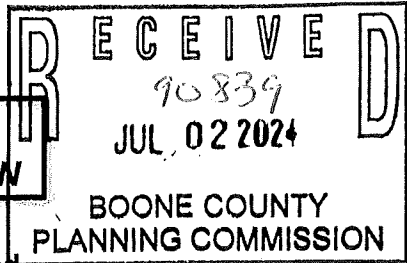


**ZONING MAP AMENDMENT  
BOONE COUNTY PLANNING COMMISSION**



Seven (7) copies of submitted drawings are required

**An application consists of all fees paid in full, submitted drawings, and a completed application form**

**SECTION A:** (To be completed by applicant)

1. Name of Project: Hickory Burlington and Poplar Pointe Apartments
2. Location of Project: Hickory Burlington and Poplar Pointe Apartments
3. Total Acreage of Project: Off Lariat Way
4. Current Zoning of Property: 44.37 acres
5. Proposed Zoning of Property (classification being requested): SR-1 PD
6. Proposed Use(s) (specify each use):  
~~Patio Rental Homes and Apartments~~
7. Proposed Building Intensities (specify for each building):  
~~139 Patio Rental homes and 157 Multi-Family Units, totally 296 units.~~
8. Are you applying for any of the following (check all that apply):  
 Conditional Use Permit     Variance
9. Current Owner: Delaware Crossing, LLC  
Address: 25 Town Center Boulevard, Suite 104  
Crestview Hills                      KY                      41017  
City    State    Zip Code  
Phone Number: pmanger@gallensteincompanies.com      Fax Number: N/A  
Email: pmanger@gallensteincompanies.com
10. Applicant: Charter Commercial, LLC  
Address: 25 Town Center Boulevard, Suite 104  
Crestview Hills                      KY                      41017  
City    State    Zip Code  
Phone Number: 513-403-1335                      Fax Number: N/A  
Email: pmanger@gallensteincompanies.com
11. Are there any existing buildings on the site:     Yes                       No  
If yes, indicate how many: \_\_\_\_\_  
One residence and couple auxiliary structures

1115

572

12. 1196 93 2021  
Deed Book Page Number Group Number

13. Have you had a pre-application meeting with the BCPC staff:  Yes  No

14. Have you submitted a Concept Development Plan:  Yes  No

15. Have you met or discussed your proposed development with any of the following organizations/agencies (check all that apply):

- |  |   |
|--|---|
| <input checked="" type="checkbox"/> Boone County Building Department   | <input type="checkbox"/> Local Fire District                          |
| <input type="checkbox"/> Boone County Public Works Department  | <input type="checkbox"/> Local School District                        |
| <input type="checkbox"/> Boone County Water District   | <input type="checkbox"/> Northern Kentucky Health Department          |
| <input type="checkbox"/> Cincinnati Bell   | <input type="checkbox"/> Owen Cooperative Electric, Inc.              |
| <input checked="" type="checkbox"/> Cincinnati/Northern Kentucky International Airport (Kentucky Airport Zoning Commission for height restrictions near the airport) | <input type="checkbox"/> Sanitation District No. 1                    |
| <input type="checkbox"/> Duke Energy   | <input type="checkbox"/> USDA NRCS/Boone County Conservation District |
| <input type="checkbox"/> Florence Public Services Department   | <input type="checkbox"/> Other: _____                                 |
| <input type="checkbox"/> Kentucky Division of Water  |   |
| <input type="checkbox"/> Kentucky Transportation Cabinet   |   |

16. Concept Development Plan Jurisdiction/Location (check all that apply):  
 Unincorporated Boone  Florence  Walton  Union

17. Waiver of 60 Day Time Requirement by Originator for Final Planning Commission Action:

In accordance with the provisions of KRS 100.211, the applicant(s) and property owner(s) or originator(s) hereby waive the 60 day time limit for the Boone County Planning Commission to take final action on my (our) Zoning Map Amendment/Concept Development Plan application. This time limit waiver is considered effective immediately upon receipt by the Boone County Planning Commission and expires on September 19, 2024

Property Owner's Signature:

Paul Mang

Applicant's Signature:

Paul Mang

**SECTION B:** (To be completed by Planning Commission staff)

1. Date Received: 7/2/24 Fee Received: \$3203.31 Receipt #: 90839
2. Number of Copies Received: \_\_\_\_\_
3. Has the following been submitted (check all that apply):
  - Completed Application
  - Concept Development Plan
  - Legal Description
  - Names and Mailing Addresses of Adjacent Property Owners
4. Date the application is Administratively Complete (as defined in KRS 100.211): \_\_\_\_\_
5. Staff Reviewer: M. Schwent
6. Committee Chairperson: C. Gulick
7. Scheduled Public Hearing Date: 8/21/2024
8. Boone County Planning Commission Action: \_\_\_\_\_ Date of Action: 10/16/2024
  - \_\_\_\_\_ Approved
  - \_\_\_\_\_ Approved with Conditions
  - Denial
  - \_\_\_\_\_ Other
9. Resolution Number: \_\_\_\_\_

Boone County Planning Commission  
Boone County Administration Building  
2950 Washington Street, Room 317  
P.O. Box 958  
Burlington, Kentucky 41005  
Phone: 859-334-2196 Fax: 859-334-2264  
[plancom@boonecountky.org](mailto:plancom@boonecountky.org)  
[www.boonecountky.org](http://www.boonecountky.org)

# EXHIBIT

“A”

## STAFF REPORT

#1

Request of **Charter Commercial, LLC (applicant)** for **Delaware Crossing, LLC (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

August 21, 2024

### REQUEST

- A. The request is to rezone an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way from SR-1 to SR-1/PD to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

### SITE HISTORY

- 1969-1981 Based on information contained in the Boone County GIS, the buildings located at 1841 Holbrook Lane were constructed.
- 2018 On April 27, 2018, the Boone County planning Commission approved a Preliminary Plat for Valley Creek Farms subdivision.
- 2022 On January 31, 2022, the Boone County Planning Commission approved a Conveyance Plat creating the southern lot of the site in question as a non-buildable lot.

### APPLICABLE REGULATIONS

- A. Section 308 of the Boone County Zoning Regulations states that before any map amendment is granted, the Planning Commission and legislative body shall use the following criteria:
1. The map amendment is in agreement with the adopted comprehensive plan and any specific study designed to further detail the Boone County Comprehensive Plan for the location in question; or
  2. The existing zoning classification is inappropriate and that the proposed zoning classification is appropriate; or
  3. There have been major changes of an economic, physical, or social nature not anticipated in the adopted comprehensive plan that substantially alter the area's character.
- B. Section 930 of the Boone County Zoning Regulations states that "the purpose of the Suburban Residential One district is to provide a low density, residential environment whose dwelling types and densities are typical of a suburban character. It is also to provide limited or passive and active recreational uses that are appropriate to the permitted uses in the district. Suburban Residential One districts will be located on lands

within established urban areas where adequate infrastructure facilities and services are available or proposed.”

C. Section 931 of the Boone County Zoning Regulations does not identify attached single-family dwellings or multi-family dwellings as a principally permitted use in the SR-1 district.

D. Section 1506 of the Boone County Zoning Regulations provides for the following planned development criteria:

1. **Mixed Use Development and Pedestrian Orientation:** Planned Developments shall generally have a mixed use orientation (combination of differing types of residential, commercial, public or civic, and/or industrial uses) both within the development itself and relative to the relationship between the proposed planned development and adjacent sites. The additional intensity allowed in a Planned Development by Section 1501 shall only be permitted when a true mixed use and/or an amenity - oriented development with community and recreation facilities as described in this standard is provided.
2. In general, Planned Developments shall have a pedestrian orientation, where it is possible to live, work, shop, and play in the same immediate vicinity without a required dependence on the automobile. This may be accomplished through the use of comprehensive pedestrian circulation networks including multipurpose paths and walks along main routes and open spaces such as stream corridors, between major destinations within the development and adjoining areas, secondary walk connections to the multi-purpose paths, the creation of a designed pedestrian environment including street trees in addition to other required landscaping, decorative street lights and other street furniture, and seating areas, and the use of integral curb walks where appropriate along streets. Disruptions in major paths due to street and drive intersections shall be minimal. Additionally in commercial areas, pedestrian orientation can be accomplished by placing buildings in close proximity to the street with parking areas to the side and rear of buildings, mixing uses within the same multi-story buildings, building entrances directly facing streets with reduced setbacks, architectural design which employs display windows, projecting signs, and awnings at street level, and designed outdoor seating and gathering spaces at the street level. Each development proposal must demonstrate in detail how the project will be made walkable throughout.
3. **Compatibility of Uses:** Measures shall be taken to assure compatibility of land uses within a Planned Development itself and adjacent sites. Such measures may include the provisions of buffer zones, common open space areas and landscape features, transitional land uses, or a mixed-use development in which no specific type of land use is dominant. When applicable, the design methods recommended in the “Development Layout, Lot Sizes, and Setbacks” section of the Comprehensive Plan’s Land Use Element shall be employed. Compatibility measures/mitigation measures shall exceed the usual minimum standards of these regulations when needed to address impacts of the proposed development.
4. **Open Space:** Useable open space(s), in an amount over and above setback

and buffer yard areas and open areas required by the underlying district, shall be provided. These spaces may be provided in the form of parks, plazas, arcades, commons, trails, sports courts or other athletic and recreational areas, outdoor areas for the display of sculptural elements, etc. Land reservations for community facilities may be considered in lieu of useable open space. The use of single loaded streets to provide multi-purpose paths, park areas, or to protect stream corridors, may be proposed for this purpose. Open Space areas are encouraged to have street frontage and visibility. Any site proposed to be publicly dedicated or donated for park or open space purposes shall comply with the appropriate legislative body's requirements for acceptance of such dedications or donations.

5. Multi-Modal Transportation System: Planned Developments shall incorporate multi-modal transportation elements through the development, depending on the foreseeable needs of future residents and users of the site, and the relationship of the project site to the community at large. Such multi-modal elements may include provisions for mass transit stops or stations, car pooling lots, pedestrian and bicycle paths and lanes, bicycle parking areas, etc. Multi-modal facilities are encouraged to be combined with the pedestrian systems and open spaces described in Sections 1 and 4. Multi-modal facilities should connect to existing and future facilities that lie outside of the site.
6. Preservation of Existing Site Features: Existing topography, significant tree cover, tree lines along property lines, cemeteries, and water courses and water bodies shall be largely preserved and incorporated into the project design, where appropriate and consistent with the remainder of this article. The retention of such features may also fulfill portions of the requirements in Section 4 "Open Space" and Section 7 "Landscaping."
7. Landscaping: Substantial landscaping shall be provided in a Planned Development with emphasis given to street scape areas, buffer zones, and the provision of significant landscaping (in terms of size of landscape areas, and quantity and quality of landscape materials) within the developed portions of the site. The use of landscape design guidelines is required for multi-phased projects. The retention of existing healthy, substantial trees should occur wherever possible. Properly designed street tree plantings may be permitted to fulfill some landscaping requirements as part of an overall amenity package.
8. Architecture: Sites which are subject to architectural requirements through adopted overlay districts or land use studies shall follow said requirements. For all other sites, a consistent architectural theme shall be provided in planned developments. The theme shall largely use traditional, regionally influenced architectural forms and elements and shall allow variations within it. Traditional styles such as Georgian, Federal, I-House, Cape Cod, Craftsman, Tudor, Queen Anne, Italianate, early 20th century commercial structures, and local farm structures may be used as models. Contemporary or transitional styles of comparable quality may also be considered. The architectural theme shall also relate to existing structures on the project site and adjacent sites, especially if such existing structures are historic.

9. For attached or multi-family residential developments and commercial or office developments, the predominant building materials shall have a solid appearance, such as could be achieved with brick, stone, and architectural grade cast concrete products designed to replicate natural materials. Roof designs shall have a finished appearance through the use of three dimensional pitched roof forms with architectural grade roofing and/or the use of defined parapets with cornice lines. Long building facades and roof planes shall be interrupted through the use of three dimensional jogs in the building footprint and secondary roof forms such as hips, dormers, and gables. Such buildings shall include architectural detailing for cosmetic enhancement, largely use natural colors, and use a consistent design treatment on all facades. The use of architectural guidelines or building prototypes is required for all multi-phased projects. Developments should be mixed-use in character with multi-level buildings where commercial services are proposed. Walkability must be planned for when locating commercial and residential uses in proximity to each other. Office and residential uses are strongly advocated above commercial uses in business districts to decrease dependence on the automobile.
10. Historic and Prehistoric Features: Historic and prehistoric features on the project site shall be retained, utilized, and incorporated into the overall project design if physically and economically feasible.
11. Signage: A consistent signage theme shall be provided within a Planned Development. Building mounted signs shall be the predominate signage on the project site. Freestanding signs shall be monument style and of a limited size and height. Signage shall visually correlate to the planned architectural theme by the use of consistent design details, materials, and colors. The use of signage design guidelines is required for multi-phased projects.
12. Transportation Connections and Entry Points: The provision of transportation connections (street connections, pedestrian paths, multi-purpose trails, sidewalks, and bicycle facilities) shall be provided in all planned development unless physically unfeasible or unsafe. This shall include connections to adjoining properties and developments, and inter-connectivity within the development itself, and contain minimal use of cul-de-sacs or other dead-end types of streets only when necessary. Transportation connections shall account for the County's adopted Transportation Plan and any adopted bikeway and/or pedestrian plans. In addition, the various entry points (streets, paths, etc.) into a Planned Development shall be marked or otherwise defined through the use of landscaping, low-key signage on retaining walls, architectural or sculptural elements, archways, markers, etc. Any structures used to demarcate entry points shall visually correlate to the planned architectural theme by the use of consistent design details, materials, and colors.
13. Conformance with Comprehensive Plan: All Planned Developments shall conform to the provisions of the adopted Comprehensive Plan and take into account the limitations of existing or planned infrastructure.
14. Further, Concept Development Plan proposals within areas that are subject to

a specific land use or corridor study shall be evaluated against the criteria or requirements of such study as well as the criteria in this section.

15. A Planned Development and its uses, buildings or structures shall be minimally subject to the supplemental performance and development standards of this order, unless superseded by any special requirements, conditions, variances or other particulars imposed by the Planning Commission during the concept or preliminary application and hearing phases described in this article. Such special conditions may include provisions governing, common open space, lands or facilities, disposition of open land, infrastructure provisions including any physical design and/or any other requirement found to be necessary, appropriate or desirable for the purposes of this district.
- E. Section 1509 of the Boone County Zoning Regulations states that the Planning Commission shall require that a Concept Development Plan be submitted for the development or redevelopment of property located in a Planned Development district. If the Concept Development Plan involves a zone change, then the proposal is subject to the criteria then the proposal is subject to the criteria in Section 308 as well as the provisions of this article. If the Concept Development Plan does not involve a zone change, or only involves uses that are permitted in the underlying district (formerly called a "Utilization of an Underlying District Within a Planned Development"), then the proposal is subject only to the provisions of this article. The contents of the Concept Development Plan submittal shall include the items listed in Section 303.

#### SITE CHARACTERISTICS

- A. The approximate forty-four (44) acre area can be broken into two portions:
  1. The northern portion, containing approximately 19.97 acres; and
  2. The southern portion, containing approximately 24.40 acres.
- B. The northern portion of the site is currently occupied by a single-story detached single-family residential dwelling and three (3) accessory barn structures.
- C. The southern portion of the site is currently vacant and heavily wooded.
- D. Gunpowder Creek runs along the eastern edge of the site in question.
- E. The western edge of the site in question is located within the six hundred (600) foot buffer of an underground pipeline.
- F. Access is provided from the following street stubs: Holbrook Lane, Watson Lane, and Lariat Way.
- G. Topographically, a valley bisects the site with slopes having a grade change of seventy (70) feet.
- H. The eastern edge of the site in question is located within the 100-year floodplain of

Gunpowder Creek.

- I. Water service is available from 8" water lines within the Shaker Run and Valley Creek Farms subdivisions.
- J. Sanitary sewer service is available from 8" sewer lines within the Shaker Run and Valley Creek Farms subdivisions.

ADJACENT LAND USES AND ZONES

North: Detached single-family (SR-1)  
South: Detached single-family (Valley Creek Farms) and detached single-family/agricultural land (SR-1)  
East: Detached single-family (Shaker Run) (SR-1) and attached single-family (Oakbrook Cliffs (SR-1/PD)  
South: Detached single-family (Valley Creek Farms) and detached single-family/agricultural land (SR-1)

RELATIONSHIP TO COMPREHENSIVE PLAN

- A. The Our Boone County Plan 2040 Future Land Use Plan designates the site for "Rural Density Residential", "Suburban Density Residential", and "Developmentally Sensitive" uses.
  - 1. Rural Density Residential is described as low density residential uses of up to one dwelling unit per acre.
  - 2. Suburban Density Residential is described as single family housing of up to four units per acre.
  - 3. Developmentally Sensitive is described as areas that have an existing slope of twenty percent or greater for a height of 20 meters (67.6 feet), or have unique soil or flooding characteristics which limit the ability of an area to support urban development, or contain significant wooded areas, creeks, wildlife habitat, or other natural features that are important to a site's stability and visual character.
- B. The following Our Boone County – Plan 2040 Goals and Objectives apply to this application:
  - 1. Land uses and zoning decisions shall strive to balance the rights of landowners with the rights of neighbors and the community (Overall Goal A, Objective 4).
  - 2. Development policies shall not discriminate against any person (Demographics Goal A, Objective 1).
  - 3. Provide appropriate services, housing, employment, and shopping opportunities in order to meet the needs of the population in all geographic areas of the County (Demographics Goal A, Objective 4).
  - 4. A broad range of housing opportunities shall be provided to meet the needs and desires of all household types and shall be balanced with affordability, employment centers, commercial areas, and educational needs. [Unincorporated Boone County, City of Union, City of Walton only] (Demographics Goal B, Objective 1).
  - 5. A broad range of housing opportunities shall be provided to meet the needs and

desires of all household types and shall be balanced with affordability, employment centers, commercial areas, and educational needs. The City of Florence acknowledges the need for affordable housing opportunities, but also desires an equal balance of housing options to maintain a diverse city. [City of Florence only] (Demographics Goal B, Objective 1).

6. Residential developments shall be evaluated for impact on infrastructure, environment, health and safety considerations, buffering, and design with secondary consideration given to dwelling unit type and density (Demographics Goal B, Objective 4).
7. Mixed use, higher density neighborhoods shall be encouraged near existing or proposed transportation nodes and shall be designed to establish neighborhoods with a mix of retail, public, and recreational uses (Demographics Goal B, Objective 6).
8. Residential developments shall be encouraged to plan and build in a manner compatible with the general housing density and design planned for the area, with the existing conditions of the site, and significant site features (Demographics Goal B, Objective 6).
9. Development shall attempt to utilize existing topography and vegetation (including mature trees where possible) and preserve the existing character of the land where practical. [Unincorporated Boone County, City of Union, City of Walton only] (Environment Goal A, Objective 7).
10. Development shall utilize existing topography and vegetation (including mature trees) and preserve the existing character of the land. [City of Florence only] (Environment Goal A, Objective 7).
11. Priority shall be given towards maintaining, protecting, and improving the capacity and safety of the existing road system across jurisdictions (Transportation Goal A, Objective 1).
12. New land development shall be closely coordinated with the local transportation system to assure efficiency and continuity within Boone County and with the regional multi-modal transportation system (Transportation Goal B, Objective 9).

C. The following are excerpts from Our Boone County - Plan 2040:

1. The main need of a population base is for safe, sound, and sanitary housing. Meeting this involves considering both housing supply and demand in order to determine what those future needs will be. It is important to forecast housing growth by areas in order to ensure that an equal balance of options exist to meet the needs of a diverse population. Furthermore, in order for Boone County to retain its youth, the county must provide housing choices that complement the educational opportunities, the current job market, and commercial attractions. Otherwise, an out-migration of the young population will occur. A variety of multi-family housing options should include apartments, townhouses, patio homes, condominiums, nursing homes, and retirement housing all ranging greatly in price per unit to accommodate a wide range of relative affordability. These types of units are increasing in demand beyond the established urban areas such as Florence and should be encouraged to occur in the unincorporated portions of Boone County as viable options for both the building community and the prospective resident, especially as the county's land values rise. Older housing stock that can be rehabilitated without undue effort or expense should be retained and in some

cases, incorporated into new subdivision design rather than being demolished. There are concentrations of older housing stock in need of restoration or rehabilitation in many of the older communities like Florence, Belleview-McVillie, Burlington, Constance, Hebron, Petersburg, Stringtown, Taylorsport, Union, Verona, and Walton. Home ownership, maintenance, and rehabilitation assistance of older homes in these areas should be incentivized and encouraged through Federal or local grants. New residential development, regardless of the type, should be evaluated for its impact on the county's existing infrastructure and natural environment to ensure that it fits in with existing surrounding development through proper buffering and design. High density developments should be close to thoroughfares and urban services and be sensitively developed in terms of building height, setbacks, mass, and visual impact. A variety of housing opportunities within the established urban (and suburban) areas of Boone County would encourage mixing of different income levels. Most new construction of typical single-family housing should be encouraged to occur within undeveloped areas within the established suburban portion of the county in order to take advantage of existing infrastructure and services already in place and avoid placing strain on the more rural transportation network in the undeveloped portions of the county. Consideration must be given to existing conditions that may affect new development in established areas such as adjoining land uses and proximity to the Cincinnati-Northern Kentucky International Airport where commerce and airplane noise may be an issue.

Established single-family housing areas should not experience an influx of multi-family or duplex construction unless adequate buffering or proper development design can be provided to ease the transition. Duplexes are often more successful than multi-family housing in blending in with single-family residential surroundings. Therefore, when buffering is minimal between single-family housing areas and proposed multi-family units, an appropriate transitional area of duplexes may be designed. New subdivision development should include design considerations and gradation of lot sizes to lessen the impact on the character of the area. This is particularly true in areas of agricultural zoning and could include larger lots or green space along the main road(s), larger lots or a buffer along adjacent low density residential uses, strategic open space, and detailed attention to the orientation of housing units. The bottom line is that infrastructure partners must work together and correlate improvements in order to ensure capacity and be sensitive to the surrounding area. A progression of densities of residential uses from high (multi-family) to low (single family) shall be encouraged. Where traditional progressions of high to low net density are not possible through creative development design, an appropriate and attractive visual transition should be achieved. This could include existing vegetation or new landscaping and/or fencing. Existing vegetation (such as wooded fence rows) should be retained as much as possible to provide buffer strips and stormwater filters. The density of typical residential developments in Boone County over the past couple of decades has been consistent with the suburban residential densities reflected on the 2035 Future Land Use Map (up to 4 units per acre). However, there is a large segment of the population that desires to live in more rural areas while still having convenient access to centers of commercial, services, and employment activity. For residential development to occur in these outlying areas, adequate and proper

infrastructure must be provided, and impact on the natural environment must be minimized. If significant large lot residential subdivision development does occur in some of these rural areas, roads and other components of infrastructure will have to be improved. Existing and planned water, stormwater, and sanitary sewer services influence the locations of new residential construction. However, just because one form of public infrastructure exists or has been improved, does not automatically mean the area is ready for suburban development. Other forms of infrastructure may also be needed or improved first. Development phasing is an option to ensure that the timing of a new development corresponds with the provision of adequate infrastructure. In order to support the densities needed to sustain mass transit and to foster new affordable housing options, average minimum densities should be encouraged for mixed-use, planned developments. This would allow a variety of housing types but also enable a logical progression of intensities. The highest residential densities would be in the activity centers of these mixed-use developments and would include neighborhood commercial, employment, public uses, as well as transit stations or stops. (Demographics, Conclusions and Recommendations, Housing Types and Locations, pages 20-21).

2. The demand for more affordable residences, generated by the commercial and industrial employment growth centers, has resulted in the development of many new condominium and apartment complexes. However, attempts at more affordable new construction single-family or apartment developments are often met with localized resistance from the public. Creative site design and architecture can help overcome some of this opposition. Clustered housing or Open Space Subdivisions shall be promoted (via density bonuses) as they enable the development industry and local government to address density concerns and strengthen the sense of community, as well as meet the housing demands of a changing population. Mixed-use, planned developments that can provide a mix of housing options, commercial opportunities, offices, public uses, recreational uses, and multiple transportation options (i.e. transit, pedestrian, park-and-ride) could be one strategy for meeting this demand for higher density. Areas of employment growth in Boone County should experience development pressure for housing options relative in affordability to the jobs and shopping opportunities being created in the area. Manufactured (or modular) homes are an economical alternative to conventional homes and can lower the cost of construction to help offset rising land costs. These manufactured homes have historically been in demand in the rural, western half of the county. Most manufactured homes are permitted throughout all of Boone County and can occur separately or in a subdivision, unless regulated by private deed restrictions or restrictive covenants. Mobile homes, however, are strictly limited to the few remaining mobile home parks in Boone County or in the Agricultural (A-1) zoning district which is primarily located in the western portion of the county. However, A-1 zoning requires a minimum five acre lot size making this a less economical housing alternative with the increasing land prices. (Demographics, Conclusions and Recommendations, Affordability, page 21).
3. Developments in Boone County must recognize the potential impacts upon adjoining land uses and incorporate a transition of land uses, building setbacks, and/or landscaping to minimize these impacts. Potential impacts include visual,

noise or vibrations, odors, dust, smoke, and light. Buffering to mitigate these impacts should be an integral part of the design of proposed projects; where appropriate, existing site features should be used in meeting this guideline. Developments should provide buffering along public roadways, to soften the visual impact. Appropriate wooded areas and stream valleys should remain as open space within developments and between developments. Developments proposed adjacent to planned or established open spaces should provide pedestrian access where appropriate. Natural green space benefits the community as well as encourages developers to create innovative development designs through clustering of buildings and impermeable area. Typically, buffering is required and provided between both like and unlike land uses. However, as the development of Boone County fills in previously rural areas, different lot sizes and designs of residential development sometimes impact each other. Where an appropriate gradation of lot size and setbacks cannot be designed into a proposed residential subdivision development of a significantly higher density than existing adjacent residential uses, deliberate vegetation buffering may need to be incorporated into the design to help protect low density residential areas. Buffer areas should use and supplement existing site features where possible (Land Use, Future Land Use Development Guidelines, Buffering, pages 95-96).

4. Developments in Boone County must include landscaping to accompany the proposed project. Retention of existing healthy vegetation is considered a component of landscaping and is encouraged. This landscaping should be designed to improve the public view of a development, and should be incorporated into parking lots and other vehicle circulation areas, as well as within open spaces and around structures. Landscaping is intended to soften the visual impacts of the development from adjoining properties and roadways. The amount of heat absorbed by impervious cover from sun radiation is decreased by landscaping, which reduces energy costs. Landscaping helps purify the air of harmful pollutants, thus reducing health impacts. It also helps reduce the quantity and improve the quality of storm water runoff, including temperature. The use of bioretention islands (water filtering basins) and grass swales should be used where possible in place of raised islands as described in Northern Kentucky's Storm Water Best Management Practices Manual (2012). Native species are often heartier than nursery stock and should be used wherever possible to minimize pesticides and other high impact forms of maintenance. Developments along major roadways in Boone County must include landscaping between the development and the right-of-way in order to promote the aesthetic appearance from the roads and to facilitate the compatibility of differing land uses (Land Use, Future Land Use Development Guidelines, Landscaping, page 96).
5. The developing residential area south of KY 18, west of Oakbrook Drive and east of Camp Ernst Road must include street connections that enable traffic generated by the area to travel in all four directions. This should allow Oakbrook traffic to proceed to KY 237 in the Rogers Lane area, and Shaker Run traffic to access KY 18 without traveling on Camp Ernst Road or Oakbrook Road. Development in the area around Rogers Lane should be carefully planned. The remaining portions of this section of Boone County should develop in a Suburban Residential fashion, with the exception of planned High Suburban Density Residential uses between

Hickory Hill Subdivision and KY 18. Subdivision street connections are vital to this area around Burlington to give alternative routes for residents (Land Use, Description and Purpose of Land Use Maps, Future Land Use Geographic Areas, Conclusions and Recommendations, 11 – Burlington, page 120).

- D. Holbrook Lane is a county maintained local street providing for two way traffic within two driving lanes. A sidewalk exists along the south side of the roadway.
- E. Watson Lane is a county maintained local street providing for two way traffic within two driving lanes. Sidewalks exist along both sides of the roadway.
- F. Lariat Way is a county maintained local street providing for two way traffic within two driving lanes. Sidewalks are being provided as individual lots are developed.

STAFF COMMENTS

- A. The submitted Concept Development Plan indicates the following:
  - 1. Construction of 139 attached single-family residential dwellings in twenty-four (24) single-story buildings.
  - 2. Construction of 157 multi-family residential dwellings in twenty-four (24) three-story buildings and thirteen (13) two-story garages).
  - 3. Construction of public streets within the northern portion of the site.
  - 4. Street connections at Watson Lane, Holbrook Lane, and Lariat Way.
  - 5. Construction of street extensions for Holbrook Lane and Lariat Way.
  - 6. Provision for stormwater retention.
  - 7. Provision for amenity space.
  - 8. Construction of sidewalks.
  - 9. Construction of a private access drive within the southern portion of the site, extending from the terminus of Lariat Way.
  - 10. Provision for a walking train in the southern portion of the site.
  - 11. Provision for public utilities.
- B. The following table provides a summary of intensity for the proposed development as well as for the existing adjacent areas:

	INTENSITY (du/ac)
Shaker Run Subdivision	4.55
Valley Creek Farms Subdivision	
Final Platted	4.12
Remaining Preliminary Plat	2.20
Total	3.20
Hickory Burlington	6.96
Poplar Point Apartments	6.43
Total Development	6.67

- C. The applicant has provide a Project Narrative and Traffic Assessment. The Traffic Assessment provides anticipated peak hour traffic generation numbers and trip distribution. However, the report does not include any intersection analyses or recommendations.
- D. Staff has reviewed the submitted Concept Development Plans against the Boone County Zoning and Subdivision Regulations, and offers the following comments (some of these requirements can be adjusted by the Planning Commission and Fiscal Court pursuant to the Planned Development (PD) regulations):

1. Section 505.2 of the zoning regulations prohibits townhouse and multi-family residential dwellings within the underlying SR-1 district.
2. Section 1504 of the zoning regulations state that the intensity of use in a Planned Development district may exceed the maximum intensity permitted in the underlying zoning district by up to fifty percent (50%).

