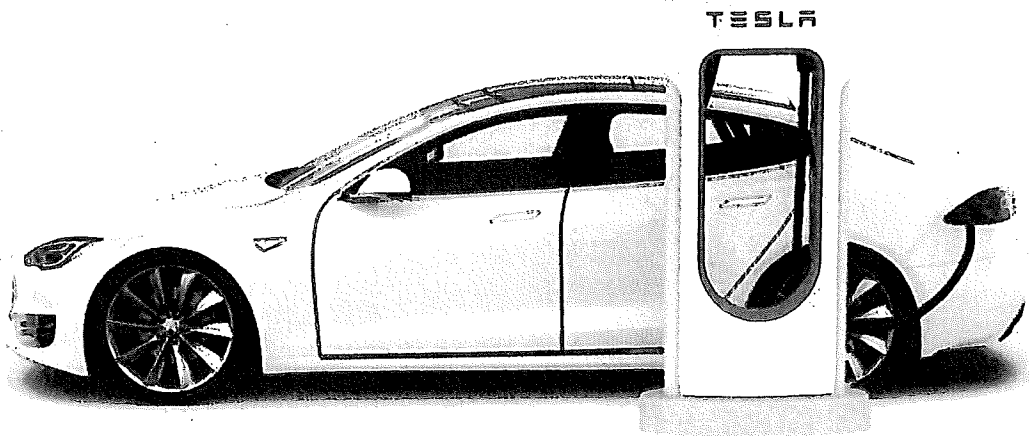
SECTION B (To be completed by BCPC Staff)

1. Date Received 10-2-19 Fee Received \$2,283.80 RH 80 335
2. Check what has been submitted:
  - Application  Fee  Legal Description
  - Concept Development Plan  Addresses of Adjoining Property Owners
  - No. of copies of plan received \*\*
3.  Date Application is Administratively Complete as Defined in KRS 100.211
4. Staff Reviewer Kelli Wall
5. Committee Chairperson \_\_\_\_\_
6. Scheduled Public Hearing Date 11/16/19
7. Boone County Planning Commission Action:
  - Approved  Approved With Conditions
  - Denied
8. Other: \_\_\_\_\_

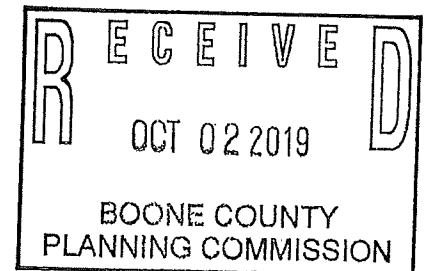
**Boone County Planning Commission**  
**Boone County Administration Building**  
**2950 Washington Street, Room 317**  
**P.O. Box 958**  
**Burlington, Kentucky 41005**  
 Phone (859) 334-2196 - Fax (859) 334-2264  
 plancom@boonecountyky.org - E-Mail  
 www.boonecountyky.org - Web Page

Note: See Boone County Planning Commission Fee Schedule for Concept Development Plan Fees. An application consists of all fees paid in full, submitted drawings and a completed application form.

# SUPERCHARGER NETWORK



Project Narrative  
Tesla Supercharger Station  
Florence, KY



**Tesla Installation Manager**  
William Busch  
Tesla Midwest Installation Program Manager  
[wbusch@tesla.com](mailto:wbusch@tesla.com)  
(818)-571-0705

**Project Contact**  
Zachary Whitney  
GPD Planning Specialist  
[zwhitney@gpdgroup.com](mailto:zwhitney@gpdgroup.com)  
206-204-7397

## Statement of Proposal

Tesla, Inc. (Tesla) is proposing the installation of an electric vehicle charging station on land zoned as Commercial Two (C-2). The use is in conformance with all required standards and criteria of the local code. This project will not have an adverse impact on the surrounding area as detailed in the below narrative.

<b>At A Glance: Idaho Springs, CO Supercharger Station</b>	
<b>Location</b>	70 Spiral Drive, Florence, KY, 41042
<b>Land Owner</b>	Meijer
<b>Utility Service Territory</b>	Duke Energy
<b># of Charging Stalls</b>	12

### Existing Conditions

#### *Location and Access*

Located off of Interstate 75 passing to and from Cincinnati, OH this project involves the installation of electric vehicle charging infrastructure that will serve the Tesla community traveling along I-75. The site is located within the Commercial Two zone, generally for commercial uses such as gas stations, shopping centers, etc. The project address is 70 Spiral Drive, Florence, KY 41042

#### *Property Owner*

Meijer Grocers

#### *Zoning*

All parcels associated with the Project are zoned as Commercial Two (C-2) by Boone County. Tesla will comply with all local, state, and federal requirements in the permitting required for this project.

#### *Current Use of Land and Adjacent Areas*

The project area is currently an existing parking lot that serves the Meijer Grocer on site. The area includes a large greenspace that Tesla will be installing new parking stalls within to accommodate the installation of Electric Vehicle Charging Infrastructure. All electrical equipment will be placed within existing greenspace, with new parking stalls being developed in the greenspace as well.

The location includes an existing grocer, and local shops that offers amenities that will be utilized by Tesla owners. This area offers an excellent location for Tesla's supercharging infrastructure, as the current market in the Kentucky area is growing rapidly and Tesla users are looking for more places to charge. This site will provide charging for customers as well as help Boone County pave the way for electric vehicle charging development in this area of Kentucky.

## **Project Overview**

This area of Boone County is an ideal location for Supercharger given its location off of Interstate 75. Given the strategic value of this location, this will benefit both regional and long-distance travelers visiting the greater area while also serving the local ownership community.

When exploring potential Supercharger locations, Tesla searches for locations that offer amenities matching the needs of our owners and their expected dwell-time, provide a safe and enjoyable environment, and have utility infrastructure sufficient to accommodate the loads of our station.

## **Project Site Plan**

As proposed, the project will provide (8) new parking stalls with charging units to the local and regional community. Additionally, the site will be developed with (4) additional new parking spaces to support the future EV charging needs in this area. No parking stalls or existing infrastructure will be removed in the process of this project.

The project will utilize Tesla Superchargers and charge posts to provide power for Tesla vehicles. Additional project components will include required utility equipment such as a new Utility transformer and switchgear assembly. All equipment will be secure and tamper proof. The installation of a Trex Enclosure to protect and screen electrical equipment will be part of this project.

## **Parking Calculation**

<b>PARKING STALL SCHEDULE</b>	
EXISTING STANDARD STALLS UTILIZED AS A RESULT OF THIS PROJECT	0
PROPOSED TESLA STALLS	8
PROPOSED FUTURE STALLS	4
NET STALL COUNT	+12

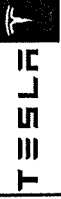
## **Location**

Tesla has specifically coordinated with the property owner to select stalls that will have the least amount of impact on the overall area. This ensures that existing clientel base does not experience any adverse impacts from the installation and reduces the likelihood of non-electric vehicles parking in the Tesla Supercharger stalls. Tesla has worked with Meijer to find a location that is best suited for the parking area and still accomodated a premium charging experince for electric vehicle owners.

## **Interconnection**

The Project is located in Duke Energy's service territory. The electrical service required for this project will be supplied by a dedicated transformer for the project shown on the site plan.

To accommodate 8 charging stalls, Tesla will install 2 Supercharger Cabinets (to be screened and secured) and 8 Charge Posts at the stall locations.



3880 LEXINGTON AVENUE  
FLORENCE, KY 41042  
502.891.1000



**GPD GROUP, INC.**

500 South Main Street  
Arlon, OH 43011  
380.972.2100 Fax 380.972.2122

GPD PROJECT NUMBER: 201814106  
DRAWN BY: LJM  
CHECKED BY: RJP/PA  
INSTALLATION MANAGER: WILLIAM BUSCH

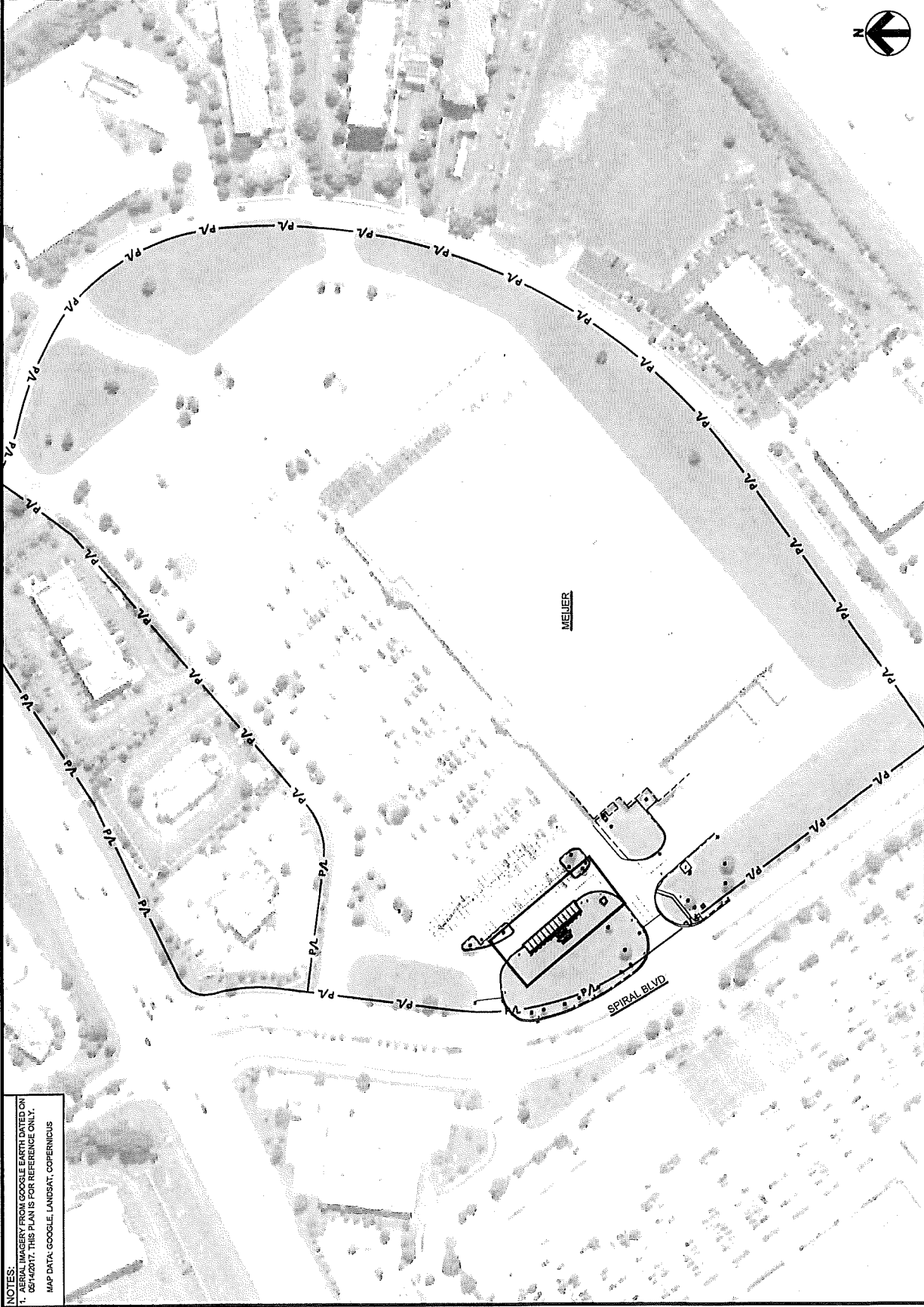
REV#	DATE	DESCRIPTION
B	10.01.19	ISSUED FOR TOWN REVIEW
A	09.20.19	ISSUED FOR SOW REVIEW

**NOT FOR CONSTRUCTION**

70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
TITLE SHEET &  
PROJECT DATA

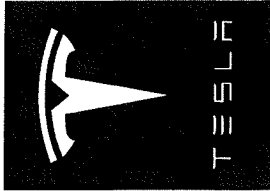
SHEET NUMBER  
T-M



SCALE: 1"=150'

MEIJER'S AERIAL SITE PLAN (STORE# - 151)

NOTES:  
1. AERIAL IMAGERY FROM GOOGLE EARTH DATED ON 05/14/2017. THIS PLAN IS FOR REFERENCE ONLY.  
2. MAP DATA: GOOGLE, LANDSAT, COPERNICUS



# TESLA

## CHARGING STATION (TESLA STATION)

70 SPIRAL DR. (TESLA STATION)  
FLORENCE, KY 41042

### SITE INFORMATION

ADDRESS:  
70 SPIRAL DR.  
TESLA STATION  
FLORENCE, KY 41042

POWER COMPANY:  
DUKE ENERGY  
1111 W. MORRIS  
CITY, KY 40301

PROPERTY OWNER:  
TESLA, INC.  
3500 DEER CREEK ROAD  
PALO ALTO, CA 94304  
(650) 671-4000

PERMITTING JURISDICTION:  
BOONE COUNTY  
2950 WASHINGTON ST., RM 317  
BURLINGTON, KY 41005  
CONTACT: KEVIN WALL  
(609) 394-2186

COUNTY:  
BOONE

DESIGN CRITERIA:  
WIND SPEED = 115 MPH 3-SECOND GUST, ULTIMATE  
EXPOSURE = C  
FROST DEPTH = 20"

GPD GROUP, INC. CONTACTS:  
PROJECT MANAGER:  
ZAGHREY SHEETS, PE - OH# 77870  
(502) 572-2148  
ZSHEETS@gpdgroup.com

PROJECT COORDINATOR:  
JOSHUA HUFFINE  
(502) 572-2153  
JHUFFINE@gpdgroup.com

UTILITY COORDINATOR:  
BRIAN GUTHRIE  
850 THURIDGE @ GPDGROUP.COM

PERMIT COORDINATOR:  
ZAGHREY SHEETS, PE  
850 THURIDGE @ GPDGROUP.COM



Know what's below  
Call before you dig.

### FLOOD HAZARD AREA NOTE

THE SITE IS LOCATED IN FLOOD ZONE "X" (AREA DETERMINED TO BE UNDESIRABLE FOR FLOOD INSURANCE). FLOOD INSURANCE MAP NUMBER 31015001200, EFFECTIVE DATE 10/04/2007.

### BEFORE SCALING & PLAN REPRODUCTION WARNING

CONTRACTORS SHALL VERIFY ALL PLANS, EXISTING DIMENSIONS, AND FIELD CONDITIONS ON THE JOB SITE AND SHALL IMMEDIATELY NOTIFY THE ENGINEER IN WRITING OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK OR BE RESPONSIBLE FOR SAME.

### APPLICABLE CODES

ALL WORK SHALL COMPLY WITH THE FOLLOWING APPLICABLE CODES:  
2018 KENTUCKY BUILDING CODE (2015 IRC)  
2011 NATIONAL ELECTRICAL CODE

IN THE EVENT OF CONFLICT, THE MOST RESTRICTIVE CODE SHALL PREVAIL.

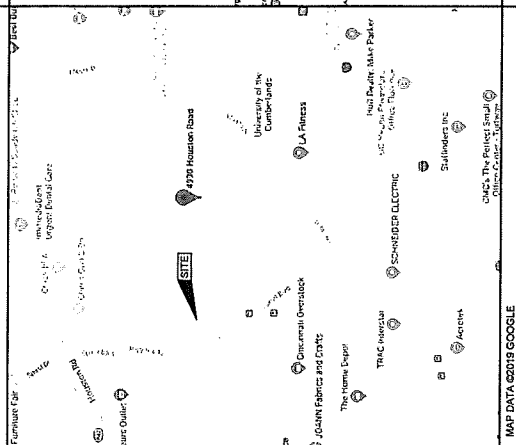
### PROJECT DESCRIPTION

- INSTALL (2) V3 SUPERCHARGER CABINETS
- INSTALL (8) V3 CHARGING POSTS
- INSTALL (1) MASTER CONTROLLER
- INSTALL (1) SWITCHGEAR ASSEMBLY
- INSTALL (1) PEDESTAL LIGHT FIXTURE(S)
- INFRASTRUCTURE FOR (1) FUTURE V3 SUPERCHARGER CABINETS
- INFRASTRUCTURE FOR (4) FUTURE V3 CHARGING POSTS

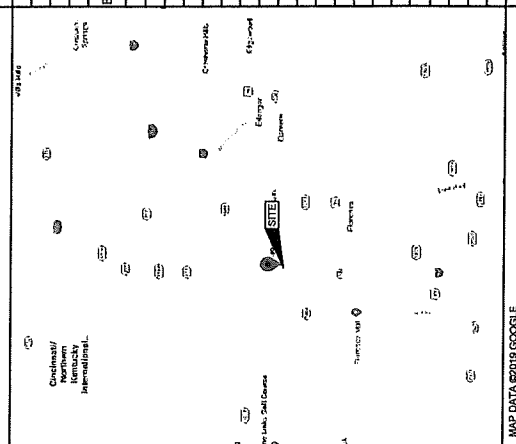
### DRAWING INDEX

T.M.	TITLE SHEET & PROJECT DATA
T-1	TITLE SHEET & PROJECT DATA
CIVIL	SHEET TITLE
GN-1	GENERAL NOTES
GN-2	GENERAL NOTES
GN-3	GENERAL NOTES
C-1	TOPOGRAPHIC SURVEY (BY OTHERS)
C-2	OVERALL SITE PLAN
C-3	DEMOLITION PLAN
C-4	SITE PLAN
C-5	EQUIPMENT DIMENSION PLAN
C-6	CIVIL DETAILS
C-7	CIVIL DETAILS
C-8	CIVIL DETAILS
ELECTRICAL	SHEET TITLE
EM-1	ELECTRICAL GENERAL NOTES
EM-2	ELECTRICAL GENERAL NOTES
E-1	OVERALL UTILITY PLAN
E-2	ELECTRICAL EQUIPMENT PLAN
E-3	SYSTEM ONE-LINE DIAGRAM
E-4	PANEL SCHEDULES
E-5	ELECTRICAL DETAILS
E-6	ELECTRICAL DETAILS
E-7	UTILITY DETAILS
G-1	GROUNDING DETAILS

### LOCATION MAP



### VICINITY MAP



NOT FOR CONSTRUCTION

IT IS VIOLATION OF THE LAW FOR ANY PERSON UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER TO ALTER THIS DOCUMENT.

70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
TITLE SHEET &  
PROJECT DATA

SHEET NUMBER  
T-1





GOVERNING BUILDING CODE: 2018 KENTUCKY BUILDING CODE (2015 IBC)

**DESIGN LOADS:**

LATERAL LOAD DESIGN DATA:  
 WIND DESIGN DATA (WDE): 7-10;  
 BASIC WIND SPEED (V): 115 MPH  
 BASIC WIND SPEED (V): 110 MPH  
 IMPORTANCE FACTOR (I): 1.0  
 EXPOSURE CATEGORY: C

SEISMIC DESIGN DATA (ASCE 7-16):  
 SEISMIC RISK CATEGORY: II (AS RISK)  
 SEISMIC IMPORTANCE FACTOR (I): 1.0  
 SHORT PERIODS (S<sub>1</sub>): 0.147  
 SHORT PERIODS (S<sub>2</sub>): 0.080  
 SHORT PERIODS (S<sub>3</sub>): 0.157  
 SHORT PERIODS (S<sub>4</sub>): 0.128

SEISMIC DESIGN CATEGORY: 0

**FOUNDATION SYSTEMS:**

**GENERAL:**

THE CONTRACTOR SHALL VISIT THE SITE PRIOR TO THE START OF ANY WORK. THE CONTRACTOR SHALL VERIFY ANY EXISTING FIELD CONDITIONS THAT MAY AFFECT THE INSTALLATION OF THE FOUNDATION SYSTEM.

THE CONTRACTOR SHALL EXERCISE GREAT CARE DURING EXCAVATION, UNDERGROUND UTILITY LOCATIONS AND NOTIFY THE ENGINEER IMMEDIATELY IF DEVIATION FROM PLANS EXIST. THE CONTRACTOR IS RESPONSIBLE FOR THE SAFE SUPPORT OF UTILITIES ACROSS EXCAVATIONS, SHEETING, SHORING, AND DEWATERING IS THE RESPONSIBILITY OF THE CONTRACTOR.

NO GEOTECHNICAL REPORT WAS PROVIDED AT THE TIME OF FOUNDATION DESIGN. A SOILS TESTING LABORATORY SHALL BE RETAINED BY THE OWNER TO PROVIDE CONSTRUCTION REVIEW TO ENSURE FOUNDATION PHASES OF THE PROJECT.

**SHIELD/TRENCH FOOTINGS:**

DETERMINATION OF FINAL BEARING ELEVATIONS, TOPSOIL AND EXCAVATION STRIPPING DEPTH, INSPECTION OF ALL SUBSOIL EXPOSED DURING STRIPPING, SITE GRADING, EXCAVATION OPERATIONS, APPROVAL OF FILL MATERIALS, DENSITY TESTING OF FILLS TO ENSURE PLACEMENT PER APPROVED SPECIFICATIONS, AND VERIFICATION OF ALLOWABLE BEARING PRESSURES ARE THE TESTING LABORATORY'S RESPONSIBILITY.

ALL FOUNDATIONS ARE TO REST ON FIRM UNDISTURBED SOIL OR COMPACTED FILL FREE FROM ORGANIC MATTER. EXCEPT WHERE AN EXCAVATION DEPTH IS SHOWN, NOTIFY OWNER'S REPRESENTATIVE BEFORE PROCEEDING WITH CONSTRUCTION.

CONTRACTOR SHALL COMPACT SUBGRADE. SEE FROSTING FROST DESIGN NOTES BELOW. FOUNDATIONS HAVE BEEN DESIGNED BASED ON AN ASSUMED ALLOWABLE SOIL BEARING CAPACITY OF 1500 PSF.

NEW FOOTINGS PLACES IS ADJACENT TO EXISTING FOOTINGS SHALL BEAR AT THE SAME ELEVATION, UNLESS NOTED OTHERWISE.

STEP FOOTINGS AT A RATIO OF ONE (1) VERTICAL TO TWO (2) HORIZONTAL WITH A MAXIMUM VERTICAL STEP OF 2'-0" UNLESS NOTED OTHERWISE.