Section 903.3 of the zoning regulations state that the maximum intensity of the underlying SR-1 district shall not exceed four (4) units per acre.

Therefore, the maximum intensity allowed in the proposed SR-1/PD district is six (6) units per acre.

The submitted Concept Development Plan indicates an overall development intensity of 6.67 units per acre.

3. Section 1506 of the Boone County Zoning Regulations includes a set of criteria that is to be used to evaluate the appropriateness of proposed developments. The following is an analysis of this criteria against the submitted plan:
  - a. Mixed Use Development and Pedestrian Orientation – The submitted plan includes two different residential types and includes open space areas.
  - b. Compatibility of Uses – The submitted plan indicates that development of the overall project will have an intensity of 6.67 units per acre. The adjacent Shaker Run Subdivision has an intensity of 4.55 units per acre and the Valley Creek Farms Subdivision has an intensity of 4.12 units per acre. The submitted plan provides for a minimum perimeter building setback of ten (10) feet adjacent to these subdivisions.
  - c. Open Space – Within the northern portion of the site, the open space is concentrated to the interior of the development, situated with and around the stormwater retention facility. Within the southern portion of the site, the open space is concentrated along the hillsides.
  - d. Multi-Modal Transportation System – The site will be accessed by passenger vehicles. The plan also includes sidewalks and walking trails.
  - e. Preservation of Existing Site Features – The development has been designed to preserve the hillsides located within the southern portion of the site.
  - f. Landscaping – The submitted plan does not provide for any specific

- g. landscaping.  
Architecture – The proposed townhomes will have masonry knee walls along the front and portions of the side elevations, paneled walls, pitched roofs, and architectural grade garage doors. Insufficient information has been provided to determine the building materials of the multi-family residential buildings.
  - h. Signage – Insufficient information has been submitted to determine if the proposed signage will visually correlate to the architectural theme of the proposed development. However, the submitted concept development plan does indicate that names of the two proposed distinct development areas are to be added to the existing signage for Valley Creek Farms, at Camp Ernst Road and that additional signage is to be installed at the entrances to the development at the Holbrook Lane extension, Watson Lane, and Lariat Way extension.
  - i. Transportation Connections and Entry Points – The submitted plan indicates that access will be provided from Watson Lane, an extension of Lariat Way, and a future extension of Holbrook Lane.
4. Section 3151 of the zoning regulations provides requirements for trash enclosure areas. Insufficient information has been submitted to determine compliance with these regulations.
  5. Section 3316 of the zoning regulations provides requirements for exterior lighting. Insufficient information has been submitted to determine compliance with these regulations.
  6. Section 3321 of the zoning regulations requires sidewalks that are immediately adjacent to off-street parking spaces to have a minimum width of six (6) feet. The submitted plan indicates that the sidewalks immediately adjacent to parking spaces within the southern portion of the site will have a width of four (4) feet.
  7. Section 3325 of the zoning regulations provide for minimum parking space requirements for residential uses, based on the number of bedrooms in each unit. This requirement will be verified as part of a Major Site Plan application.
  8. Section 3408.7.A of the zoning regulations states that in residential and residential planned development districts, entrance signs shall be permitted subject to the following restrictions:
    - a. Shall only be a monument sign.  
  
Insufficient information has been submitted to determine compliance with this requirement.
    - b. Shall only be permitted for a residential subdivision that involves the construction of a new street or for a multi-family development, or section of a development, that contains twenty (20) or more dwelling units.  
  
The submitted proposal meets this requirement.

- c. Shall be limited to one (1) sign, up to a maximum sign area of one hundred (100) square feet, or two (2) signs, each having a maximum sign area of fifty (50) square feet.

Insufficient information has been submitted to determine compliance with this requirement.

- d. The maximum height of the sign shall not exceed ten (10) feet.

Insufficient information has been submitted to determine compliance with this requirement.

- e. Shall be located at the entrance into the residential development from either an arterial, collector, or subcollector street.

The submitted proposal indicates that the proposed signage will be at the entrances to the proposed development from local streets.

- 9. Section 402.1.1 of the subdivision regulations state that maximum grade of public streets shall not exceed twelve (12) percent.

The submitted plan indicates that the grade of proposed street "D", at the entrance from Holbrook Lane, will have a grade of approximately thirty-four (34) percent.

- 10. The subdivision regulations do not allow individual parking spaces to be accessed from a public street.

The submitted plan indicates the provision for individual parking spaces along proposed streets "A" and "C".

- E. Staff sent out an Agency Memo to the Boone County Building Department, Community Development Division, Boone County Public Works, Boone County Schools, Boone County Water District, Burlington Fire District, Florence Fire Department, the Kentucky Transportation Cabinet, and SD1.

- 1. Bridget Striker, Boone County Planning Commission Community Development Division, replied that there are neither structures nor sites of historic significance within the project area.
- 2. Randy Childress, Florence Fire Department, replied that he spoke with the Burlington Fire District and that they will be doing the plan review on this project.
- 3. Chad Eha, Burlington Fire Protection District, replied as follows: Just a few quick questions regarding the sight plan submitted for Hickory Burlington/Poplar Pointe. Would it be possible to cul-de -sac Lariat Way before the private drive to allow traffic not committing to the private drive for the new development a turn around especially larger trucks like our equipment. Also, would it be possible to connect Lariat Way into Trevino Lane allowing a second egress in the event of a larger scale incident. Residents could become stuck in the event of a large-scale incident once the fire department secures a water supply line from a hydrant blocking the

- only proposed access point to the proposed development. Other than that, it looks like a very nice development for the area.
4. Mike Rouse, Boone County Water District, replied that the master meter should be supplied by Trevino Lane.
  5. Rob Franxman, County Engineer, replied that a full traffic impact study should be completed and that KY 237 and Cannondale should be part of that study. He would request the intersection of Oakbrook and Holbrook be looked at.

### STAFF CONCERNS

- A. The following list provides a summary of those requirements that are being requested to be waived/altered using the PD district regulations:
  1. Allow townhouse and multi-family residential dwellings.
  2. Reduce the width of sidewalks immediately adjacent to off-street parking areas from six (6) feet to four (4) feet.
  3. Allow entrance signs along local streets rather from arterial, collector, or subcollector streets.
  4. Increase the maximum grade allowed for public streets from twelve (12) percent to thirty-four (34) percent.
  5. Allow individual parking spaces along public streets.
  6. Allow for off-site signage.
- B. The submitted plans provide notations stating that there will be approximately 139 townhome units and approximately 157 multi-family units. The project narrative states that there will 139 townhome units and 157 multi-family units. The applicant needs to address the discrepancy between the generalized notation on the plans and the specific numbers provided in the project narrative.
- C. Given the number of proposed dwelling units, the applicant should address what, if any, amenities will be provided as part of the project.
- D. The applicant should address the timing of the proposed extension of Holbrook Lane, both to the project entrance as well as further to the west.
- E. The applicant has indicated that they would like to have the names of the two proposed distinct development areas added to the existing signage for Valley Creek Farms, at Camp Ernst Road. The applicant should address the ownership of these signs and the owner's willingness to accommodate the request.
- F. The following modifications should be made to the proposed concept development plan:
  1. Holbrook Lane should be extended to the western boundary of the site.
  2. Sidewalks should be provided along both sides of the proposed Holbrook Lane extension.
  3. Sidewalks should be provided along both sides of the proposed Lariat Way extension.
  4. Sidewalks should be provided along both sides of the proposed private access drive, from Lariat Way to the multi-family dwelling area.

5. An additional walking trail should be provided from the multi-family development area to the proposed walking trail.
6. Additional perimeter buffer yard width should be provided within the northern portion of the site.

CONCLUSION

This request needs to be evaluated by the Boone County Planning Commission and Boone Fiscal Court in terms of Articles 3 and 15 of the Boone County Zoning Regulations, the appropriate planning documents noted herein, and the potential impacts on existing and planned uses in the area.

Respectfully submitted,



Michael D. Schwartz  
Director, Zoning Services

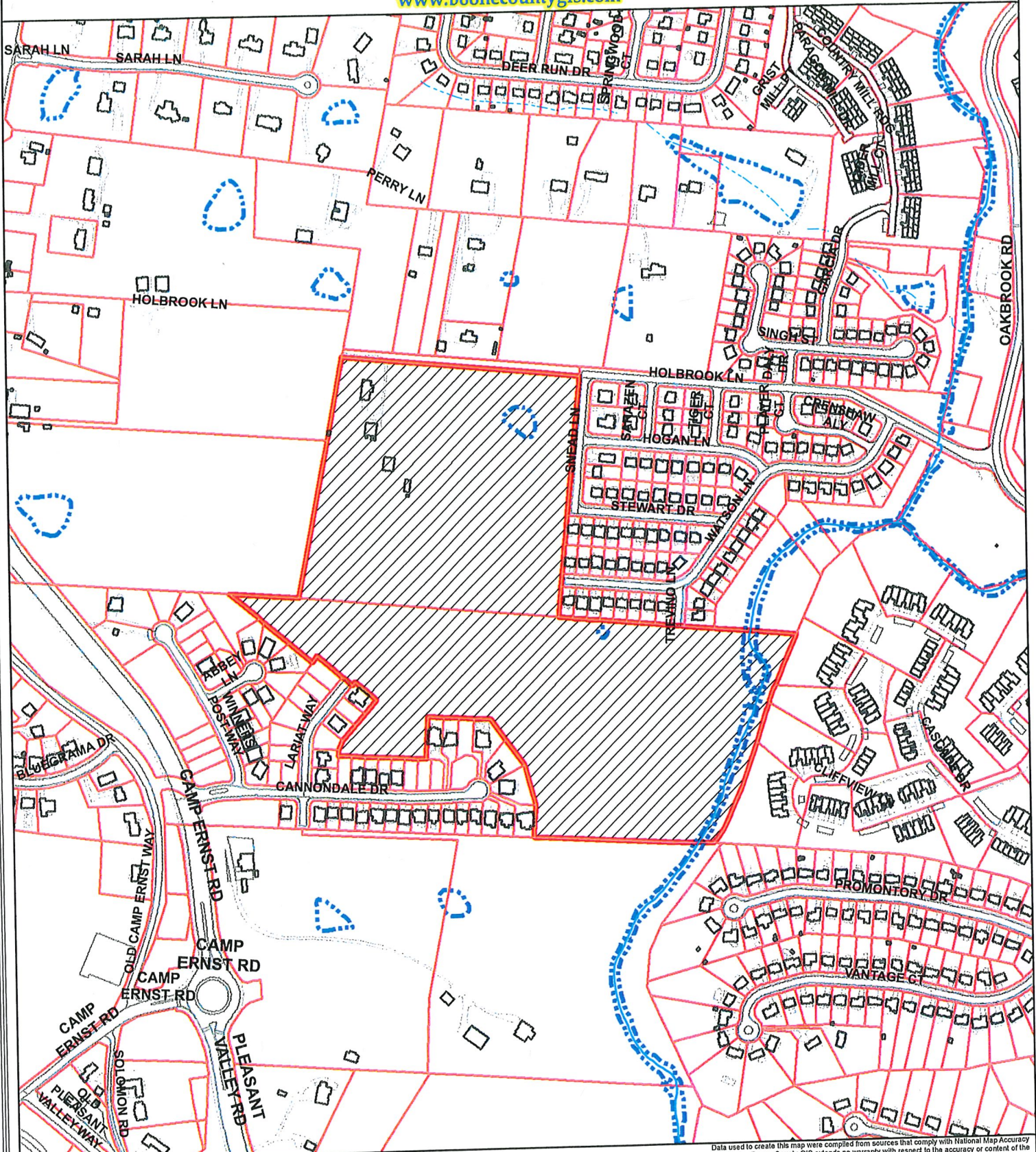
MDS/ss

Attachments:

- \*Vicinity Map
- \*Valley Creek Farms Preliminary Plat Map
- \*Aerial Map
- \*Topographical and Floodplain Map
- \*Zoning Map
- \*2040 Future Land Use Map
- \*Pipeline Buffer Map
- \*Concept Plan Map
- \*Application
- \*Project Narrative
- \*Traffic Assessment
- \*Concept Development Plan

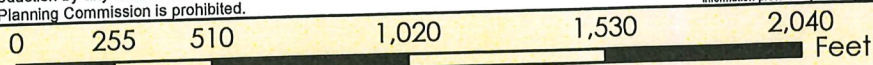
# Vicinity Map

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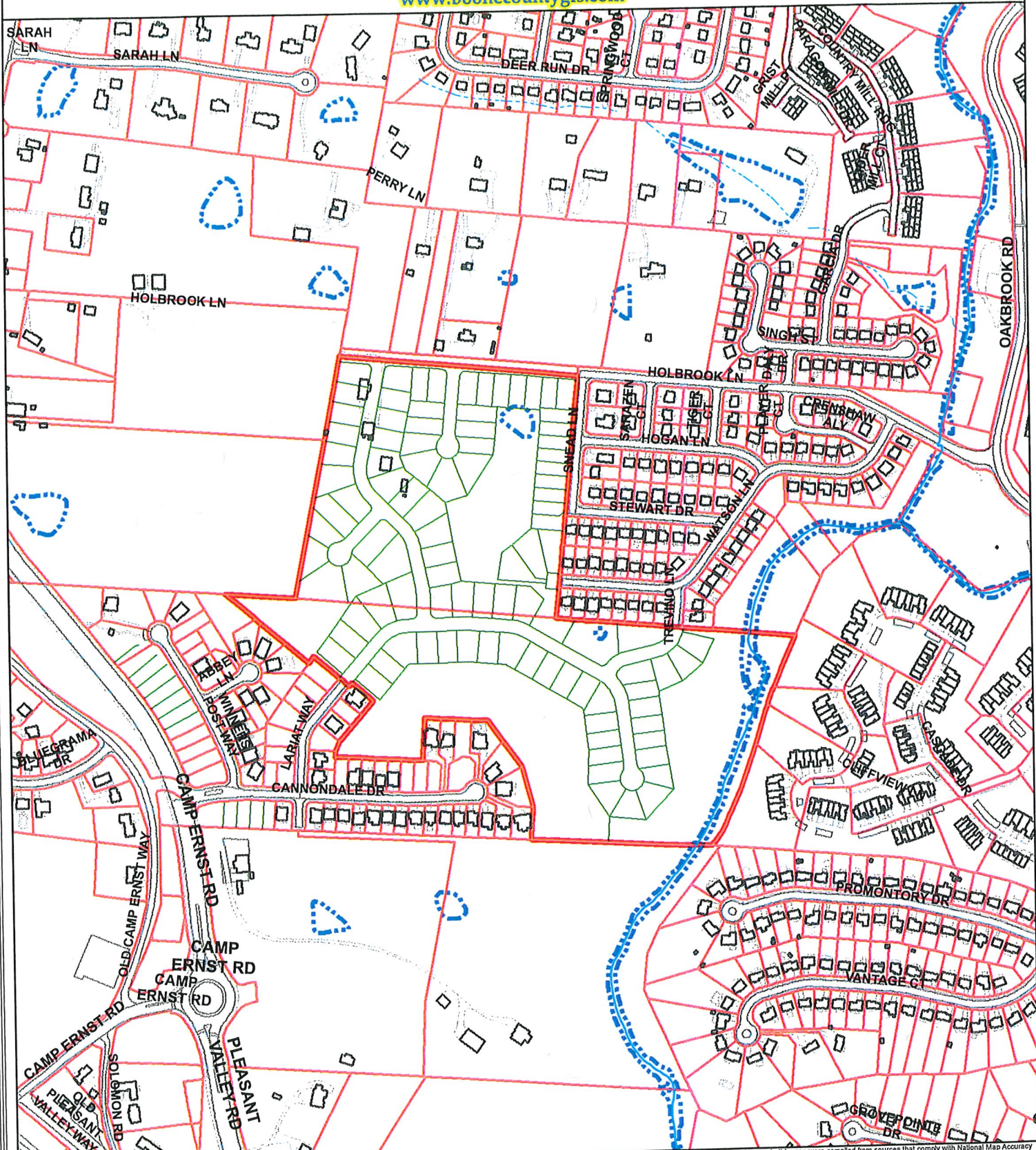
**Boone County GIS - Putting Northern Kentucky on the Map**

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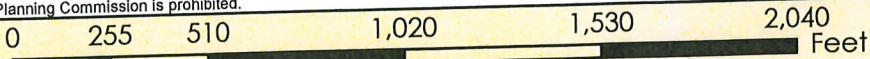
# Valley Creek Farms Preliminary Plat Map

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1 inch = 500 feet



**Boone**



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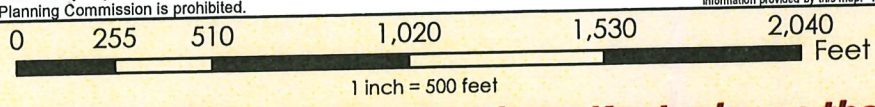
# Aerial Map

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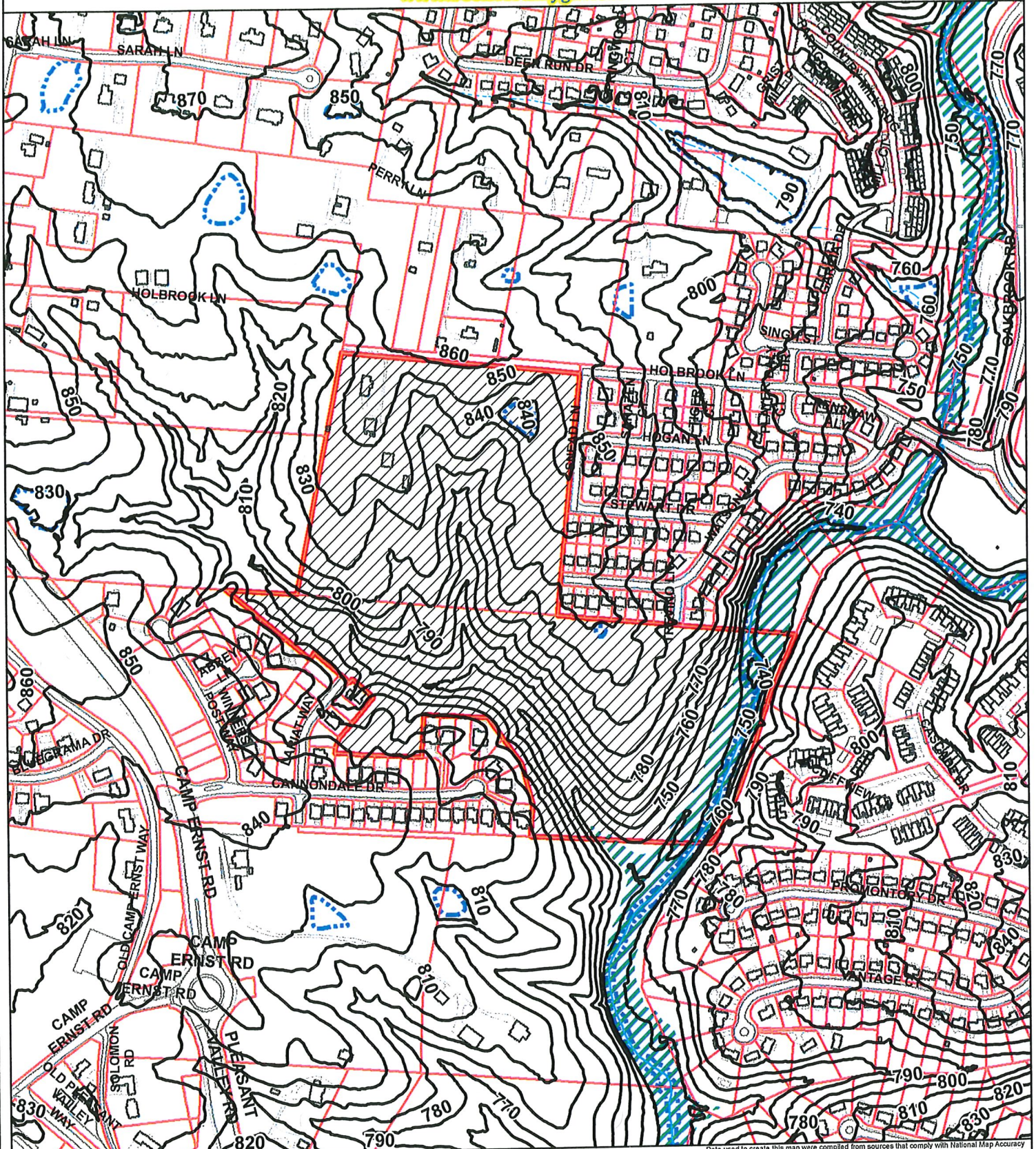


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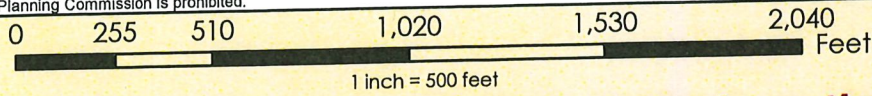
# Topographic and Floodplain Map

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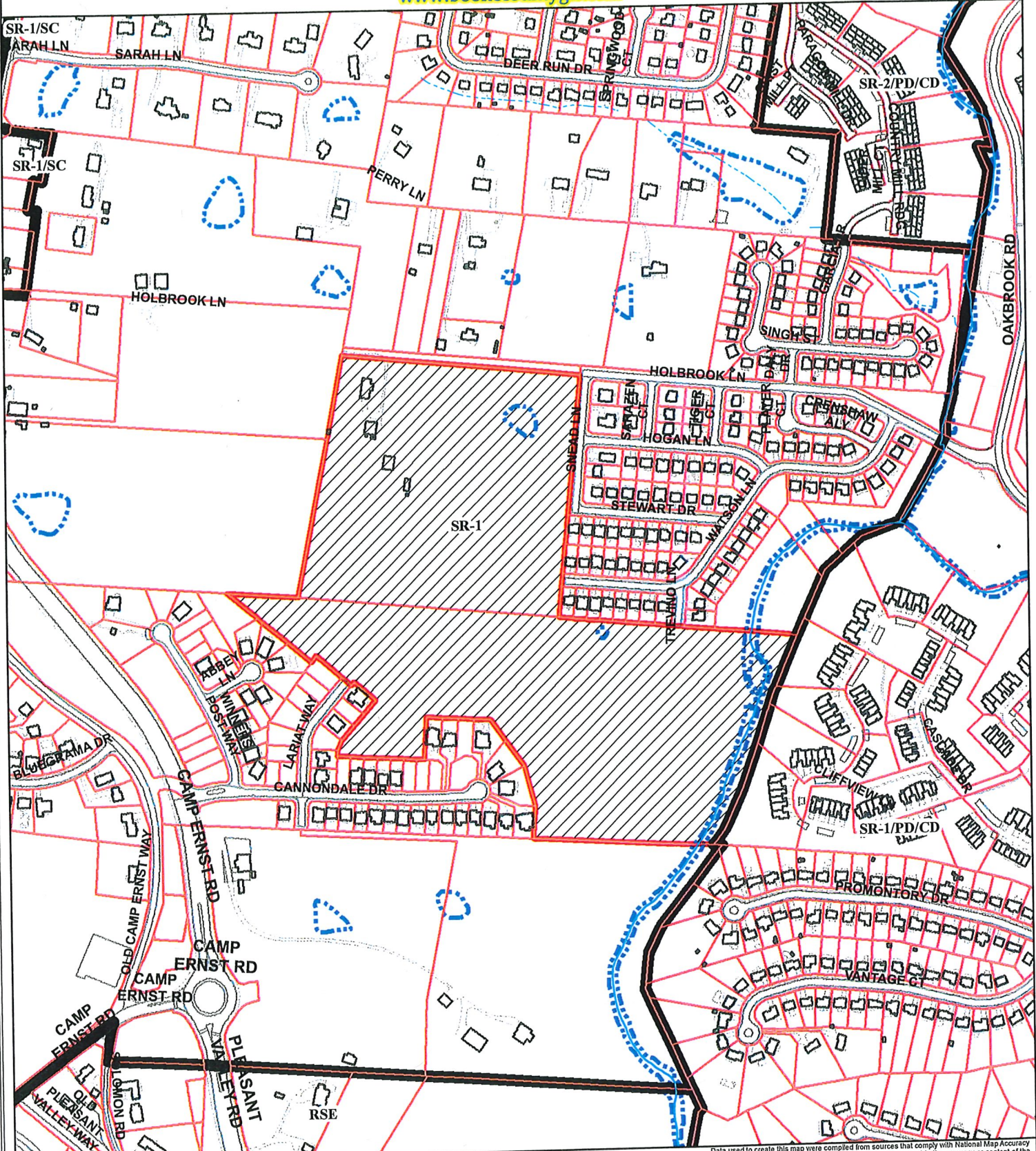
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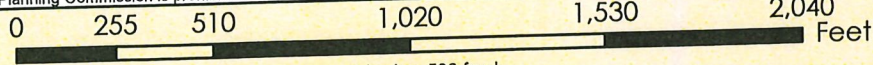
# Zoning Map

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# 2040 Future Land Use Map

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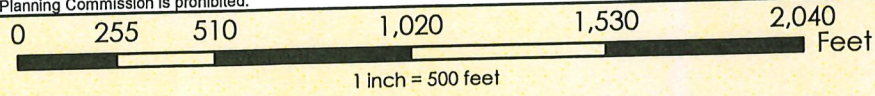


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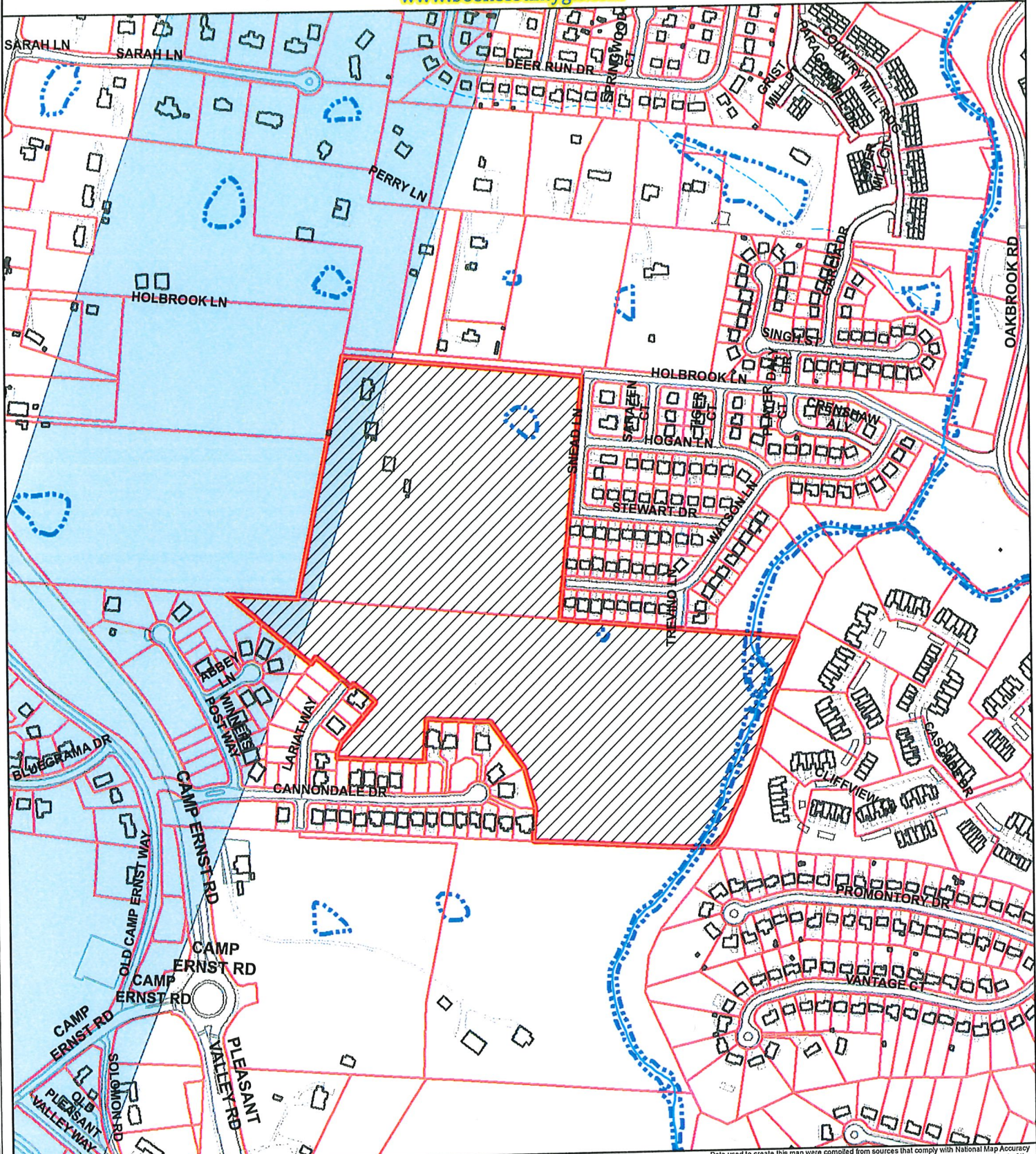


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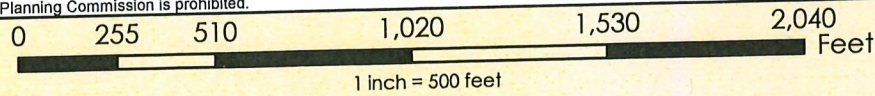
# Pipeline Buffer Map