INUNDATION AND LONG TERM EXPOSURE OF BEARING SURFACES, WHICH WILL RESULT IN DETERIORATION OF BEARING FORMATIONS, SHALL BE PREVENTED. FOOTINGS SHALL BE PLACED IMMEDIATELY FOLLOWING FOOTING EXCAVATIONS AND BEARING SURFACE INSPECTION.

UTILITY LINES SHALL NOT BE PLACED THROUGH OR BELOW FOUNDATIONS WITHOUT THE APPROVAL OF THE ENGINEER OF RECORD.

GROUNDWATER ASSUMED TO BE BELOW EXCAVATION DEPTH. IF GROUNDWATER IS ENCOUNTERED DURING EXCAVATION ON SITE, CONTRACTOR SHALL PROVIDE FOR ANY SITE DRAINAGE AND DE-WATERING REQUIREMENTS.

CONTRACTOR TO VERIFY LOCATION OF ALL EXISTING PUBLIC AND PRIVATE UTILITIES PRIOR TO EXCAVATION. IF NECESSARY, UTILITIES SHALL BE RELOCATED PRIOR TO FOUNDATION INSTALLATION.

CHARGING CABINET PRE-FABRICATED ASSEMBLY FOUNDATION & ALL CHARGING POST FOUNDATIONS - CONCRETE FOUNDATIONS SHOULD BE SUPPORTED ON A 6 INCH COMPACTED LAYER OF APPROVED FREE-DRAINING GRANULAR MATERIAL.

APPROVED MATERIAL SHOULD BE PLACED OVER THE FULL WIDTH OF THE INFILL AREA UNTIL VISIBLE DEFORMATION OF THE BACKFILL CEASES.

**SPECIAL INSPECTIONS:**  
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR SCHEDULING AND OBSERVING OF ALL SPECIAL INSPECTIONS REQUIRED BY THE AUTHORITY HAVING JURISDICTION. SPECIAL INSPECTIONS MUST BE COMPLETED PRIOR TO FINAL INSPECTION APPROVAL.

EXISTING SLAB REINFORCEMENT INVESTIGATION: CONTRACTOR SHALL VERIFY POST TENSIONING AND REINFORCEMENT LOCATION IN EXISTING CONCRETE SLAB PRIOR TO DRILLING

**CONCRETE:**

**GENERAL:**

ALL CONCRETE CONSTRUCTION SHALL CONFORM TO ACI 301-10, "STANDARD SPECIFICATION FOR STRUCTURAL CONCRETE" AND ACI 302, 305 AND 308 UNLESS NOTED OTHERWISE.

ALL DETAILING, FABRICATION AND PLACING OF CONCRETE SHALL CONFORM TO ACI 318-14, "BUILDING CODE REQUIREMENTS FOR STRUCTURAL CONCRETE" AND THE LATEST ACI "MANUAL OF STANDARD PRACTICE FOR DETAIL REINFORCED CONCRETE STRUCTURES" UNLESS NOTED OTHERWISE.

SAFETY AND PERFORMANCE OF THE STRUCTURE ARE THE RESPONSIBILITY OF THE CONTRACTOR INsofar AS THEY ARE AFFECTED BY THE LOCATION AND DETAILS OF CONSTRUCTION JOINTS. SHOP DRAWINGS OF THE PROPOSED CONSTRUCTION JOINT LOCATIONS AND DETAILS ARE TO BE SUBMITTED TO THE ARCHITECT FOR APPROVAL.

MAXIMUM SIZE OF AGGREGATE SHALL NOT EXCEED SIZE SUITABLE FOR INSTALLATION METHOD USED OR TO CLEAR DISTANCE BETWEEN OR BETWEEN REINFORCING. MAXIMUM SIZE MAY BE INCREASED TO 1 1/2 TIMES THE SIZE OF THE AGGREGATE. METHODS OF CONSOLIDATION SUCH AS VIBRATING SHALL PREVENT HONEYCOMBS OR VOIDS.

ALL CONCRETE SHALL DEVELOP A MINIMUM COMPRESSIVE STRENGTH IN 28 DAYS AS FOLLOWS: 4000 PSI. ALL CONCRETE EXPOSED TO WEATHER SHALL CONTAIN 6% (± 1%) AIR ENTRAINMENT.

REINFORCING BARS SHALL CONFORM TO ASTM A615, GRADE 60.

WELDED WIRE FABRIC REINFORCING SHALL CONFORM TO ASTM A1064 AND BE FURNISHED IN FLAT SHEETS AND INSTALLED ON CHAIRS OR PRECAST CONCRETE BLOCKS.

NO TACK WELDING OF REINFORCING IN THE FIELD IS PERMITTED.

PROVIDE CORNER BARS AT ALL LOCATIONS WHERE REINFORCEMENT CHANGES DIRECTION.

PROVIDE STRAIGHT AND DIAGONAL BARS AT EDGES OF ALL OPENINGS.

REINFORCING EMBEDMENT AND LAP SPLICES (INCHES) FOR 4000 PSI CONCRETE

BAR SIZE	ANCHORAGE	SPICE	ANCHORAGE	TOP	SPLICE
# 3	15	19	19	24	30
# 4	19	25	25	30	36
# 5	23	31	31	36	42
# 6	28	37	37	42	48

\* HORIZONTAL BARS WITH MORE THAN 12" OF CONCRETE BELOW BAR

NON-SHRINK GROUT SHALL MEET A MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 6000 PSI.

**FIBER CONCRETE MESH NOTES:**

4000 PSI CONCRETE  
 MINIMUM CEMENT FACTOR: 6.02  
 MAXIMUM W/C: 0.44  
 ENTRAINED AIR: 6.5% AVG.  
 WATERS REDUCER: NONE UNLESS HRWR OR MID RANGE WR. THEN 6" - 8"  
 RETARDER: NORMAL TYPE D AS NEEDED (REQUIRED IF CONCRETE TEMPERATURE EXCEEDS 85° F)  
 ACCELERATOR: NONE - 50% MAX. UNLESS TYPE ONLY THE USE OF CALCIUM CHLORIDE IS PROHIBITED.  
 FIBER: 1.5' @ 1.5 LBS. PER CUBIC YARD (AS FIBERMESH 300 OR EQUIVALENT)

\*ONS: DESIGNATES A CONCRETE MIX DESIGN WITH 2 GALLON PER CUBIC YARD OF CALCIUM NITRIDE CORROSION-INHIBITOR AT 7.5% SILICA FUME.

\*F: DESIGNATES A CONCRETE MIX DESIGN WITH 1.5 LBS. FIBERGLASS MONOPOLYMER FIBER 1.5 INCHES IN LENGTH REINFORCEMENT PER CUBIC YARD OF CONCRETE

**STRUCTURAL STEEL**

**GENERAL:**

MATERIAL PROPERTIES:  
 ASTM A36 UNO  
 PLATE:  
 PIPE:  
 TUBE:  
 ASTM A106 GRADE A (Fy = 50 ksi)

DETAILING, FABRICATION, AND ERECTION SHALL CONFORM TO THE 2010 AISC (602-10) SPECIFICATIONS.

ALL WELDING SHALL BE DONE USING E-70XX ELECTRODES IN ACCORDANCE WITH AWS D1.1 SPECIFICATIONS.

FIELD VERIFY ALL CONDITIONS AT AND CONNECTIONS TO THE EXISTING CONSTRUCTION BEFORE FABRICATION.

ALL EXPOSED STRUCTURAL STEEL, ANCHOR RODS AND BOLTS SHALL BE HOT DIP GALVANIZED PER ASTM A123.

UNLESS NOTED OTHERWISE ON THE DRAWING, ALL ANCHOR RODS SHALL CONFORM TO ASTM F1954 G-05 WITH HEAVY HEXAGONAL NUT.

SUBMIT FABRICATION AND ERECTION DRAWINGS SHOWING ALL DETAILS, CONNECTIONS, MATERIAL DESIGNATIONS, AND TOP STEEL ELEVATIONS FOR APPROVAL. THE SHOP DRAWINGS WILL BE REVIEWED FOR GENERAL CONFORMANCE TO THE CONTRACT DRAWINGS. SUCH APPROVAL SHALL NOT RELIEVE THE FABRICATOR/CONTRACTOR OF THE OBLIGATION TO PROVIDE THE BEST QUALITY WORK AND TO PROVIDE THE DETAILS AND ERECTION DRAWINGS OR THE GENERAL FIT-UP OF PARTS THAT ARE TO BE ASSEMBLED IN THE FIELD.



GPD PROJECT NUMBER: 2019 44 06  
 DRAWN BY: UJL  
 CHECKED BY: RBE/PA  
 INSTALLATION MANAGER: WILLIAM BUSCH

REV	DATE	DESCRIPTION
B	10.03.19	ISSUED FOR 100% REVIEW
A	09.20.19	ISSUED FOR 50% REVIEW

**NOT FOR CONSTRUCTION**

THIS IS A PORTION OF THE PLAN FOR ANY PERSON UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT.

70 SPIRAL DR.  
 (TESLA STATION)  
 FLORENCE, KY 41042

SHEET TITLE  
 GENERAL NOTES

SHEET NUMBER  
 GN-3

No.	Description	By	Date

**SITE NAME:** Florence, KY

**PROPERTY DESCRIPTION:**  
 Remainder of Lot 8 of Florence Urban Subdivision, as recorded in Plat Book 2 of 2017, in Boone County, Kentucky.

**SCHEDULE B2 EXCEPTIONS:**

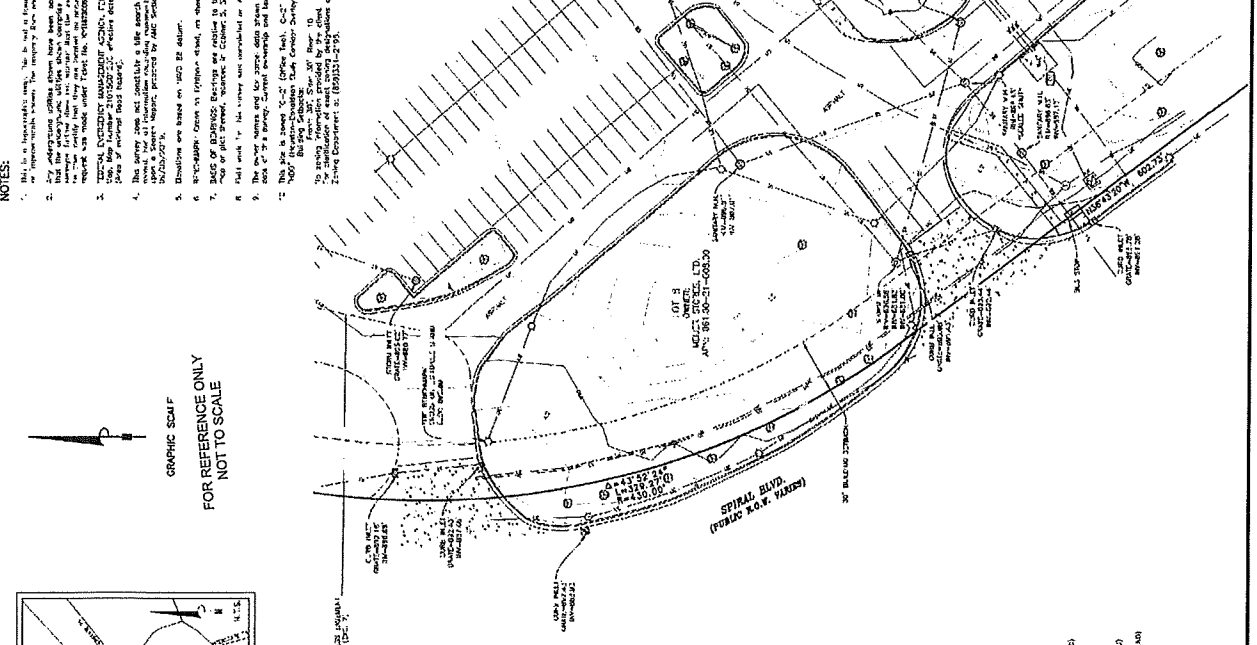
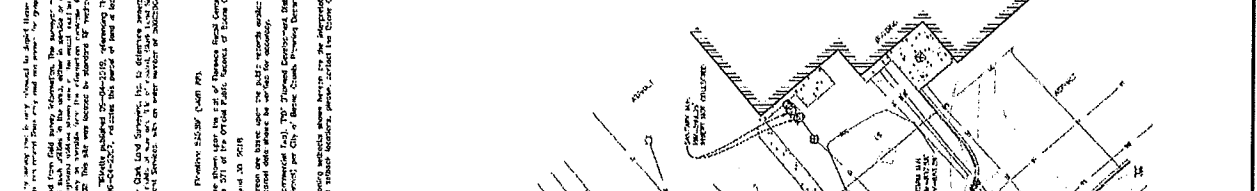
1. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
2. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
3. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
4. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
5. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
6. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
7. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
8. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
9. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
10. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
11. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
12. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
13. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
14. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
15. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
16. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
17. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
18. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
19. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.
20. THE USE OF LAND AND STRUCTURES IS SUBJECT TO ALL APPLICABLE ZONING ORDINANCES, LOCAL ORDINANCES, AND STATE AND FEDERAL LAWS, REGULATIONS, AND DECISIONS.

**SURVEYOR'S STATEMENT:**  
 I, the undersigned, being duly qualified and sworn as a Professional Land Surveyor in the State of Kentucky, do hereby certify that I am the author of the foregoing plat and that the same is a true and correct representation of the facts as shown to me by the client and as shown on the ground. I have not been furnished with any title insurance policy or other evidence of title, and I have not been furnished with any other information that would require me to make any correction or addition to the foregoing plat. I have not been furnished with any other information that would require me to make any correction or addition to the foregoing plat. I have not been furnished with any other information that would require me to make any correction or addition to the foregoing plat.

**NOTES:**

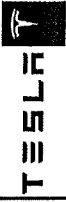
1. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
2. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
3. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
4. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
5. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
6. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
7. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
8. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
9. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
10. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
11. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
12. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
13. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
14. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
15. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
16. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
17. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
18. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
19. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.
20. The lot lines shown on this plat were determined by a traverse survey conducted by the undersigned on or about the date shown on the plat. The bearings and distances were measured in the field and reduced to mean sea level. The bearings were measured with a total station and the distances were measured with a steel tape. The bearings were measured to the nearest second and the distances were measured to the nearest 0.01 foot.

**GRAPHIC SCALE**  
 FOR REFERENCE ONLY  
 NOT TO SCALE



**LEGEND**

1	1" FT FINISH
2	2" FT FINISH
3	3" FT FINISH
4	4" FT FINISH
5	5" FT FINISH
6	6" FT FINISH
7	7" FT FINISH
8	8" FT FINISH
9	9" FT FINISH
10	10" FT FINISH
11	11" FT FINISH
12	12" FT FINISH
13	13" FT FINISH
14	14" FT FINISH
15	15" FT FINISH
16	16" FT FINISH
17	17" FT FINISH
18	18" FT FINISH
19	19" FT FINISH
20	20" FT FINISH



300 SOUTH HIGH STREET  
PULASKI, OHIO 44131  
419.241.1000



300 South High Street  
Apt. 101, OH 44131  
330.572.1101 Fax: 330.572.1102

GPD PROJECT NUMBER: 2018K106  
DRAWN BY: LJM  
CHECKED BY: BHEPM  
INSTALLATION MANAGER: WILLIAM BUSH

REV	DATE	DESCRIPTION
B	10.20.18	ISSUED FOR 100% REVIEW
A	09.20.18	ISSUED FOR 50% REVIEW

NOT FOR  
CONSTRUCTION

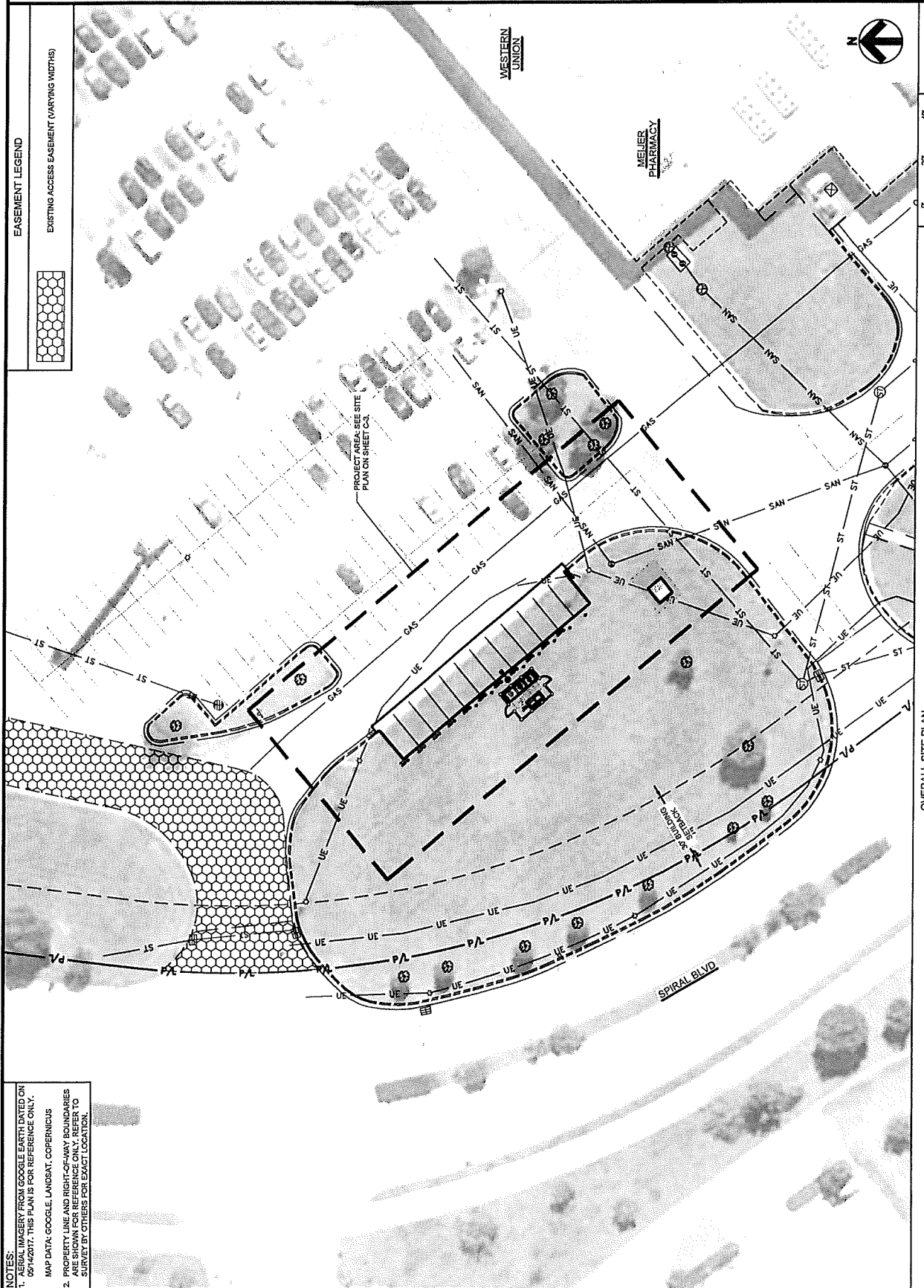
THIS PLAN IS THE PROPERTY OF GPD GROUP, INC. AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF GPD GROUP, INC.

70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
OVERALL SITE  
PLAN

SHEET NUMBER  
C-1

EASEMENT LEGEND  
EXISTING ACCESS EASEMENT (VARYING WIDTHS)



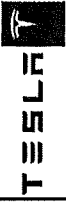
SCALE: 1"=40'

OVERALL SITE PLAN

NOTES:

1. AERIAL IMAGERY FROM GOOGLE EARTH DATED ON 05/14/2017. THIS PLAN IS FOR REFERENCE ONLY.
2. MAP DATA: GOOGLE, LANDSAT, COPERNICUS
3. PROPERTY LINE AND RIGHT-OF-WAY BOUNDARIES SHOWN ARE BASED ON SURVEY BY OTHERS FOR EXACT LOCATION.





PAVED UNDERGROUND CONDUITS AND TRENCHES SHALL BE INSTALLED PER THE LATEST EDITION OF THE NATIONAL ELECTRICAL CODE (NEC).



300 South Main Street  
Aurora, OH 44211  
336.572.2100 Fax 336.572.2102

GPD PROJECT NUMBER: 2019-01-04  
DRAWN BY: JLM  
CHECKED BY: BDEPM  
INSTALLATION MANAGER: WILLIAM BUECH

REV	DATE	DESCRIPTION
8	10.31.19	ISSUED FOR 100% REVIEW
7	10.20.19	ISSUED FOR 60% REVIEW
6		
5		
4		
3		
2		
1		

NOT FOR CONSTRUCTION

IT IS A POLICY OF THE UNIVERSITY OF KY. THAT PERSONS WHOSE NAMES ARE LISTED ON THIS DOCUMENT, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, SHALL NOT ALTER THIS DOCUMENT.