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**Boone**

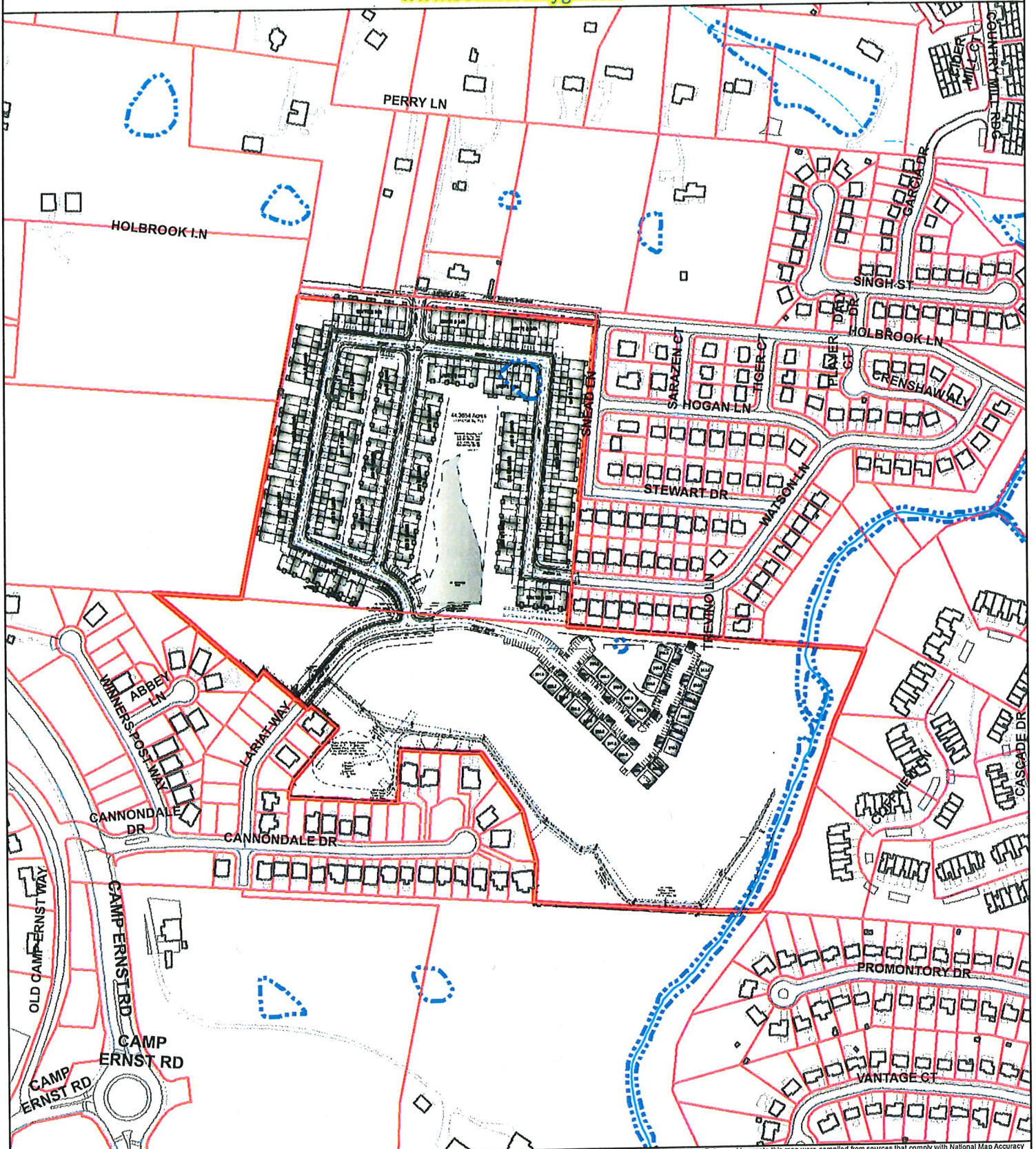


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ArcMap Document (\*.mxd)

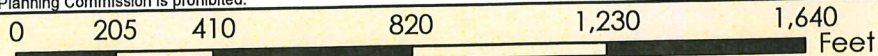
# Concept Plan Map

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12. 1196 93 2021  
Deed Book Page Number Group Number

13. Have you had a pre-application meeting with the BCPC staff:  Yes  No

14. Have you submitted a Concept Development Plan:  Yes  No

15. Have you met or discussed your proposed development with any of the following organizations/agencies (check all that apply):

- |  |   |
|--|---|
| <input checked="" type="checkbox"/> Boone County Building Department   | <input type="checkbox"/> Local Fire District                          |
| <input type="checkbox"/> Boone County Public Works Department  | <input type="checkbox"/> Local School District                        |
| <input type="checkbox"/> Boone County Water District   | <input type="checkbox"/> Northern Kentucky Health Department          |
| <input type="checkbox"/> Cincinnati Bell   | <input type="checkbox"/> Owen Cooperative Electric, Inc.              |
| <input checked="" type="checkbox"/> Cincinnati/Northern Kentucky International Airport (Kentucky Airport Zoning Commission for height restrictions near the airport) | <input type="checkbox"/> Sanitation District No. 1                    |
| <input type="checkbox"/> Duke Energy   | <input type="checkbox"/> USDA NRCS/Boone County Conservation District |
| <input type="checkbox"/> Florence Public Services Department   | <input type="checkbox"/> Other: _____                                 |
| <input type="checkbox"/> Kentucky Division of Water  |   |
| <input type="checkbox"/> Kentucky Transportation Cabinet   |   |

16. Concept Development Plan Jurisdiction/Location (check all that apply):  
 Unincorporated Boone  Florence  Walton  Union

17. Waiver of 60 Day Time Requirement by Originator for Final Planning Commission Action:

In accordance with the provisions of KRS 100.211, the applicant(s) and property owner(s) or originator(s) hereby waive the 60 day time limit for the Boone County Planning Commission to take final action on my (our) Zoning Map Amendment/Concept Development Plan application. This time limit waiver is considered effective immediately upon receipt by the Boone County Planning Commission and expires on September 19, 2024

Property Owner's Signature:

Paul May

Applicant's Signature:

Paul May

**SECTION B:** (To be completed by Planning Commission staff)

1. Date Received: 7/2/24 Fee Received: \$3203.31 Receipt #: 90839
2. Number of Copies Received: \_\_\_\_\_
3. Has the following been submitted (check all that apply):
  - Completed Application
  - Concept Development Plan
  - Legal Description
  - Names and Mailing Addresses of Adjacent Property Owners
4. Date the application is Administratively Complete (as defined in KRS 100.211): \_\_\_\_\_
5. Staff Reviewer: \_\_\_\_\_
6. Committee Chairperson: \_\_\_\_\_
7. Scheduled Public Hearing Date: \_\_\_\_\_
8. Boone County Planning Commission Action: \_\_\_\_\_ Date of Action: \_\_\_\_\_
  - \_\_\_\_\_ Approved
  - \_\_\_\_\_ Approved with Conditions
  - \_\_\_\_\_ Denial
  - \_\_\_\_\_ Other
9. Resolution Number: \_\_\_\_\_

Boone County Planning Commission  
Boone County Administration Building  
2950 Washington Street, Room 317  
P.O. Box 958  
Burlington, Kentucky 41005  
Phone: 859-334-2196 Fax: 859-334-2264  
[plancom@boonecountyky.org](mailto:plancom@boonecountyky.org)  
[www.boonecountyky.org](http://www.boonecountyky.org)

# Valley Creek Farms—

## Hickory Burlington, and Poplar Pointe Apartments

### Project Overview:

Charter Commercial, LLC (applicant) is submitting a Concept Development Plan for a residential development in Boone County, east of Camp Ernst Rd. which will connect via an extension of Lariat Way in a previously approved subdivision known as Valley Creek Farms-Phase A-Section 2A. The applicant will extend Lariat Way approximately 450' feet to a new street which will be directed northward to a series of streets that would connect back in with the Shaker Run Subdivision to the east, at Watson Lane. We also would anticipate a future extension of Holbrook Lane which would potentially connect to the northwest to Camp Ernst Rd. The development will reside on Boone County Auditor Parcel Numbers 049.00-00-073.00 and 050.00-00-001.05, comprising approximately 44.37 acres. The development will be comprised of two product types, a patio home rental product, and a second area with small multi-family unit buildings. The Patio Home rental product will be referred to as "Hickory Burlington" and the multi-family portion of this project will be referred to as "Poplar Pointe Apartments". Overall, the proposed request is an appropriate transitional use to the existing residential and will provide the residents of Burlington with an alternative housing option amidst the national housing shortage.

The Hickory Burlington project is lower density than traditional multi-family and compatible with the surrounding single-family neighborhoods adjacent to its location. Hickory Burlington will consist of 139 single-story, ranch style, rental patio homes, each with two bedrooms, two baths, and private garages, in attached groupings of 4-8 units. The development will have a distinct neighborhood feel in a quiet setting with heavy landscaping, walking trails, and a retention pond which will be used as an amenity as well as stormwater management. While Hickory Burlington is not age restricted, it is anticipated that many renters will include baby boomers who are downsizing but want to remain in the community and are choosing the flexibility of a low-maintenance lifestyle.

The Poplar Pointe Apartments will be located further east of Valley Creek Farms, and south of Shaker Run Subdivision. It will be situated away from these subdivisions to provide distance between the multi-family buildings and the existing single-family residences. We anticipate the buildings to be located stepping into the topography while providing beautiful view corridors of the wooded area backing up to Gunpowder Creek to the east

and south. We are proposing twenty-four (24) small, multi-family buildings, each with one-bedroom unit apartments. We are proposing thirteen (13) detached, three-car garages (carriage houses), each with a one-bedroom apartment above the garage units. We are proposing 157 units which are broken down to be approximately one hundred thirty-seven (137) 1-Bedroom units. We are proposing approximately 178 parking spaces, including 39 garage spaces, providing approximately a ratio of 1.13 parking spaces per unit.

Anticipated is an overall density of approximately 6.69 units per acre, based upon 296 units on 44.37 acres. This would be reasonably compatible with the SR-1 density plus a 50% bonus for density. The development would provide approximately 75 % Open Space, walkability with the sidewalks on the proposed streets as well as the proposed walking trails that will interconnect with the trails proposed for Valley Creek Farms. Ultimately, we envision providing connectivity to Shaker Run Subdivision which will provide a pathway to get to Overland Park which is situated to the north and east of this development.

#### Community Facilities:

Boone County School District- Contact will be made to coordinate.

Fire Department: Contact will be made to coordinate.

Sanitary Sewer is present on the property to the south and east part. Connection could be made to the main in Watson Lane, and additionally mains to the south can serve this development. We anticipate a sanitary main extension to follow the new road network and will connect to the main.

Water is available via connections at Lariat Way and Watson Lane. Applicant's Engineer has been in contact with Boone County Water and understands that water capacity is available for this development.

Duke Energy has gas and electric facilities adjacent to this development which will provide electric and gas to the development.

#### Construction:

It is anticipated that the patio home portion will precede the apartment construction. Roadway construction would be anticipated in late 2024 and early 2025 and patio home construction would commence after mass grading and roadway and utility construction.

Apartment construction would be anticipated mid to late 2025 with completion in 2026.

#### Signage:

Applicant is proposing names to be added to the existing signage for Valley Creek Farms (at Camp Ernst Rd.) with lettering for Hickory Burlington and Poplar Pointe Apartments. Additional signage for Hickory Burlington is proposed at the intersection of Lariat Way and Street "A", as well as small monument sign at the entrance to the Hickory Burlington section at Watson Lane.

It would be anticipated to have signage at the end of Lariat Way as it leads to the private road servicing the Poplar Pointe Apartments.

Access:

Access to the development has been previously mentioned but formally, access to the project will come from Camp Ernst Road via Cannondale Dr., then Lariat Way. The new road network will connect the Hickory Burlington to Shaker Run Subdivision by Watson Lane. Future extension of Holbrook Lane to Camp Ernst Rd. is anticipated in the future as development occurs on those lots north and west of our proposed project.

Traffic Analysis has been performed by Diana Zimmerman and is attached as Exhibit "A"

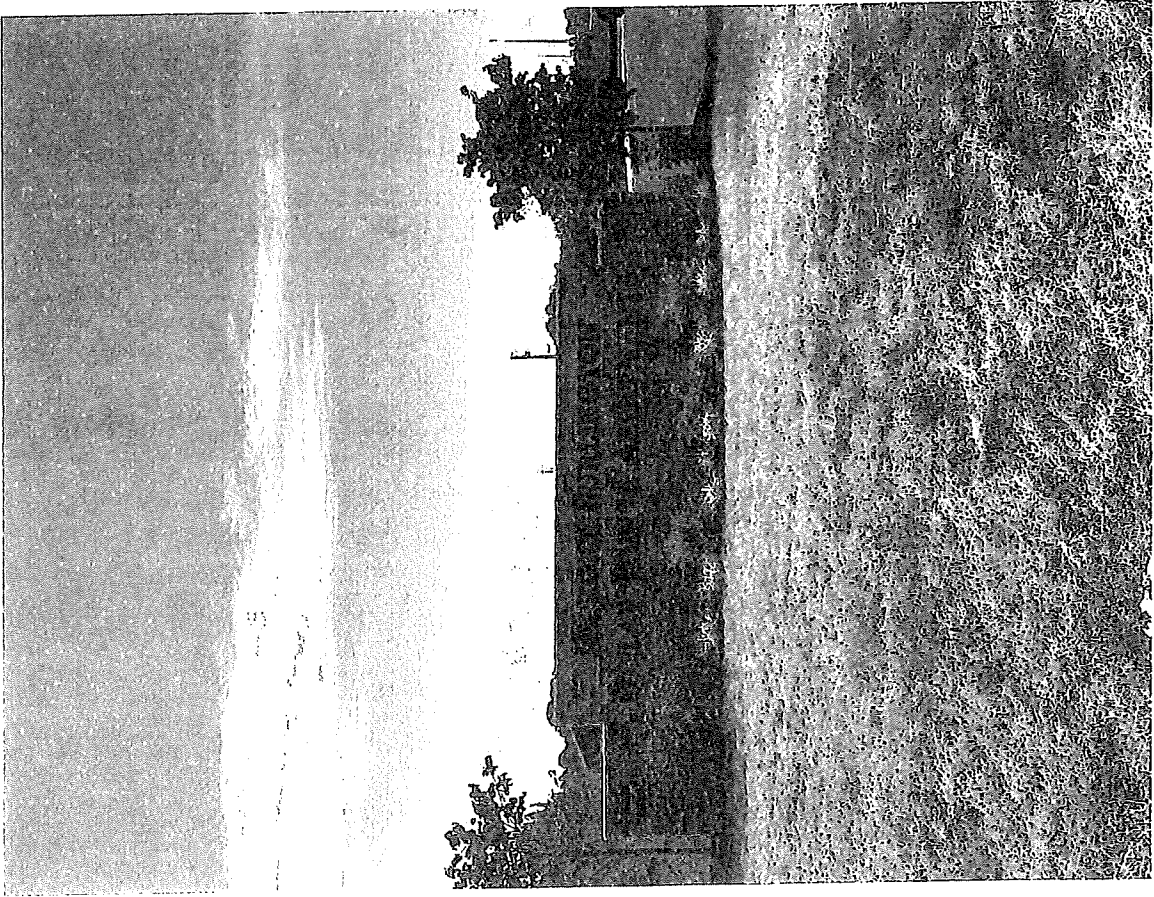
Open Space:

75% Open Space

Development Standards:

We are proposing to follow SR-1 PD Standards which would require setbacks as follows:

Front Setback=	30 ft, 20 ft proposed
Rear Setback=	30 ft, 20 ft proposed
Side Setback=	5', 15 ft total
	0 ft for attached units



final report

July 1, 2024

## Traffic Assessment

Hickory Burlington & Poplar Pointe Aparments  
Burlington, KY

Prepared for

Boone County Planning Commission



**DIANE B. ZIMMERMAN**  
Traffic Engineering, LLC

12803 High Meadows Pike  
Prospect, KY 40059  
502.648.1858  
diane zim@att.net



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## INTRODUCTION

The preliminary site plan for apartments on Holbrook Lane in Burlington, KY shows 157 apartment units and 139 rental townhouses. **Figure 1** displays a map of the site. Access to the development will be from Holbrook Lane, Watson Lane, and Lariat Way. The purpose of this study is to identify the traffic characteristics of this development and the adjacent road.

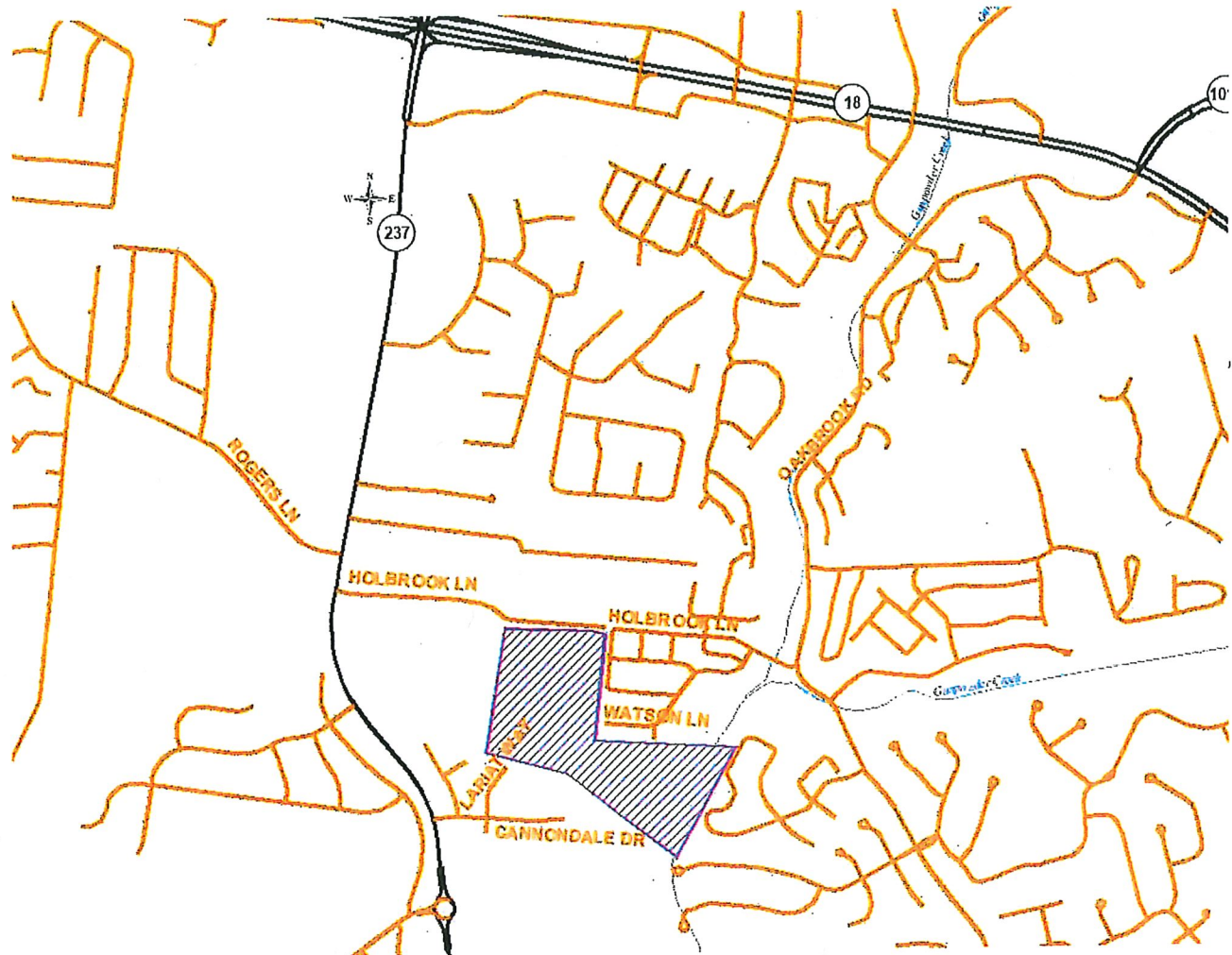


Figure 1. Site Map

## EXISTING CONDITIONS

Holbrook Lane is maintained by Boone County and is part of Shaker Run subdivision. The road has two eleven-foot lanes with curb and gutter, and an eight-foot multi-use path along the south side. The road is classified as a local road. The speed limit is 25 mph. The intersection with Oakwood Road is controlled with a stop sign on Holbrook Lane. There is a right and left turn lane on Holbrook Lane.

Lariat Way is maintained by Boone County and is part of Valley Creek Farms subdivision. The road has two eleven-foot lanes with curb and gutter, and sidewalks. The road is classified as a local road. The speed limit is 25 mph. The intersection with Cannondale Drive is controlled with a stop sign on Cannondale Drive. There are no turn lanes.

## TRIP GENERATION

The Institute of Transportation Engineers Trip Generation Manual, 11<sup>th</sup> Edition contains trip generation rates for a wide range of developments. The land uses of "Single-Family Attached (215)" and "Multifamily Housing (Low-Rise) (220)" were reviewed and determined to be the best match. The trip generation results are listed in **Table 1**. This distribution is based upon the existing traffic counts on KY 237 and Oakwood Road. **Figure 2** shows the trip distribution throughout the road network during the peak hours.

**Table 1. Peak Hour Trips Generated by Site**

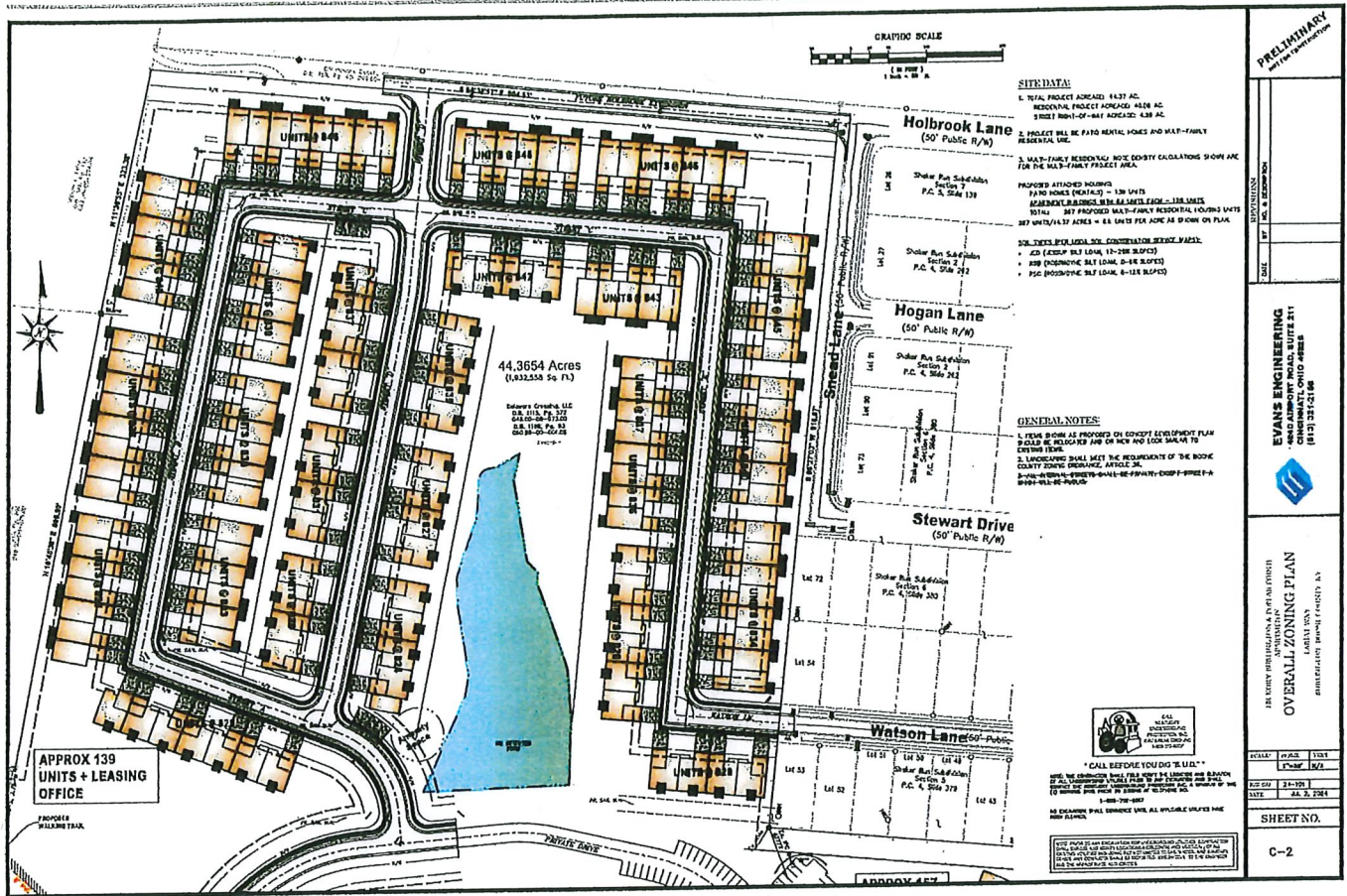
Land Use	A.M. Peak Hour			P.M. Peak Hour			Daily Trips
	Trips	In	Out	Trips	In	Out	
Single Family Attached (139 units)	67	17	50	79	47	32	1,009
Multifamily Low Rise (157 units)	72	17	55	88	55	33	1,082
Total	139	34	105	167	102	65	2,091

Hickory Burlington & Poplar Pointe Apartments  
Traffic Assessment

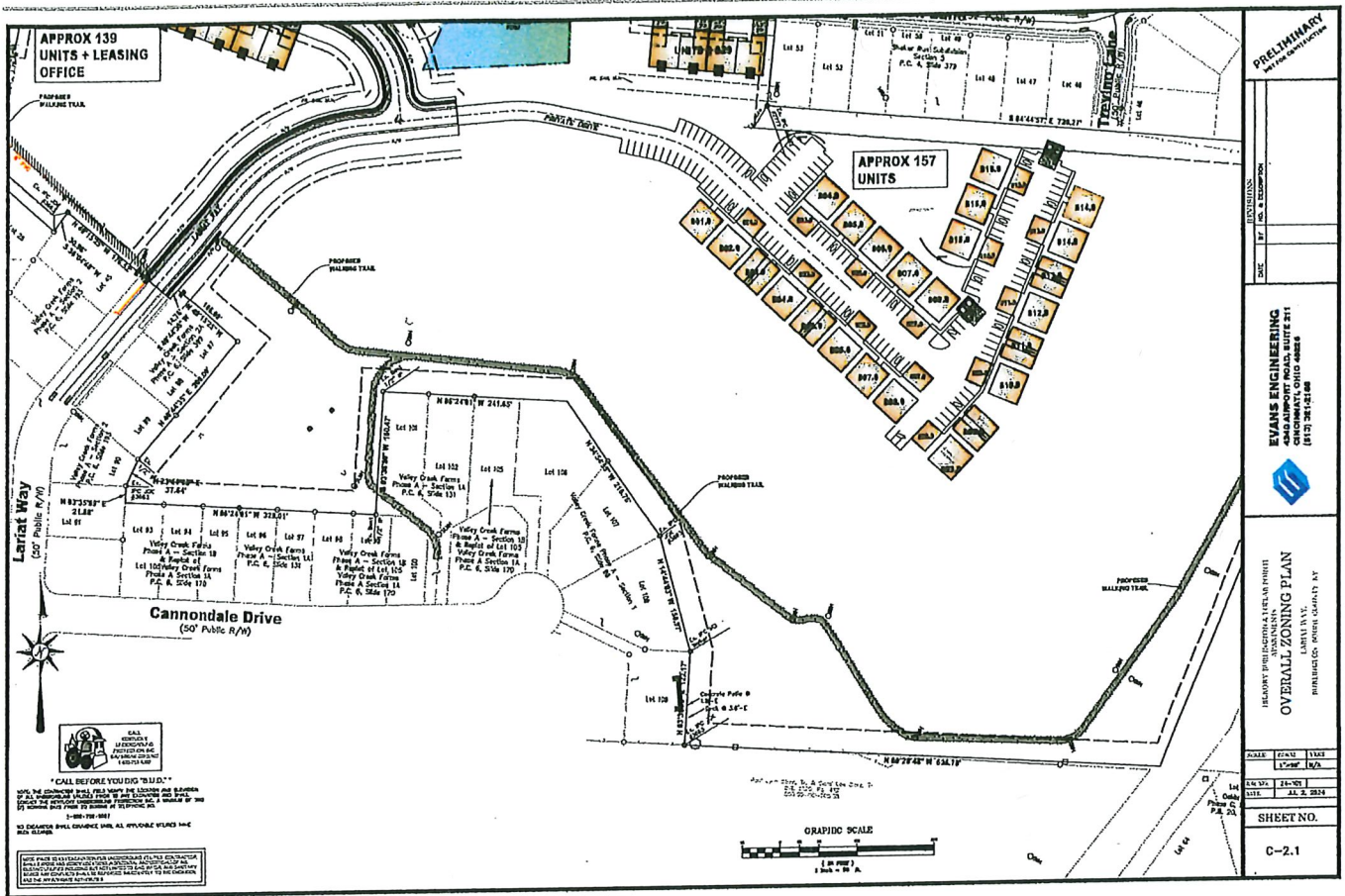


Figure 2. Peak Hour Trips Distribution

# Hickory Burlington & Poplar Pointe Apartments Traffic Assessment



# Hickory Burlington & Poplar Pointe Apartments Traffic Assessment



**PRELIMINARY**  
FOR CONSTRUCTION

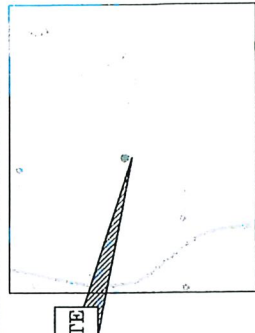
**EVANS ENGINEERING**  
4440 AIRPORT ROAD, SUITE 211  
CHICKASAW, OHIO 43005  
(614) 267-1218

**OVERALL ZONING PLAN**  
LEAVITT WAY  
BURLINGTON, WINDHAM COUNTY, NY

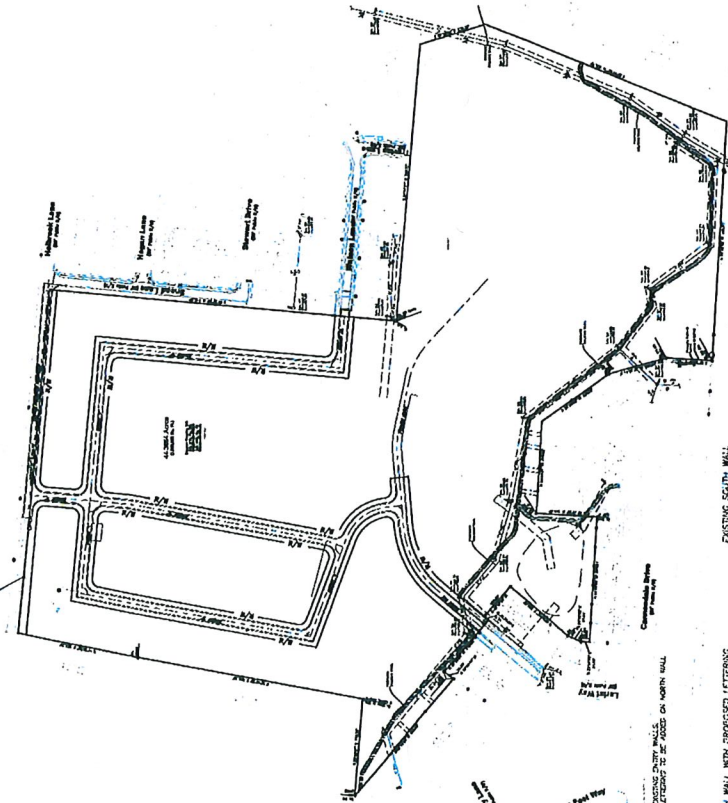
DATE	REVISION
11.2.2024	1

**SHEET NO.**  
C-2.1

PLANS FOR THE CONSTRUCTION OF A NEW RESIDENTIAL, PLANNED DEVELOPMENT KNOWN AS:  
**HICKORY BURLINGTON & POPLAR POINTE APARTMENTS**  
 LARLAI WAY, BOONE COUNTY, KY  
 JUNE, 2024



PROPOSED SITE



**RECEIVED**  
 JUNE 02 2024  
 BOONE COUNTY PLANNING COMMISSION

**DEPARTMENT/UTILITY TABLE:**  
 BOONE COUNTY BUILDING DEPARTMENT  
 BOONE COUNTY PUBLIC WORKS DEPARTMENT  
 BOONE COUNTY WATER DEPARTMENT  
 BOONE COUNTY FIRE DEPARTMENT  
 BOONE COUNTY SANITATION DEPARTMENT  
 BOONE COUNTY UTILITIES DEPARTMENT  
 BOONE COUNTY ZONING DEPARTMENT

**DEPARTMENT/UTILITY TABLE:**  
 BOONE COUNTY BUILDING DEPARTMENT  
 BOONE COUNTY PUBLIC WORKS DEPARTMENT  
 BOONE COUNTY WATER DEPARTMENT  
 BOONE COUNTY FIRE DEPARTMENT  
 BOONE COUNTY SANITATION DEPARTMENT  
 BOONE COUNTY UTILITIES DEPARTMENT  
 BOONE COUNTY ZONING DEPARTMENT

**OWNER:**  
 HICKORY BURLINGTON & POPLAR  
 25 DORN CENTER BLVD. SUITE 104  
 CRESTVIEW - HILLS NY 10817

**CIVIL ENGINEERING:**  
 EVANS ENGINEERING  
 4440 AIRPORT ROAD, SUITE 211  
 CINCINNATI, OH 45226  
 (513) 321-2168

**IMPERVIOUS SURFACE RATIO:**  
 TOTAL SITE = 10.00 ACRES  
 PAVEMENT = 4.50 ACRES  
 ROADS = 1.00 ACRES  
 DRIVEWAYS = 1.00 ACRES  
 DRIVE = 0.25 ACRES  
 TOTAL = 7.75 ACRES

**ZONE INFORMATION:**  
 ZONE: SUBURBAN RESIDENTIAL ONE (SR-1)  
 MAX GROUND COVER = 30%  
 MAX HEIGHT = 35 FT  
 MAX SIDE YARD SETBACK = 5' MIN / 15' TOTAL  
 MAX FRONT YARD SETBACK = 30' MIN / 35' TOTAL  
 MAX REAR YARD SETBACK = 30' MIN / 35' TOTAL  
 MAX OVERHANG = 5' MIN / 15' TOTAL

**NOTES:**  
 1. OWNER, BURLINGTON CROSSING LLC, 25 TOWN CENTER BLVD SUITE 104, CRESTVIEW HILLS NY 10817.  
 2. PROPERTY ADDRESS LARLAI WAY, WATSON LN., HIGHLAND KY.  
 3. PRESENT ZONING OF PROPERTY = SR-1.  
 4. PRESENT USE = VACANT.  
 5. ALL PROPOSED UTILITIES TO BE UNDERGROUND.  
 6. EXISTING SOIL TYPES = ROCKWORTH SILTY LOAM (R8), BROWN JESSUP SILT LOAM (S8), CHICKAMAUGA CLAY LOAM (C7).  
 7. FLOODING MAY BE ON ONE SIDE OF THE STREET ONLY FOR ALL STREETS.  
 8. ALL UTILITIES TO BE UNDERGROUND.  
 9. ALL UTILITIES TO BE UNDERGROUND.  
 10. ALL UTILITIES TO BE UNDERGROUND.  
 11. ALL UTILITIES TO BE UNDERGROUND.  
 12. ALL UTILITIES TO BE UNDERGROUND.  
 13. ALL UTILITIES TO BE UNDERGROUND.  
 14. ALL UTILITIES TO BE UNDERGROUND.  
 15. ALL UTILITIES TO BE UNDERGROUND.  
 16. ALL UTILITIES TO BE UNDERGROUND.  
 17. ALL UTILITIES TO BE UNDERGROUND.  
 18. ALL UTILITIES TO BE UNDERGROUND.  
 19. ALL UTILITIES TO BE UNDERGROUND.  
 20. ALL UTILITIES TO BE UNDERGROUND.

**LEGEND:**  
 ■ MANHOLE  
 □ CATCH BASIN  
 ○ TELEPHONE POLE  
 ○ ELECTRIC POLE  
 ○ TRANSFORMER  
 ○ GAS VALVE  
 ○ WATER SPOUT  
 ○ WATER METER  
 ○ WATER VALVE  
 ○ FIRE HYDRANT  
 ○ CATCH BASIN  
 ○ SINGLE GATE INLET  
 ○ DOUBLE GATE INLET  
 ○ STAFF PIPE  
 ○ STORM MANHOLE  
 ○ SANITARY MANHOLE  
 ○ SANITARY MANHOLE  
 ○ MANHOLE - TYPE UNDETERMINED  
 ○ CLEARCUT  
 ○ GUY WIRE  
 ○ UTILITY POLE  
 ○ FENCE POST  
 ○ TREE-INDICATOR  
 ○ BUILDING  
 ○ OPEN OR OUTLET UNDETERMINED  
 ○ OVERHEAD UTILITY  
 ○ UNDERGROUND TELEPHONE LINE-APPROXIMATE LOCATION SHOWN PER RECORD  
 ○ GAS LINE-APPROXIMATE LOCATION SHOWN PER RECORD  
 ○ WATER LINE-APPROXIMATE LOCATION SHOWN PER RECORD  
 ○ WATER LINE PER MARKINGS IN RESPONSE TO RIGHT RECORD OR 3-3-2024  
 ○ STORM LINE/PIPE-APPROXIMATE LOCATION SHOWN PER RECORD  
 ○ SANITARY LINE/PIPE-APPROXIMATE LOCATION SHOWN PER RECORD  
 ○ SANITARY LINE/PIPE-APPROXIMATE LOCATION SHOWN PER RECORD

**SHEET INDEX:**  
 C-1 COVER SHEET  
 C-2 MAIN PLAN  
 C-3 EXISTING CONCEPT DEVELOPMENT PLAN  
 C-4 PROPOSED CONCEPT DEVELOPMENT PLAN  
 C-5 UTILITY & STAGING PLAN

**REVISIONS:**  
 NO. & DESCRIPTION  
 DATE

**EVANS ENGINEERING**  
 4440 AIRPORT ROAD, SUITE 211  
 CINCINNATI, OHIO 45226  
 (513) 321-2168

**HICKORY BURLINGTON & POPLAR POINTE APARTMENTS**  
 LARLAI WAY

**COVER SHEET**  
 SHEET NO. C-1

**SCALE:** 1" = 100' N/A

**DATE:** JUL 2, 2024

**DATE:** JUL 2, 2024

**DATE:** JUL 2, 2024

NO.	DATE	DESCRIPTION
1	10/15/2024	PRELIMINARY

**EVANS ENGINEERING**  
 4240 AIRPORT ROAD, SUITE 211  
 CINCINNATI, OHIO 45226  
 (513) 321-2160

**OVERALL ZONING PLAN**  
 APARTMENTS  
 HICKORY BURLINGTON & PORTLAND NORTH  
 LAMAR WAY  
 DUBLIN, OHIO 43007 COUNTY, OHIO

SHEET NO. C-2

**SITE DATA:**

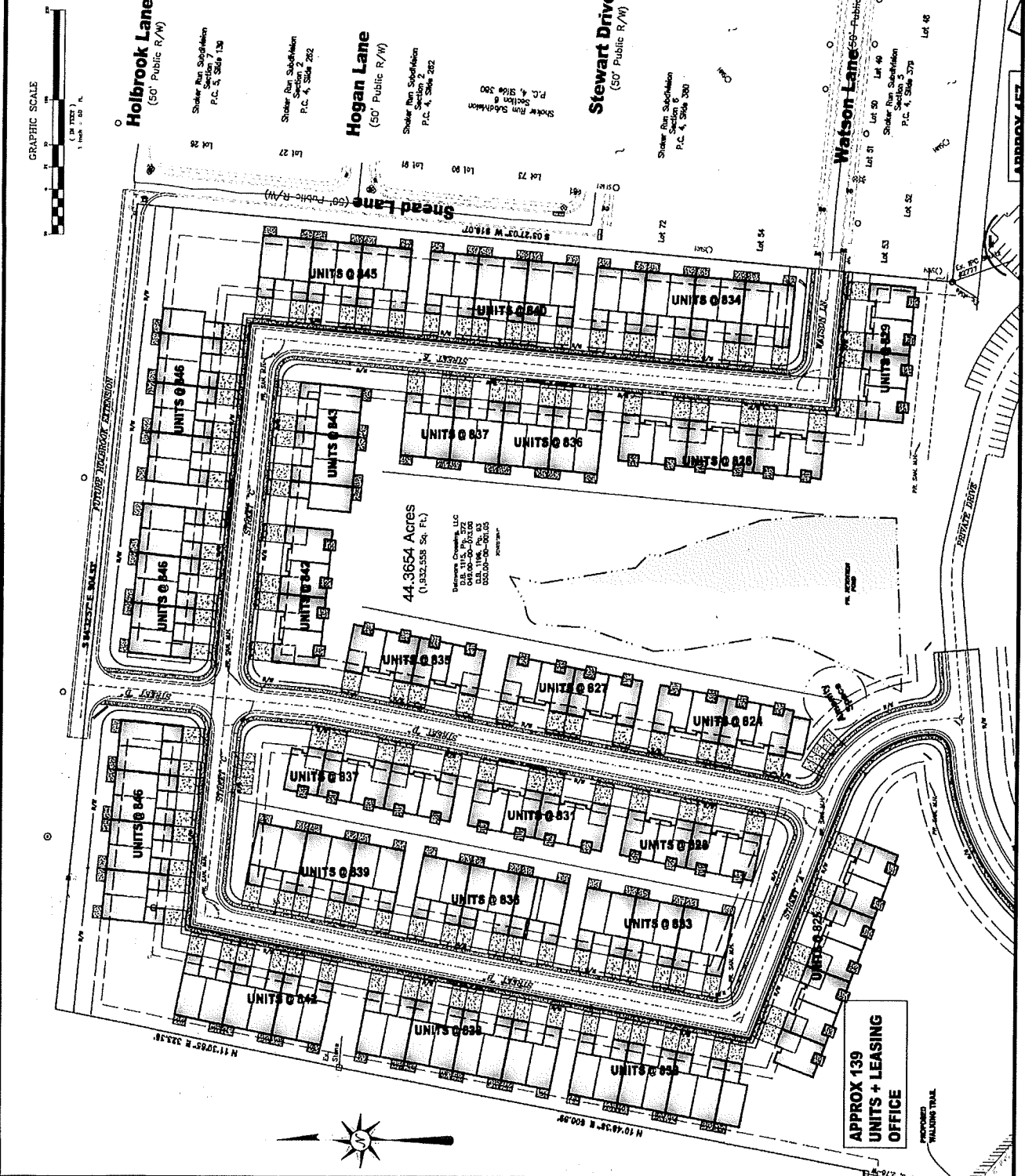
1. 10' AL PROJECT ASPHALT, 45.17 AC RESIDENTIAL, PROJECT AREA: 10.00 AC. STREET FRONTAGE: 287' 0" AC-CADD, 4.29 AC. RESIDENTIAL UNIT: 10' x 10' DENSITY AND MULTI-FAMILY RESIDENTIAL UNIT.
2. MULTI-FAMILY RESIDENTIAL: NET DENSITY CALCULATIONS SHOWN ARE FOR THE MULTI-FAMILY PROJECT AREA.
3. PROPOSED ATTACHED LOSING.
4. ADJ. (CDD) OF LOT 10, 17-20% SUPPLIES.
5. 100% (CDD) OF LOT 10, 17-20% SUPPLIES.
6. 100% (CDD) OF LOT 10, 17-20% SUPPLIES.

**GENERAL NOTES:**

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF CINCINNATI ZONING ORDINANCE AND ALL APPLICABLE CODES.
2. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF CINCINNATI ZONING ORDINANCE AND ALL APPLICABLE CODES.
3. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF CINCINNATI ZONING ORDINANCE AND ALL APPLICABLE CODES.

**APPROX 139 UNITS + LEASING OFFICE**

PROPOSED WALKING TRAIL



**APPROX 139 UNITS + LEASING OFFICE**

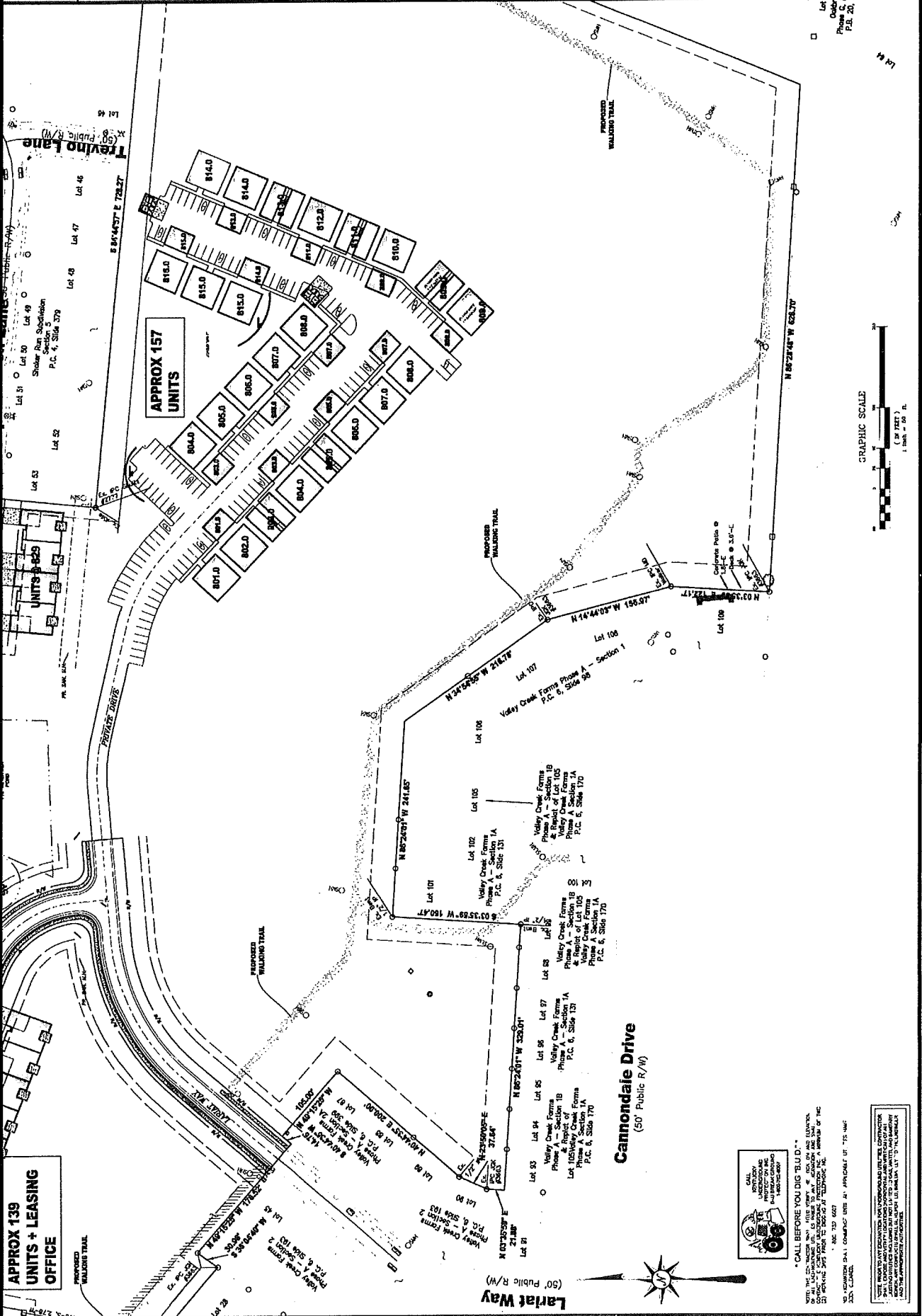
PROPOSED WALKING TRAIL

NO. & DESCR' CH	DATE	REVISIONS

**EVANS ENGINEERING**  
 4240 AIRPORT ROAD, SUITE 211  
 CINCINNATI, OHIO 45226  
 (613) 231-2158

**OVERALL ZONING PLAN**  
 APARTMENTS  
 LINDSEY J. DUNN & POLY CHINE  
 LARA VASA  
 1001 MCGONIGLE BLVD., SUITE 100  
 CINCINNATI, OHIO 45226

SCALE	HORIZ.	VERT.
1" = 50'	1" = 50'	N/A
SUB. NO.	2	101
DATE	JUL 2, 2024	
SHEET NO.		
C-2.1		



**APPROX 139 UNITS + LEASING OFFICE**

**APPROX 157 UNITS**

**Cannondale Drive**  
 (50' Public R/W)



**\* CALL BEFORE YOU DIG "B.U.D." \***  
 ANY EXISTING UTILITIES SHOWN ON THIS PLAN ARE THE PROPERTY OF THE CITY OF CINCINNATI. THE CITY OF CINCINNATI IS NOT RESPONSIBLE FOR THE ACCURACY OF THE UTILITIES SHOWN ON THIS PLAN. THE CITY OF CINCINNATI IS NOT RESPONSIBLE FOR THE ACCURACY OF THE UTILITIES SHOWN ON THIS PLAN. THE CITY OF CINCINNATI IS NOT RESPONSIBLE FOR THE ACCURACY OF THE UTILITIES SHOWN ON THIS PLAN.

NO. 702 6027  
 15 WASHINGTON ST. CINCINNATI, OH 45202  
 513.261.1111  
 WWW.CINCINNATI.ORG



**PRELIMINARY**  
 FOR THE CONNECTION

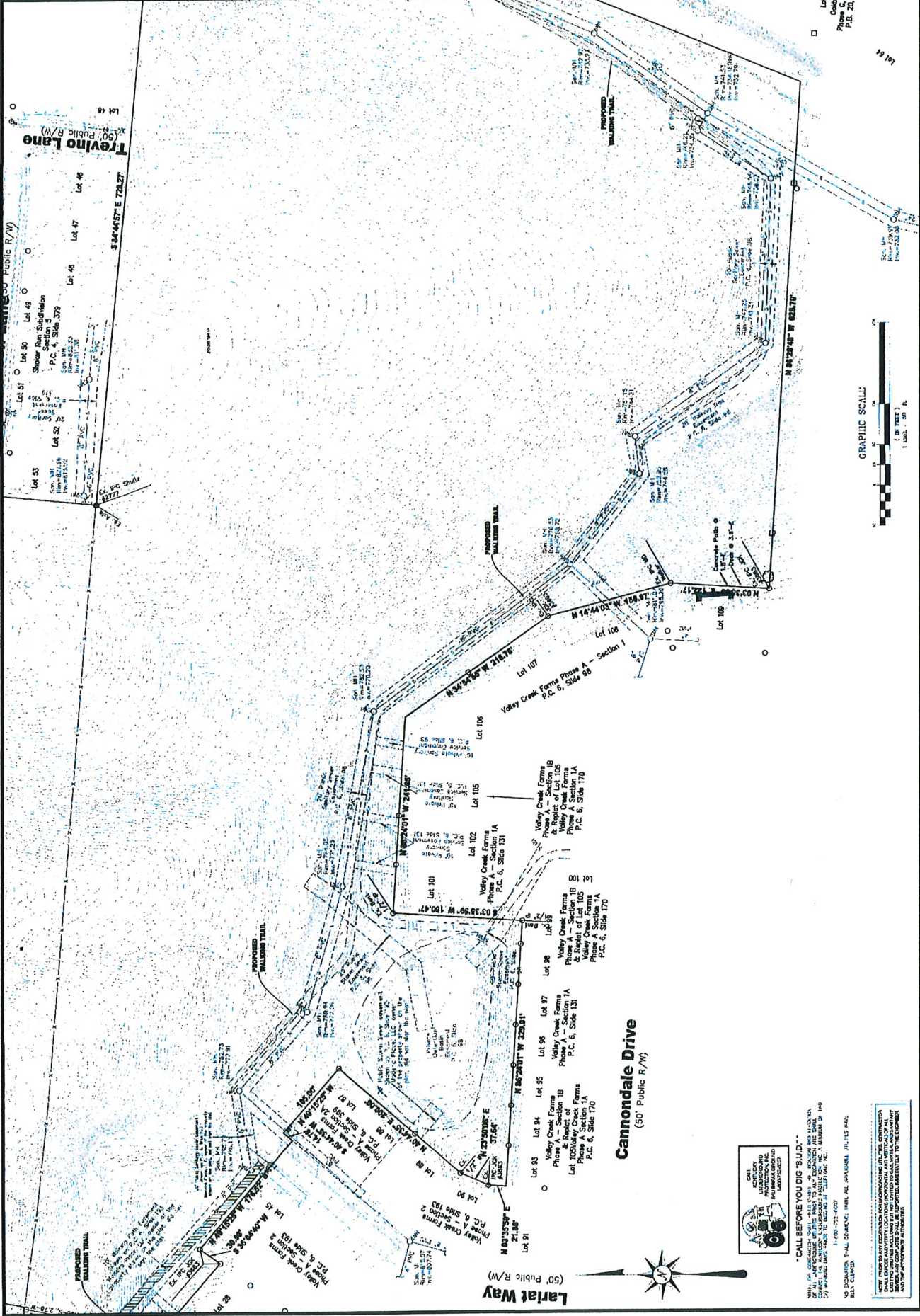


PRELIMINARY Not for construction	
DATE	03/24/17
DRAWN BY	NO. 8 6528-70A
CHKD BY	
REVISIONS	

**EVANS ENGINEERING**  
 4240 AIRPORT ROAD, SUITE 211  
 CINCINNATI, OHIO 45226  
 (513) 321-2188

**EXISTING CONCEPT DEVELOPMENT PLAN**  
 HICKORY HILL EIGHTON & NEIGHBORS  
 ARLINGTON, VIRGINIA  
 HICKORY HILL EIGHTON & NEIGHBORS  
 HICKORY HILL EIGHTON & NEIGHBORS  
 HICKORY HILL EIGHTON & NEIGHBORS  
 HICKORY HILL EIGHTON & NEIGHBORS

SHEET NO.  
 C-3.1



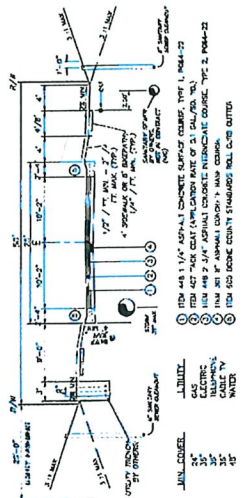
PRELIMINARY Not for construction	
DATE	03/24/17
DRAWN BY	NO. 8 6528-70A
CHKD BY	
REVISIONS	

**EVANS ENGINEERING**  
 4240 AIRPORT ROAD, SUITE 211  
 CINCINNATI, OHIO 45226  
 (513) 321-2188

**EXISTING CONCEPT DEVELOPMENT PLAN**  
 HICKORY HILL EIGHTON & NEIGHBORS  
 ARLINGTON, VIRGINIA  
 HICKORY HILL EIGHTON & NEIGHBORS  
 HICKORY HILL EIGHTON & NEIGHBORS  
 HICKORY HILL EIGHTON & NEIGHBORS

SHEET NO.  
 C-3.1

**\*CALL BEFORE YOU DIG "B.U.D."\***  
 A utility locator service is available for hire. Call 1-800-485-4848. Call before you dig. Digging without proper permits may result in injury, property damage, and/or death. Digging without proper permits may also result in criminal and civil penalties. Call before you dig. Call before you dig. Call before you dig.



**CALL BEFORE YOU DIG 'B.U.D.' !!**  
 WE ALL KNOW YOU SHOULD CALL BEFORE YOU DIG. BUT WE DON'T WANT TO HEAR ABOUT AN UNLAWFUL DIGGING OPERATION THAT RESULTS IN A SERVICE DISRUPTION. CALL US TODAY TO GET THE INFORMATION YOU NEED TO AVOID THESE PROBLEMS. WE'LL BE GLAD TO ASSIST YOU.

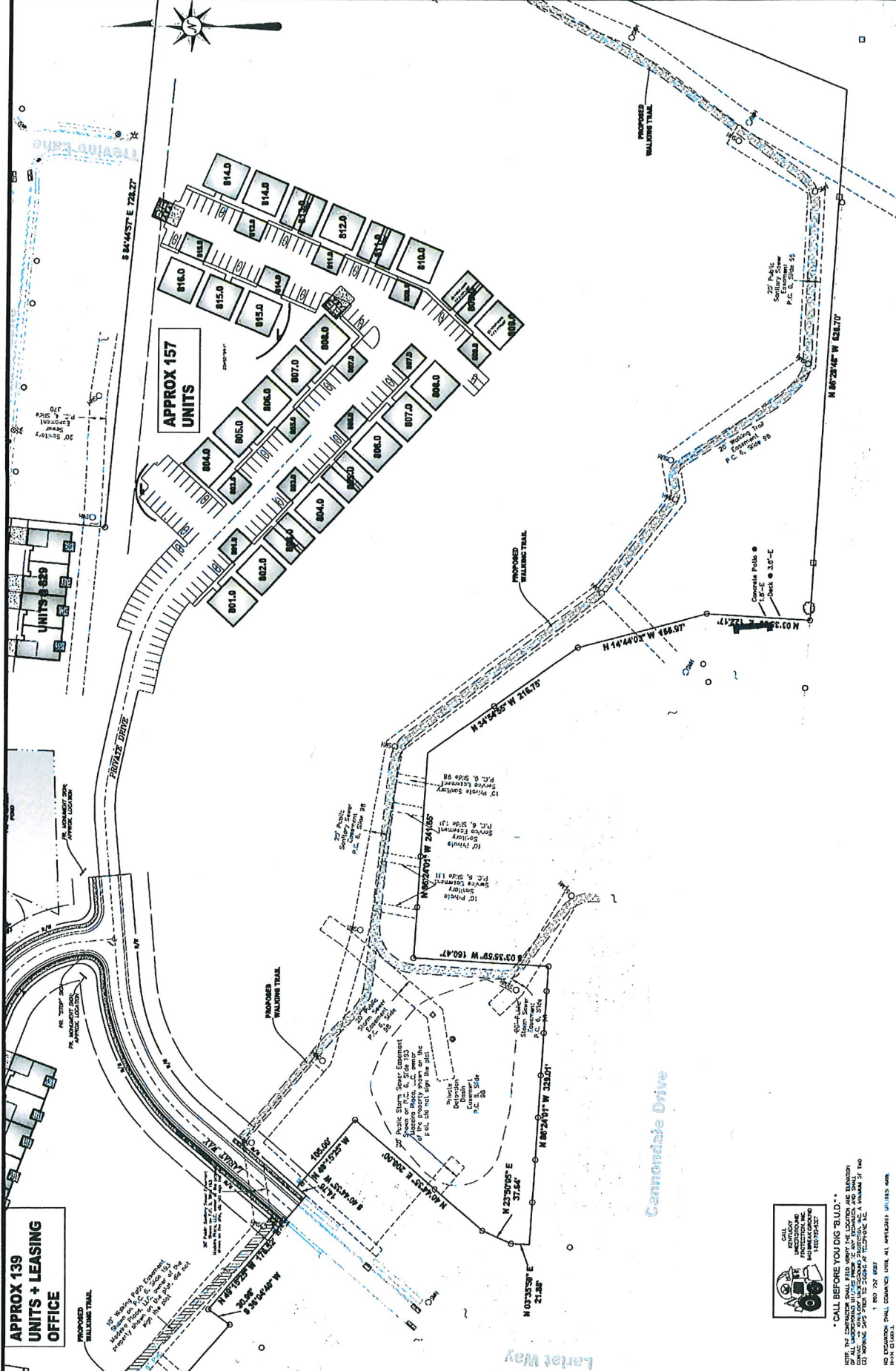
**1-800-922-6007**  
 WE'LL BE GLAD TO ASSIST YOU.

NO OPERATION SHALL COMMENCE UNTIL ALL APPLICABLE UTILITY HAS BEEN LOCATED AND MARKED.

NOTE: THIS PLAN IS FOR INFORMATION ONLY. IT IS NOT A CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL UTILITIES AND FOR OBTAINING ALL NECESSARY PERMITS FROM THE APPROPRIATE AGENCIES AND THE APPLICABLE AUTHORITIES.