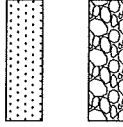
70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
SITE PLAN

SHEET NUMBER  
C-3

CONSTRUCTION KEY NOTES AND LEGEND (P)

- PROPOSED PAD MOUNTED ELECTRICAL UTILITY TRANSFORMER (BY UTILITY). CONTRACTOR SHALL PROVIDE CONCRETE PAD PER UTILITY REQUIREMENTS. SEE ELECTRICAL PLANS FOR PROPOSED ROUTING.
- PROPOSED EQUIPMENT CLEAR SPACE (TYPICAL).
- PROPOSED ELECTRIC METER MOUNTED TO SWITCHGEAR PER ELECTRIC COMPANY'S SPECIFICATIONS AND DETAILS ON ELECTRICAL SHEETS.
- PROPOSED SWITCHGEAR ASSEMBLY PER ELECTRICAL DRAWINGS. SEE DETAIL ON SHEET C-7 FOR ANCHORING.
- FOUNDATION (TYPICAL OF 8). SEE DETAILS ON SHEET C-8.
- PROPOSED TESLA NON-ILLUMINATED PARKING SIGN (TYPICAL OF 8). SEE DETAILS ON SHEET C-6. SEE CHARGING POST SCHEDULE THIS SHEET FOR SIGN TYPE. WHERE APPLICABLE, CONTRACTOR SHALL MOUNT SIGN TO LIGHT POLE IN VIEW OF POST.
- PROPOSED TESLA CHARGING CABINET WITH CONCRETE FOUNDATION (TYPICAL OF 2). SEE DETAILS ON SHEET C-6 & C-7.
- PROPOSED CONCRETE PAD. SEE DETAIL ON SHEET C-7.
- PROPOSED LIGHT POLE. SEE DETAIL ON SHEET C-7. SEE ELECTRICAL DRAWINGS FOR POLE AND FIXTURE SPECIFICATIONS AND WIRING.
- PROPOSED TREX "SECURISION" FENCING. SEE DETAILS ON SHEET C-8.
- PROPOSED TREX "SECURISION" SINGLE GATE WITH LESPAD AND PANIC HARDWARE (TYPICAL OF 2). SEE DETAILS ON SHEET C-8.
- PROPOSED PAINTED 4" WIDE SOLID WHITE STRIPES. SEE STRIPING NOTES ON SHEET C-8.
- PROPOSED 6" CONCRETE CURB. SEE DETAIL ON SHEET C-7.
- ALL DISTURBED AREAS NOT TO BE PAVED OR MULCHED SHALL BE SODED PER LANDSCAPE/IRRIGATION NOTES ON SHEET G-2.
- PROPOSED MASTER CONTROLLER MOUNTED ON I-FRAME. SEE ELECTRICAL SHEETS.
- PROPOSED INFRASTRUCTURE FOR FUTURE CHARGING CABINET.
- PROPOSED INFRASTRUCTURE FOR FUTURE CHARGING POSTS (TYPICAL OF 4).
- EXISTING ELECTRICAL LINE TO BE RELOCATED AS NECESSARY.



V3 (250kW) TESLA OUTDOOR EQUIPMENT SCHEDULE

DESCRIPTION	PART NUMBER	QUANTITY
TESLA EQUIPMENT		
CHARGING CABINET	1450796-00-D	2
CHARGING POST	1088565-00-D	8
VERSION 3		
SITE MASTER CONTROLLER	1137202-02-C	1
INDUSTRIAL SITE CONTROLLER		
UNIVERSAL PRECAST	1478598-00-A	8
CHARGING POST FOUNDATION		
ASTY CONST. TEMPLATE	1521278-00-A	2

CHARGING POST CIRCUIT SCHEDULE

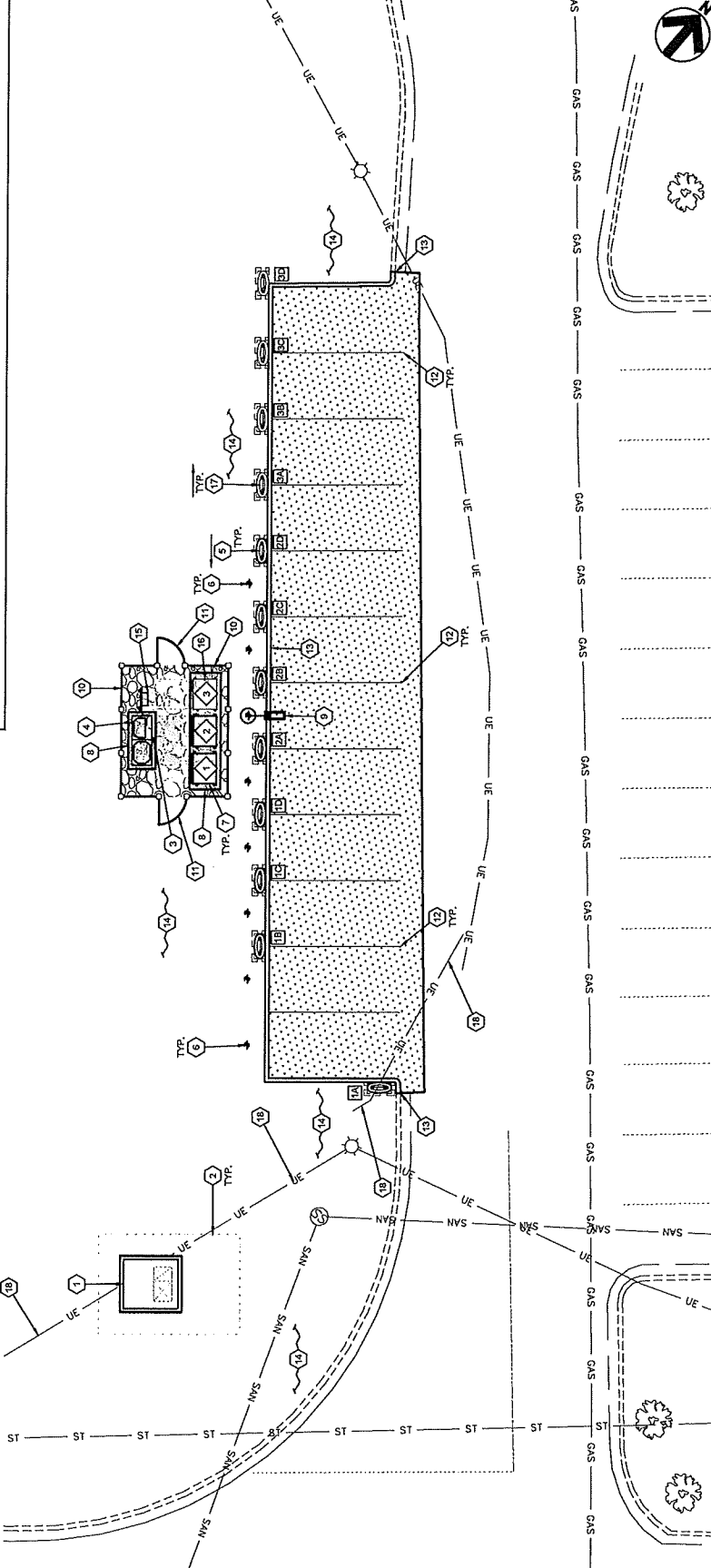
CHARGING CABINET	CHARGE POST	DEDICATED/ENABLED
1	1A	DEDICATED
	1B	DEDICATED
	1C	DEDICATED
	1D	DEDICATED
	1E	ENABLED
	1F	ENABLED
	1G	ENABLED
	1H	ENABLED
	1I	ENABLED
	1J	ENABLED
	1K	FUTURE
	1L	FUTURE
	1M	FUTURE
	1N	FUTURE

PARKING STALL SCHEDULE

EXISTING STANDARD	EXISTING PROJECT	RESULT OF THIS PROJECT
0	0	DEDICATED
8	8	PROPOSED TESLA STALLS
4	4	PROPOSED FUTURE STALLS
+12		NET STALL COUNT

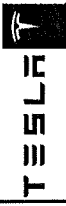
GENERAL SHEET NOTES

- CONTRACTOR SHALL REMOVE EXISTING PAVEMENT AND INSTALL PROPOSED PAVED UNDERGROUND CONDUITS AND TRENCHES. REPLACE PAVEMENT AND/OR CURB AFTER CONDUITS HAVE BEEN INSTALLED. SEE ELECTRICAL DRAWINGS FOR CONDUIT RUN LENGTHS AND TRENCH DETAILS. CONTRACTOR SHALL MEET OR EXCEED EXISTING PAVEMENT SPECIFICATIONS. NOTIFY ENGINEER OF ANY DISCREPANCIES PRIOR TO PERFORMING WORK.
- APPLY LIQUID ASPHALT AT ALL JOINTS BETWEEN CONCRETE AND ASPHALT AND WHERE PROPOSED ASPHALT MEETS EXISTING, INCLUDING SAW CUT JOINTS.
- FOR TRAFFIC CONTROL PROCEDURES (IF APPLICABLE), SEE TRAFFIC CONTROL NOTES ON SHEET G-1.



SCALE: 1"=15'

SITE PLAN



7000 LEXINGTON AVENUE  
FLORENCE, KY 41042  
859.251.2100



**GPD GROUP, INC.**  
503 South Main Street  
Apt. 01, 44311  
330.672.2100

GPD PROJECT NUMBER: 2018A1038  
DRAWN BY: LMJ  
CHECKED BY: R992PM  
INSTALLATION MANAGER: WILLIAM BUSH

REV	DATE	DESCRIPTION
B	10.01.18	ISSUED FOR 100% REVIEW
A	09.20.18	ISSUED FOR 50% REVIEW

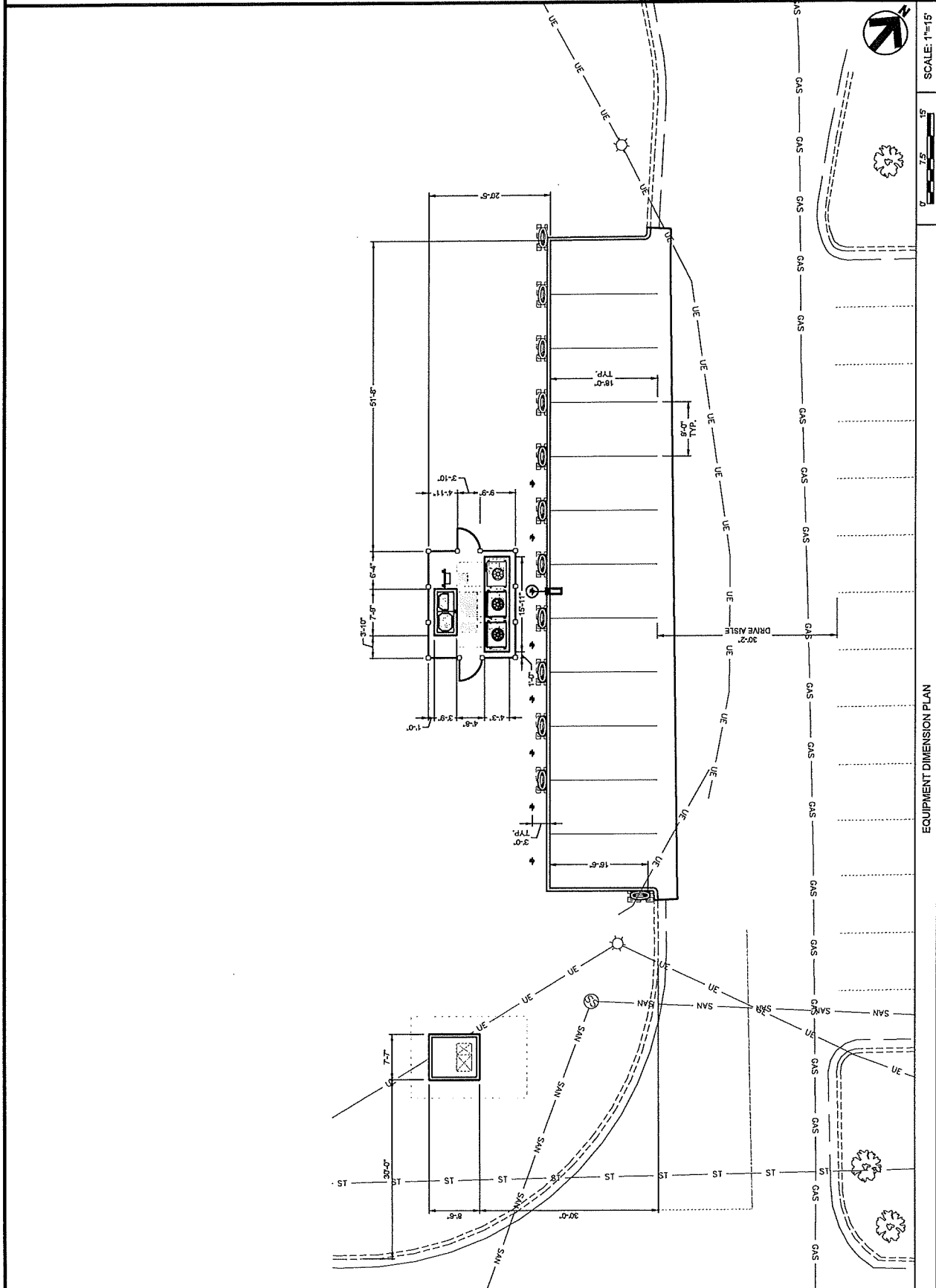
**NOT FOR CONSTRUCTION**

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNDER PENALTY OF FINE AND/OR IMPRISONMENT, TO ALTER THIS DOCUMENT.

70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
EQUIPMENT  
DIMENSION PLAN

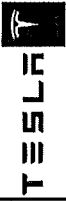
SHEET NUMBER  
C-5



EQUIPMENT DIMENSION PLAN

SCALE: 1"=15'





THE CHARGE CABLES AND  
POLYMER CONCRETE  
FORMS ARE NOT TO SCALE



500 South Main Street  
Aurora, OH 44201  
330.972.2100 Fax 330.972.2102

GPD PROJECT NUMBER:	201914159
DRAWN BY:	LJL
CHECKED BY:	RFP/NU
INSTALLATION MANAGER:	WILLIAM BURCH

REV	DATE	DESCRIPTION
B	10.01.18	ISSUED FOR 100% REVIEW
A	09.23.18	ISSUED FOR 50% REVIEW

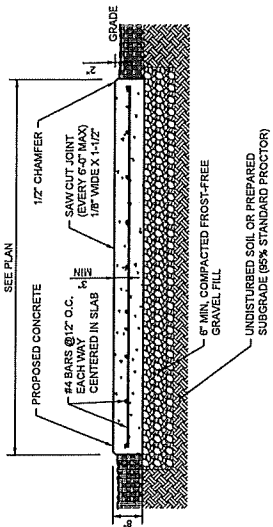
NOT FOR  
CONSTRUCTION

THE LOCATION OF THE LINES FOR ANY PERIOD  
UNLESS THEY ARE ACTING UNDER THE DIRECTION  
OF A LICENSED PROFESSIONAL ENGINEER.

70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
CIVIL DETAILS

SHEET NUMBER  
C-7

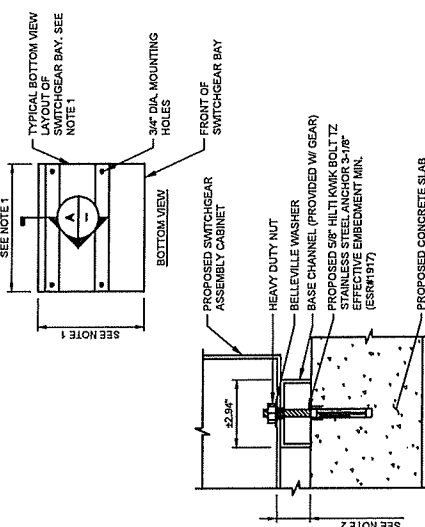


- CONCRETE PAD NOTES & CONCRETE SPECIFICATIONS:**
- FOUNDATION AREA SHALL BE EXCAVATED TO THE DEPTH AND DIMENSIONS SHOWN ON THE PLANS. OTHER UNSUITABLE MATERIAL SHALL BE REMOVED AND LEGALLY DISPOSED OF OFF-SITE. THE SOIL SHALL BE ROLLED WITH A 1-TON VIBRATORY WALKBEHIND ROLLER AT A SPEED OF 3 MPH TO 4 MPH. THE UNDERCUT SOFT OR "WEAVING" AREAS A MINIMUM OF 12 INCHES DEEP. BACKFILL UNDERCUT AREA WITH FILL MEETING THE SPECIFICATIONS OF STRUCTURAL FILL.
  - CONCRETE TO HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH (f'c) = 4000 PSI.
  - ALL REINFORCING TO HAVE MINIMUM CONCRETE COVER PER ACI SPECIFICATIONS.
  - ALL CONCRETE MATERIALS AND WORKMANSHIP SHALL CONFORM TO ACI 318-14 AND APPLICABLE STATE BUILDING CODE.
  - MINIMUM SOIL SURFACE SLOPES TO MAINTAIN A MINIMUM OF 6" OF SOIL COVER TO THE BOTTOM OF THE PAD.

CONCRETE PAD DETAIL

1

N.T.S.



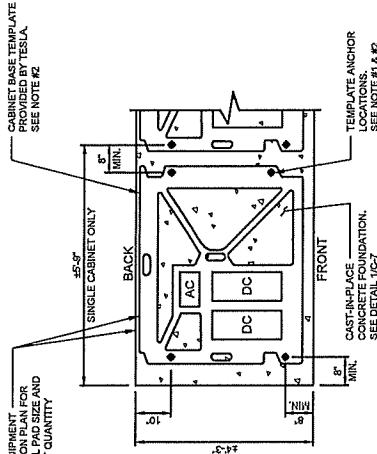
- NOTES:**
- SEE FLOOR PLAN ON ELECTRICAL SHEETS FOR MOUNTING HOLE LOCATION AND SWITCHGEAR BAY DIMENSIONS.
  - SEE ELEVATION ON ELECTRICAL SHEETS FOR BASE CHANNEL DIMENSION.
  - MINIMUM EDGE DISTANCE FOR PROPOSED ANCHORAGE SHALL BE 6".

SWITCHGEAR MOUNTING DETAIL

2

N.T.S.

- NOTES:**
- CHARGING CABINET SHALL BE ANCHORED TO CAST-IN-PLACE FOUNDATION WITH (4) 5/8" DIA. HILT. ANCHORS. HILTS SHALL BE SPACED AND INSTALLED BY CONTRACTOR. PROVIDE 6" MIN. EDGE DISTANCE AND 6" MIN. BOLT SPACING.
  - CHARGING CABINET BASE TEMPLATE SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL COORDINATE WITH TESLA FOR EXACT DIMENSIONS AND SPECIFICATIONS.

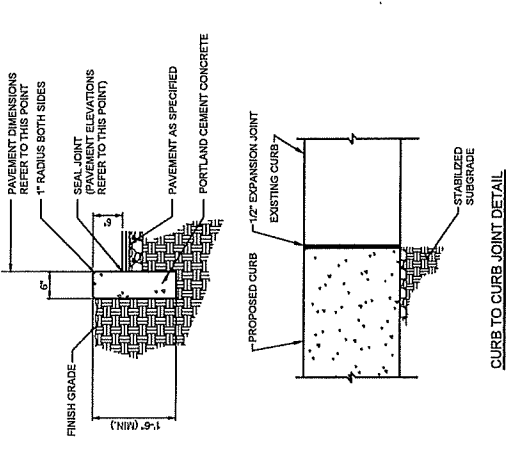


PLAN VIEW

CHARGING CABINET FOUNDATION DETAIL

3

N.T.S.



- NOTE:**  
CONTRACTOR TO ASSESS EXISTING CURBS AND INSTALL PROPOSED CURBS TO MATCH. ABOVE DETAILS TO BE USED AS MINIMUM STANDARDS.

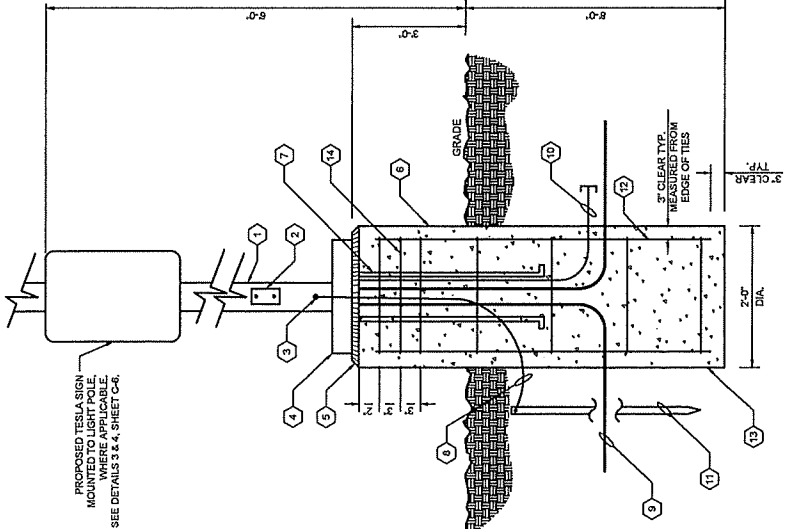
P.C.C. CURB DETAIL

4

N.T.S.

**KEYED NOTES**

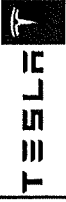
- POLE TO MATCH EXISTING IN HEIGHT, TYPE AND COLOR.
- HAND HOLE
- GROUND LUG
- BOLT COVER
- 3/4" CHAMFER
- RIB CONCRETE TO REMOVE SOURCE FORM LINES AND FILL ALL HOLES FOR SMOOTH FINISH. WEATHERSEAL ALL EXPOSED CONCRETE.
- ANCHOR BOLTS SUPPLIED WITH POLE. INSTALL PER POLE VENDOR DRAWINGS.
- #6 CU. BASE GROUND CONDUCTOR
- BRANCH CIRCUIT WIRING IN CONDUIT AT 24" (MINIMUM) BELOW GRADE
- 1" SPARE CONDUIT STUBBED INTO POLE BASE & CAPPED 12" FROM BASE
- 3/4"x16-0" CU. CLAD GROUND ROD
- (Ø) #6 REBAR (VERTICAL) TIE-WIRED TO #4 REBAR TIES AT 12" ON CENTER, U.N.O.
- REINFORCED CONCRETE, 4000 PSI COMPRESSIVE STRENGTH
- ADDITIONAL #4 REBAR TIES @ 5" ON CENTER



LIGHT POLE FOUNDATION DETAIL

6

N.T.S.