APPROX 139 UNITS + LEASING OFFICE  
 PROPOSED WALLING THAIL  
 10' Deep Post & Beam  
 Foundation  
 Proposed by Evans Engineering  
 Property of Evans Engineering



**APPROX 139  
UNITS + LEASING  
OFFICE**

**APPROX 157  
UNITS**



**\* CALL BEFORE YOU DIG TO U.D. \***  
NOTE: THE CONTRACTOR SHALL FIELD VERIFY THE LOCATION AND ELEVATION OF ALL UTILITIES AND RECORD THEM ACCORDING TO THE STANDARD PRACTICES OF THE INDUSTRY. A RECORD DRAWING SHALL BE SUBMITTED IMMEDIATELY TO THE ENGINEER.  
NO EXCAVATION SHALL COMMENCE UNTIL ALL UTILITIES ARE IDENTIFIED AND MARKED.  
BY: [Signature]

NOTE: THIS PLAN IS FOR INFORMATION ONLY AND DOES NOT REPRESENT A CONTRACT. THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION OF ALL UTILITIES AND RECORD THEM ACCORDING TO THE STANDARD PRACTICES OF THE INDUSTRY. A RECORD DRAWING SHALL BE SUBMITTED IMMEDIATELY TO THE ENGINEER.  
NO EXCAVATION SHALL COMMENCE UNTIL ALL UTILITIES ARE IDENTIFIED AND MARKED.  
BY: [Signature]



**PRELIMINARY**  
NOT FOR CONSTRUCTION

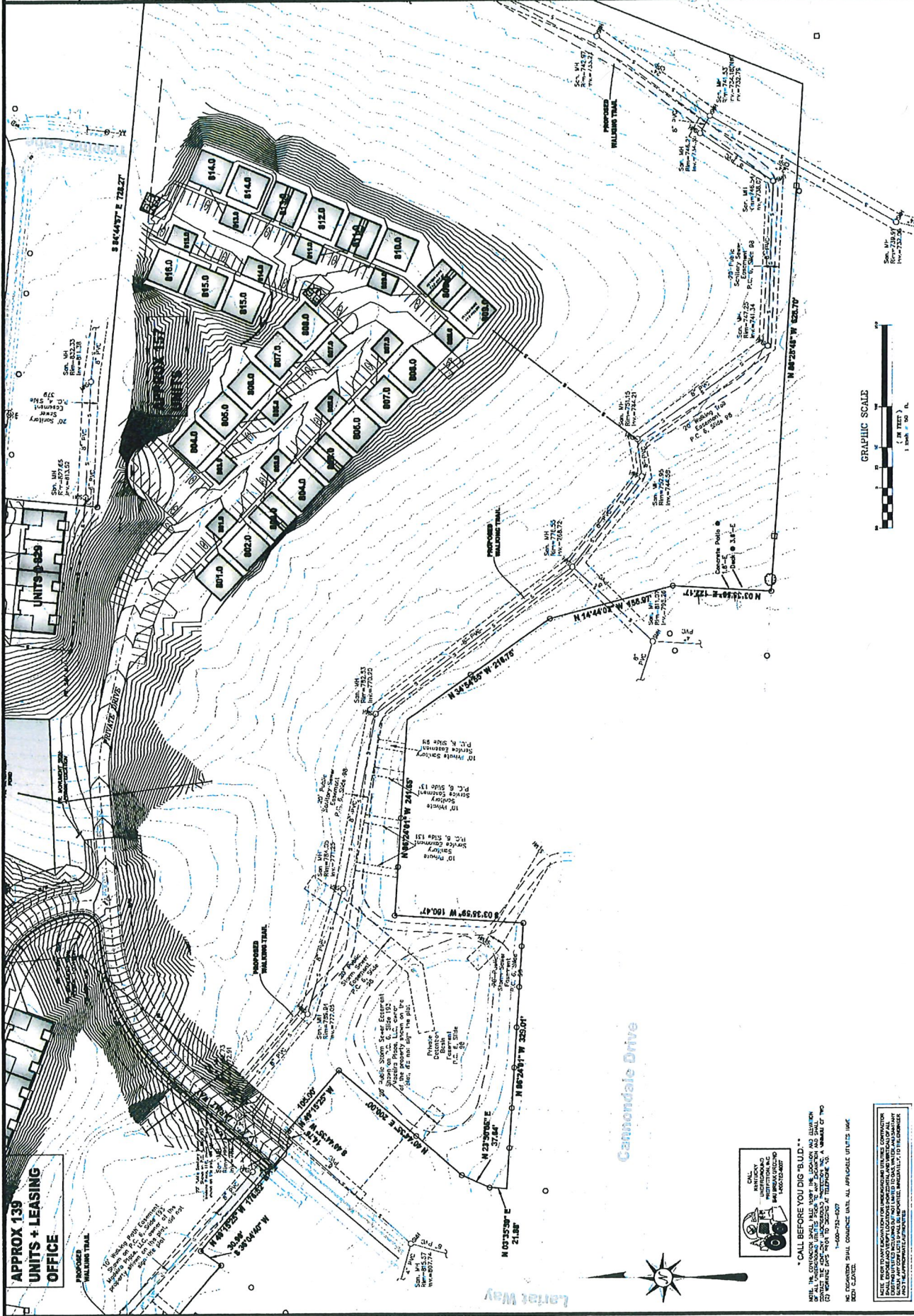
DATE	BY	NO. & DESCRIPTION

**EVANS ENGINEERING**  
4240 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45228  
(513) 221-2168

**UTILITY & GRADING PLAN**  
HICKORY BUILDINGS & TOWN CENTER  
APARTMENTS  
LARAMY WAY  
MURKINBORO, OHIO 45228

SCALE	DATE
1" = 40'	M/V
DATE	BY
24-1-11	
DATE	BY
JUL 2, 2009	

SHEET NO.  
C-5.1



**APPROX 139 UNITS + LEASING OFFICE**



**\* CALL BEFORE YOU DIG \* B.U.D. \*\***  
ALL UTILITIES SHOWN ARE BASED ON RECORD DRAWINGS AND FIELD SURVEY. THE CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO ANY EXCAVATION OR DRILLING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS.

NO EXCAVATION SHALL COMMENCE UNTIL ALL APPLICABLE UTILITIES HAVE BEEN LOCATED.  
DATE: 07/02/09  
BY: M/V

PROPOSED 1-STORY RESIDENCES FOR: DELAWARE CROSSING, LLC  
**HOLBROOK LN • LOT 00**  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042 • BOONE CO. PARCEL ID: 049.00-00-073.00  
 "THE BTR COLLECTION" • CONSTRUCTION DOCUMENTS-1

**BROOKSTONE HOMES, LLC.**  
 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200

**B**  
*Virtual f/x*  
 10351 KENWOOD RD.  
 BLUE ASH, OHIO 45242  
 (513) 393-0857

**BROOKSTONE**  
 H O M E S



Virtual f/x View • Elevation-A

**SHEET INDEX**

NO.	DESCRIPTION	DATE
1	Title Sheet	06/24/24
CDAT	ET 25' Unit • Front Elevation-A & B	
CDKZ	ET 25' Unit • Other Elevations	
CDL1	1st Floor Slab	
CDL2	1st Floor Slab • Virtual Tour-A	
CDL3	1st Floor Slab • Virtual Tour-B	
CDL4	1st Floor Slab • Virtual Tour-C	
CDL5	1st Floor Slab • Virtual Tour-D	
CDL6	1st Floor Slab • Virtual Tour-E	
CDL7	1st Floor Slab • Virtual Tour-F	
CDL8	1st Floor Slab • Virtual Tour-G	
CDL9	1st Floor Slab • Virtual Tour-H	
CDL10	1st Floor Slab • Virtual Tour-I	
CDL11	1st Floor Slab • Virtual Tour-J	
CDL12	1st Floor Slab • Virtual Tour-K	
CDL13	1st Floor Slab • Virtual Tour-L	
CDL14	1st Floor Slab • Virtual Tour-M	
CDL15	1st Floor Slab • Virtual Tour-N	
CDL16	1st Floor Slab • Virtual Tour-O	
CDL17	1st Floor Slab • Virtual Tour-P	
CDL18	1st Floor Slab • Virtual Tour-Q	
CDL19	1st Floor Slab • Virtual Tour-R	
CDL20	1st Floor Slab • Virtual Tour-S	
CDL21	1st Floor Slab • Virtual Tour-T	
CDL22	1st Floor Slab • Virtual Tour-U	
CDL23	1st Floor Slab • Virtual Tour-V	
CDL24	1st Floor Slab • Virtual Tour-W	
CDL25	1st Floor Slab • Virtual Tour-X	
CDL26	1st Floor Slab • Virtual Tour-Y	
CDL27	1st Floor Slab • Virtual Tour-Z	
CDL28	1st Floor Slab • Virtual Tour-AA	
CDL29	1st Floor Slab • Virtual Tour-AB	
CDL30	1st Floor Slab • Virtual Tour-AC	
CDL31	1st Floor Slab • Virtual Tour-AD	
CDL32	1st Floor Slab • Virtual Tour-AE	
CDL33	1st Floor Slab • Virtual Tour-AF	
CDL34	1st Floor Slab • Virtual Tour-AG	
CDL35	1st Floor Slab • Virtual Tour-AH	
CDL36	1st Floor Slab • Virtual Tour-AI	
CDL37	1st Floor Slab • Virtual Tour-AJ	
CDL38	1st Floor Slab • Virtual Tour-AK	
CDL39	1st Floor Slab • Virtual Tour-AL	
CDL40	1st Floor Slab • Virtual Tour-AM	
CDL41	1st Floor Slab • Virtual Tour-AN	
CDL42	1st Floor Slab • Virtual Tour-AO	
CDL43	1st Floor Slab • Virtual Tour-AP	
CDL44	1st Floor Slab • Virtual Tour-AQ	
CDL45	1st Floor Slab • Virtual Tour-AR	
CDL46	1st Floor Slab • Virtual Tour-AS	
CDL47	1st Floor Slab • Virtual Tour-AT	
CDL48	1st Floor Slab • Virtual Tour-AU	
CDL49	1st Floor Slab • Virtual Tour-AV	
CDL50	1st Floor Slab • Virtual Tour-AW	
CDL51	1st Floor Slab • Virtual Tour-AX	
CDL52	1st Floor Slab • Virtual Tour-AY	
CDL53	1st Floor Slab • Virtual Tour-AZ	
CDL54	1st Floor Slab • Virtual Tour-BA	
CDL55	1st Floor Slab • Virtual Tour-BB	
CDL56	1st Floor Slab • Virtual Tour-BC	
CDL57	1st Floor Slab • Virtual Tour-BD	
CDL58	1st Floor Slab • Virtual Tour-BE	
CDL59	1st Floor Slab • Virtual Tour-BF	
CDL60	1st Floor Slab • Virtual Tour-BG	
CDL61	1st Floor Slab • Virtual Tour-BH	
CDL62	1st Floor Slab • Virtual Tour-BI	
CDL63	1st Floor Slab • Virtual Tour-BJ	
CDL64	1st Floor Slab • Virtual Tour-BK	
CDL65	1st Floor Slab • Virtual Tour-BL	
CDL66	1st Floor Slab • Virtual Tour-BM	
CDL67	1st Floor Slab • Virtual Tour-BN	
CDL68	1st Floor Slab • Virtual Tour-BO	
CDL69	1st Floor Slab • Virtual Tour-BP	
CDL70	1st Floor Slab • Virtual Tour-BQ	
CDL71	1st Floor Slab • Virtual Tour-BR	
CDL72	1st Floor Slab • Virtual Tour-BS	
CDL73	1st Floor Slab • Virtual Tour-BT	
CDL74	1st Floor Slab • Virtual Tour-BU	
CDL75	1st Floor Slab • Virtual Tour-BV	
CDL76	1st Floor Slab • Virtual Tour-BW	
CDL77	1st Floor Slab • Virtual Tour-BX	
CDL78	1st Floor Slab • Virtual Tour-BY	
CDL79	1st Floor Slab • Virtual Tour-BZ	
CDL80	1st Floor Slab • Virtual Tour-CA	
CDL81	1st Floor Slab • Virtual Tour-CB	
CDL82	1st Floor Slab • Virtual Tour-CC	
CDL83	1st Floor Slab • Virtual Tour-CD	
CDL84	1st Floor Slab • Virtual Tour-CE	
CDL85	1st Floor Slab • Virtual Tour-CD	
CDL86	1st Floor Slab • Virtual Tour-CE	
CDL87	1st Floor Slab • Virtual Tour-CD	
CDL88	1st Floor Slab • Virtual Tour-CE	
CDL89	1st Floor Slab • Virtual Tour-CD	
CDL90	1st Floor Slab • Virtual Tour-CE	
CDL91	1st Floor Slab • Virtual Tour-CD	
CDL92	1st Floor Slab • Virtual Tour-CE	
CDL93	1st Floor Slab • Virtual Tour-CD	
CDL94	1st Floor Slab • Virtual Tour-CE	
CDL95	1st Floor Slab • Virtual Tour-CD	
CDL96	1st Floor Slab • Virtual Tour-CE	
CDL97	1st Floor Slab • Virtual Tour-CD	
CDL98	1st Floor Slab • Virtual Tour-CE	
CDL99	1st Floor Slab • Virtual Tour-CD	
CDL100	1st Floor Slab • Virtual Tour-CE	

**Square Footages:**

Category	28' Units	38' Units
• 1st Floor Slab	1,352 sqft	1,439 sqft
• 1st Floor Slab (Excl Unit)	1,329 sqft	N/A sqft
• Garage Slab	372 sqft	372 sqft
• Front Porch Slab	40 sqft	52 sqft
• ROOF-TEX & FRID	96 sqft	96 sqft
• First Floor-A (Excl Unit)	1,382 sqft	N/A sqft

**CODE INFORMATION**

- REFERENCED CODE: INTERNATIONAL CODE OF BUILDING
- BUILDING CLASSIFICATION: USE OCC.P.
- BUILDING CONSTRUCTION TYPE: 98 (UNPROTECTED)
- WIND LOAD: 20 PSF
- SNOW LOAD: 20 PSF
- WIND LOAD: 20 PSF
- FLOOR LOAD: 40 PSF

**OWNER**  
 DELAWARE CROSSING, LLC  
**RESIDENTIAL SERVICES**  
 VIRTUAL F/X  
 MICHAEL P. FISHER

**CONTRACTOR**  
 BROOKSTONE HOMES, LLC

**DATE:** 06/24/24 • D01  
**REVISED:**  
**RELEASE:**  
**PERMIT RELEASE:**

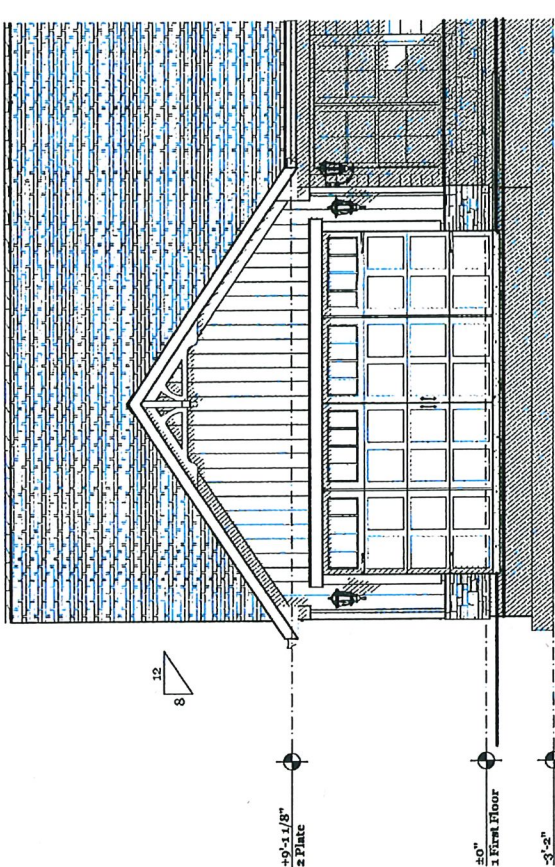
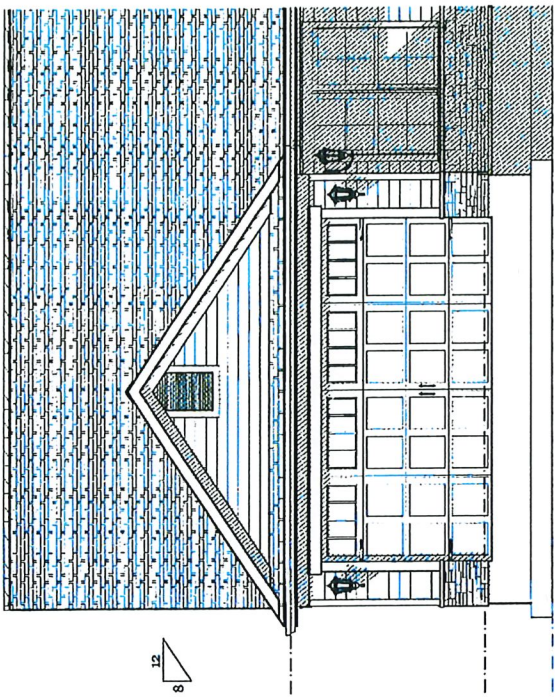
**PROPERTY INFORMATION**  
 20 AC ON HOLBROOK LN (48-73)

SHEET NO. 1-629  
**T1**  
 SHEET TITLE  
 Title Sheet

**Virtual f/x**  
 1031 Kenwood Rd. • Suite 201 • Blue Ash, Ohio 45242  
 Drafted By: T. Miller • (513) 393-0857  
 Virtual f/x  
 (513) 393-0857

**BROOKSTONE**  
 The Multi-Fam's  
 ELA-G. Varies  
 For: Delaware Crossing, LLC  
 1841 Holbrook Lane • Florence, Kentucky 41042  
 BROOKSTONE HOMES  
 7203 Wooster Pike • Cincinnati, OH 45227 • 513-340-7200

06/27/24  
 Title Request  
 06/24/24



**E1** Holbrook Ln • 28' Unit • Front Elevation-A  
 3/16" = 1'-0"

**E1** Holbrook Ln • 28' Unit • Front Elevation-B  
 3/16" = 1'-0"

**CODE INFORMATION CONT...**

STRUCTURAL MEMBER ALLOWABLE DEFLECTION  
 • Floor joists: L/240 with no finished ceiling intended for rooms L/180  
 • Joists and planked ceilings: L/240  
 • All other structural members: L/240  
 • Exterior walls—wind loads with flexible finishes: L/240  
 • Exterior walls—wind loads with flexible finishes: L/120  
 • Lateral bracing: measure vector walls: L/600

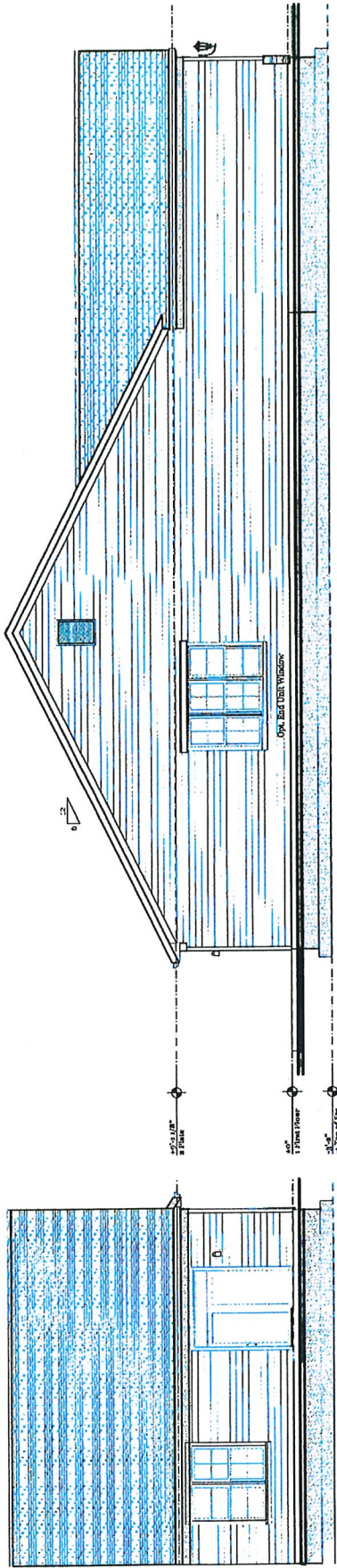
**Note:**  
 All Downspouts connected  
 to approved locations

**WALL BRACING METHOD**  
 PER RCO 602.2.4.4  
 6x4x8 PER RCO 602.2.4.4  
 WOOD STRUCTURAL PANEL: 3/4" MIN.  
 THK. CONNECTION REQUIREMENTS:  
 FASTENERS COMPLY WITH RCO 602.3.3)  
 FASTENED 6" ON EDGES AND 12" IN FIELD.  
 SHALL BE 3/4" MIN. GROUND BOLTS FASTENED  
 IN ACCORDANCE WITH RCO TABLE 702.2.5

**EXHAUST FAN & SMOKE DETECTOR NOTE:**

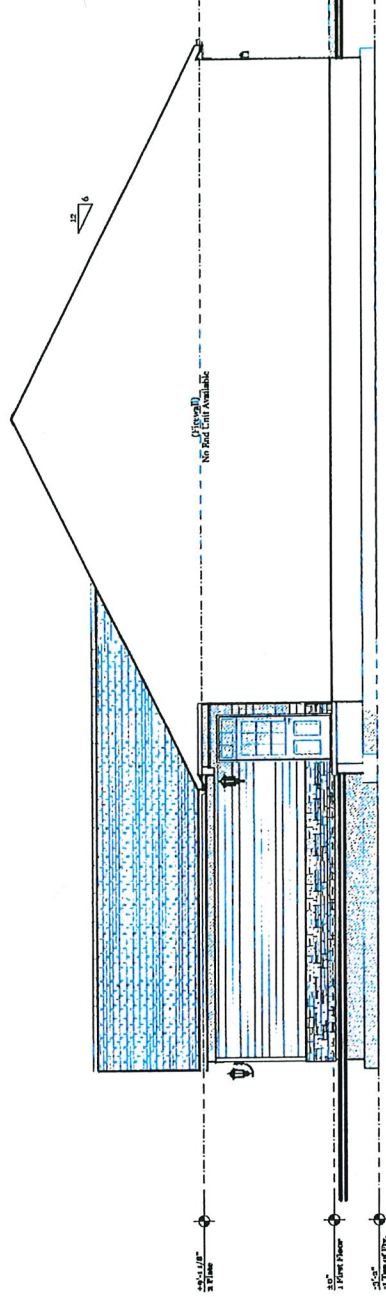
- ⊙ (SEE NOTES, D EXHAUST FAN LOCATION SHEET A-5)
- ⊙ (SEE NOTES, D EXHAUST FAN LOCATION SHEET A-5)
- ⊙ (SEE NOTE IN SHEET A-4 THE NOTE TO THE LEFT FOR ALL SMOKE DETECTOR (CODE INFO)
- ⊙ (DENOTES SMOKE/CO COMBO DETECTOR LOCATIONS PROGRAMMABLE THERMOSTAT REQUIRED FOR ALL FORCED AIR FUNCTIONS)

ALL SMOKE ALARMS SHALL BE LISTED IN  
 WITHIN EACH DWELLING UNIT SMOKE  
 ALARMS UTILIZING PHOTOVOLTAGE AND  
 IONIZATION TECHNOLOGIES SHALL BE  
 INSTALLED. BEDROOMS TO USE IONIZATION  
 TYPE, OUTSIDE BEDROOMS TO BE  
 PHOTOVOLTAGE TYPE. SEPARATE OR DUAL-  
 SENSING SMOKE ALARMS MAY BE USED



**E2** 28' Unit • Rear Elevation-A  
1/8" = 1'-0"

**E3** Holbrook Ln • 28' Unit • Left Elevation-A  
1/8" = 1'-0"

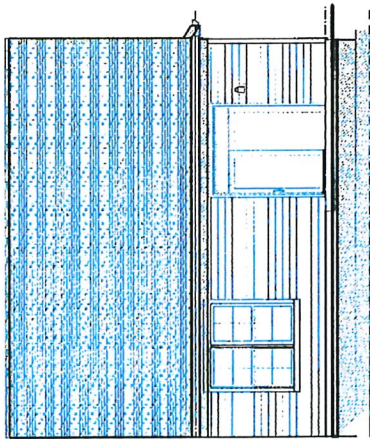


**E4** Holbrook Ln • 28' Unit • Right Elevation-A  
1/8" = 1'-0"

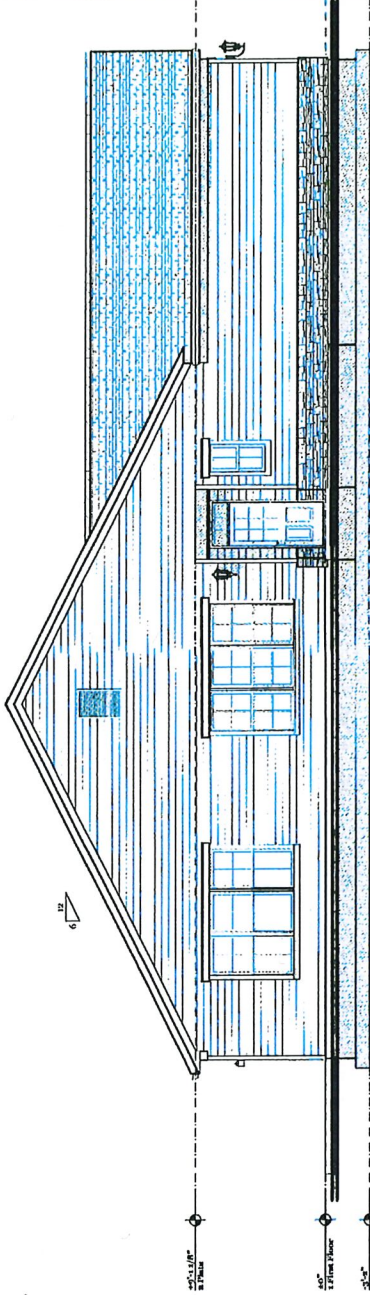
**HBRO**  
 28' Unit - Other Elevations  
**CD**  
**A.2**  
 SHEET NO. 316/20

**Brookstone**  
 2024-0807  
 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-200-1200  
**BROOKSTONE HOMES**  
 Fort Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
**Bl-A-C, Various**  
**The Multi-Farm's**

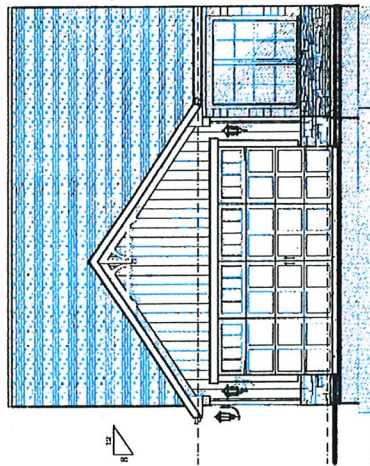
**Ohmud f/x**  
 10351 Kenwood Rd. • Sylvania • Blue Ash, Ohio 45424  
 (614) 393-0857  
 Email: info@ohmud.com  
**Vf/x**  
 MANUFACTURED BY: 06/07/24  
 Club Request: 06/07/24  
 Design Date: 06/07/24



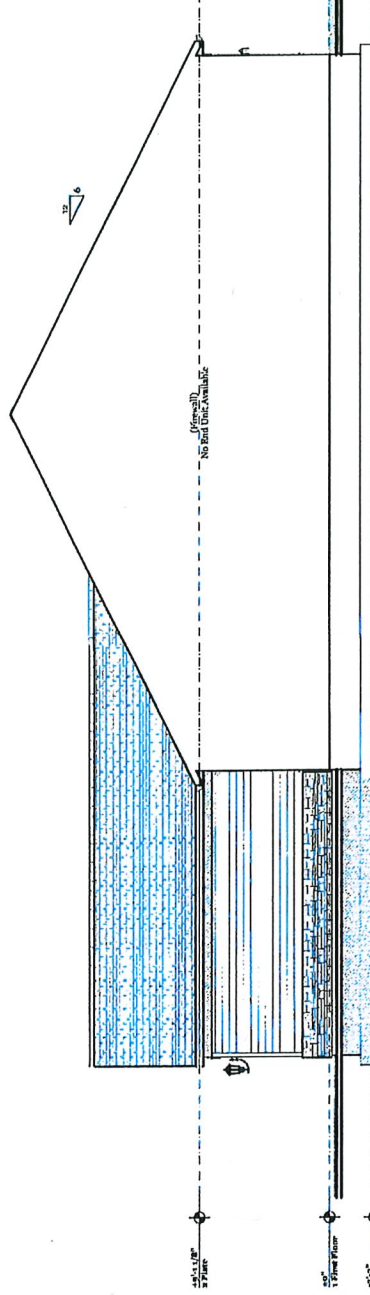
**E2** 28' Unit • Rear Elevation-A  
1/8" = 1'-0"



**E3** Holbrook Ln • 28' End Unit • Left Elevation-A  
1/8" = 1'-0"



**E1** 28' End Unit • Front Elevation-A  
1/8" = 1'-0"



**E4** Holbrook Ln • 28' End Unit • Right Elevation-A  
1/8" = 1'-0"

**HB00** The Multi-Fam's  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
 BROOKSTONE HOMES, L.L.C.  
 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-208-0800