SMALL BUSINESS OWNED  
PUBLICLY TRADED COMPANY  
NASDAQ: TSLA



520 South Main Street  
Aurora, OH 44201  
380.572.2100 Fax 380.572.2102

GPD PROJECT NUMBER	2018141-14
DRAWN BY:	UM
DESIGNED BY:	RP/EM
INSTALLATION MANAGER:	WILLIAM BUSH

REV	DATE	DESCRIPTION
B	10.01.18	ISSUED FOR 10% REVIEW
A	02.20.18	ISSUED FOR 50% REVIEW

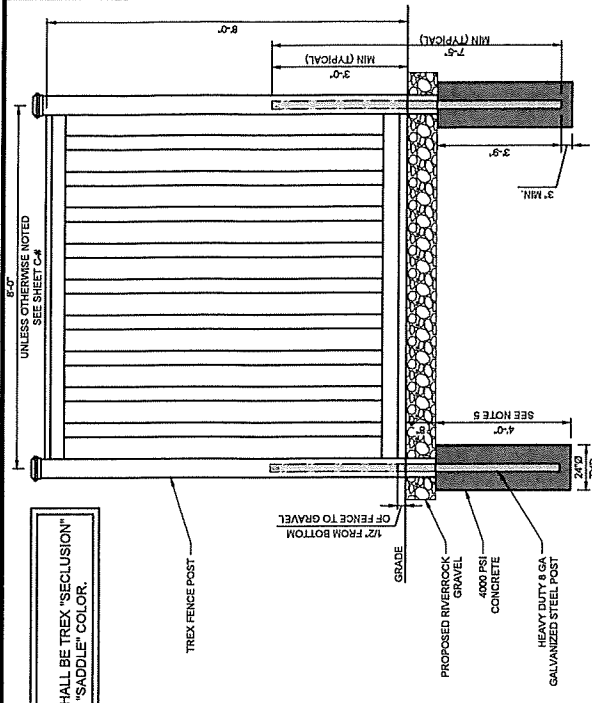
**NOT FOR  
CONSTRUCTION**

IT IS A VIOLATION OF THE LAW FOR ANY PERSON  
UNLESS THEY ARE ACTING UNDER THE DIRECTION  
OF A LICENSED PROFESSIONAL ENGINEER TO  
ALTER THIS DOCUMENT.

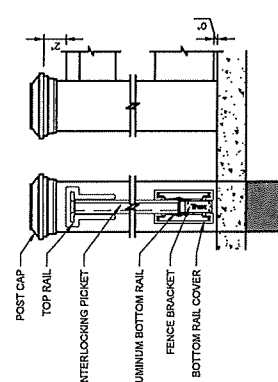
70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
CIVIL DETAILS

SHEET NUMBER  
C-8



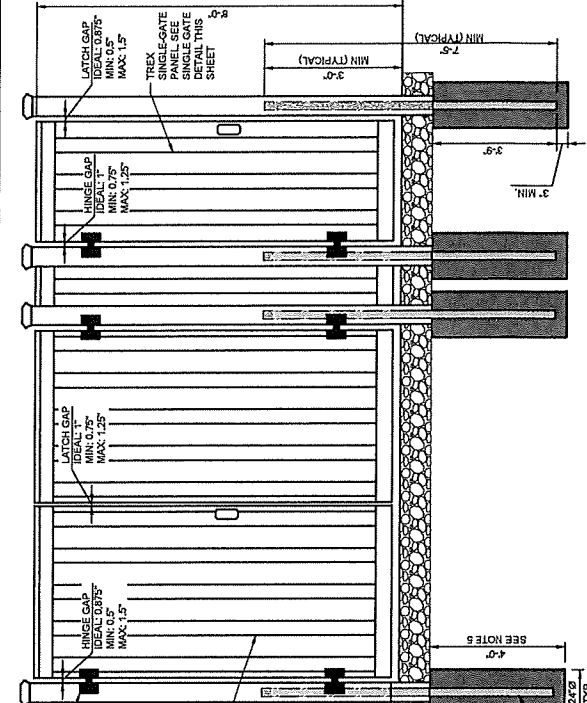
NOTE:  
FENCING SHALL BE TREX "SECLUSION"  
TYPE WITH "SADDLE" COLOR.



- NOTES:
1. INSTALLATION TO BE COMPLETED PER MANUFACTURER'S SPECIFICATION.
  2. THIS DRAWING IS PROVIDED FOR PLANNING PURPOSES. REFER TO MANUFACTURER'S INSTALLATIONS FOR CONSTRUCTION DETAILS.
  3. REFER TO MANUFACTURER'S WEBSITE FOR PRODUCT INFORMATION.
  4. DRAWING NOT TO SCALE.
  5. IF FROST DEPTH EXCEEDS 4'-0", FOOTER TO EXCEED FROST DEPTH.
  6. ENGINEER OF RECORD SHALL BE CONTACTED IMMEDIATELY IF GROUND WATER IS ENCOUNTERED DURING CONSTRUCTION.

N.T.S.

TREX FENCE DETAIL



- GATE NOTES:
- TREX SECLUSION SINGLE GATE OPENING AT 46-1/4" BETWEEN EDGE OF COMPOSITE POSTS
  - TREX SECLUSION DOUBLE GATE OPENING AT 136-1/2" BETWEEN EDGE OF COMPOSITE POSTS
  - REFER TO SITE PLAN FOR APPLICABLE GATE TYPES AND QUANTITIES

GENERAL NOTE

CONTRACTOR SHALL INSTALL SIGNAGE ON INTERIOR SIDE OF WIDE SWING GATE INDICATING THE FOLLOWING:

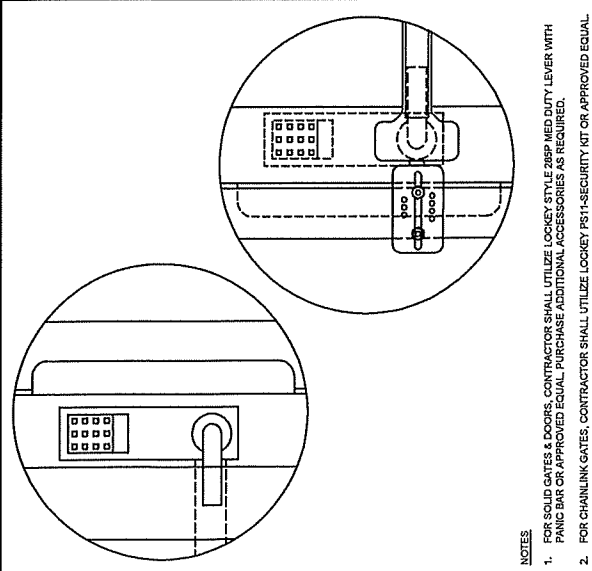
24"

DOUBLE GATE SHALL REMAIN OPEN AT ALL TIMES WHILE SERVICING ELECTRICAL EQUIPMENT

- INSTALLATION NOTES:
1. INSTALLATION TO BE COMPLETED PER MANUFACTURER'S RECOMMENDATIONS.
  2. THIS DRAWING IS PROVIDED FOR PLANNING PURPOSES. REFER TO MANUFACTURER'S WEBSITE FOR CONSTRUCTION DETAILS.
  3. REFER TO MANUFACTURER'S WEBSITE FOR PRODUCT INFORMATION.
  4. DRAWING NOT TO SCALE.
  5. IF FROST DEPTH EXCEEDS 4'-0", FOOTER TO EXCEED FROST DEPTH.

N.T.S.

TREX GATE DETAIL



N.T.S.

SINGLE GATE KEYPAD DETAIL W/ PANIC HARDWARE

- NOTES:
1. ALL PAVER MARKINGS TO BE WHITE PAVERMENT PAINT, UNLESS STATED OTHERWISE.
  2. MARKING (STRIPING) PAINT FOR PARKING SPACES, TRAFFIC ARROWS, ADA PARKING AND SYMBOLS, ETC. PER LOCAL REQUIREMENTS AND AS FOLLOWS:
    - 2.1. EXISTING SURFACES WITHOUT ANY SEAL COATING, OIL BASE (ALK/AC) RESIN TYPE TO MEET FEDERAL SPECIFICATION TPA-1822
    - 2.2. NEW OR EXISTING SURFACES WITH A TOP COATING OR SEAL COATING (USUALLY WATER BASE FAST DRYING 100% ACRYLIC TYPE) WATER BASE TYPE TO MEET FEDERAL SPECIFICATION TPA-1822 AND ACH 11425, 1582 AND D711, IN ACCORDANCE WITH ASTM D2269, D972, D1492, D582 AND D711.
  3. PROVIDE A NON-SLIP AGGREGATE ADDITIVE TO MARKING PAINT USED AT ADA ACCESS RAMPS.
  4. APPLY 2 COATS WITH STRAIGHT EDGES, YELLOW ON CONCRETE/WHITE ON ASPHALT EXCEPT WHEN MATCHING ADJACENT OR EXISTING COLOR WHEN THE PAVING IS AN EXPANSION OR SEGMENT OF A LARGER LOT.

N.T.S.

STRIPING NOTES

**EXHIBIT**

**“B”**

**ZONE CHANGE/CONCEPT PLAN COMMITTEE REPORT**

TO: Boone County Planning Commission

FROM: Brad Shipe, Chairman

DATE: December 4, 2019

RE: Request of **GPD Group c/o Zachary Whitney (applicant)** for **Meijer Stores Ltd. (owner)** for a Change in Concept Development Plan in a Office Two/Commercial Two/Planned Development/Houston Donaldson Study Corridor Overlay (O-2/C-2/PD/HDO) zone for an approximate 0.09 acre portion of the 26.32 acre lot located at 4990 Houston Road, Florence, Kentucky (Meijer). The request is for a Change in Concept Development Plan to allow an electric vehicle charging station.

**REMARKS:**

We, the Committee, recommend approval of the above referenced request based on the following findings of fact and with the following conditions.

**FINDINGS OF FACT**

1. The Committee has concluded that the proposed Concept Development Plan is in agreement with the Our Boone County - 2040 Plan, which is the adopted Comprehensive Plan, due to the following reasons.

The Our Boone County - 2040 Plan's Future Land Use Map designates the site as "Commercial." This designation is described as "retail, corporate and professional office, interchange commercial, indoor commercial recreation, restaurants, services, etc." The Land Use Element ("14. Florence Commercial," p. 125) states, "A substantial portion of this section is located within the 2013 Houston-Donaldson Study area and has excellent development and/or redevelopment opportunity due to its high visibility from and easy access to I-75. Existing parcels should be developed/redeveloped to be aesthetically harmonious with existing surrounding land uses and cleaned up environmentally as much as is needed for new development" (p. 125). This section also states, "The visual appearance of commercial areas in general and of Mall Road in particular, should be enhanced through the design of buildings, landscaping, signs, and arrangement of buildings. Attention to aesthetic impacts of proposed developments should be part of all phases of the review process" (p. 126).

The Future Land Use Development Guidelines recommend the provision of landscaping in all developments ("Landscaping," p. 96). The Guidelines also state that "developments in Boone County should give consideration to the overall design

of the area. Site, landscape, and building design should be a primary concern at the early stages of the development, with an emphasis on the aesthetic impact of the proposed use" ("Design, Signs, and Cultural Resource Preservation," p. 97). Further, the Goals and Objectives ("Economy," Goal B) state, "Boone County businesses are developed in appropriate locations and are compatible with surrounding land uses."

The proposal corresponds to the Future Land Use Map's "Commercial" designation and is in agreement with the 2013 Houston-Donaldson Study as discussed below. The proposed charging station represents a minor expansion of an existing parking lot at an established business and is not indicative of a stereotypical fueling station since it does not include a canopy, sales of convenience goods, etc., and is not open to the motoring public at large. The proposal, with the agreed conditions, also addresses the aesthetic concerns of the Comprehensive Plan through the provision of landscaping and an enclosure structure around the electronic equipment which meets the Houston-Donaldson Study standards. The proposal does not include any signage which would advertise the facility to the public at large. Due to these facts, the proposed charging station will be aesthetically harmonious and compatible with the surrounding land uses.

2. The Committee has concluded that the proposed Concept Development Plan is in agreement with the 2013 Houston-Donaldson Study. The subject site is within "Subarea Four" as outlined in the Study. The recommendations for this subarea include, "future development and/or redevelopment in Subarea Four should be of commercial or office type land uses and not of light industrial or residential uses, except for the light industrial area on Spiral Drive" (p. 33), and "redevelopment in Subarea Four aesthetically fit in with the existing land uses" (p. 33). The proposed charging station involves a minor expansion to an existing parking lot in a subdivision comprised of commercial and office uses and is reflective of a standard parking lot at a retail establishment. As mentioned above, the proposed facility will be aesthetically harmonious and compatible with the surrounding land uses and the enclosure structure will meet the Houston-Donaldson Study's design standards. Additionally, the remaining Meijer lot will still comfortably meet the Study's green space standard which requires at least 22 percent of the overall lot to "be devoted to landscaping as permanent green space."
3. The Committee has concluded that the Concept Development Plan fulfills the applicable requirements of Article 15 "Planned Development District" of the Boone County Zoning Regulations, including Section 1514 "Planned Development Criteria."
4. The Committee has concluded that the attached conditions are necessary to achieve consistency with the specific goals, objectives, and policies of the Our Boone County - 2040 Plan, 2013 Houston-Donaldson Study, and Article 15 "Planned Development District" of the Boone County Zoning Regulations. The Committee has also concluded that the attached conditions are necessary to mitigate foreseeable community impacts that may be created by the development.

The property owner has signed a letter demonstrating agreement with these conditions.

CONDITIONS

1. Landscaping from Buffer Yard A with the large shrub option shall be provided across the front of the charging station including the parking spaces which face Spiral Boulevard. A screen of large shrubs shall be provided around the freestanding transformer.
2. The design of the equipment enclosure structure shall follow the Mechanical Equipment Screening requirements in the Houston-Donaldson Study (p. 57) and correlate to the Meijer building.

A copy of the Public Hearing minutes accompanies the findings and recommendation serving as a summary of the evidence and testimony presented by the proponents and opponents of this request. Attached is the signature page for the Zone Change Committee Vote

## ZONE CHANGE/CONCEPT PLAN COMMITTEE VOTE

TO: Boone County Planning Commission

FROM: Brad Shipe, Chairman

DATE: November 20, 2019

2. Request of **GPD Group c/o Zachary Whitney (applicant)** for **Meijer Stores Ltd. (owner)** for a Change in an Concept Development Plan in a Office Two/Commercial Two/Planned Development/Houston Donaldson Study Corridor Overlay (O-2/C-2/PD/HDO) zone for an approximate 0.09 acre portion of the 26.32 acre lot located at 4990 Houston Road, Florence, Kentucky (Meijer). The request is for a Change in Concept Development Plan to allow an electric vehicle charging station.

### REMARKS:

We, the Committee Members were present at the Committee Meeting and voted on the above request or else were absent from voting. Further, based upon the vote, the Committee directs the Staff to draft the findings of fact and conditions if deemed necessary in order to complete the Committee Report.

\_\_\_\_\_  
**Kim Bunger**

For Project \_\_\_\_ Absent \_\_\_\_  
Against Project \_\_\_\_  
Abstain \_\_\_\_ Deferred \_\_\_\_

\_\_\_\_\_  
**Mark Hicks (Alternate)**

For Project \_\_\_\_ Absent \_\_\_\_  
Against Project \_\_\_\_  
Abstain \_\_\_\_ Deferred \_\_\_\_

\_\_\_\_\_  
**Janet Kegley**

For Project \_\_\_\_ Absent \_\_\_\_  
Against Project \_\_\_\_  
Abstain \_\_\_\_ Deferred \_\_\_\_

\_\_\_\_\_  
**Lori Heilman (Alternate)**

For Project \_\_\_\_ Absent \_\_\_\_  
Against Project \_\_\_\_

*Rick Lunnemann*  
**Rick Lunnemann**

For Project  Absent \_\_\_\_  
Against Project \_\_\_\_  
Abstain \_\_\_\_ Deferred \_\_\_\_

\_\_\_\_\_  
**Steve Turner (Alternate)**

For Project \_\_\_\_ Absent \_\_\_\_  
Against Project \_\_\_\_  
Abstain \_\_\_\_ Deferred \_\_\_\_

*Kim Patton*  
**Kim Patton**

For Project  Absent \_\_\_\_  
Against Project \_\_\_\_  
Abstain \_\_\_\_ Deferred \_\_\_\_

\_\_\_\_\_  
**Randy Bessler (Alternate)**

For Project \_\_\_\_ Absent \_\_\_\_  
Against Project \_\_\_\_  
Abstain \_\_\_\_ Deferred \_\_\_\_

*Brad Shipe*  
**Brad Shipe (Chairman)**

For Project  Absent \_\_\_\_  
Against Project \_\_\_\_  
Abstain \_\_\_\_ Deferred \_\_\_\_

**TOTAL:** \_\_\_\_ DEFERRED \_\_\_\_ FOR PROJECT \_\_\_\_ ABSENT \_\_\_\_  
\_\_\_\_ AGAINST PROJECT \_\_\_\_ ABSTAIN \_\_\_\_

**COMMISSION MEMBERS PRESENT:**

Mr. Randy Bessler  
Ms. Corrin Gulick  
Mr. Steve Harper  
Mrs. Lori Heilman  
Mr. Mark Hicks  
Mrs. Janet Kegley  
Mr. Don McMillian  
Mr. Kim Patton  
Mr. Charlie Rolfsen, Chairman  
Mr. Brad Shipe  
Mr. Tom Szurlinski  
Mr. Steve Turner, Temporary Presiding Officer

**COMMISSION MEMBERS NOT PRESENT:**

Mr. Kim Bunger, Secretary/Treasurer  
Mr. Rick Lunnemann  
Mr. Bob Schwenke

**LEGAL COUNSEL PRESENT:**

Mr. Dale T. Wilson

**STAFF MEMBERS PRESENT:**

Kevin P. Costello, AICP, Executive Director  
Mr. Kevin T. Wall, AICP, Director, Zoning Services  
Mr. Todd K. Morgan, AICP, Senior Planner

Chairman Rolfsen introduced the second item on the Agenda at 8:01 p.m.

**CHANGE IN CONCEPT DEVELOPMENT PLAN - Kevin Wall, Staff**

2. Request of **GPD Group c/o Zachary Whitney (applicant)** for **Meijer Stores Ltd. (owner)** for a Change in an Concept Development Plan in a Office Two/Commercial Two/Planned Development/Houston Donaldson Study Corridor Overlay (O-2/C-2/PD/HDO) zone for an approximate 0.09 acre portion of the 26.32 acre lot located at 4990 Houston Road, Florence, Kentucky (Meijer). The request is for a Change in Concept Development Plan to allow an electric vehicle charging station

Staff Member, Kevin Wall, presented the Staff Report, which included a PowerPoint presentation. The request is a Change in Concept Development Plan. The overall plan for Houston Lakes Subdivision was approved in 1990. Since then, there were some modifications made by the Zoning Administrator over time. The site is zoned O-2/C-2/PD/HDO. The Future Land Use Map designates the site as Commercial (C). A charging station is proposed for 12 electric vehicles that is exclusive to Tesla customers. The site is located on the side of the Meijer store facing Spiral Boulevard. It is within the existing big landscape area. It consists of 12 parking spaces. Each space has a 5.5 foot high charging post as well as a regulatory sign (Vehicle Charging Only). It has a compound that includes the electric charging equipment. It is 18' x 18.5' in size and will be fenced. There is also a separate transformer. The signs are 6 feet high and 1.5 square feet in size. The fence is 8 feet high. Mr. Wall showed photos of the existing site and adjoining properties.

In regard to Staff Comments, Mr. Wall referred to the Houston-Donaldson Study standards as noted in the Staff Report. He also referred to Section 1514 of the Zoning Regulations - the Planned Development (PD) standards. Not all of the standards apply since it is a small site. No landscaping plan was submitted with the application. He recommended landscaping from Buffer Yard A across the front of the charging station. There are some existing street trees along Spiral Boulevard. Mr. Wall further recommended the large shrub option, including screening the transformer and an equipment enclosure consistent with the Meijer building. The overall Meijer site has 8.25 acres of green space, which is about 28%. The Houston-Donaldson Study recommends 22% green space. With the proposed development, the overall site would still be above the 22% requirement. No comments were received from the City of Florence regarding the proposed project. The proposed project must be evaluated based on the Boone County Comprehensive Plan, the Houston-Donaldson Study and Article 15 of the Boone County Zoning Regulations.

At this time, Chairman Rolfsen asked if the applicant was present and wanted to proceed with their presentation?

Mr. Jordan Rhyne, representing Tesla, stated that his company has 2 super charger sites north of Cincinnati and the proposed one would be the first south of the City. There is a lot of demand in the area. Mr. Rhyne agreed to provide the large bushes for landscaping.

At this time, Chairman Rolfsen asked if there was anyone in the audience who wanted to speak in favor or against the request?

Seeing no one in the audience, Chairman Rolfsen asked if any Board members had any comments or questions?

Mr. McMillian stated that he likes the request because just like gas, people will run out of electric.


Ms. Gulick asked if the chargers were only for Tesla vehicles? Mr. Rhyne replied yes. They are Level 3 chargers or super chargers. It is similar to the ones that were placed at Walmart. It takes about 20-40 minutes to charge a vehicle. It is a 250 kilowatt charger. Charging can also be set up at home. It is a good location for local and interstate customers. Some customers are charged for electric. It depends on the vehicle type. The higher end models include free charging. A full charge is \$8-10 for about 300 miles of driving. When a customer plugs in, the chargers are data connected and a credit card is on file. There are no point of sale terminals. They have 30 sites associated with Meijer and over 750 sites across the nation. They like Meijer because the store is open 24/7 and they have restaurants located near the charging stations.

Seeing no further questions or comments, Chairman Rolfsen announced that the Committee Meeting for this item will be on November 20, 2019 at 5:00 P.M. in the Fiscal Courtroom. This item will be on the Agenda for the Business Meeting on December 4, 2019 at 7:00 P.M. Chairman Rolfsen closed the Public Hearing at 8:14 P.M.

APPROVED:

  
\_\_\_\_\_  
Charlie Rolfsen  
Chairman

Attest:

  
Kevin P. Costello, AICP  
Executive Director

# **SUPPORTING INFORMATION**



# BOONE COUNTY PLANNING COMMISSION

[www.boonecountky.org/pc](http://www.boonecountky.org/pc)  
[www.boonecountkygis.com](http://www.boonecountkygis.com)

Boone County Administration Building  
2950 Washington Street, Room 317  
P.O. Box 958  
Burlington, KY 41005

Phone (859) 334-2196; Fax (859) 334-2264  
[plancom@boonecountky.org](mailto:plancom@boonecountky.org)

November 25, 2019

Mr. Zachary Whitney  
GPD Group  
520 S. Main Street  
Akron, OH 44311

RE: Recommended Conditions of Approval for Tesla Charging Station Change in Concept Development Plan Application, 4990 Houston Road (Meijer), Florence, Kentucky.

Dear Mr. Whitney:

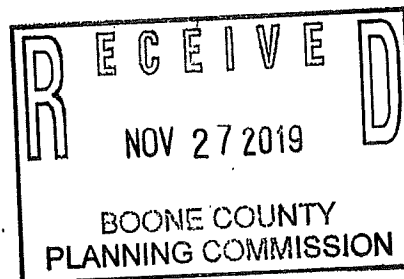
The following represents the recommended conditions of approval for the above referenced application as discussed by the Planning Commission's Zone Change Committee at their November 20, 2019 meeting. If you, as the authorized representative of the property owner agree to these conditions, please so indicate by signing in the space provided at the end of this letter and return the original letter to the Planning Commission's office by Monday 12/2/19.

## CONDITIONS

1. Landscaping from Buffer Yard A with the large shrub option shall be provided across the front of the charging station including the parking spaces which face Spiral Boulevard. A screen of large shrubs shall be provided around the freestanding transformer.
2. The design of the equipment enclosure structure shall follow the Mechanical Equipment Screening requirements in the Houston-Donaldson Study (p. 57) and correlate to the Meijer building.

Sincerely,

Kevin T. Wall, AICP  
Director, Zoning Services



## AGREEMENT

I, the authorized representative of the property owner of the tract located at 4990 Houston Road, Florence, Kentucky, agree to the conditions listed herein for the above referenced Change in Concept Development Plan application.

Zachary Whitney, Authorized Representative for  
Meijer Stores, LTD

11/25/2019  
Date

KTW/tlb

**ORDINANCE NO. O-1-20**

**AN ORDINANCE ADOPTING AND APPROVING A CHANGE IN A CONCEPT DEVELOPMENT PLAN IN AN OFFICE TWO/COMMERCIAL TWO/PLANNED DEVELOPMENT/HOUSTON DONALDSON STUDY CORRIDOR OVERLAY (O-2/C-2/PD/HDO) ZONE FOR AN APPROXIMATE 0.09 ACRE PORTION OF THE 26.32 ACRE LOT LOCATED AT 4990 HOUSTON ROAD, FLORENCE, KENTUCKY, TO ALLOW AN ELECTRIC VEHICLE CHARGING STATION. (MEIJER)**

WHEREAS, the City of Florence, Kentucky, is a member of the county-wide planning unit with a county-wide planning commission known as the Boone County Planning Commission, and

WHEREAS, the Boone County Planning Commission by Resolution No. R-19-026-A recommended approval for a change in a concept development plan, and

WHEREAS, the recommendation for approval by the Boone County Planning Commission is based upon certain findings and conditions attached to the Commission's Resolution, all of which have been reviewed by the City Council for the City of Florence, Kentucky.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY OF FLORENCE, KENTUCKY, AS FOLLOWS:

**SECTION I**

That the request of GPD Group c/o Zachary Whitney (Applicant) for Meijer Stores Ltd. (Owner), for a change in a concept development plan in an Office Two/Commercial Two/Planned Development/Houston Donaldson Study Corridor Overlay (O-2/C-2/PD/HDO) zone, for an approximate 0.09 acre portion of the 26.32 acre lot located at 4990 Houston Road, Florence, Kentucky, to allow an electric vehicle charging station, shall be and is hereby approved. The zoning map of Boone County, Kentucky, as it applies to the City of Florence, Kentucky, shall be designated to reflect the approval of this change in the concept development plan for this subject property.

**SECTION II**

The approval of this change to the concept development plan is granted subject to the conditions, terms and provisions of the Boone County Planning Commission recommendation, including its Committee Report, made a part of that recommendation, and all provisions comprising the development plan herein, a copy of that recommendation and Committee Report of the Boone County Planning Commission being attached, marked Exhibit "A" and incorporated herein as if fully set out, this recommendation being in the form of Resolution No. R-19-026-A of the Boone County Planning Commission, including the exhibits referenced to that Resolution.

**SECTION III**

This approval is granted based on the Findings of Fact made by the Boone County Planning Commission and contained within its recommendation that is attached and part of the Commission's Resolution No. R-19-026-A, marked Exhibit "A", and attached hereto.

**SECTION IV**

If the approval for this change in the concept development plan shall be held invalid, in whole or in part, by any Court of proper jurisdiction, that invalidity shall not affect the validity of any of the other zoning regulations, zoning map, comprehensive plan, or other portions, provisions or terms of the approved concept development plan for this property as they are severable from this Ordinance and are intended to have full force and effect regardless of any invalidity relating to this particular Ordinance.

**SECTION V**

This Ordinance shall be published by posting on the City's internet website.

PASSED AND APPROVED ON FIRST READING THIS 21<sup>st</sup> DAY OF January, 2020.

PASSED AND APPROVED ON SECOND READING AND PUBLICATION ORDERED THIS 28 DAY OF January, 2020.

APPROVED: ,

Deane E. Whalen  
MAYOR

ATTEST:

Amy Zelman  
CITY CLERK



2400 DEER CREEK RD  
PALO ALTO, CA 94304  
(650) 997-4600



520 South Main Street  
CINCINNATI, OH 45202  
352.572.2100 Fax 352.572.2102

GPD PROJECT NUMBER: 201914136  
DRAWN BY: JMJ  
CHECKED BY: RPE/PM  
INSTALLATION MANAGER: WILLIAM BUSCH

REV#	DATE	ISSUED FOR	DESCRIPTION
1	10/27/19	ISSUED FOR 100% REVIEW	
2	12/14/19	ISSUED FOR 100% REVIEW	
3		ISSUED FOR 100% REVIEW	
4		ISSUED FOR 100% REVIEW	
5		ISSUED FOR 100% REVIEW	
6		ISSUED FOR 100% REVIEW	
7		ISSUED FOR 100% REVIEW	
8		ISSUED FOR 100% REVIEW	
9		ISSUED FOR 100% REVIEW	
10		ISSUED FOR 100% REVIEW	

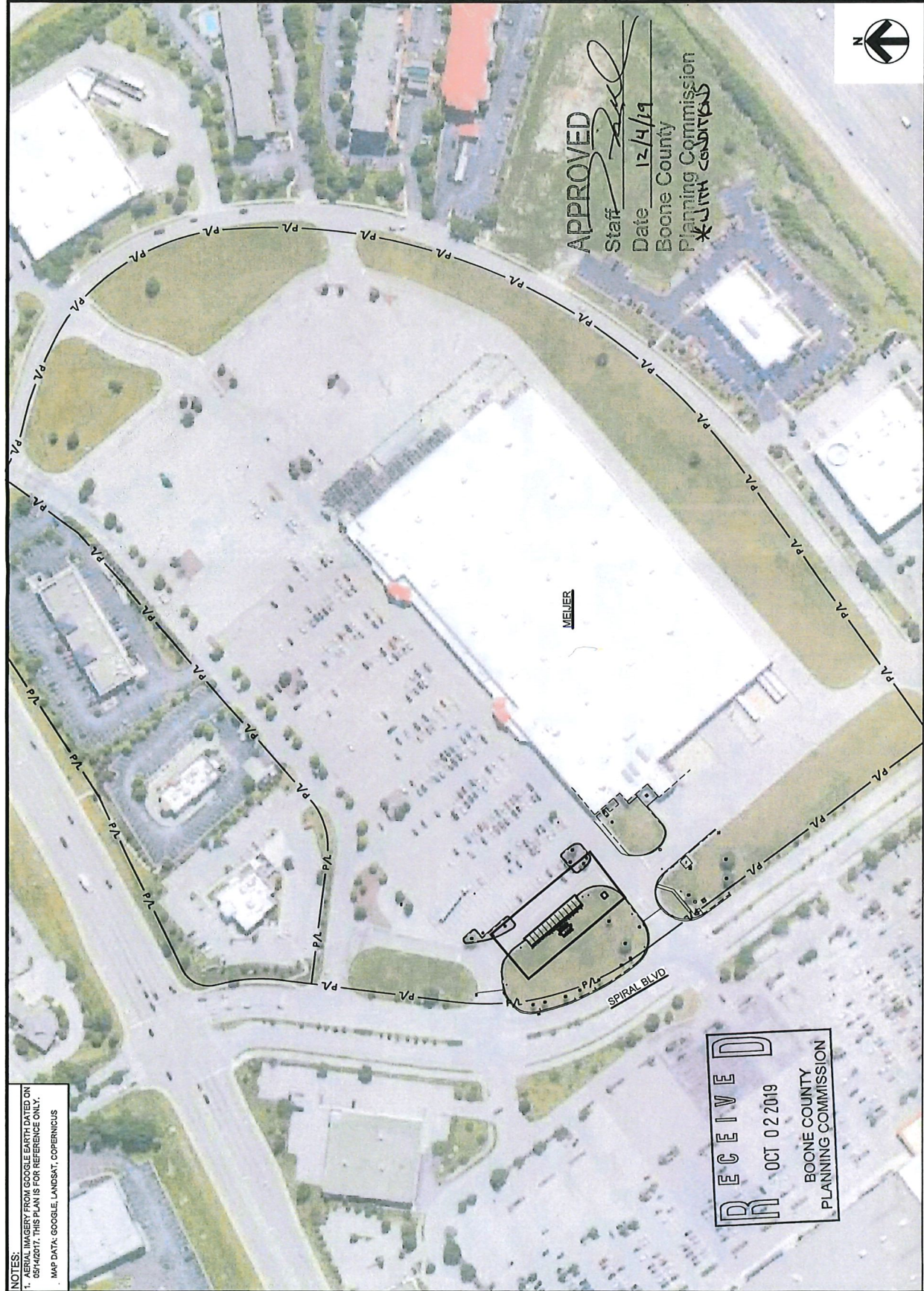
**NOT FOR CONSTRUCTION**

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, FIRM OR COMPANY TO REPRODUCE, TRANSMIT, OR OTHERWISE DISSEMINATE THE CONTENTS OF THIS DOCUMENT WITHOUT THE WRITTEN PERMISSION OF THE PROFESSIONAL ENGINEER, ARCHITECT OR SURVEYOR IDENTIFIED HEREIN. TO ALTER THIS DOCUMENT.

70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
TITLE SHEET &  
PROJECT DATA

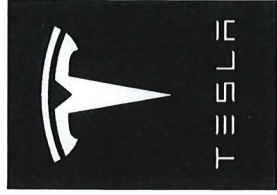
SHEET NUMBER  
T-M



**APPROVED**  
Staff: *[Signature]*  
Date: 12/14/19  
Boone County  
Planning Commission  
*[Signature]*

NOTES:  
1. IMAGES FROM GOOGLE EARTH DATED ON 11/26/2017. THIS PLAN IS FOR REFERENCE ONLY.  
2. MAP DATA: GOOGLE, LANDSAT, COPERNICUS

**RECEIVED**  
OCT 02 2019  
BOONE COUNTY  
PLANNING COMMISSION



# TESLA

## CHARGING STATION (TESLA STATION)

### 70 SPIRAL DR. (TESLA STATION) FLORENCE, KY 41042



3400 DEER CREEK RD  
PALO ALTO, CA 94304  
(650) 961-1000



520 South Main Street  
336.572.2100 Fax: 336.572.2102

GPD PROJECT NUMBER: 2019-04-06

DRAWN BY: JMU  
CHECKED BY: RPEPM  
INSTALLATION MANAGER: WILLIAM BUSCH

REV	DATE	DESCRIPTION
1	10.27.19	ISSUED FOR 100% REVIEW
2	11.13.19	ISSUED FOR 50% REVIEW
3	11.13.19	ISSUED FOR 50% REVIEW

NOT FOR  
CONSTRUCTION

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS SPECIFICALLY AUTHORIZED BY THE LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT.

70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
TITLE SHEET &  
PROJECT DATA

SHEET NUMBER  
T-1

#### DRAWING INDEX

T-M	TITLE SHEET & PROJECT DATA
T-1	TITLE SHEET & PROJECT DATA
CIVIL	SHEET TITLE
GN-1	GENERAL NOTES
GN-2	GENERAL NOTES
GN-3	GENERAL NOTES
-	TOPOGRAPHIC SURVEY (BY OTHERS)
C-1	OVERALL SITE PLAN
C-2	DEMOLITION PLAN
C-3	SITE PLAN
C-5	EQUIPMENT DIMENSION PLAN
C-6	CIVIL DETAILS
C-7	CIVIL DETAILS
C-8	CIVIL DETAILS
ELECTRICAL	SHEET TITLE
EM-1	ELECTRICAL GENERAL NOTES
EM-2	ELECTRICAL GENERAL NOTES
E-1	OVERALL UTILITY PLAN
E-2	ELECTRICAL EQUIPMENT PLAN
E-3	SYSTEM ONE-LINE DIAGRAM
E-4	PANEL SCHEDULES
E-5	ELECTRICAL DETAILS
E-6	ELECTRICAL DETAILS
E-7	UTILITY DETAILS
G-1	GROUNDING DETAILS

#### PROJECT DESCRIPTION

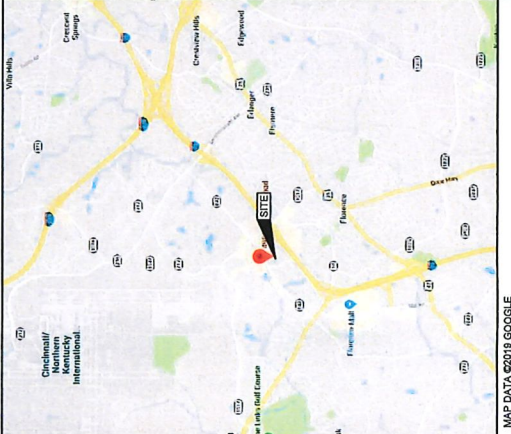
- INSTALL (2) V3 SUPERCHARGER CABINETS
- INSTALL (6) V3 CHARGING POSTS
- INSTALL (1) MASTER CONTROLLER
- INSTALL (1) PEDESTRIAN LIGHT FIXTURE(S)
- INSTALL (1) UTILITY TRANSFORMER
- INFRASTRUCTURE FOR (1) FUTURE V3 SUPERCHARGER CABINETS
- INFRASTRUCTURE FOR (4) FUTURE V3 CHARGING POSTS

#### APPLICABLE CODES

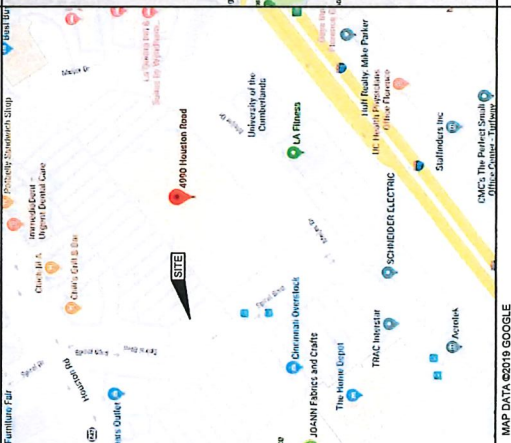
ALL WORK SHALL COMPLY WITH THE FOLLOWING APPLICABLE CODES:  
2019 KENTUCKY BUILDING CODE (2015 IBC)  
2011 NATIONAL ELECTRICAL CODE  
IN THE EVENT OF CONFLICT, THE MOST RESTRICTIVE CODE SHALL PREVAIL.

DESIGN CRITERIA:  
WIND SPEED = 115 MPH 3-SECOND GUST, ULTIMATE  
ICE CATEGORY = II  
FROST DEPTH = 20"

#### VICINITY MAP



#### LOCATION MAP



#### SITE INFORMATION

ADDRESS:  
70 SPIRAL DR.  
FLORENCE, KY 41042

POWER COMPANY:  
CONTACT: MELVIN MORRIS  
(317) 893-1737

PROPERTY OWNER:  
MELVIN MORRIS  
P.O. BOX 988  
BURLINGTON, KY 41005  
CONTACT: KEVIN WALL  
(859) 334-2186

EQUIPMENT SUPPLIER:  
TESLA, INC.  
3500 DEER CREEK ROAD  
PALO ALTO, CA 94304  
(650) 961-1000

GPD GROUP INC. CONTACTS:  
PROJECT MANAGER:  
ZACHERY SHEETS, PE - OHP 77870  
(502) 572-2148  
ZSHEETS@GPDGROUP.COM

PROJECT COORDINATOR:  
JOSHUA RUFFINE  
(502) 572-2193  
JRUFFINE@GPDGROUP.COM

UTILITY COORDINATOR:  
BRIAN GUTHRIE  
(502) 572-2193  
BGUTHRIE@GPDGROUP.COM

PERMIT COORDINATOR:  
ZACHERY WHITNEY  
(502) 572-2193  
ZWHITNEY@GPDGROUP.COM



Know what's below  
Call before you dig.

#### FLOOD HAZARD AREA NOTE

THE SITE IS LOCATED IN FLOOD ZONE "X" (AREA DETERMINED TO BE OUTSIDE OF AN ANNUAL CHANCE FLOODPLAIN) PER FLOOD INSURANCE MAP NUMBER 210505240, EFFECTIVE DATE: 06/04/2007.

#### BEFORE SCALING & PLAN REPRODUCTION WARNING

CONTRACTORS SHALL VERIFY ALL PLANS, EXISTING DIMENSIONS, AND FIELD CONDITIONS ON THE JOB SITE AND SHALL IMMEDIATELY NOTIFY THE ENGINEER IN WRITING OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK OR BE RESPONSIBLE FOR SAME.

MAP DATA ©2019 GOOGLE

MAP DATA ©2019 GOOGLE



**GENERAL SITE WORK NOTES CONTINUED**

**PART 1 - GENERAL**

CLEANING, GRUBBING, STRIPPING, EROSION CONTROL, SURVEY, LAYOUT, SUBGRADE PREPARATION AND FINISH GRADING AS REQUIRED TO COMPLETE THE PROPOSED WORK SHOWN IN THESE PLANS.

**1.1 REFERENCES:**

- A. STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION).
- B. ASTM (AMERICAN SOCIETY FOR TESTING AND MATERIALS).
- C. OSHA (OCCUPATION SAFETY AND HEALTH ADMINISTRATION).

**1.2 INSPECTION AND TESTING:**

- A. FIELD TESTING OF EARTHWORK COMPACTATION AND CONCRETE CYLINDERS SHALL BE PERFORMED BY AN INDEPENDENT TESTING LAB. THIS WORK TO BE COORDINATED BY THE CONTRACTOR.
- B. ALL WORK SHALL BE INSPECTED AND RELEASED BY THE GENERAL CONTRACTOR WHO SHALL CARRY THE RESPONSIBILITY TO REQUEST TIMELY INSPECTIONS PRIOR TO PROCEEDING WITH FURTHER WORK THAT WOULD MAKE PARTS OF WORK INACCESSIBLE OR DIFFICULT TO INSPECT.

**1.3 SITE MAINTENANCE AND PROTECTION:**

- A. PROVIDE ALL NECESSARY JOB SITE MAINTENANCE AND PROTECTION FROM COMMENCEMENT OF WORK UNTIL COMPLETION OF THE CONTRACT.
- B. AVOID DAMAGE TO THE SITE AND TO EXISTING FACILITIES, STRUCTURES, TREES, AND SHRUBS DESIGNATED TO REMAIN. TAKE PROTECTIVE MEASURES TO PREVENT EXISTING FACILITIES THAT ARE NOT DESIGNATED FOR REMOVAL FROM BEING DAMAGED BY THE WORK.
- C. KEEP SITE FREE OF ALL PONDING WATER.
- D. PROVIDE EROSION CONTROL MEASURES IN ACCORDANCE WITH STATE DOT, LOCAL PERMITTING AGENCY AND EPA REQUIREMENTS.
- E. PROVIDE AND MAINTAIN ALL TEMPORARY FENCING, BARRICADES, WARNING SIGNALS AND SIMILAR DEVICES NECESSARY TO PROTECT AGAINST THEFT FROM PROPERTY DURING THE ENTIRE PERIOD OF CONSTRUCTION. REMOVE ALL SUCH DEVICES UPON COMPLETION OF THE WORK.
- F. EXISTING UTILITIES: DO NOT INTERRUPT EXISTING UTILITIES SERVING FACILITIES OCCUPIED BY THE OWNER OR OTHERS, EXCEPT WHEN PERMITTED IN WRITING BY THE CONSTRUCTION MANAGER AND ONLY AFTER ADEQUATE TEMPORARY UTILITY SERVICES HAVE BEEN PROVIDED.
- G. PROVIDE A MINIMUM 48-HOUR NOTICE TO THE CONSTRUCTION MANAGER AND RECEIVE WRITTEN NOTICE TO PROCEED BEFORE INTERRUPTING ANY UTILITY SERVICE.

**PART 2 - PRODUCTS**

- 2.1 GRANULAR BACKFILL SHALL MEET THE FOLLOWING GRADATION PER THE TABLE BELOW:
- 2.2 GRANULAR BEDDING AND TRENCH BACKFILL: WELL-GRADED SAND MEETING THE GRADATION REQUIREMENTS OF ASTM D2487 (S&G OR S&M).
- 2.3 UNSUITABLE MATERIAL: HIGH AND MODERATELY PLASTIC SILTS AND CLAYS (LL>45), MATERIAL CONTAINING REFUSE, FROZEN LUMPS, DEMOLISHED BITUMINOUS MATERIAL, VEGETATIVE MATTER, AND OTHERS. MATERIALS CONTAINING ORGANIC MATTER SHALL BE REMOVED AND THE AREA REPAIRED BY THE CONSTRUCTION MANAGER. TYPICAL THESE WILL BE SOILS CLASSIFIED BY ASTM AS PT, MH, OH, OH, ML, AND OL.