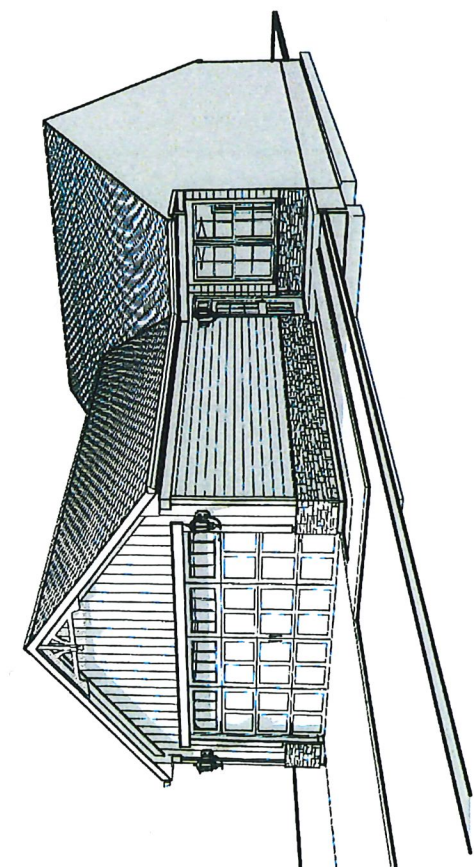
**BROOKSTONE** HOMES

REVISED: 06/07/24  
 Job Number: 06/07/24  
 Design Date: 06/07/24

10321 Kenwood Rd. • Suite 2001 • Blue Ash, Ohio 45242  
 Designed By: P. Miller  
 Email: pml@cloud.com

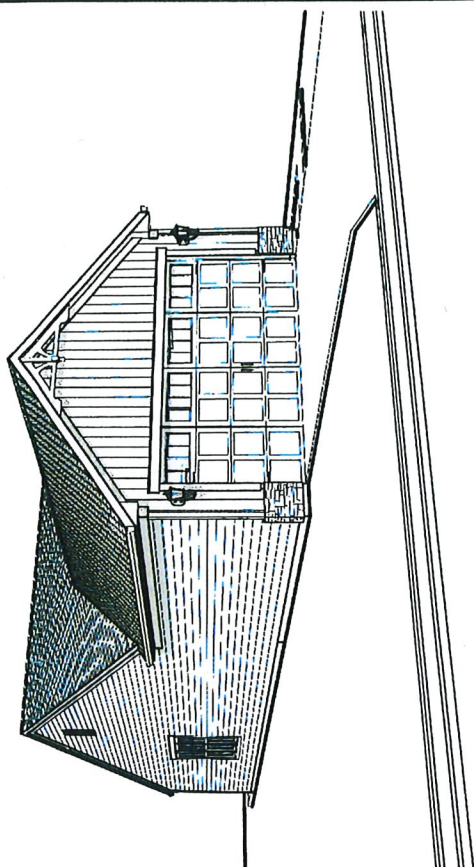
V/f/x  
 @cloud.com

CD  
 A.3  
 SHEET NO. 4-20



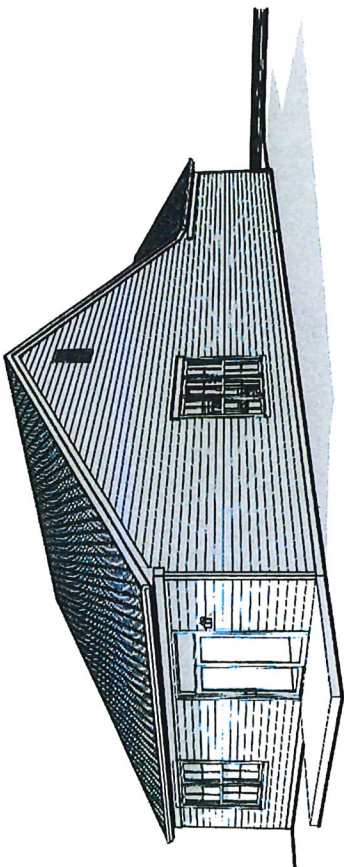
Vfx1

Holbrook 28' Ext • Vfx 3D Tour



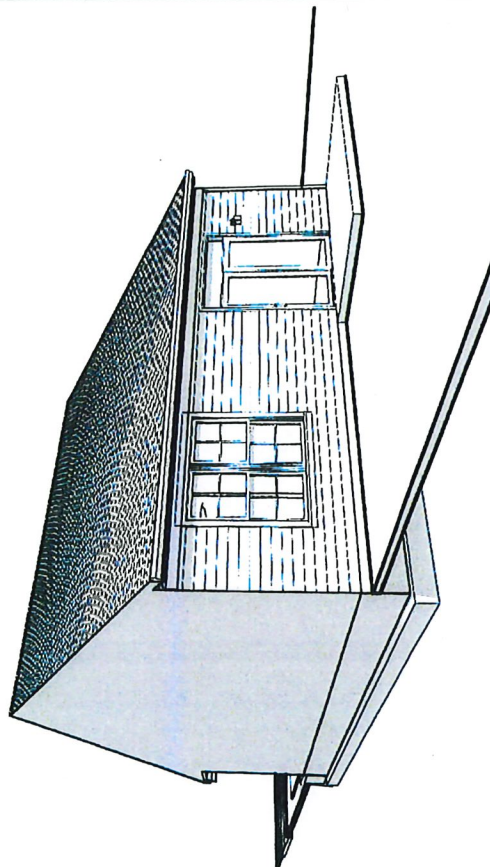
Vfx2

Holbrook 28' Ext • Vfx 3D Tour



Vfx3

Holbrook 28' Ext • Vfx 3D Tour



Vfx4

Holbrook 28' Ext • Vfx 3D Tour

**HBOO** The Multi-Farm's  
 E.A. • G. Vatter  
 FORT DELAWARE CROSSING, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
 7703 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-301-0007

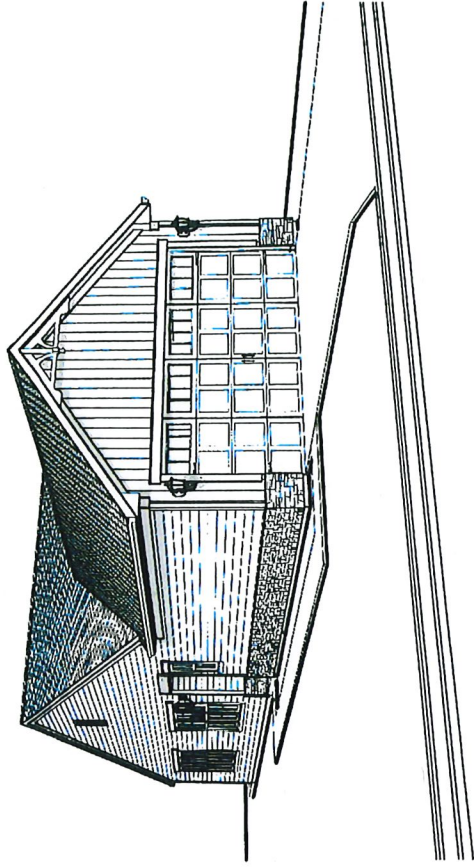
**BROOKSTONE**  
 A R V A

REVISED: 06/27/24  
 Job Request: 06/27/24  
 Design Date: 06/27/24

© Mutual f/x  
 10551 Kenwood Rd. • Suite 2001 • Blue Ash, Ohio 45424  
 Phone: (513) 993-0857  
 Email: info@mutualfx.com

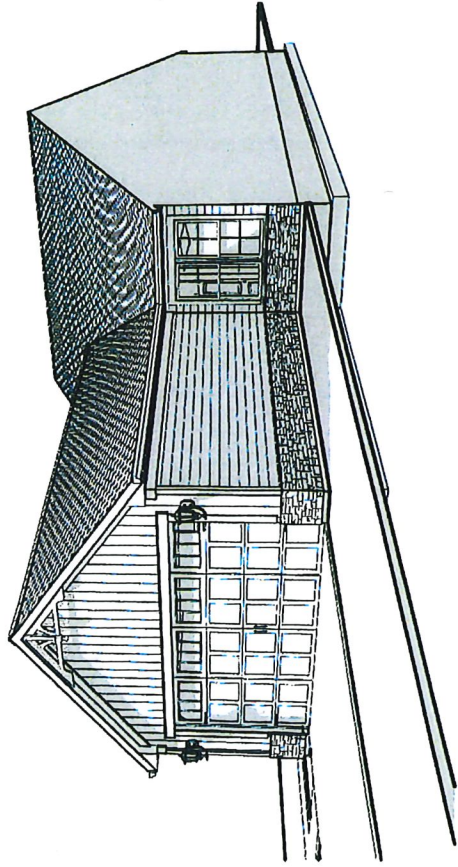
**Vfx/x**

CD  
 A.4  
 SHEET NO. 3 OF 20



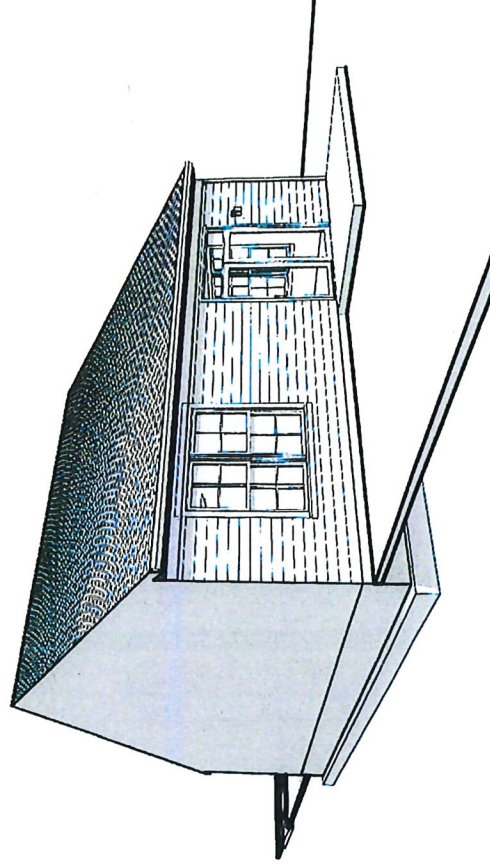
Vfx22

Holbrook 28' End Ext • Vfx 3D Tour



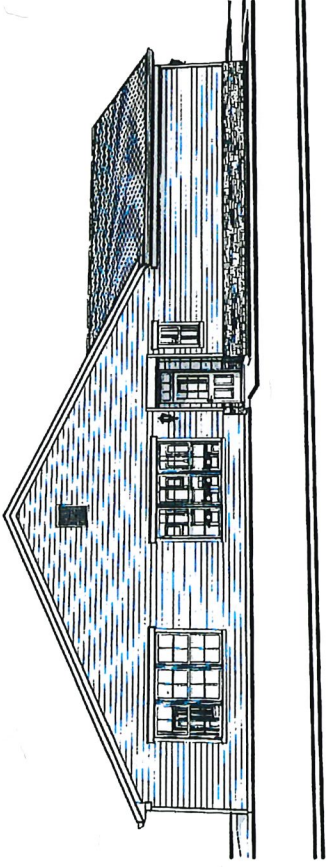
Vfx1

Holbrook 28' End Ext • Vfx 3D Tour



Vfx4

Holbrook 28' End Ext • Vfx 3D Tour



Vfx3

Holbrook 28' End Ext • Vfx 3D Tour

**HBOO**  
 The Multi-Fam's  
 RI-A • G. Varies  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • TOMPKINS, KENTUCKY 41042  
 2021-0607

**BROOKSTONE**  
 2021-0607

REVISIONS: 06/17/22  
 01-08-2022  
 Design Date: 06/17/22

10551 Kenwood Rd. • Suite 2001 • Blue Ash, Ohio 45242  
 Design By: P. A. *Wynn f/x*  
 513-993-0857  
 wynn@wrynf/x.com

**Vfx/x**  
 CD  
 A-5  
 SHEET NO. 0150

**CD**  
**B1**  
 SHEET NO. 2 of 2

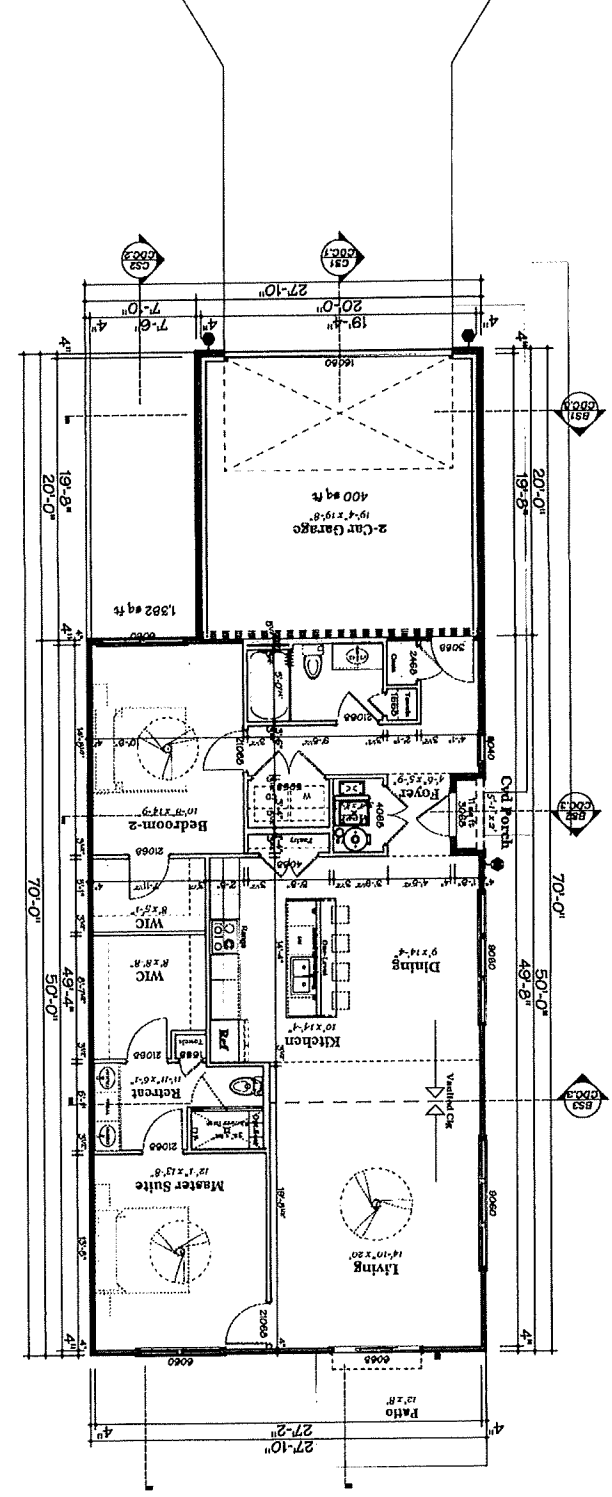
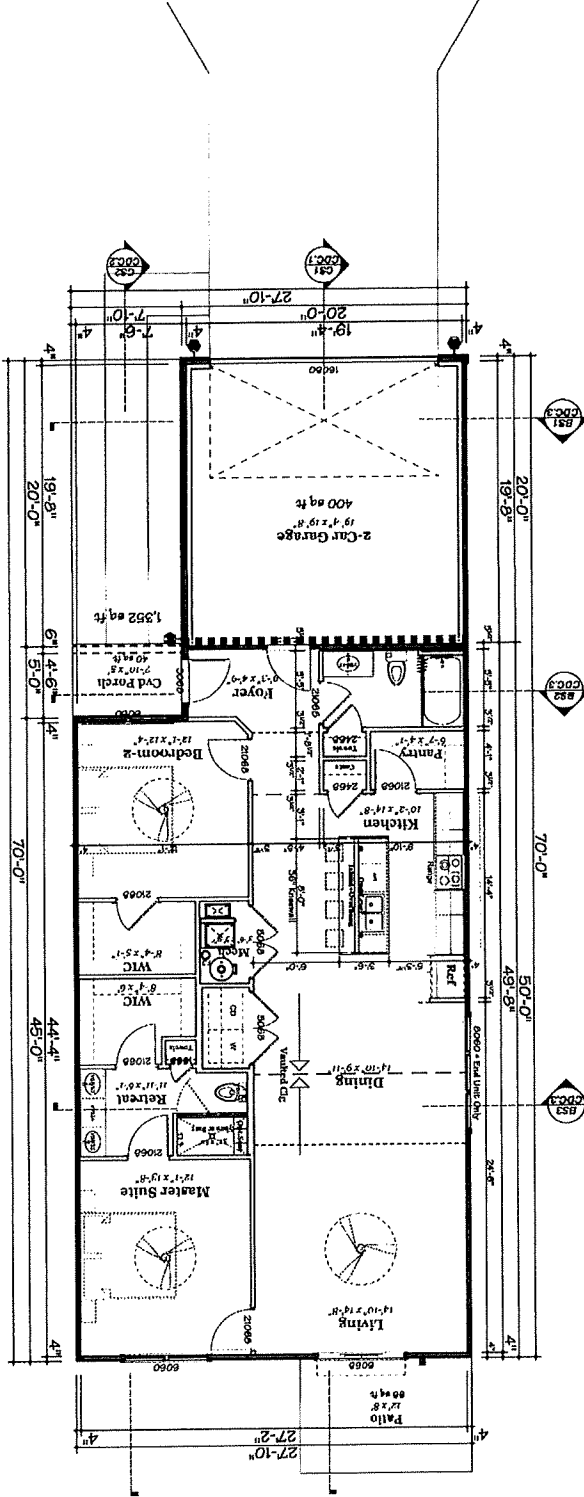
**BROOKSTONE**  
 The Multi-Fam's  
 E.A. G. Varies  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
 BROOKSTONE HOMES, L.L.C.  
 2203 WOODRICK Pkwy • CINCINNATI, OH 45227 • 513-451-0000

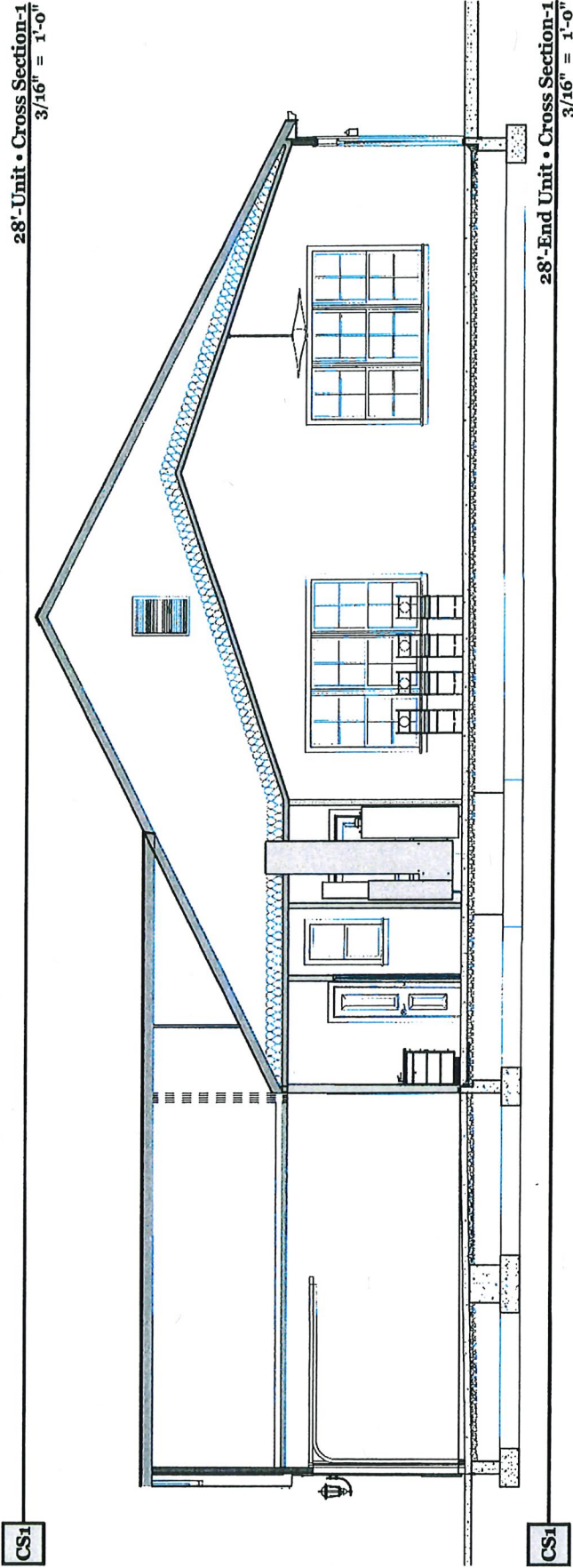
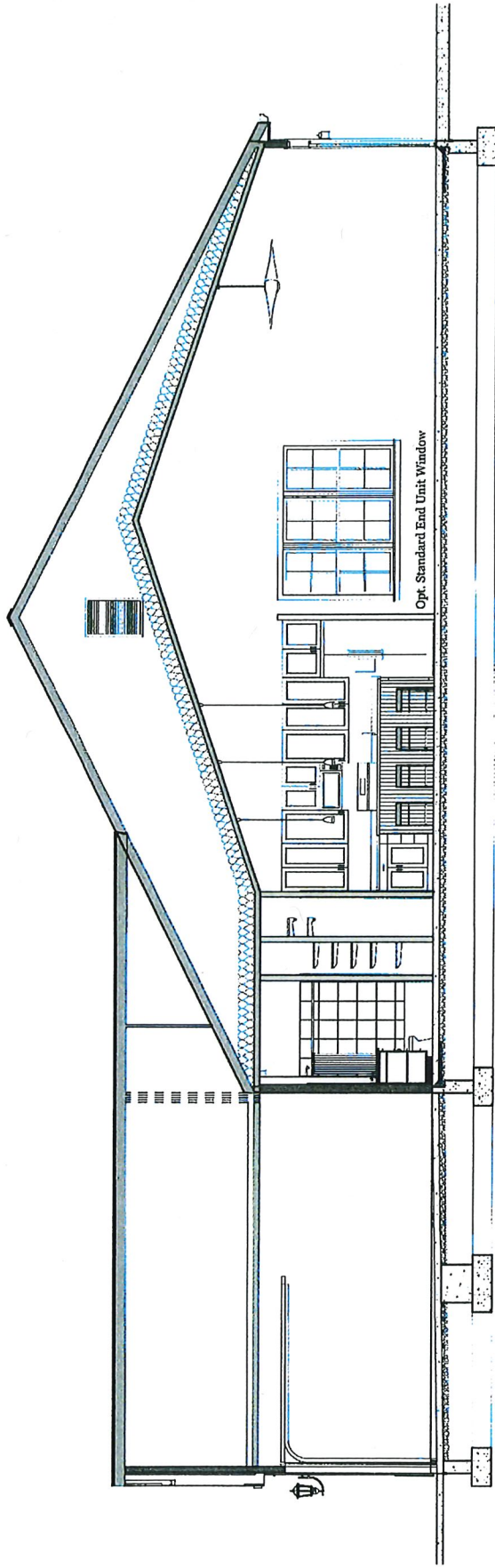
**Brookstone**  
 ARCHITECTS

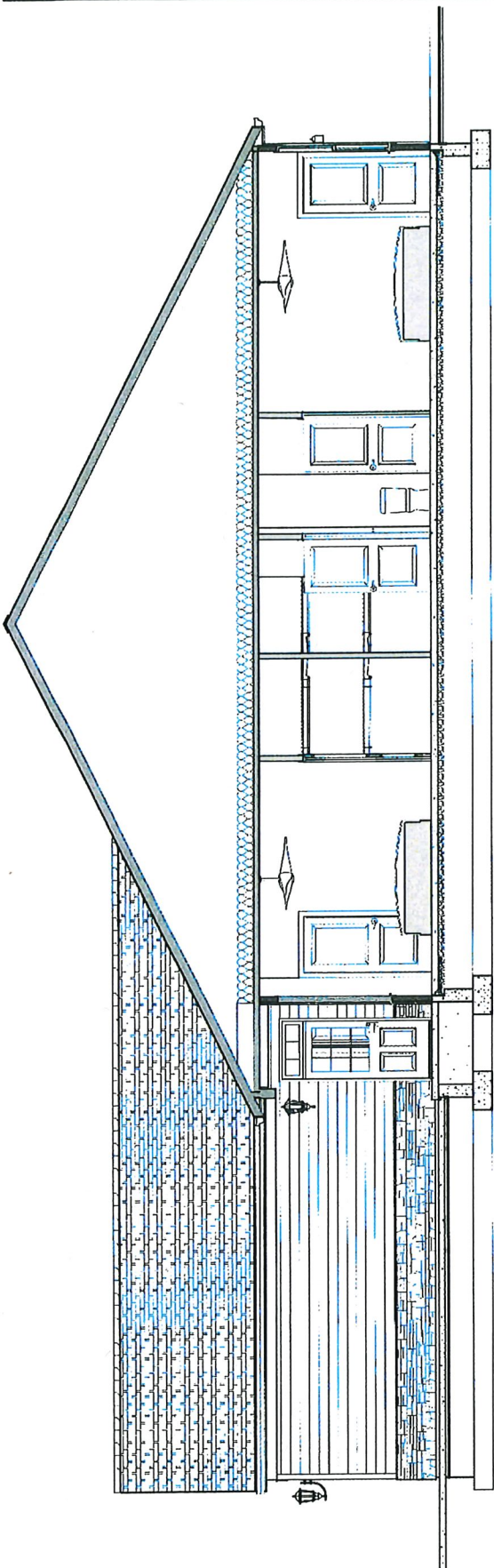
Designed By: E. M. Fisher • (615) 393-0857  
 10531 Kenwood Rd. • Suite 2001 • Mass. Ave., Ohio 45424  
 Email: efisher@brookstone.com

**Vf/x**  
 Vf/x

**1.** Holbrook Ln • 28' Unit • First Floor Plan 1/8" = 1'-0"  
**1.** Holbrook Ln • 28' Unit • First Floor Plan 1/8" = 1'-0"

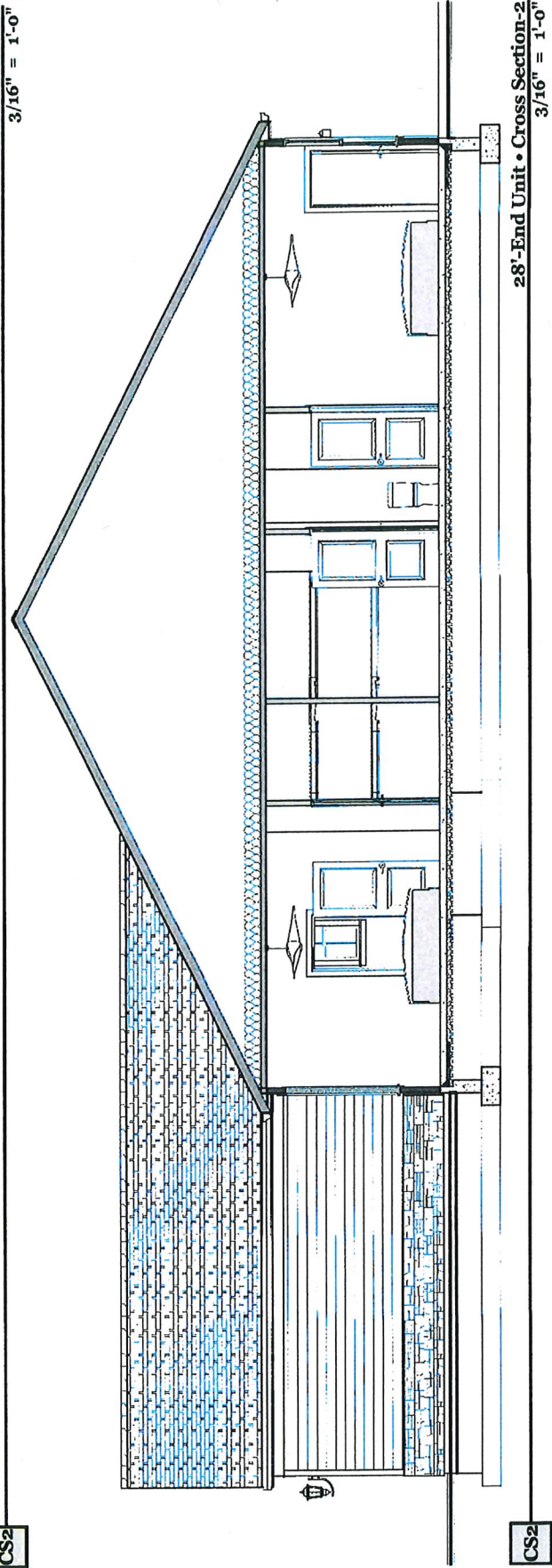






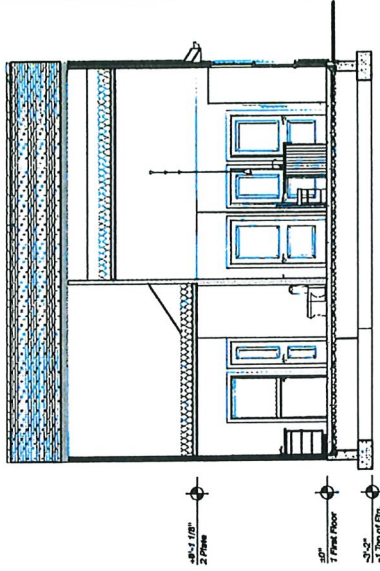
**CS2**

28'-Unit • Cross Section-2  
 3/16" = 1'-0"

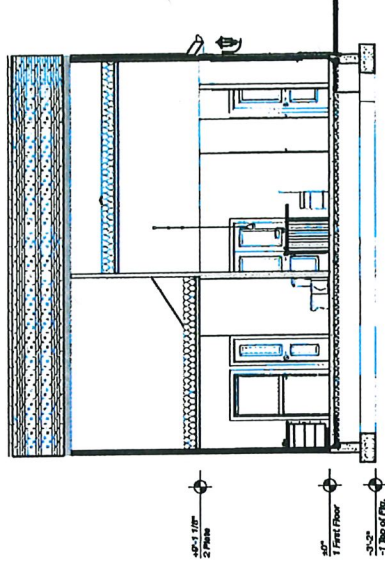


**CS2**

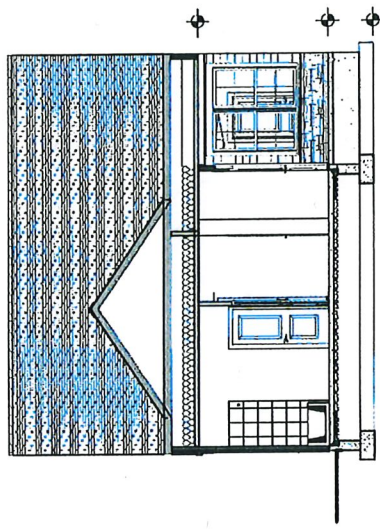
28'-End Unit • Cross Section-2  
 3/16" = 1'-0"



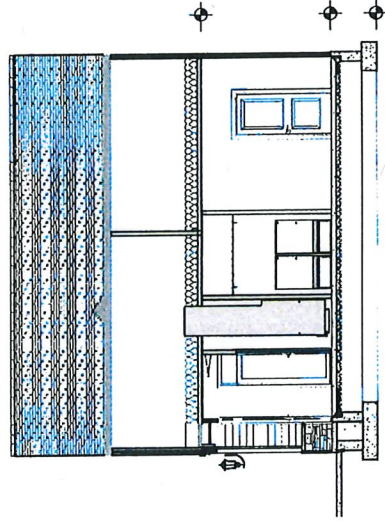
**BS3** 28'-Unit • Building Section-3  
1/8" = 1'-0"