SIEVE SIZE	TOTAL PERCENT PASSING
1.18 INCH (27.5 MM)	100
3/4 INCH (19.0 MM)	75 TO 100
3/8 INCH (9.5 MM)	60 TO 100
NO. 4 (4.75 MM)	35 TO 75
NO. 10 (2.00 MM)	30 TO 60
NO. 20 (0.75 MM)	7 TO 30
NO. 30 (0.60 MM)	3 TO 15

**PART 3 - EXECUTION**

- 3.1 GENERAL:
  - A. BEFORE STARTING GENERAL SITE PREPARATION ACTIVITIES, INSTALL EROSION AND SEDIMENT CONTROL MEASURES PER APPLICABLE PLAN PREPARED BY CONTRACTOR. THE WORK AREA SHALL BE PROTECTED AND MAINTAINED IN SUCH CONDITION THAT IN THE EVENT OF RAIN THE SITE WILL BE DRAINAGE FREE AT ALL TIMES.
  - B. BEFORE ALL SURVEY, LAYOUT, STAKING, AND MARKING, ESTABLISH AND MAINTAIN ALL LINES, GRADES, ELEVATIONS AND BENCHMARKS NEEDED FOR EXECUTION OF THE WORK.
  - C. CLEAR AND GRUB THE AREA WITHIN THE LIMITS OF THE SITE. REMOVE TREES, BRUSH, STUMPS, RUBBISH AND OTHER DEBRIS AND VEGETATION RESTING ON OR PROTRUDING THROUGH THE SURFACE OF THE SITE AREA TO BE CLEARED.
  - D. REMOVE THE FOLLOWING MATERIALS TO A DEPTH OF NO LESS THAN 12 INCHES BELOW THE ORIGINAL PROTRUDING SURFACE: ROOTS, STUMPS, AND OTHER DEBRIS, BRUSH, AND REFUSE EMBEDDED IN OR PROTRUDING THROUGH THE GROUND SURFACE. RAKE DISK OR PLOW THE AREA TO A DEPTH OF NO LESS THAN 12 INCHES, AND REMOVE TO A DEPTH OF 12 INCHES ALL ROOTS AND OTHER DEBRIS THEREBY EXPOSED.

E. REMOVE TOPSOIL MATERIAL COMPLETELY FROM THE SURFACE UNTIL THE SOIL NO LONGER MEETS THE DEFINITION OF TOPSOIL. AVOID MIXING TOPSOIL WITH SUBSOIL OR OTHER UNSUITABLE MATERIALS.

F. EXCEPT WHERE EXCAVATION TO GREATER DEPTH IS INDICATED, FILL DEPRESSIONS RESULTING FROM CLEANING, GRUBBING AND DEMOLITION WORK COMPLETELY WITH SUITABLE FILL.

G. REMOVE FROM THE SITE AND DISPOSE IN AN AUTHORIZED LANDFILL ALL DEBRIS RESULTING FROM CLEANING AND GRUBBING OPERATIONS. BURNING WILL NOT BE PERMITTED.

H. PRIOR TO EXCAVATING, THOROUGHLY EXAMINE THE AREA TO BE EXCAVATED AND/OR TRENCHED TO VERIFY THE LOCATIONS OF FEATURES INDICATED ON THE DRAWINGS AND TO ASCERTAIN THE EXISTENCE AND LOCATION OF ANY STRUCTURE, UNDERGROUND STRUCTURE, OR OTHER ITEM NOT SHOWN ON THE DRAWINGS. NOTIFY THE CONSTRUCTION MANAGER IMMEDIATELY OF ANY FEATURES INDICATED ON THE DRAWINGS THAT WILL PREVENT ACCOMPLISHMENT OF THE WORK AS INDICATED ON THE DRAWINGS.

I. SEPARATE AND STOCK PILE ALL EXCAVATED MATERIALS SUITABLE FOR BACKFILL. ALL EXCESS EXCAVATED AND UNSUITABLE MATERIALS SHALL BE DISPOSED OF OFF-SITE IN A LEGAL MANNER.

J. DURING EXCAVATION, THE CONTRACTOR SHALL PROVIDE SHORING, SHEETING, AND BRACING AS REQUIRED TO PREVENT CAVING OR SLOUGHING OF EXCAVATION.

K. THE BASE OF ALL FOUNDATION EXCAVATIONS SHOULD BE FREE OF WATER AND LOOSE SOIL PRIOR TO PLACING CONCRETE. CONCRETE SHOULD BE PLACED AS SOON AS POSSIBLE AFTER EXCAVATING TO AVOID EXCESSIVE SOIL DRYING. EXCESSIVE SOIL DRYING SHOULD BE CORRECTED BY ADDING WATER TO SOIL. UNLESS OTHERWISE SPECIFIED, THE AFFECTED SOIL SHOULD BE REMOVED PRIOR TO PLACING CONCRETE.

**3.2 BACKFILL:**

- A. AS SOON AS PRACTICAL, AFTER COMPLETING CONSTRUCTION OF THE RELATED STRUCTURE INCLUDING CURING PERIOD OF THE FINISHED CONCRETE, BACKFILL WITH APPROVED FINISHED GRADE.
- B. PRIOR TO PLACING BACKFILL AROUND STRUCTURES, ALL FORMS SHALL BE REMOVED AND THE EXCAVATION CLEANED OF ALL TRASH, DEBRIS, AND UNSUITABLE MATERIALS.
- C. DO NOT PLACE FROZEN MATERIAL IN AS BACKFILL.
- D. BACKFILL BY PLACING AND COMPACTING SUITABLE BACKFILL MATERIAL OR SELECT GRANULAR BACKFILL MATERIAL WHEN REQUIRED IN UNIFORM HORIZONTAL LAYERS OF NO GREATER THAN 8-INCHES THICKNESS AND COMPACTED. WHERE HAND OPERATED COMPACTORS ARE USED, THE FILL MATERIAL SHALL BE PLACED IN LIFTS NOT TO EXCEED 4 INCHES IN LOOSE DEPTH AND COMPACTED.
- E. WHENEVER THE DENSITY TESTING INDICATES THAT THE CONTRACTOR HAS NOT OBTAINED THE SPECIFIED DENSITY, THE SUCCEEDING LAYER SHALL NOT BE PLACED UNTIL THE SPECIFICATION REQUIREMENTS ARE MET. THE CONTRACTOR SHALL TAKE WATERSHED APPROVED MEASURES TO CORRECT DRYING, ADDING WATER, OR INCREASING THE COMPACTIVE EFFORT TO MEET THE MINIMUM COMPACTATION REQUIREMENTS.
- F. TRENCH BACKFILL SHALL BE TO A MINIMUM OF 95 PERCENT OF THE MAXIMUM DRY DENSITY AS PROVIDED BY THE STANDARD PROCTOR TEST, ASTM D 698.

3.3 TRENCH EXCAVATION:
 

- A. UTILITY TRENCHES SHALL BE EXCAVATED TO THE LINES AND GRADES SHOWN ON THE DRAWINGS OR AS DIRECTED BY THE GENERAL CONTRACTOR. PROVIDE SHORING, SHEETING AND BRACING AS REQUIRED TO PREVENT CAVING OR SLOUGHING OF THE TRENCH WALLS.
- B. EXTEND THE TRENCH WIDTH A MINIMUM OF 8 INCHES BEYOND THE OUTSIDE EDGE OF THE OUTERMOST CONDUIT.
- C. WHEN SOFT YIELDING, OR OTHERWISE UNSTABLE SOIL CONDITIONS ARE ENCOUNTERED, CONTACT ENGINEER IMMEDIATELY.

3.4 TRENCH BACKFILL:
 

- A. PROVIDE GRANULAR BEDDING MATERIAL IN ACCORDANCE WITH THE DRAWINGS AND THE UTILITY REQUIREMENTS.
- B. NOTIFY THE GENERAL CONTRACTOR 24 HOURS IN ADVANCE OF BACKFILLING.
- C. CONDUCT UTILITY CHECK TESTS BEFORE BACKFILLING. BACKFILL AND COMPACT TRENCH BEFORE ACCEPTANCE TESTING.

D. PLACE GRANULAR TRENCH BACKFILL UNIFORMLY ON BOTH SIDES OF THE CONDUITS IN 6-INCH UNCOMPACTED LIFTS UNTIL 12 INCHES OVER THE CONDUITS. SOLIDLY RAM AND TAMP BACKFILL INTO SPACE AROUND CONDUITS.

E. PROTECT CONDUIT FROM LATERAL MOVEMENT, IMPACT DAMAGE, OR UNBALANCED LOADING.

F. ABOVE THE CONDUIT EMBEDMENT ZONE, PLACE AND COMPACT SATISFACTORY BACKFILL MATERIAL IN 6-INCH MAXIMUM LOOSE THICKNESS LIFTS TO RESTORE THE REQUIRED FINISHED SURFACE GRADE.

G. COMPACT FINAL TRENCH BACKFILL TO A DENSITY EQUAL TO OR GREATER THAN THAT OF THE EXISTING UNDISTURBED MATERIAL IMMEDIATELY ADJACENT TO THE TRENCH BUT NO LESS THAN A MINIMUM OF 95 PERCENT OF THE MAXIMUM DRY DENSITY AS PROVIDED BY THE STANDARD PROCTOR TEST, ASTM D 698.

3.5 FINISH GRADING:
 

- A. BEFORE ALL GRADING TO PROVIDE POSITIVE DRAINAGE AWAY FROM STRUCTURES AND SMOOTH, EVEN SURFACE DRAINAGE OF THE ENTIRE CONSTRUCTION, GRADING SHALL BE COMPATIBLE WITH ALL SURROUNDING TOPOGRAPHY AND STRUCTURES.
- B. UTILIZE SATISFACTORY FILL MATERIAL RESULTING FROM THE EXCAVATION WORK IN THE CONSTRUCTION OF FILLS, EMBANKMENTS AND FOR REPLACEMENT OF REMOVED UNSUITABLE

MATERIALS.

C. REPAIR ALL ACCESS ROADS AND SURROUNDING AREAS USED DURING THE COURSE OF THIS WORK TO THEIR ORIGINAL CONDITION.

D. CONTRACTOR TO GRADE SITE TO DRAIN AND NOT POND WATER.

**3.8 ASPHALT PAVING:**

A. CONTRACTORS RESPONSIBLE FOR RESTRICTION AND APPLYING SEALCOATING PER LOCAL DOT SPECIFICATIONS, UNLESS OTHERWISE SPECIFIED.

**SIGN POST NOTES:**

1. ACCEPTABLE COLOR SUBSTITUTIONS:

2. OTHER ACCEPTABLE COLORS CAN BE FOUND ON ENCYCLOPEDIA.COM

3. CONTRACTOR SHALL COORDINATE WITH CITY WHEN SPECIAL JURISDICTIONALITY REQUESTS ARE NECESSARY FOR ANY SIGN POST INSTALLATIONS, I.E. POST MATERIAL, PAINT COLORS, HARDWARE, ETC. CONTRACTOR IS RESPONSIBLE FOR ENSURING CITY APPROVES ALL MATERIALS PRIOR TO INSTALLATION.

PAINT COLOR SUBSTITUTIONS	
BRAND	COLOR
PANTONE	COOL GRAY #7 #66B199
BENJAMIN MOORE	PURDUM / A4975 #66B199
BEHR	EQUINOX FF31-1 #66B199
SHERWIN-WILLIAMS	STAMPED CONCRETE-7655 #66B199
VALSPAR	STONE MASON GRAY #66B199

**LANDSCAPE/IRRIGATION NOTES**

1. ALL DISTURBED AREAS THAT ARE NOT TO BE PAVED OR MULCHED SHALL BE GRADED TO MATCH EXISTING CONDITIONS AND SLODDED AT A 3:1 MAXIMUM SLOPE.

2. SOG SHALL BE SELECTED PER ZONE AND MATCHED TO EXISTING SITE. SOG SHALL BE A FIRST GRADE FREE FROM ALL NOXIOUS WEEDS.

ZONES 3, 4 & 5: APPROVED BLUE GRASS BLEND  
ZONES 6, 7 & 8: APPROVED PERENNIAL BLEND  
ZONES 9 & 10: APPROVED ST AUGUSTINE FLORETAM BLEND

3. PLANT GUARANTEE (IF APPLICABLE): CONTRACTOR SHALL GUARANTEE ALL PLANTS FOR A PERIOD OF ONE (1) YEAR FROM DATE OF PROJECT ACCEPTANCE BY THE OWNER.

4. IRRIGATION RELOCATION: CONTRACTOR FIELD VERIFY IF EXISTING IRRIGATION IS PRESENT. DETERMINE POINT OF CONNECTION, SYSTEM PRESSURE, FUTURE TYPES, AND POTENTIAL FOR CONFLICTS. IRRIGATION SYSTEM SHALL BE MODIFIED TO PROVIDE 100% COVERAGE OF THE LANDSCAPE AREA. IF THE EXISTING IRRIGATION SYSTEM IS NOT CAPABLE OF EXPANSION, CONTRACTOR TO INSTALL A NEW CONTROLLER, BOOSTER PUMP, AND OTHER APPARATUS NEEDED FOR A COMPLETE IRRIGATION SYSTEM. CONTRACTOR SHALL ENSURE ALL EXISTING BUILDING WALLS AND FOUNDATION FOOTINGS BY THE SAME SUPPLIER. CONTRACTOR SHALL ENSURE PROPER SELECTION OF FITURES, SYSTEM SHALL INCLUDE ALL SPRINKLER FITURES, DRIP TUBING, PIPING, VALVES, WIRING AND CONTROLS TO PROVIDE IRRIGATION SYSTEM. CONTRACTOR SHALL VERIFY ALL CITY CODE PRIOR TO UPDATING THE IRRIGATION SYSTEM. CONTRACTOR SHALL OBTAIN APPROVAL OF SHOP DRAWINGS, THE UPDATED IRRIGATION SYSTEM SHALL BE APPROVED BY OWNER FOR FINAL ACCEPTANCE.



360 DEGREE CHECKED  
FIELD AUTO CALIBRATION  
TYPED BY: [Signature]



520 South 16th Street  
Aurora, OH 44111  
393.272.2109 Fax: 393.272.2102

CPD PROJECT NUMBER:	2019141R
DRAWN BY:	JM
CHECKED BY:	R/PEPA
INSTALLATION MANAGER:	WILLIAM BURCH

REV	DATE	DESCRIPTION
0	10.01.19	ISSUED FOR 100% DESIGN
1	10.23.19	ISSUED FOR 100% DESIGN

NOT FOR CONSTRUCTION

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, OTHER THAN THE DESIGNER, TO REPRODUCE OR ALTER THIS DOCUMENT.

70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
GENERAL NOTES

SHEET NUMBER  
GN-2

**GOVERNING BUILDING CODE:** 2018 KENTUCKY BUILDING CODE (2018 IBC)

**DESIGN LOADINGS:**

LATERAL LOAD DESIGN DATA:  
WIND DESIGN DATA (WIND SPEED 115 MPH)  
BASIC WIND SPEED (V<sub>W</sub>)  
WIND EXPOSURE CATEGORY (C)  
WIND PROTECTION FACTOR (K<sub>d</sub>)  
WIND EXPOSURE CATEGORY (C)

SEISMIC DESIGN DATA (WIND SPEED 115 MPH)  
SEISMIC IMPORTANCE FACTOR (I)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

SEISMIC DESIGN CATEGORY (B)  
SEISMIC DESIGN CATEGORY (B)

**FOUNDATION SYSTEMS:**

**GENERAL:**

THE CONTRACTOR SHALL VISIT THE SITE PRIOR TO THE START OF ANY WORK. THE CONTRACTOR SHALL VERIFY ANY EXISTING FIELD CONDITION THAT MAY AFFECT THE INSTALLATION OF THE FOUNDATION SYSTEM.

THE CONTRACTOR SHALL EXERCISE GREAT CARE DURING EXCAVATION. UNDERGROUND UTILITY LOCATIONS, IF SHOWN, ARE APPROXIMATE. THE CONTRACTOR SHALL DETERMINE UTILITY LOCATIONS BY VISUAL INSPECTION AND BY USING APPROPRIATE METHODS. THE CONTRACTOR IS RESPONSIBLE FOR THE SAFE SUPPORT OF UTILITIES ACROSS EXCAVATIONS.

SHEETING, SHORING, AND Dewatering IS THE RESPONSIBILITY OF THE CONTRACTOR.

NO GEOTECHNICAL REPORT WAS PROVIDED AT THE TIME OF FOUNDATION DESIGN. A SOILS TESTING LABORATORY SHALL BE RETAINED BY THE OWNER TO PROVIDE CONSTRUCTION REVIEW TO ENSURE CONFORMANCE WITH THE CONSTRUCTION DOCUMENTS DURING THE EXCAVATIONS, BACKFILL, AND FOUNDATION PHASES OF THE PROJECT.

**SPREAD/TRENCH FOOTINGS:**

DETERMINATION OF FINAL BEARING CAPACITY, TORSION, AND EXCAVATION STRIPPING DEPTH, INSPECTION OF ALL SUBSOIL EXPOSED DURING STRIPPING, SITE DRAINAGE OPERATIONS, APPROVAL OF FILL MATERIALS, DENSITY TESTING OF FILLS TO ENSURE PLACEMENT PER SPECIFICATION REQUIREMENTS, INSPECTION OF FOUNDATION BEARING SURFACES, AND VERIFICATION OF ALLOWABLE BEARING PRESSURES ARE THE TESTING LABORATORY'S RESPONSIBILITY.

ALL FOUNDATIONS ARE TO REST ON FIRM UNDISTURBED SOIL OR COMPACTED FILL FREE FROM ORGANIC MATTER. IF FIRM SOIL CONDITIONS ARE ENCOUNTERED AT FOUNDATION DEPTHS SHOWN, NOTIFY OWNER'S REPRESENTATIVE BEFORE PROCEEDING WITH CONSTRUCTION.

CONTRACTOR SHALL COMPACT SUBGRADE. SEE FROSTING FROST DESIGN NOTES BELOW.

FOUNDATIONS HAVE BEEN DESIGNED BASED ON AN ASSUMED ALLOWABLE SOIL BEARING CAPACITY OF 1500 PSF.

NEW FOOTINGS PLACED ADJACENT TO EXISTING FOOTINGS SHALL BEAR AT THE SAME ELEVATION, UNLESS NOTED OTHERWISE.

STEP FOOTINGS AT A RATIO OF ONE (1) VERTICAL TO TWO (2) HORIZONTAL WITH A MAXIMUM VERTICAL STEP OF 2'-0" UNLESS NOTED OTHERWISE.

INDICATION AND LONG TERM EXPOSURE OF BEARING SURFACES, WHICH WILL RESULT IN DETERIORATION OF BEARING FORMATIONS, SHALL BE PREVENTED. FOOTINGS SHALL BE PLACED IMMEDIATELY FOLLOWING FOOTING EXCAVATIONS AND BEARING SURFACE INSPECTION.

UTILITY LINES SHALL NOT BE PLACED THROUGH OR BELOW FOUNDATIONS WITHOUT THE APPROVAL OF THE ENGINEER OF RECORD.

GROUNDWATER ASSUMED TO BE BELOW EXCAVATION DEPTH. IF GROUNDWATER IS ENCOUNTERED DURING EXCAVATION ON SITE, CONTRACTOR SHALL PROVIDE FOR ANY SITE DRAINAGE AND DE-WATERING REQUIREMENTS.

EXCAVATION TO VERIFY LOCATION OF ALL EXISTING PUBLIC AND PRIVATE UTILITIES PRIOR TO CONSTRUCTION. IF NECESSARY, UTILITIES SHALL BE RELOCATED PRIOR TO FOUNDATION INSTALLATION.

**CHARGING CABINET (PRE-FABRICATED ASSEMBLY FOUNDATION) & ALL CHARGING POST FOUNDATIONS: FROST DESIGN NOTES (IE, BOTTOM OF FOUNDATION ABOVE FROST LEVEL)**

CONCRETE FOUNDATIONS SHOULD BEAR DIRECTLY ON A PROPERLY COMPACTED FINE-DRAINING GRANULAR FILL CONSISTING OF NO. 57 STONE OR AN APPROVED EQUIVALENT.

GRANULAR FILL SHOULD EXTEND VERTICALLY TO THE MINIMUM RECOMMENDED REGIONAL FROST DEPTH (SEE SHEET 14 FOR LOCAL FROST DEPTH). GRANULAR FILL SHOULD BE PLACED IN 8 INCH LOOSE LIFTS AND COMPACTED WITH A VIBRATORY COMPACTOR. THE COMPACTOR EQUIPMENT SHOULD BE OPERATED OVER THE FULL WIDTH OF THE FOUNDATION UNDERCUT AREA UNTIL VISIBLE DEFORMATION OF THE BACKFILL CEASES. SEE SHEET 14 FOR LOCAL FROST DEPTH.

GEOTEXTILE (FILTER FABRIC) SHOULD BE PLACED BETWEEN THE GRANULAR BACKFILL AND COHESIVE SOILS TO PREVENT THE INFILTRATION OF FINES.

**CHARGING CABINET (PRE-FABRICATED ASSEMBLY FOUNDATION) & ALL CHARGING POST FOUNDATIONS: NO FROST DESIGN NOTES (IE, BOTTOM OF FOUNDATION BELOW FROST LEVEL)**

CONCRETE FOUNDATIONS SHOULD BE SUPPORTED ON A 6 INCH COMPACTED LAYER OF APPROVED FINE-DRAINING GRANULAR MATERIAL.

APPROVED MATERIAL SHOULD BE COMPACTED OVER THE FULL WIDTH OF THE INFILL AREA UNTIL VISIBLE DEFORMATION OF THE BACKFILL CEASES.

**SPECIAL INSPECTIONS:**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR SCHEDULING AND OVERSEEING OF ALL SPECIAL INSPECTIONS REQUIRED BY THE AUTHORITY HAVING JURISDICTION. SPECIAL INSPECTIONS MUST BE COMPLETED PRIOR TO FULL INFILTRATION APPROVAL.

**EXISTING SLAB REINFORCEMENT INVESTIGATION ONLY:**

CONCRETE SLAB PRIOR TO DRILLING

**CONCRETE:**

**GENERAL:**

ALL CONCRETE CONSTRUCTION SHALL CONFORM TO ACI 309-10, "STANDARD SPECIFICATION FOR STRUCTURAL CONCRETE" AND ACI 302, 305 AND 308 UNLESS NOTED OTHERWISE.