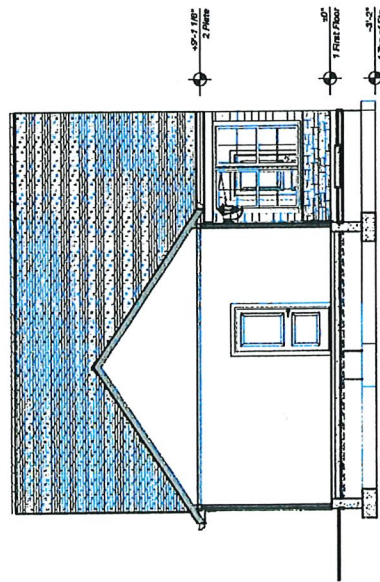
**BS3** 28'-End Unit • Building Section-3  
1/8" = 1'-0"



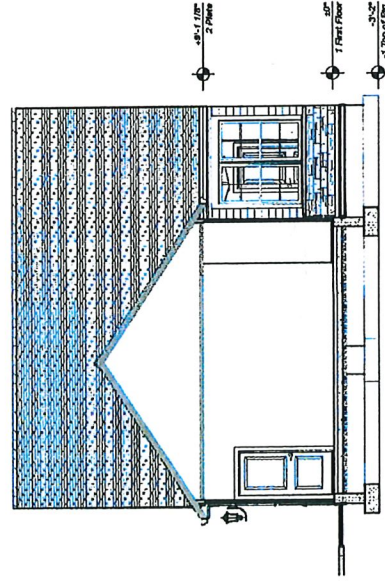
**BS2** 28'-Unit • Building Section-2  
1/8" = 1'-0"



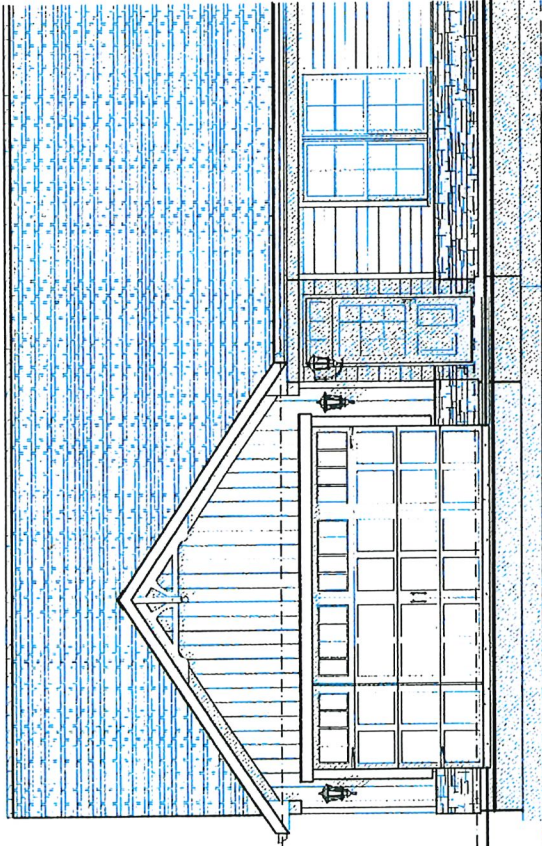
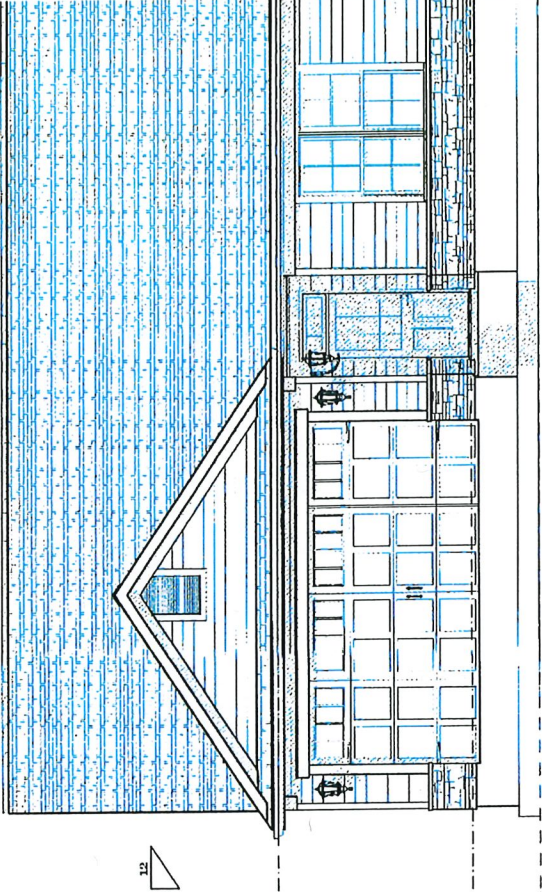
**BS2** 28'-End Unit • Building Section-2  
1/8" = 1'-0"



**BS1** 28'-Unit • Building Section-1  
1/8" = 1'-0"



**BS1** 28'-End Unit • Building Section-1  
1/8" = 1'-0"



Holbrook Ln • 38' Unit • Front Elevation-A  $\frac{3}{16}'' = 1'-0''$  E1

Holbrook Ln • 38' Unit • Front Elevation-B  $\frac{3}{16}'' = 1'-0''$  E1

ALL SMOKE ALARMS SHALL BE LISTED IN ACCORDANCE WITH UL-217, ON EACH LEVEL WITHIN EACH DWELLING UNIT. SMOKE AND IONIZATION TECHNOLOGIES SHALL BE INSTALLED. BEDROOMS TO USE IONIZATION TYPE. OUTSIDE BEDROOMS TO BE PHOTOVOLTAIC TYPE. SEPARATE OR DUAL-SENSING SMOKE ALARMS MAY BE USED

EXHAUST FAN & SMOKE DETECTOR NOTE:  
 (E) PEROTES EXHAUST FAN LOCATIONS (SEE NOTE S, D FOR CODE INFO ON SHEET A-5)  
 (S) PEROTES SMOKE DETECTOR LOCATIONS (SEE NOTE S, D FOR CODE INFO ON SHEET A-5)  
 (F) PEROTES SMOKE DETECTOR LOCATIONS (SEE NOTE S, D FOR CODE INFO ON SHEET A-5)  
 (P) PEROTES PROGRAMMABLE THERMOSTAT REZTD FOR ALL FORCED AIR FURNACES

WALL BRACING METHOD  
 PER RCO 602.10.4  
 GS-SPS: CONTINUOUS BRACING WITH 2x4 W/IN THK. CONNECTION REQUIREMENTS:  
 FASTENERS COMPLY WITH RCO 602.3(3)  
 FASTENED @ ON EDGES AND 12" IN FIELD.  
 INTERIE SHEATHING ON EXTERIE WALLS FASTENED IN ACCORDANCE WITH RCO TABLE 702.2.5

Note:  
 All Downspouts connected to approved locations

SHEET NO. 17 of 20  
**D.1**  
**CD**

**HBOO**  
**The Multi-Fam's**  
**Ri-A • G. Varies**  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLEMING, KENTUCKY 41042  
 BROOKSTONE HOMES  
 7203 WOOSTER FIRE • CINCINNATI, OH 45222 • 513-200-1500

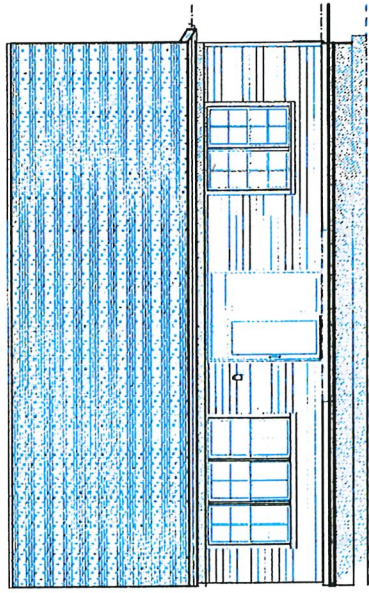
**B**  
**BROOKSTONE**

**00000**

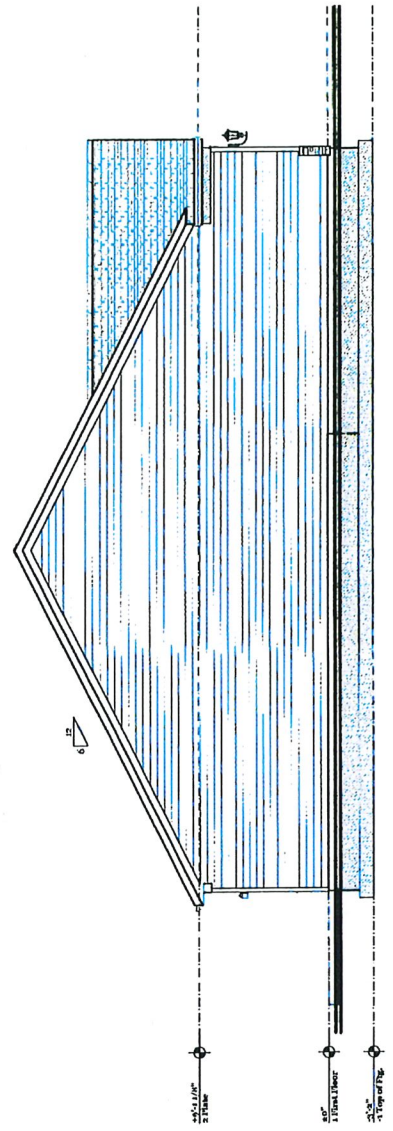
REVISONS:  
 06/07/24  
 06/07/24  
 06/07/24  
 06/07/24  
 06/07/24  
 06/07/24  
 06/07/24

Designed By: P. Fisher • (618) 393-0857  
 Email: pfisher@kfpland.com

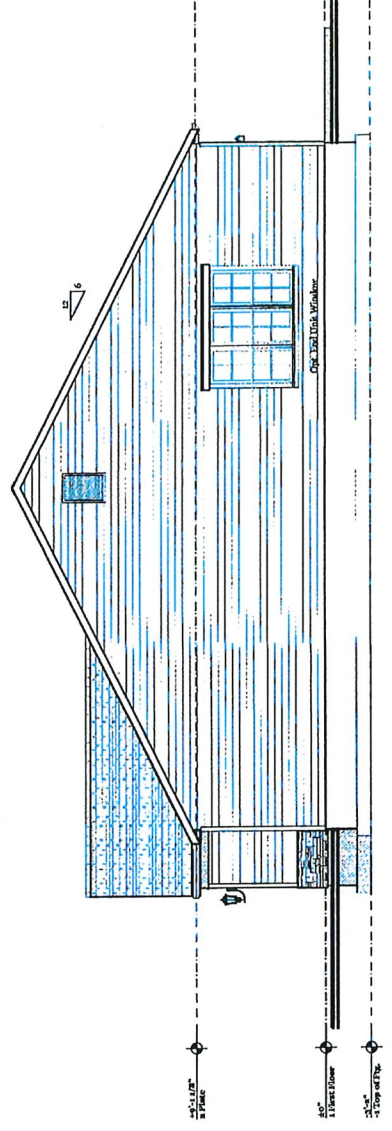
10351 Kenwood Rd. • Suite 2001 • Blue Ash, Ohio 45242  
**W/f/x**



**E2** Holbrook Ln • 38' Unit • Rear Elevation-A  
1/8" = 1'-0"



**E3** Holbrook Ln • 38' Unit • Left Elevation-A  
1/8" = 1'-0"



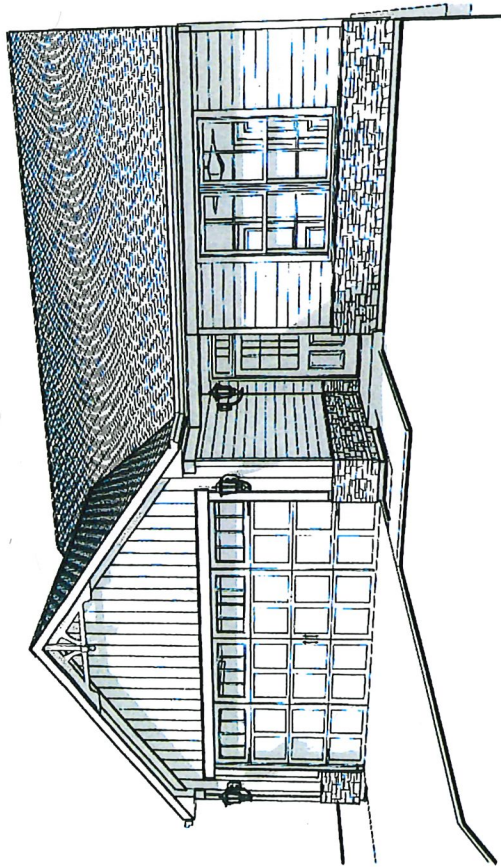
**E4** Holbrook Ln • 38' Unit • Right Elevation-A  
1/8" = 1'-0"

**HB00** The Multi-Fam's  
 E-A • G. Varies  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • HOMERIDGE, KENTUCKY 41042  
 BROOKSTONE HOMES, INC.  
 7203 WOOSTER PIKE • LINCOLN, OH 45227 • 513-200-1200

**B**  
 BROOKSTONE

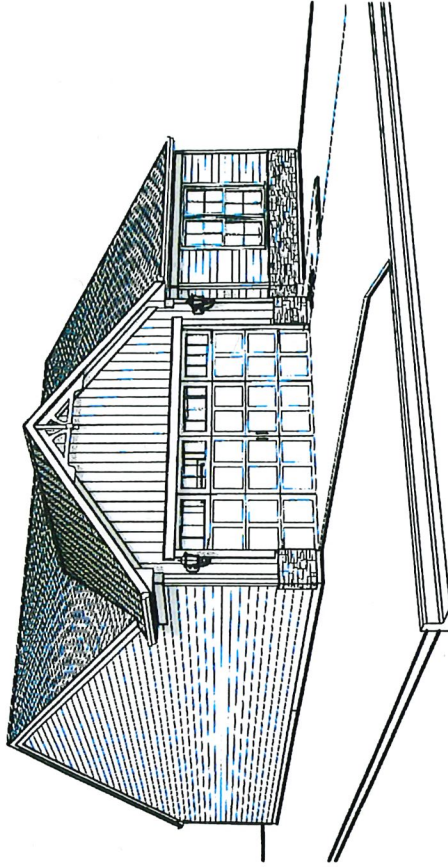
REVISIONS: 09/07/24  
 Job Request: 09/07/24  
 Design Team: 09/07/24  
 Designed By: P. M...@fisher.com  
 Email: P...@cloud.com  
 10951 Kenwood Rd. • Suite 2001 • Blue Ash, Ohio 45242  
 (913) 993-0837  
**Virtual f/x**  
**Vf/x**

CD  
 D.2  
 SHEET NO. 18 of 20



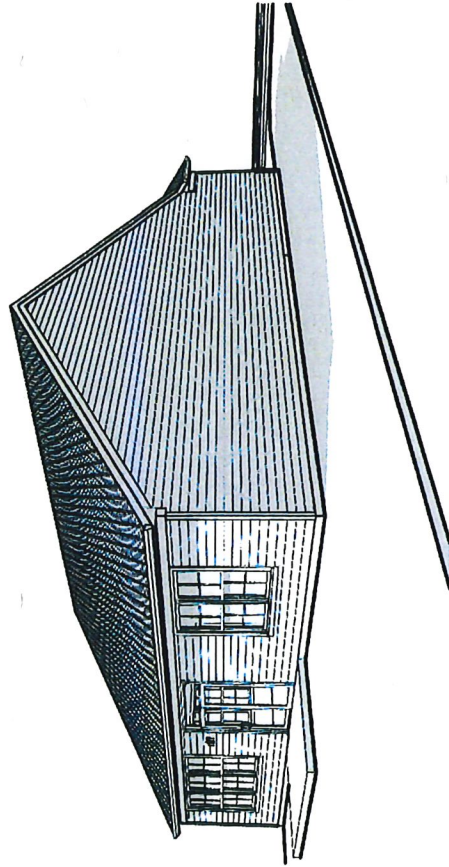
Vfx1

Holbrook 38' Ext • Vfx 3D Tour



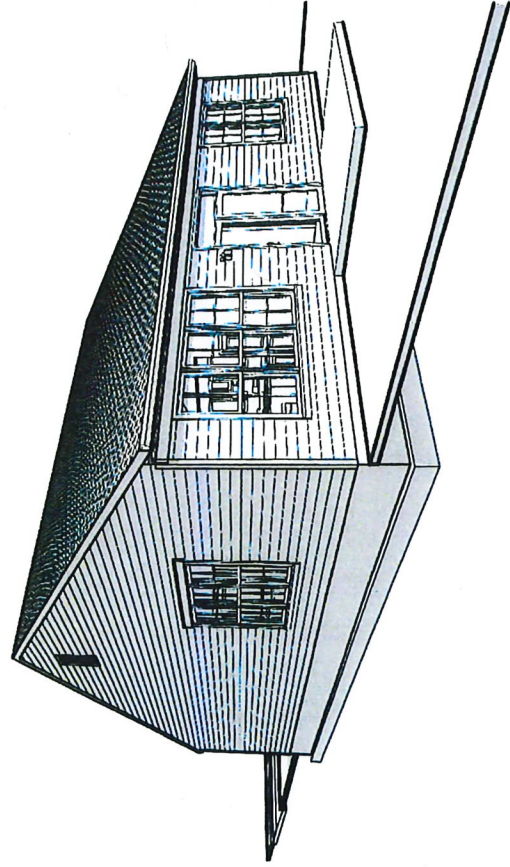
Vfx2

Holbrook 38' Ext • Vfx 3D Tour



Vfx3

Holbrook 38' Ext • Vfx 3D Tour



Vfx4

Holbrook 38' Ext • Vfx 3D Tour

**HBOO** The Multi-Farm's  
 Ft. Delaware Crossing, LLC  
 1841 HOLBROOK LANE • HOMER, KENTUCKY 41042  
 2024-0607

**BROOKSTONE**  
 L O R S

2024-0607  
 Job Request 06/07/24  
 Design Rev: 06/07/24

Vfx1  
 Vfx2  
 Vfx3  
 Vfx4

CD  
 D3  
 SHEET NO. 0148

Vfx Holbrook 38' Vertical Tour-A  
 2203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-700-1000  
**BROOKSTONE HOMES, LLC**

Email: info@cloud.com  
 Designed By: T. Fisher • (513) 393-0837  
 10851 Kenwood Rd. • Suite 2001 • Blue Ash, Ohio 45242

**Virtual f/x**  
**Vfx/x**

**HBOO The Multi-Fam's**  
 For: Delaware Crossing, LLC  
 1841 Holbrook Lane • Florence, Kentucky 41042  
 502-764-1100

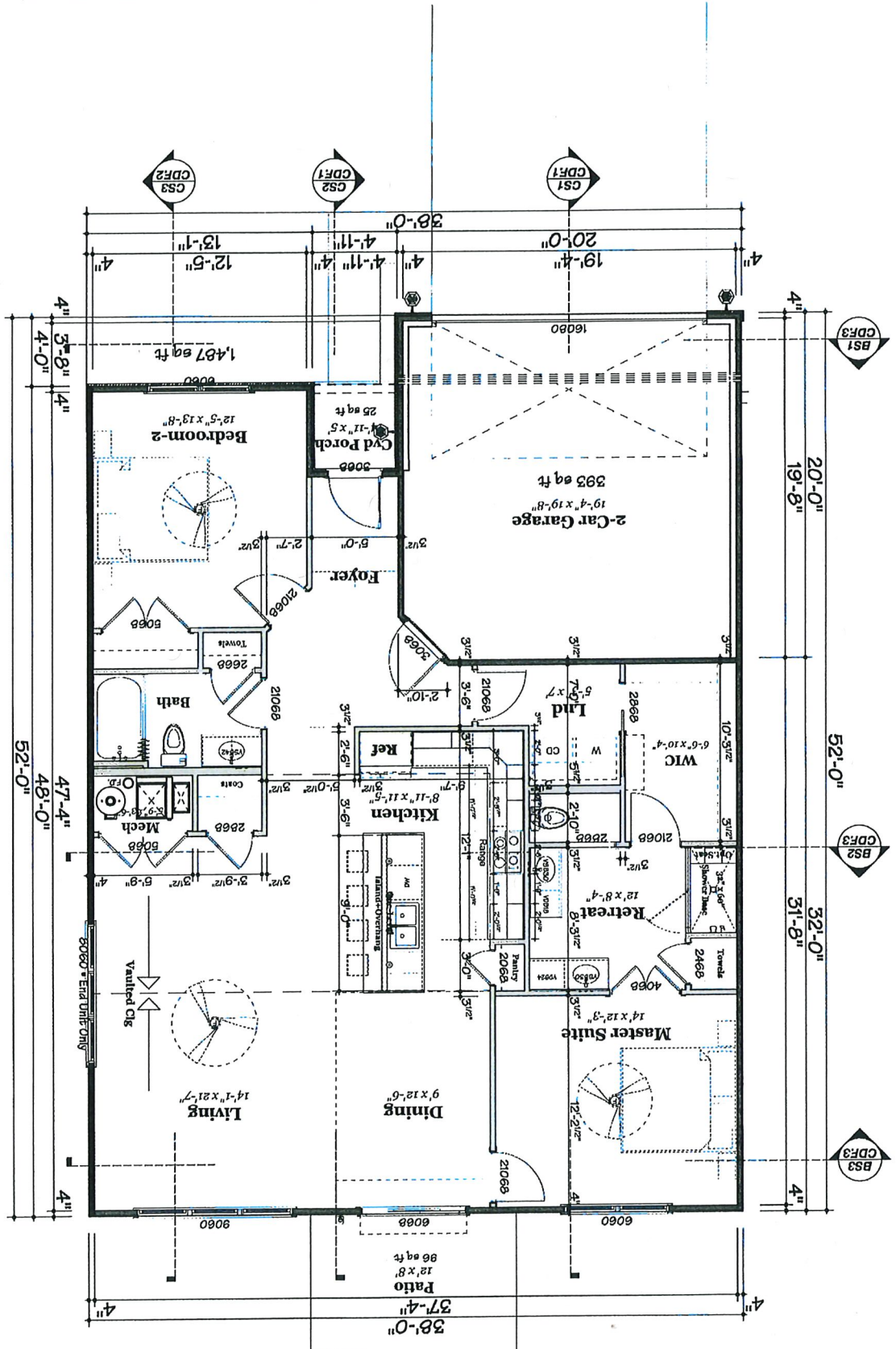
**BROOKSTONE**  
 HOMES

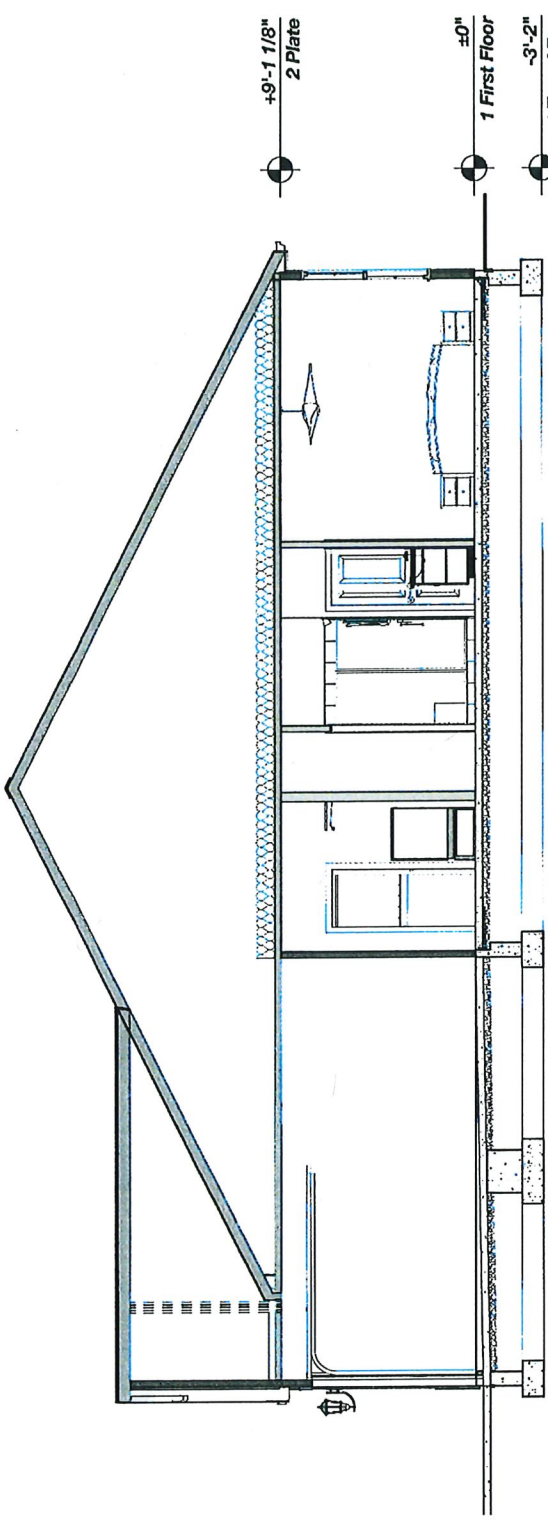
2203 WOODFIN FIRE • CINCINNATI, OH 45227 • 513-763-1900  
**BROOKSTONE Homes**  
 C. C.

6-Page Specifications  
**CD E.1**  
 Project Name  
 38' Unit - 1st Flr

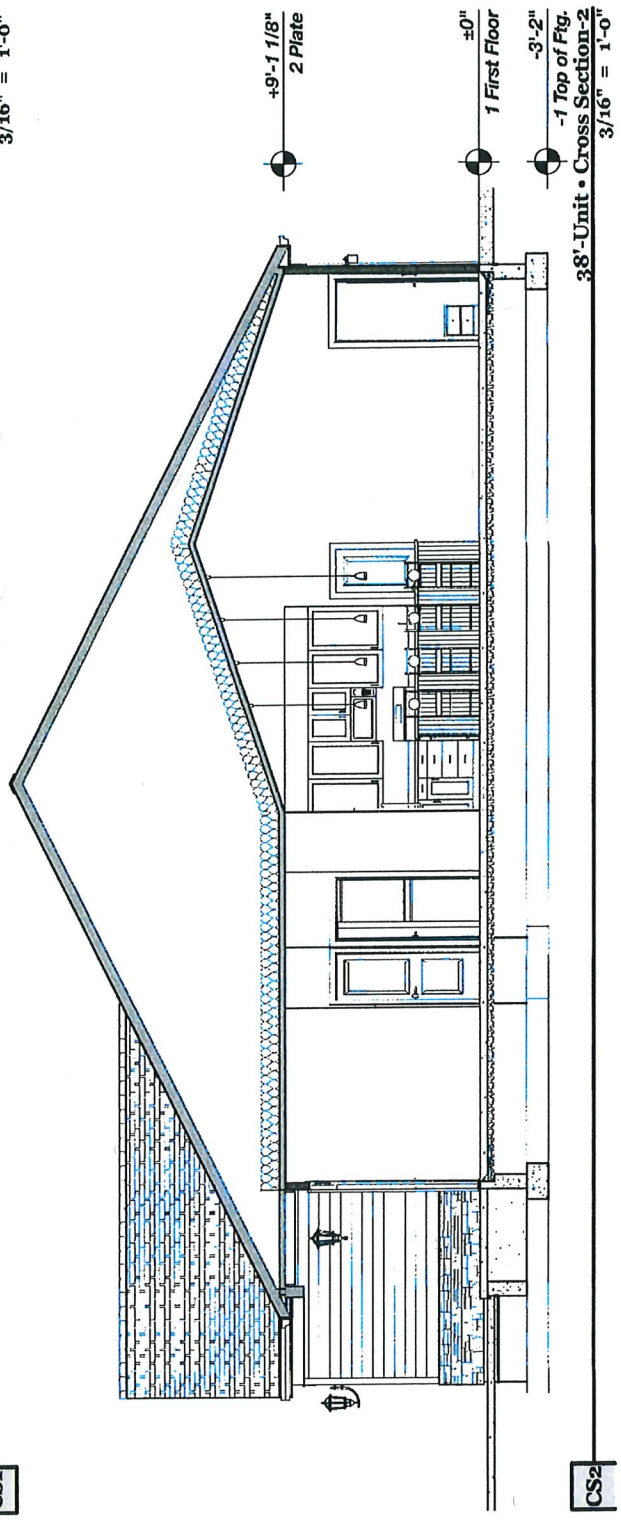
10551 Krowood Rd. • Suite 2001 • Blue Ash, Ohio 45212  
 Designed By: T. Miller • (513) 393-0852  
 Email: [tmiller@hbood.com](mailto:tmiller@hbood.com)  
**Virtual f/x**  
**Vf/x**

1. Holbrook Ln • 38' Unit • First Floor Plan  
 3/16" = 1'-0"





CS1



CS2

**W/f/x**

*W/f/x*

Designed By: P. Michael Fisher • (513) 393-0857  
 1051 Kenwood Rd. • Suite 2001 • Blue Ash, Ohio 45242  
 Email: [pf@wfx.com](mailto:pf@wfx.com)

REVISED: 06/07/24  
 03/08/2024  
 06/07/24  
 06/07/24

**BROOKSTONE**

**The Multi-Farm's**  
**EL-A • G. Varies**  
**FOR: Delaware Crossing, LLC**  
**1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042**  
**BROOKSTONE Homes, LLC**  
 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-201-0007

**CDF**  
 01  
 SUBJECT: 24-06

**HBDO** The Multi-Farm's  
 For: Delaware Crossing, LLC  
 1841 HORNHOOK LANE • FLORENCE, KENTUCKY 41042  
 2024-2025

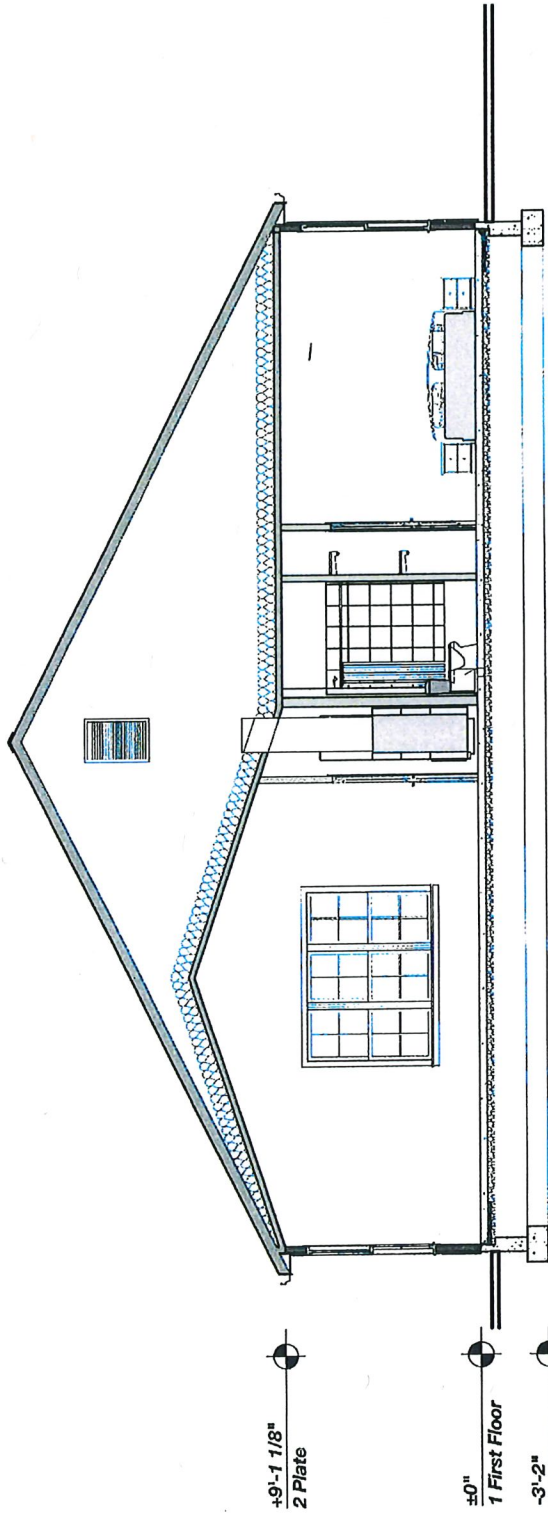
**BROOKSTONE**  
 ARCHITECTS

PROVISIONS:  Job Report  06/20/24  
 Design Dev.  08/20/24

10351 Kenwood Rd. • Suite 2001 • Blue Ash, Ohio 45212  
 Designed By: P. Mitchell  
 Email: [pjm@brookstone.com](mailto:pjm@brookstone.com)

**Vf/x**  
 Vertical f/x  
 registered professional engineer  
 (614) 933-0857

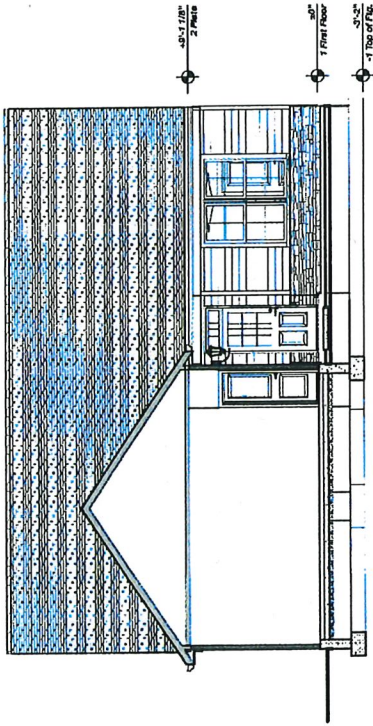
CDH  
 .2  
 SHEET NO. 26 of 30



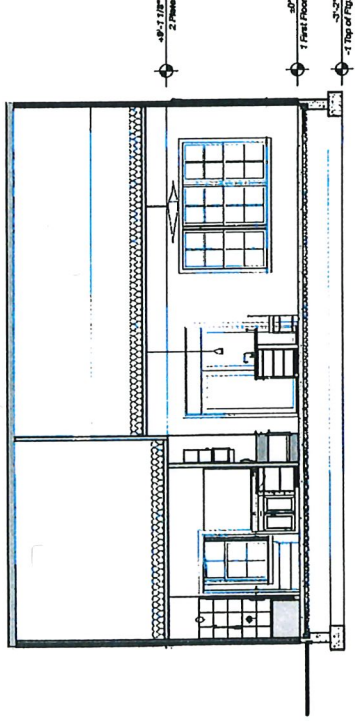
+9'-1 1/8"  
 2 Plate  
 ±0"  
 1 First Floor  
 -3'-2"  
 -1 Top of Fig.