ALL DETAILING, FABRICATION AND PLACING OF CONCRETE SHALL CONFORM TO ACI 318-14, "BUILDING CODE REQUIREMENTS FOR STRUCTURAL CONCRETE" AND THE LATEST ACI MANUAL OF STANDARD PRACTICE FOR DETAIL REINFORCED CONCRETE STRUCTURES UNLESS NOTED OTHERWISE.

SAFETY AND PERFORMANCE OF THE STRUCTURE ARE THE RESPONSIBILITY OF THE CONTRACTOR IN SO FAR AS THEY ARE AFFECTED BY THE LOCATION AND JOINTS OF CONSTRUCTION JOINTS. SHOP DRAWINGS OF THE PROPOSED CONSTRUCTION DETAIL LOCATIONS AND DETAILS ARE TO BE SUBMITTED TO THE ARCHITECT FOR APPROVAL.

MAXIMUM SIZE OF AGGREGATE SHALL NOT EXCEED SIZE SUITABLE FOR INSTALLATION METHOD UTILIZED OR 1/2 CLEAR DISTANCE BEHIND OR BETWEEN REINFORCING. MAXIMUM SIZE MAY BE INCREASED 7/8 CLEAR DISTANCE PROVIDED WORKABILITY AND METHODS OF CONSOLIDATION SUCH AS VIBRATING WILL PREVENT HONEYCOMBS OR VOIDS.

ALL CONCRETE SHALL DEVELOP A MINIMUM COMPRESSIVE STRENGTH IN 28 DAYS AS FOLLOWS: ALL CONCRETE - 4000 PSI. ALL CONCRETE EXPOSED TO WEATHER SHALL CONTAIN 9% (± 1%) AIR ENTRAINMENT.

REINFORCING BARS SHALL CONFORM TO ASTM A615, GRADE 60.

WELDED WIRE FABRIC REINFORCING SHALL CONFORM TO ASTM A1664 AND BE FURNISHED IN FLAT SHEETS AND INSTALLED ON CHAIRS OR PRECAST CONCRETE BLOCKS.

NO TACK WELDING OF REINFORCING IN THE FIELD IS PERMITTED.

PROVIDE CORNER BARS AT ALL LOCATIONS WHERE REINFORCEMENT CHANGES DIRECTION.

REINFORCING EMBEDMENT AND LAP SPLICES (INCHES) FOR 4000 PSI CONCRETE

BAR SIZE ANCHORAGE SPACE ANCHORAGE TOP SPLICE

#3 18 25 15 33

#4 19 25 15 33

#5 20 25 15 33

#6 21 25 15 33

\* HORIZONTAL BARS WITH MORE THAN 1/2 OF CONCRETE BELOW BAR

NON-SHRINK GROUT SHALL MEET A MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 8000 PSI.

**FIBER CONCRETE MESH NOTES:**

STRENGTH 4000 PSI CNS-F

MINIMUM CURING FACTOR 80%

MINIMUM CURING FACTOR 6.5% AVG.

ENTRAINED AIR 4" MAX UNLESS HWWR OR MID RANGE WR; THEN 6" - 8"

WATER REDUCER NORMAL TYPE AS NEEDED (REQUIRED IF CONCRETE TEMPERATURE EXCEEDS 86° F)

RETARDER 50° - 90° F

ACCELERATOR NON-CHLORIDE TYPE ONLY THE USE OF CALCIUM CHLORIDE EQUIVALENT

FIBER 1.5 @ 1.5 LBS PER CUBIC YARD (AS FIBERMESH 300 OR EQUIVALENT)

\* CNS DESIGNATES A CONCRETE MIX DESIGN WITH 2 GALLON PER CUBIC YARD OF CALCIUM NITRITE CORROSION INHIBITOR AT 7.5% SILICA FINE.

F' DESIGNATES A CONCRETE MIX DESIGN WITH 1.5 LBS. FIBRILLATED MONOFILAMENT FIBER 1.5 INCHES IN LENGTH REINFORCEMENT PER CUBIC YARD OF CONCRETE

**STRUCTURAL STEEL**

**GENERAL:**

**MATERIAL PROPERTIES:**

STEEL ASTM A572 GR 50

PIPE: ASTM A53 TYPE E DR S, GRADE B

TUBE: ASTM A106 GRADE A (F<sub>y</sub> = 60 KSI)

DETAILING, FABRICATION, AND ERECTION SHALL CONFORM TO THE 2010 AISC (980-19) SPECIFICATIONS.

ALL WELDING SHALL BE DONE USING E-70XX ELECTRODES IN ACCORDANCE WITH AWS D1.1 SPECIFICATIONS.

FIELD VERIFY ALL CONDITIONS AT AND CONNECTIONS TO THE EXISTING CONSTRUCTION BEFORE FABRICATION.

ALL EXPOSED STRUCTURAL STEEL, ANCHOR RODS AND BOLTS SHALL BE HOT DIP GALVANIZED PER ASTM A123.

UNLESS NOTED OTHERWISE ON THE DRAWING, ALL ANCHOR RODS SHALL CONFORM TO ASTM F164 G 35 WITH HEAVY HEXAGONAL NUT.

SUBMIT FABRICATION AND ERECTION DRAWINGS SHOWING ALL DETAILS, CONNECTIONS, MATERIAL DESIGNATIONS, AND TOP STEEL ELEVATIONS FOR APPROVAL. THE SHOP DRAWINGS SHALL BE REVIEWED FOR CONFORMANCE WITH THE SHOP DRAWING REVIEW PROCEDURE. SUCH APPROVAL SHALL NOT RELIEVE THE FABRICATOR/CONTRACTOR OF HIS RESPONSIBILITY FOR EITHER THE ACCURACY OF THE DETAILED DIMENSIONS IN THE SHOP AND ERECTION DRAWINGS OR THE GENERAL FIT-UP OF PARTS THAT ARE TO BE ASSEMBLED IN THE FIELD.



2000 SOUTH MAIN STREET  
P.O. BOX 1219  
FLORENCE, KY 40402



320 South Main Street  
Attn: OH 44211  
395.572.2100 Fax: 395.572.2102

CPD PROJECT NUMBER: 2018K11AR  
DRAWN BY: RHEPMA  
CHECKED BY: WILLIAM BUGH  
INSTALLED BY:

REV	DATE	DESCRIPTION
B	10.01.18	ISSUED FOR PERMITS REVIEW
A	08.20.18	ISSUED FOR PERMITS REVIEW

**NOT FOR CONSTRUCTION**

70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 40402

SHEET TITLE  
GENERAL NOTES

SHEET NUMBER  
GN-3





3400 OCEA CENTER RD  
PALO ALTO, CA 94304  
(650) 841-4000



520 South Main Street  
333.772.2100 Fax 333.772.2102

GPD PROJECT NUMBER: 2019H4136  
DRAWN BY: LUC  
CHECKED BY: RPEPM  
INSTALLATION MANAGER: WILLIAM BIRCH

REV.	DATE	DESCRIPTION
B	10/21/19	ISSUED FOR 100% REVIEW
A	08/28/19	ISSUED FOR 30% REVIEW
1		
2		
3		
4		
5		
6		
7		
8		
9		
10		

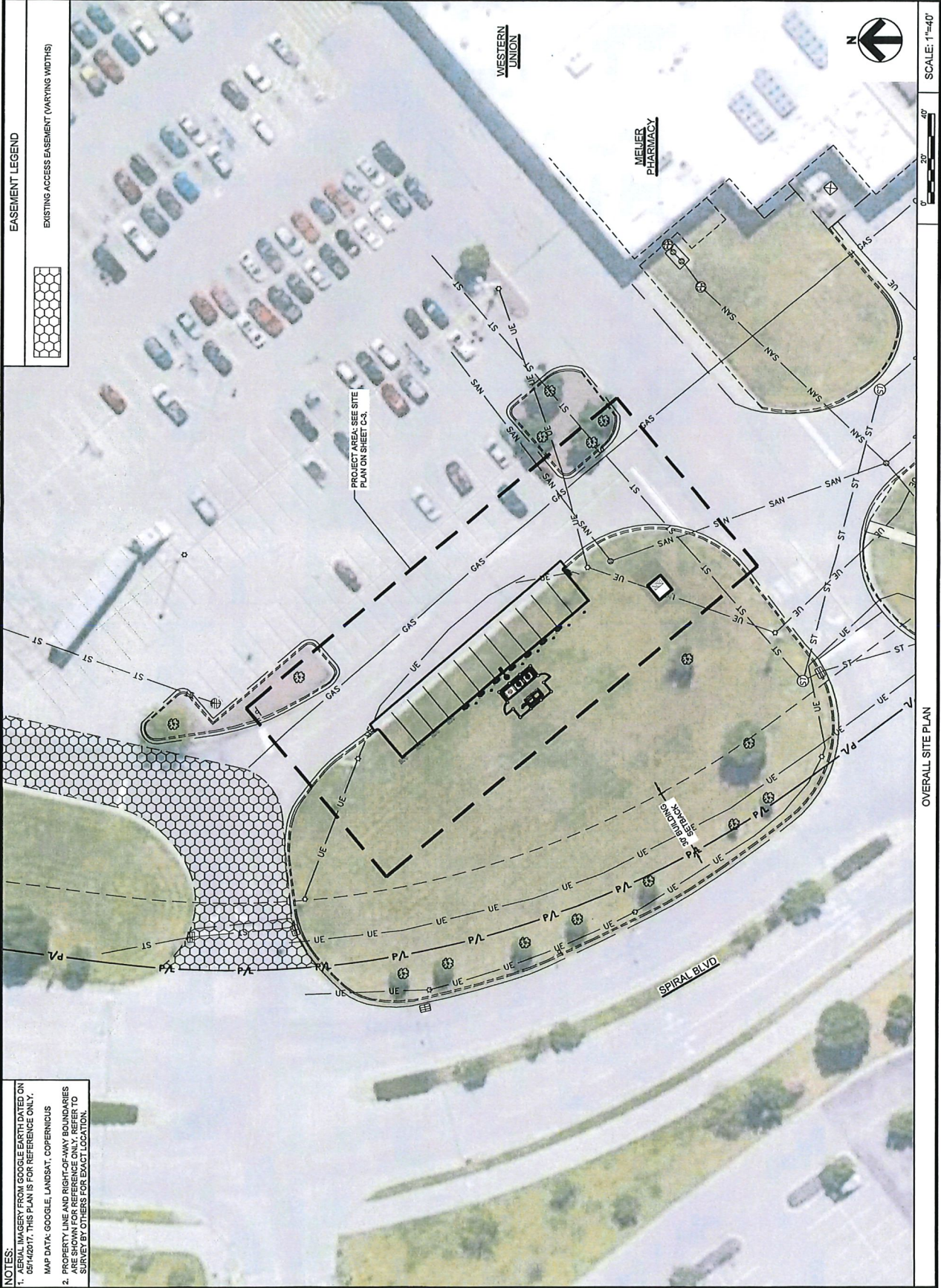
NOT FOR  
CONSTRUCTION

TITLE IS A VIOLATION OF THE LAW FOR ANY PERSON,  
COMPANY OR ORGANIZATION TO REPRODUCE OR  
USE THIS DOCUMENT WITHOUT THE WRITTEN  
CONSENT OF THE REGISTERED PROFESSIONAL ENGINEER, P.E.  
ALTER THIS DOCUMENT.

70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
OVERALL SITE  
PLAN

SHEET NUMBER  
C-1



NOTES:  
1. ALL IMAGERY FROM GOOGLE EARTH DATED ON 05/14/2017. THIS PLAN IS FOR REFERENCE ONLY.  
2. PROPERTY LINE AND RIGHT-OF-WAY BOUNDARIES ARE SHOWN FOR REFERENCE ONLY. REFER TO SURVEY BY OTHERS FOR EXACT LOCATION.

SCALE: 1"=40'

OVERALL SITE PLAN



3000 DEER CREEK RD  
PALMDALE, CA 91366  
(800) 941-1000



500 South Main Street  
330.972.2100 Fax: 330.972.2102

GPD PROJECT NUMBER: 2018141.02  
DRAWN BY: LMJ  
CHECKED BY: RP/EPM  
INSTALLATION MANAGER: WILLIAM B. BUCH

REV	DATE	DESCRIPTION
1	10/21/18	ISSUED FOR TOWN REVIEW
2	11/16/18	ISSUED FOR TOWN REVIEW

NOT FOR CONSTRUCTION


IT IS A VIOLATION OF THE LAW FOR ANY PERSON TO REPRODUCE OR TRANSMIT IN ANY MANNER OR BY ANY MEANS, WITHOUT THE WRITTEN PERMISSION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT.

70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
DEMOLITION PLAN

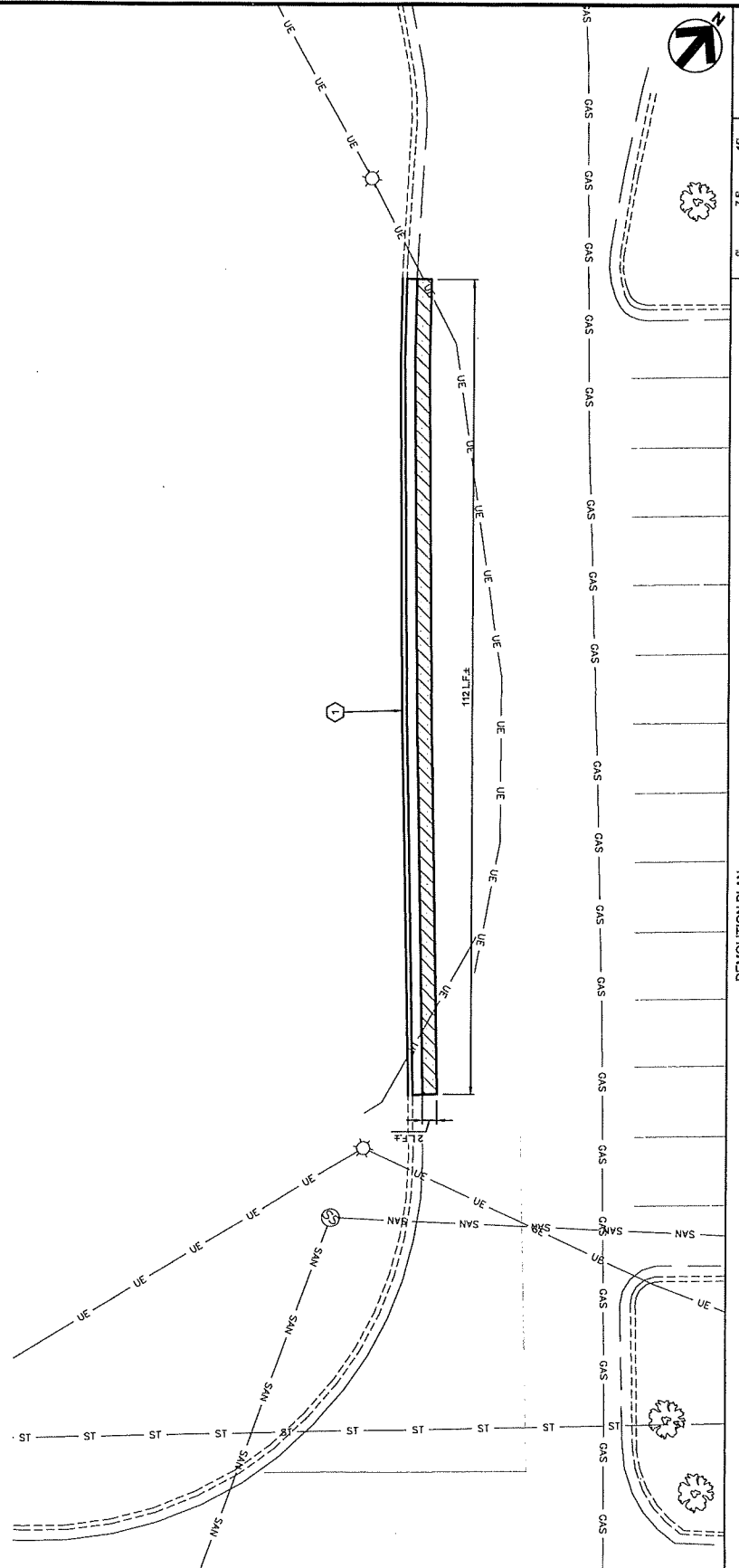
SHEET NUMBER  
C-2

DEMOLITION KEYNOTES AND LEGEND

- EXISTING CURB & GUTTER TO BE REMOVED  
 EXISTING ASPHALT TO BE REMOVED (APPROXIMATELY 225 SF) TRENCHING NOT INCLUDED  
 DENOTES LIMITS OF SAWCUT  
 1" = 20' L.F. ±

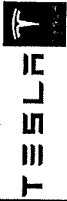
GENERAL SHEET NOTES

- CONTRACTOR SHALL REMOVE EXISTING PAVEMENT AND/OR CURB USING CLEAN SAWCUTS TO INSTALL PROPOSED UNDERGROUND CONDUITS AND REPLACE PAVEMENT AND/OR CURB AFTER CONDUITS ARE INSTALLED. SEE ELECTRICAL SHEETS FOR CONDUIT ROUTINGS. APPROXIMATE CONDUIT RUN LENGTHS AND TRENCH DETAIL. CONTRACTOR SHALL MEET OR EXCEED EXISTING STANDARDS FOR ALL TRENCHING. REPORT ANY DISCREPANCIES PRIOR TO PERFORMING WORK.
- APPLY LIQUID ASPHALT AT ALL JOINTS BETWEEN CONDUITS AND AT ALL JOINTS BETWEEN ASPHALT MEETS EXISTING, INCLUDING SAW CUT JOINTS.
- FOR TRAFFIC CONTROL PROCEDURES (IF APPLICABLE), SEE TRAFFIC CONTROL NOTES ON SHEET GN-1.



DEMOLITION PLAN

SCALE: 1"=15'



300 ZEEA CENTER DR  
PALM ALTO, CA 94496  
(415) 961-3000



520 South Main Street  
300.672.2100 Fax 300.672.2105

OPD PROJECT NUMBER: 2019141.04  
DRAWN BY: LMJ  
CHECKED BY: RPE/PM  
INSTALLATION MANAGER: WILLIAM BUDDE

REV	DATE	DESCRIPTION
B	10/21/18	ISSUED FOR 10% REVIEW
A	02/20/18	ISSUED FOR 50% REVIEW

**NOT FOR CONSTRUCTION**

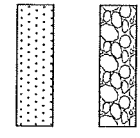
70 SPIRAL DR  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
SITE PLAN

SHEET NUMBER  
C-3

IT IS A VIOLATION OF THE LAW FOR ANY PERSON TO REPRODUCE, TRANSMIT, OR IN ANY MANNER TO DISSEMINATE THE INFORMATION CONTAINED HEREIN WITHOUT THE WRITTEN PERMISSION OF THE DESIGNER.

- ### CONSTRUCTION KEYNOTES AND LEGEND
- PROPOSED PAD MOUNTED ELECTRICAL UTILITY TRANSFORMER BY UTILITY. CONTRACTOR SHALL PROVIDE CONCRETE PAD PER UTILITY SPECIFICATIONS. COORDINATE FINAL LOCATION WITH UTILITY. SEE ELECTRICAL PLANS FOR PROPOSED ROUTING.
  - PROPOSED EQUIPMENT CLEAR SPACE (TYPICAL).
  - PROPOSED ELECTRICAL METERS MOUNTED TO SWITCHGEAR PER ELECTRICAL COMPANY SPECIFICATIONS AND DETAILS ON ELECTRICAL SHEETS.
  - PROPOSED SWITCHGEAR ASSEMBLY PER ELECTRICAL DRAWINGS. SEE DETAIL ON SHEET C-7 FOR ANCHORING.
  - PROPOSED TESLA CHARGE POST WITH INDIVIDUAL PRECAST CONCRETE FOUNDATION (TYPICAL OF B). SEE DETAILS ON SHEET C-6.
  - PROPOSED TESLA NON-ILLUMINATED PARKING SIGN (TYPICAL OF B). SEE DETAILS ON SHEET C-6. SEE CHARGING POST SCHEDULE THIS SHEET FOR SIGN TYPE. WHERE APPLICABLE, CONTRACTOR SHALL MOUNT SIGN TO LIGHT POLE IN LIEU OF POST.
  - PROPOSED TESLA CHARGING CABINET WITH CONCRETE FOUNDATION (TYPICAL OF 2). SEE DETAILS ON SHEET C-4 & C-7.
  - PROPOSED CONCRETE PAD. SEE DETAIL ON SHEET C-7.
  - PROPOSED LIGHT POLE. SEE DETAIL ON SHEET C-7. SEE ELECTRICAL DRAWINGS FOR POLE AND FIXTURE SPECIFICATIONS AND WIRING.
  - PROPOSED TREX "SECLUSION" FENCING. SEE DETAILS ON SHEET C-4.
  - PROPOSED TREX "SECLUSION" SINGLE GATE WITH KEYPAD AND PANIC HARDWARE (TYPICAL OF 2). SEE DETAILS ON SHEET C-4.
  - PROPOSED PAINTED 4" WIDE SOLID WHITE STRIPE. SEE STRIPING NOTES ON SHEET C-4.
  - PROPOSED 6" CONCRETE CURB. SEE DETAIL ON SHEET C-7.
  - ALL DISTURBED AREAS NOT TO BE PAVED OR MULCHED SHALL BE SODDED PER LANDSCAPE/IRRIGATION NOTES ON SHEET GN-2.
  - PROPOSED MASTER CONTROLLER MOUNTED ON H-FRAME. SEE ELECTRICAL SHEETS.
  - PROPOSED INFRASTRUCTURE FOR FUTURE CHARGING CABINET. SEE DETAILS ON SHEET C-7.
  - EXISTING ELECTRICAL LINE TO BE RELOCATED AS NECESSARY.



PROPOSED ASPHALT SURFACE WITH TRENCHING AND CURB. PROPOSED ASPHALT TO MATCH EXISTING IN TYPE AND DEPTH.

RIVER ROCK MULCH AREA. POLYETHYLENE FABRIC SHALL BE LAID UNDER AGGREGATE MULCH, 3/4" x 1/2" SHALE, WASHED AND ROUNDED. RIVER ROCK MULCH AREA PER THE PLAN. RIVER ROCK MULCH SHALL BE INSTALLED AT 4 INCH DEPTH.

### V3 (250kW) TESLA OUTDOOR EQUIPMENT SCHEDULE

DESCRIPTION	PART NUMBER	QUANTITY
CHARGING CABINET	1450728-00-D	2
CHARGING POST	1088685-00-D	8
SITE MASTER CONTROLLER	1137202-02-C	1
UNIVERSAL PRECAST	1478984-00-A	8
CHARGING CABINET BASE TEMPLATE	1521279-00-A	2

### CHARGING POST CIRCUIT SCHEDULE

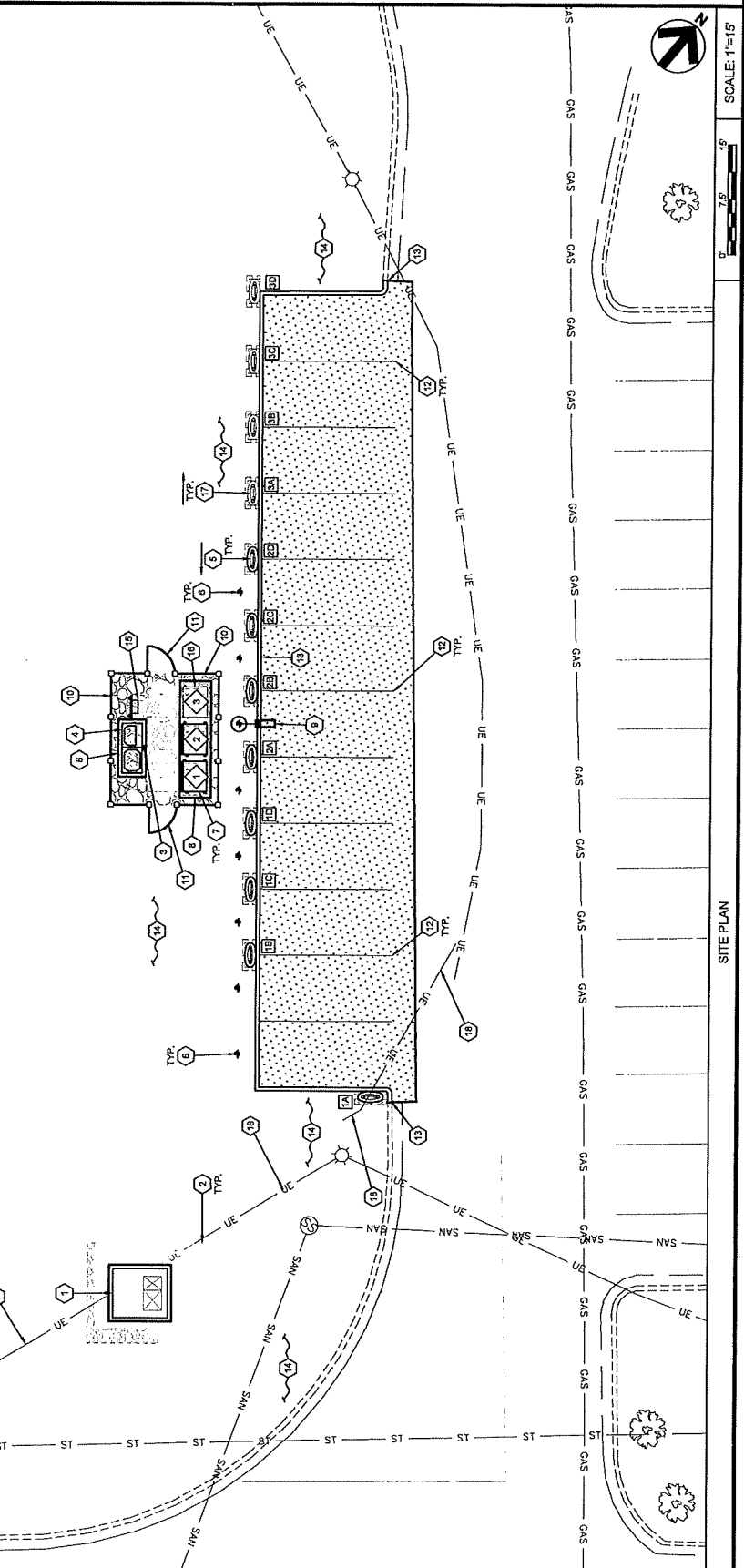
CHARGING CABINET	CHARGE POST	DEDICATED/DISABLED
1	1A	DEDICATED
	1B	DEDICATED
	1C	DEDICATED
	1D	DEDICATED
	2A	ENABLED
	2B	ENABLED
	2C	ENABLED
	3A	FUTURE
	3B	FUTURE
	3C	FUTURE
	3D	FUTURE

### PARKING STALL SCHEDULE

EXISTING STANDARD STALLS UTILIZED AS A RESULT OF THIS PROJECT	PROPOSED TESLA STALLS	PROPOSED FUTURE STALLS	NET STALL COUNT
0	8	4	+12

- ### GENERAL SHEET NOTES
- CONTRACTOR SHALL REMOVE EXISTING PAVEMENT AND CURB USING CLEAN SAWCUTS TO INSTALL NEW PAVEMENT AND CURB. REPLACE PAVEMENT AND/OR CURB AFTER CONDUITS HAVE BEEN INSTALLED. SEE ELECTRICAL SHEETS FOR CONDUIT ROUTING. APPROXIMATE LOCATIONS OF EXISTING CONDUITS SHALL BE MET OR EXCEED EXISTING PAVEMENT SPECIFICATIONS. NOTIFY ENGINEER OF ANY DISCREPANCIES PRIOR TO PERFORMING WORK.
  - APPLY LIQUID ASPHALT AT ALL JOINTS BETWEEN CONCRETE AND ASPHALT AND WHERE PROPOSED ASPHALT MEETS EXISTING, INCLUDING SAW CUT JOINTS.
  - FOR TRAFFIC CONTROL PROCEDURES AND WHERE PROPOSED ASPHALT MEETS EXISTING, INCLUDING SAW CUT JOINTS.



SCALE: 1"=15'

SITE PLAN





1000 DEER CREEK RD  
PALO ALTO, CA 94304  
1-800-874-6600



520 South Main Street  
330.272.1100 Fax 330.272.2102

CPD PROJECT NUMBER: 2019141.02  
DRAWN BY: LMJ  
CHECKED BY: RRP/PA  
INSTALLATION MANAGER: WILLIAM BUSCO

REV	DATE	DESCRIPTION
B	10/21/18	ISSUED FOR 10% REVIEW
A	10/21/18	ISSUED FOR 10% REVIEW

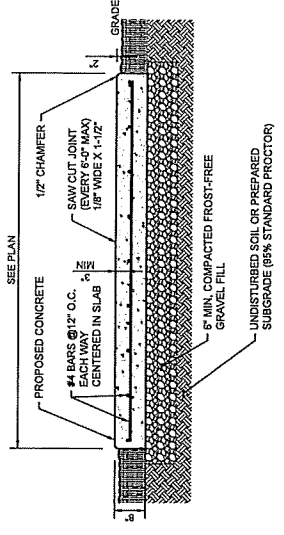
NOT FOR CONSTRUCTION

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, FIRM OR COMPANY TO REPRODUCE OR TRANSMIT ANY INFORMATION CONTAINED HEREIN WITHOUT THE WRITTEN CONSENT OF A LICENSED PROFESSIONAL ENGINEER TO ALTER THIS DOCUMENT.

70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

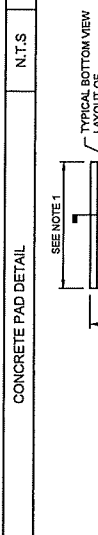
SHEET TITLE  
CIVIL DETAILS

SHEET NUMBER  
C-7

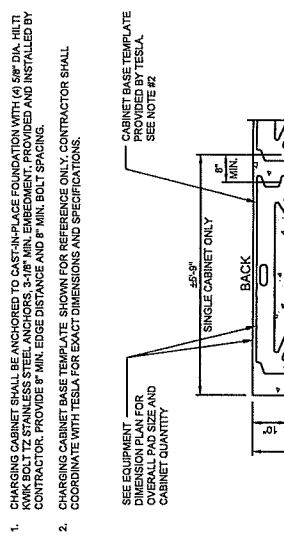


**CONCRETE PAD NOTES & CONCRETE SPECIFICATIONS:**

- FOUNDATION AREA SHALL BE EXCAVATED TO THE DEPTH AND DIMENSIONS SHOWN ON THE PLANS. OTHER UNSUITABLE MATERIAL SHALL BE REMOVED AND LEGALLY DISPOSED OF OFF-SITE. THE SUBGRADE SHALL BE ROLLED WITH A 1-TON, VIBRATORY, WALK-BEHIND ROLLER AT A SPEED OF LESS THAN 2 FPS. 8 PASSES MINIMUM, TO PROVIDE UNYIELDING SURFACE.
- WITH FILL MEETING THE SPECIFICATIONS OF STRUCTURAL FILL.
- CONCRETE TO HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH (f'c)=4000 psi.
- REINFORCING BAR TO BE ASTM A615 GRADE 60.
- ALL CONCRETE MATERIALS & MINIMUM CONCRETE COVER PER ACI SPECIFICATIONS.
- STATE BUILDING CODE.
- PERIMETER SOIL SURFACE SLOPES TO MAINTAIN A MINIMUM OF 6" OF SOIL COVER TO THE BOTTOM OF THE PAD.



CONCRETE PAD DETAIL N.T.S. 1



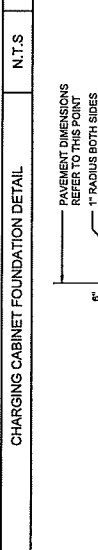
**NOTES:**

- CHARGING CABINET SHALL BE ANCHORED TO CAST-IN-PLACE FOUNDATION WITH (4) 5/8" DIA. HILTI KWIK BOLT TZ STAINLESS STEEL ANCHORS. 3-1/8" MIN. EMBEDMENT, PROVIDED AND INSTALLED BY CONTRACTOR. PROVIDE 6" MIN. EDGE DISTANCE AND 6" MIN. BOLT SPACING.
- CHARGING CABINET BASE TEMPLATE SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL COORDINATE WITH TESLA FOR EXACT DIMENSIONS AND SPECIFICATIONS.

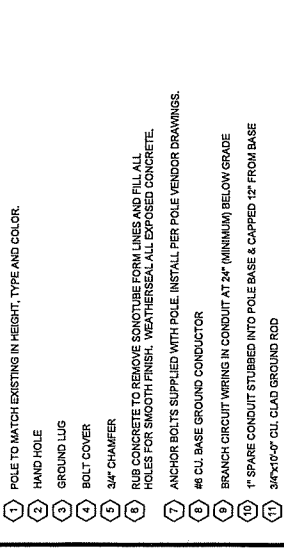
SEE EQUIPMENT DIMENSION PLAN FOR DIMENSION PLAN FOR DIMENSION PLAN AND CABINET QUANTITY. SEE NOTE #2.

CABINET BASE TEMPLATE PROVIDED BY TESLA. SEE NOTE #2.

TEMPLATE ANCHOR SEE NOTE #1 & #2.



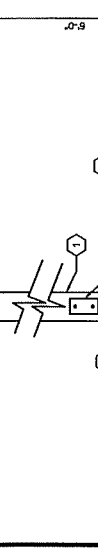
CHARGING CABINET FOUNDATION DETAIL N.T.S. 3



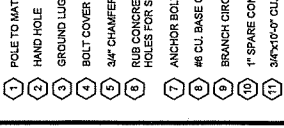
**KEYED NOTES**

- POLE TO MATCH EXISTING IN HEIGHT, TYPE AND COLOR.
- HAND HOLE
- GROUND LUG
- BOLT COVER
- 3/4" CHAMFER
- RIB CONCRETE TO REMOVE CONDUIT BORE FORM LINES AND FILL ALL HOLES FOR SMOOTH FINISH. WEATHERSEAL ALL EXPOSED CONCRETE.
- ANCHOR BOLTS SUPPLIED WITH POLE. INSTALL PER POLE VENDOR DRAWINGS.
- #6 CL. BASE GROUND CONDUCTOR
- BRANCH CIRCUIT WIRING IN CONDUIT AT 24" (MINIMUM) BELOW GRADE
- 1" SPARE CONDUIT STUBBED INTO POLE BASE & CAPPED 12" FROM BASE
- 3/4"x1/2"-9" CL. CLAD GROUND ROD
- (6) #8 REBAR (VERTICAL) TIE-WIRED TO #4 REBAR TIES AT 12" ON CENTER, U.N.O.
- REINFORCED CONCRETE, 4000 PSI COMPRESSIVE STRENGTH
- ADDITIONAL #4 REBAR TIES @ 3" ON CENTER

PROPOSED TESLA SIGN MOUNTED TO LIGHT POLE WHERE APPLICABLE. SEE DETAILS 3 & 4, SHEET C-6.



CHARGING CABINET FOUNDATION DETAIL N.T.S. 4



**NOTES:**

- SEE FLOOR PLAN ON ELECTRICAL SHEETS FOR MOUNTING HOLE LOCATION AND SWITCHGEAR BAY DIMENSIONS.
- SEE ELEVATION ON ELECTRICAL SHEETS FOR BASE CHANNEL DIMENSION.
- MINIMUM EDGE DISTANCE FOR PROPOSED ANCHORAGE SHALL BE 6".



CONCRETE PAD DETAIL N.T.S. 1

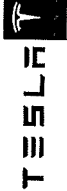
NOT FOR CONSTRUCTION

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, FIRM OR COMPANY TO REPRODUCE OR TRANSMIT ANY INFORMATION CONTAINED HEREIN WITHOUT THE WRITTEN CONSENT OF A LICENSED PROFESSIONAL ENGINEER TO ALTER THIS DOCUMENT.

70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
CIVIL DETAILS

SHEET NUMBER  
C-7



3400 DEBRA CREEK RD  
PALO ALTO, CA 94304  
(650) 991-4000



520 South Main Street  
Florence, KY 41042  
330.572.2100 Fax 330.572.2102

CPD PROJECT NUMBER: 2019A11.8  
DRAWN BY: LMJ  
CHECKED BY: RP/PH  
INSTALLATION MANAGER: WILLIAM BUSHY

NO.	DATE	ISSUED FOR	DESCRIPTION
1	10.21.19	ISSUED FOR 10% REVIEW	
2	12.20.19	ISSUED FOR 20% REVIEW	
3			
4			
5			
6			

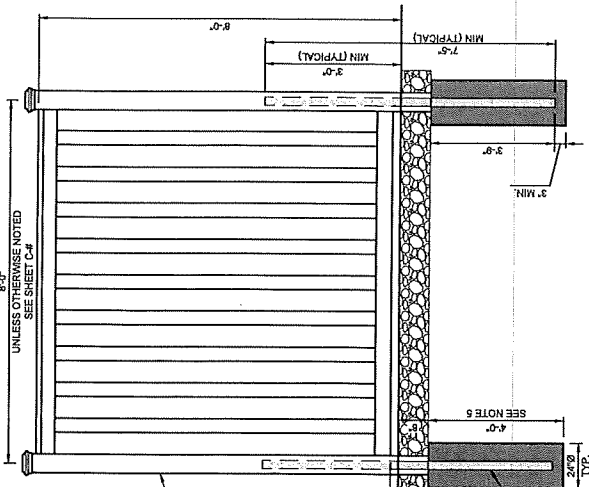
**NOT FOR CONSTRUCTION**

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, CORPORATION OR FIRM TO REPRODUCE OR TRANSMIT THIS DOCUMENT IN ANY MANNER WITHOUT THE WRITTEN PERMISSION OF THE ORIGINAL AUTHOR.

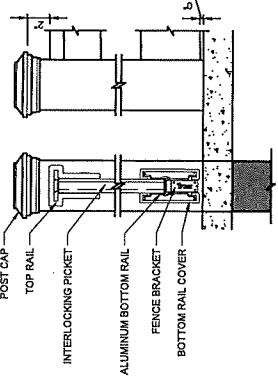
70 SPIRAL DR.  
(TESLA STATION)  
FLORENCE, KY 41042

SHEET TITLE  
CIVIL DETAILS

SHEET NUMBER  
C-8



NOTE:  
FENCING SHALL BE TREX "SECLUSION" TYPE WITH "SADDLE" COLOR.

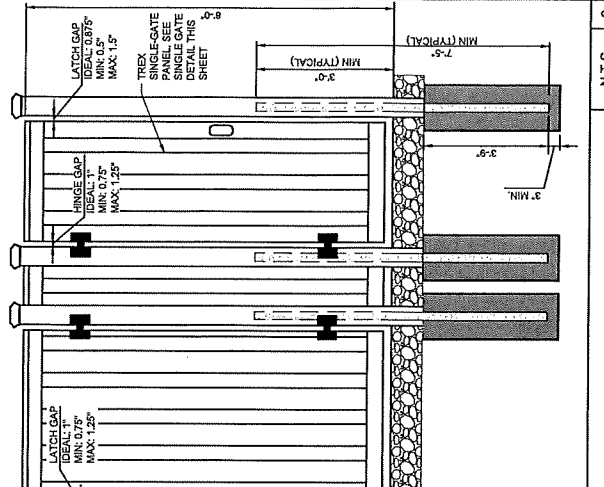


- NOTES:
1. INSTALLATION TO BE COMPLETED PER MANUFACTURER'S SPECIFICATION.
  2. THIS DRAWING IS PROVIDED FOR PLANNING PURPOSES. REFER TO MANUFACTURER'S INSTALLATIONS FOR CONSTRUCTION DETAILS.
  3. REFER TO MANUFACTURER'S WEBSITE FOR PRODUCT INFORMATION.
  4. DRAWING NOT TO SCALE.
  5. IF FROST DEPTH EXCEEDS 4'-0", FOOTER TO EXCEED FROST DEPTH.
  6. ENGINEER OF RECORD SHALL BE CONTACTED IMMEDIATELY IF GROUND WATER IS ENCOUNTERED DURING CONSTRUCTION.

TREX FENCE DETAIL

N.T.S.

N.T.S.



- GATE NOTES:
- TREX SECLUSION SINGLE GATE OPENING SHALL BE BETWEEN EDGE OF COMPOSITE POSTS
  - TREX SECLUSION DOUBLE GATE OPENING SHALL BE 1/2" BETWEEN EDGE OF COMPOSITE POSTS
  - REFER TO SITE PLAN FOR APPLICABLE GATE TYPES AND QUANTITIES

GENERAL NOTE

CONTRACTOR SHALL INSTALL MARKINGS FOR SIDE OF WIDE SWING GATE INDICATING THE FOLLOWING:

24"

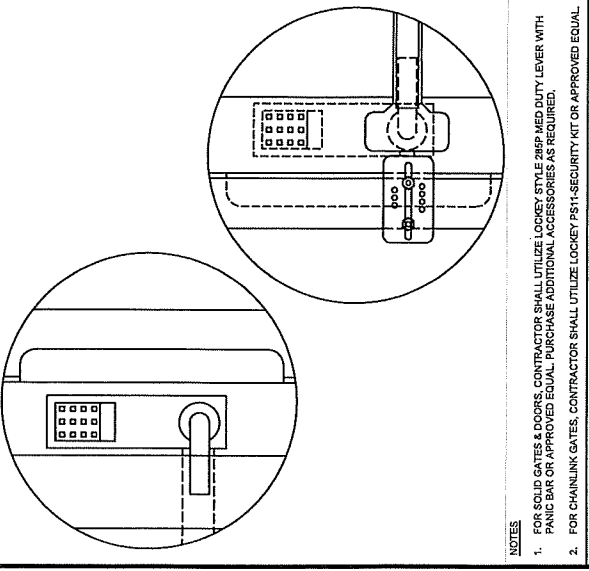
DOUBLE GATE SHALL REMAIN OPEN AT ALL TIMES UNLESS USING ELECTRICAL EQUIPMENT

- INSTALLATION NOTES:
1. INSTALLATION TO BE COMPLETED PER MANUFACTURER'S RECOMMENDATIONS.
  2. THIS DRAWING IS PROVIDED FOR PLANNING PURPOSES. REFER TO MANUFACTURER'S RECOMMENDATIONS FOR CONSTRUCTION DETAILS.
  3. REFER TO MANUFACTURER'S WEBSITE FOR PRODUCT INFORMATION.
  4. DRAWING NOT TO SCALE.
  5. IF FROST DEPTH EXCEEDS 4'-0", FOOTER TO EXCEED FROST DEPTH.

TREX GATE DETAIL

N.T.S.

N.T.S.



SINGLE GATE KEYPAD DETAIL W/ PANIC HARDWARE

N.T.S.

N.T.S.

- NOTES:
1. ALL PAVEMENT MARKINGS TO BE WHITE PAVEMENT PAINT, UNLESS STATED OTHERWISE.
  2. MARKINGS (STRIPING) PAINT FOR PARKING SPACES, TRAFFIC ARROWS, ADA PARKING AND SYMBOLS, ETC., PER LOCAL REQUIREMENTS AND AS FOLLOWS:
    - 2.1. EXISTING SURFACES WITHOUT ANY SEAL COATING: OIL BASE (ALLOY RESIN TYPE TO MEET FEDERAL SPECIFICATION TTP-1892)
    - 2.2. NEW OR EXISTING SURFACES WITH A TOP COATING OR SEAL COATING (USUALLY WATER BASE FAST DRYING 100% ACRYLIC TYPE): WATER BASE TYPE TO MEET FEDERAL SPECIFICATION TTP-01852. FOR COLD WEATHER APPLICATION PAINT PRODUCT SHALL BE IN ACCORDANCE WITH ASTM-D2588, D3723, D1492, D582 AND D711.
  3. PROVIDE A NON-SLIP AGGREGATE ADDITIVE TO MARKING PAINT USED AT ADA ACCESS RAMP.
  4. APPLY 2 COATS WITH STRAIGHT EDGES, YELLOW ON CONCRETE/WHITE ON ASPHALT EXCEPT WHEN MATCHING ADJACENT OR EXISTING COLOR WHEN THE PAVING IS AN EXPANSION OR SEGMENT OF A LARGER LOT.

STRIPING NOTES

N.T.S.