**38'-Unit • Cross Section-3**  
 3/16" = 1'-0"

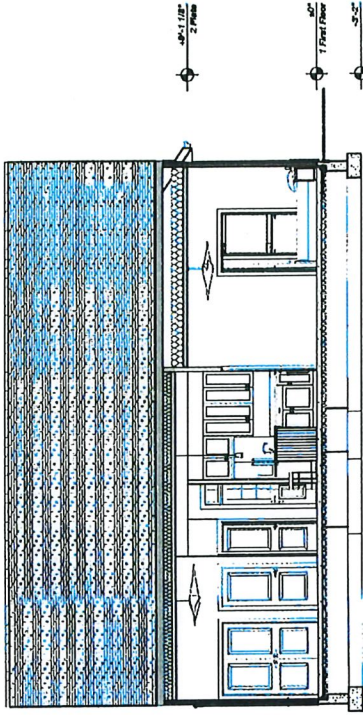
CS-3



BS1  
38'-Unit • Building Section-1  
1/8" = 1'-0"



BS2  
38'-Unit • Building Section-2  
1/8" = 1'-0"



BS3  
38'-Unit • Building Section-3

**HBRO** The Multi-Farm's  
 For: Delaware Crossing, LLC  
 1841 HOLMBROOK LANE • FARMINGTON, KENTUCKY 41042  
 7203 WOODSTOCK PIKE • CINCINNATI, OH 45227 • 513-961-0000  
**BROOKSTONE HOMES**

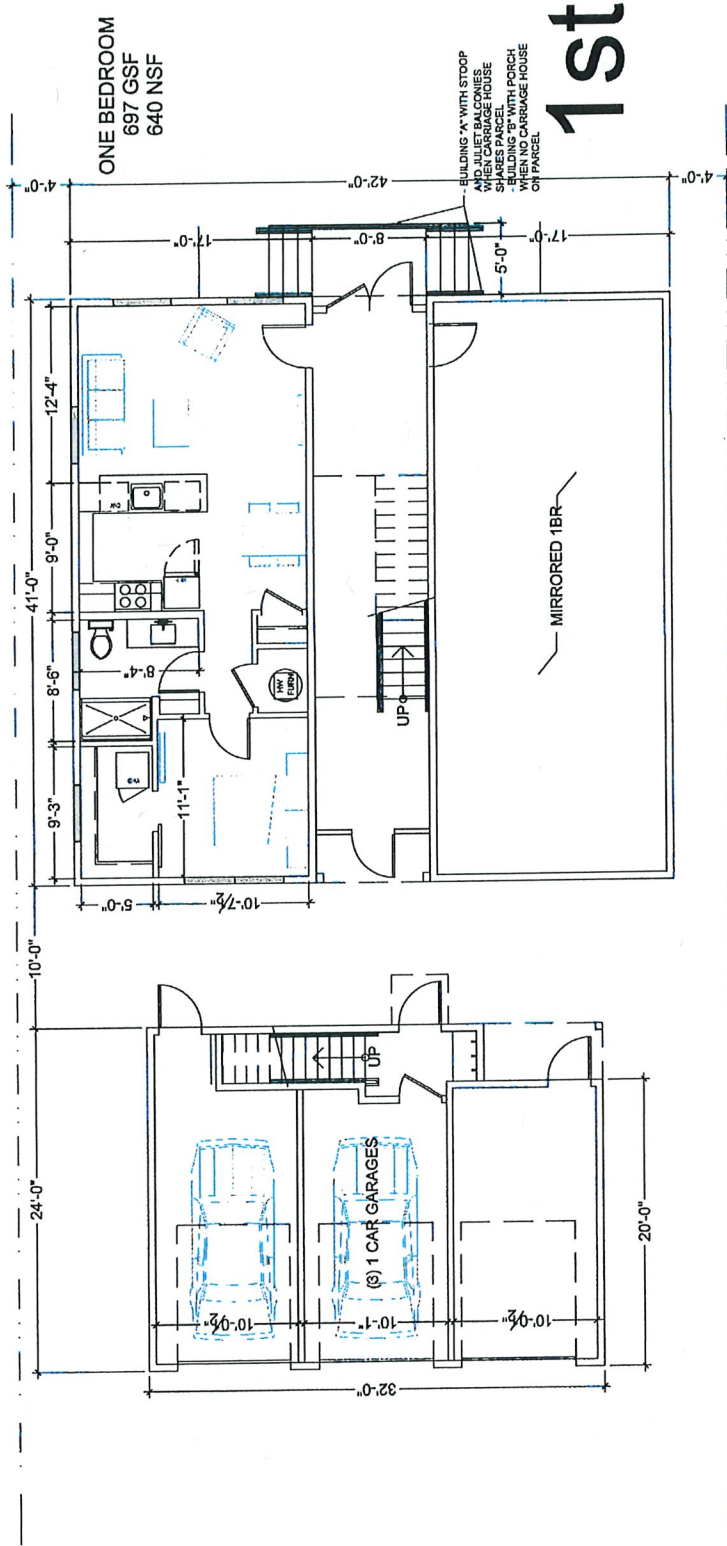
**BROOKSTONE**  
 2004 FORD  
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DESIGNED BY: 06/17/24  
 DATE: 06/17/24  
 PROJECT: 24-0000000-0000  
 DESIGN: 24-0000000-0000

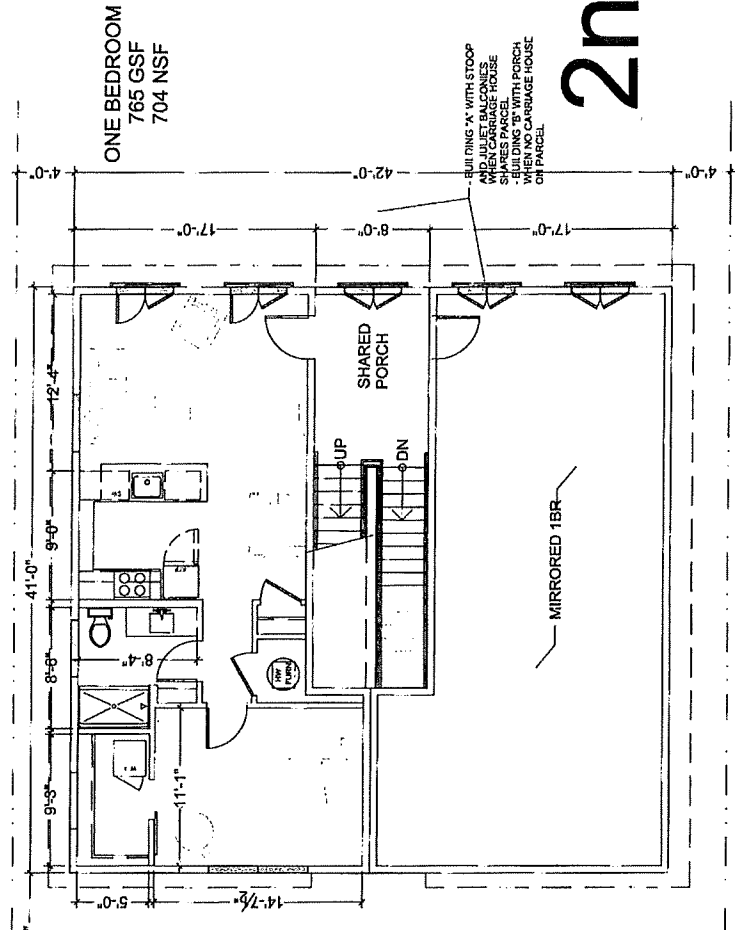
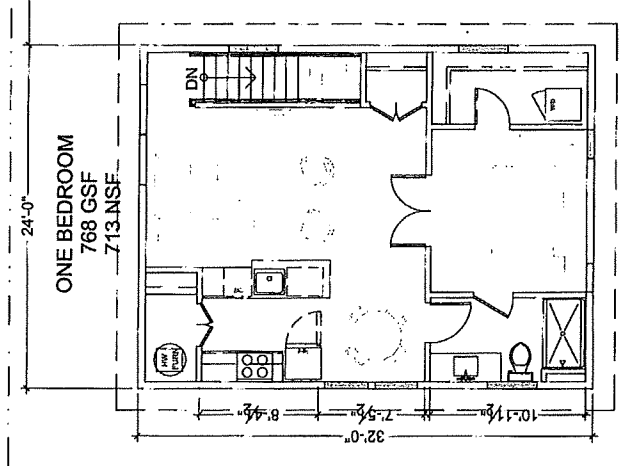
Email: [info@deland.com](mailto:info@deland.com)  
 Designed by: R. Miller • J. Fisher • (513) 398-0857  
 10551 Kenwood Rd. • Suite 2001 • Blue Ash, Ohio 45242  
**Wf/x**

**CDF**  
 BS1 • Building Section  
 SHEET NO. 2712/20

**POPLAR POINTE APARTMENTS  
SCHEMATIC DESIGN REVIEW SET**

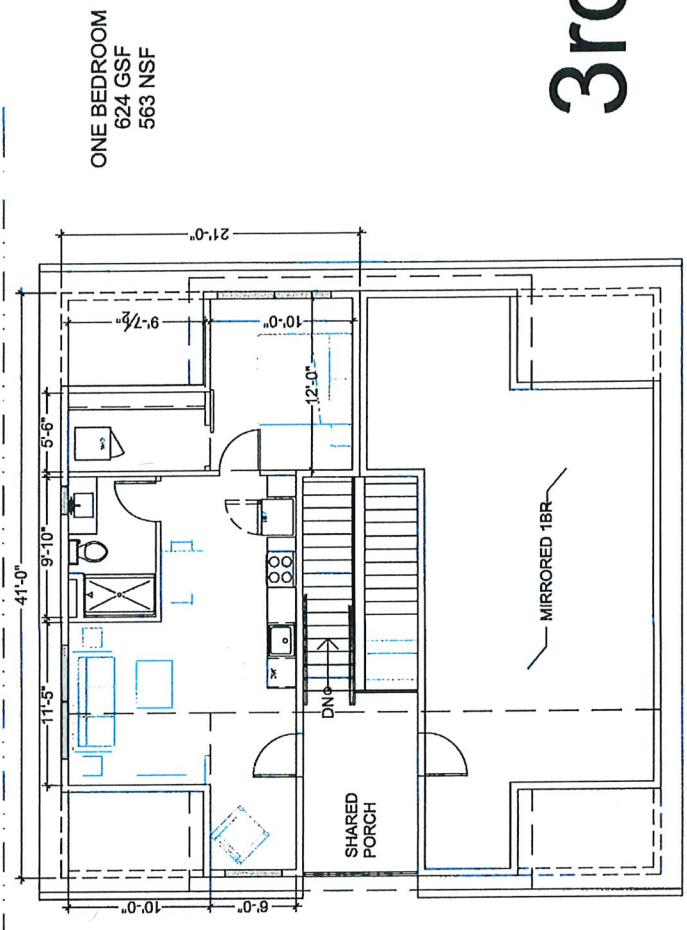


CARRIAGE HOUSE AND BUILDING A (BUILDING B SIMILAR)



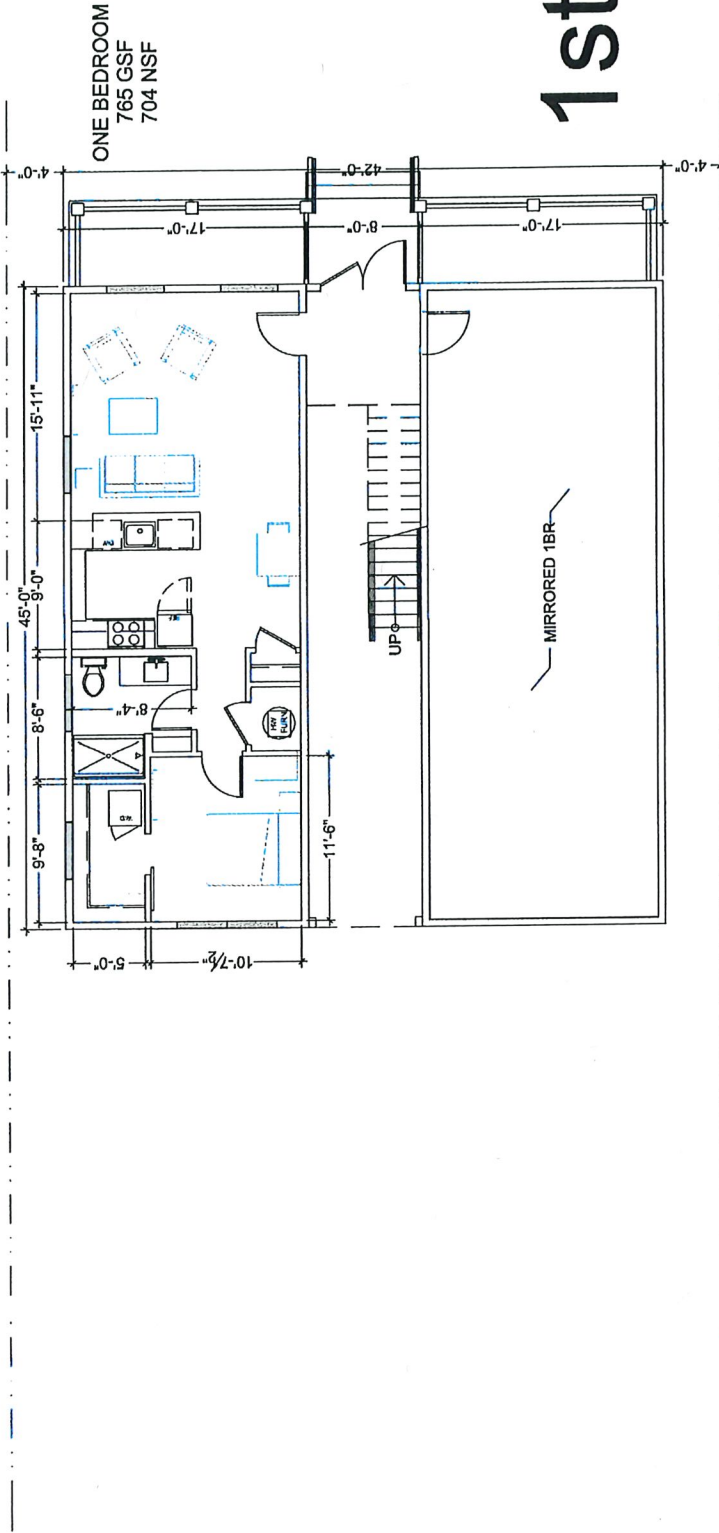
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CARRIAGE HOUSE AND BUILDING A (BUILDING B SIMILAR)



3rd

CARRIAGE HOUSE AND BUILDING A (BUILDING B SIMILAR)

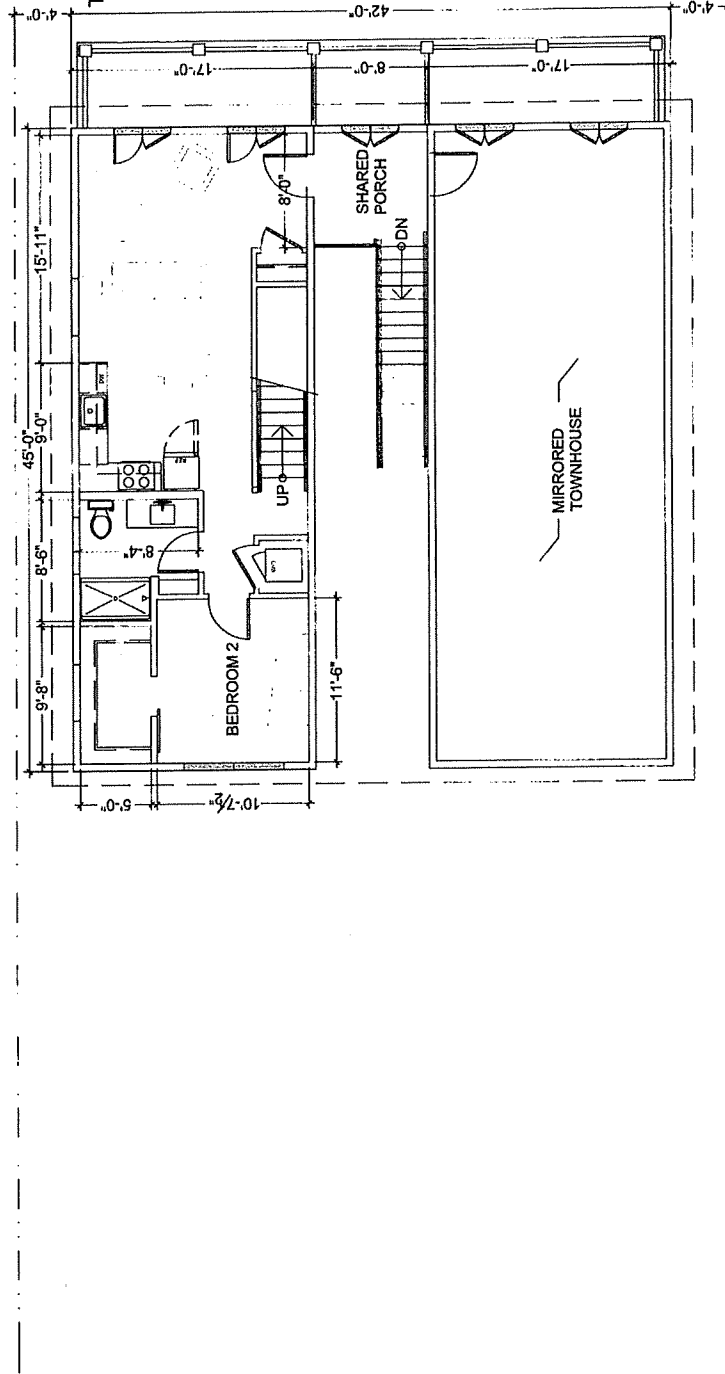


1st

BUILDING C



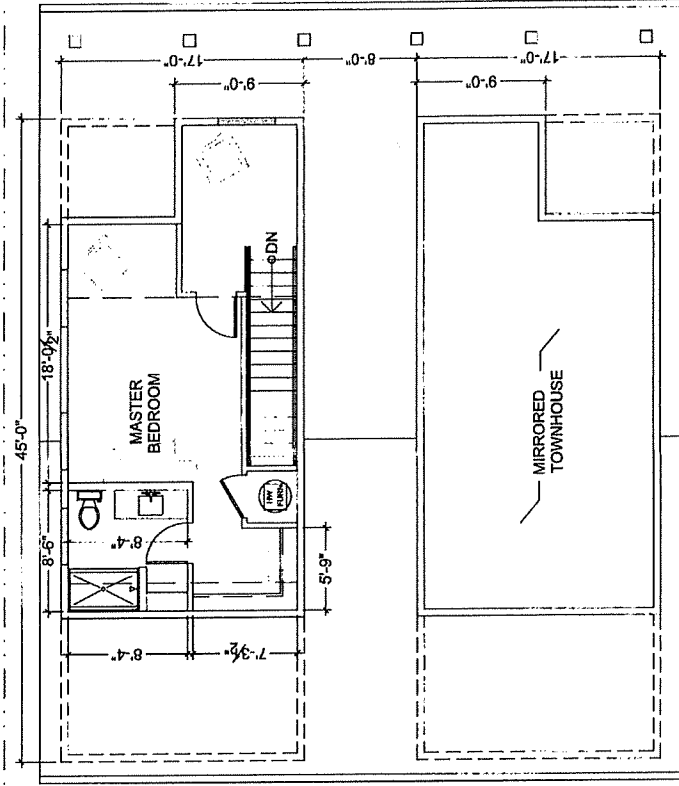
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1192 NSF



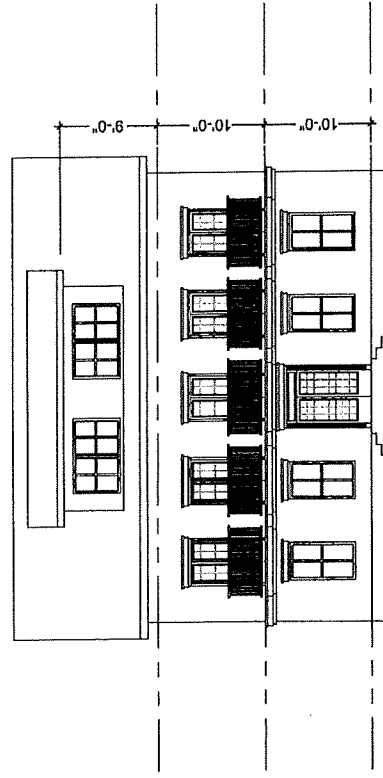
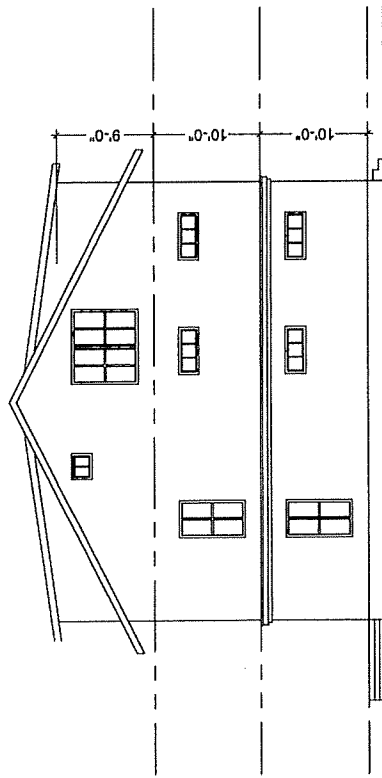
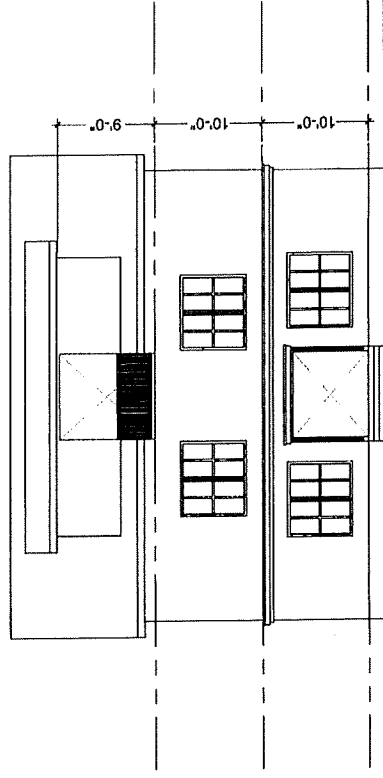
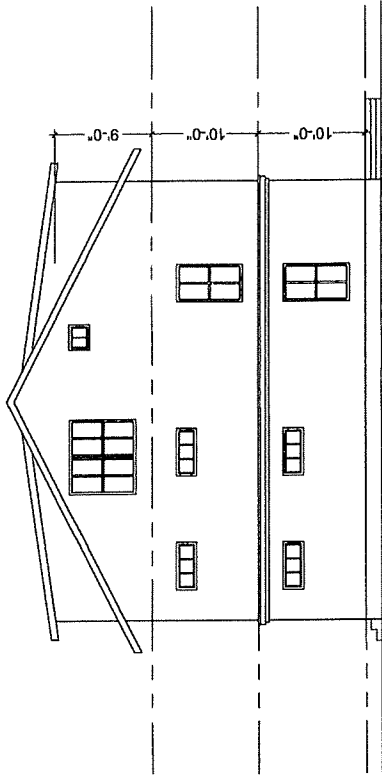
2nd

BUILDING C

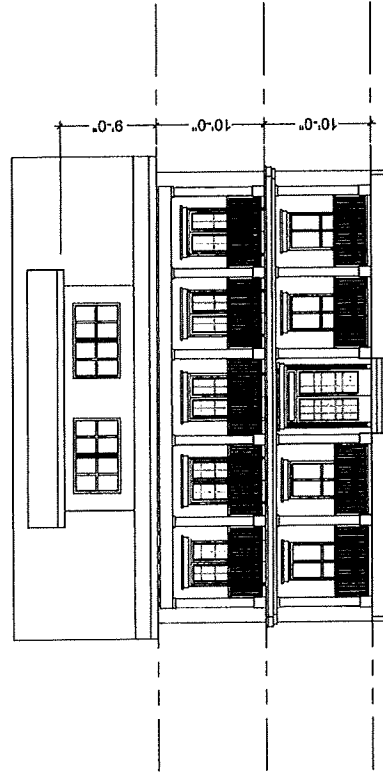
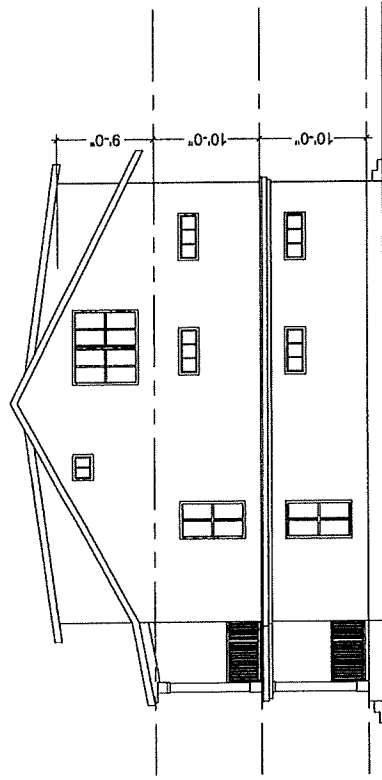
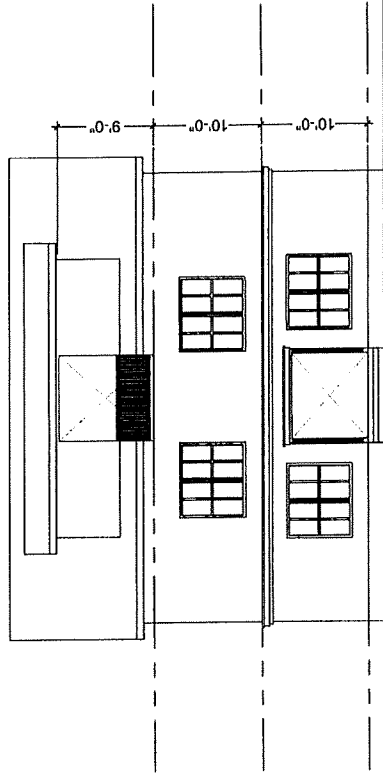
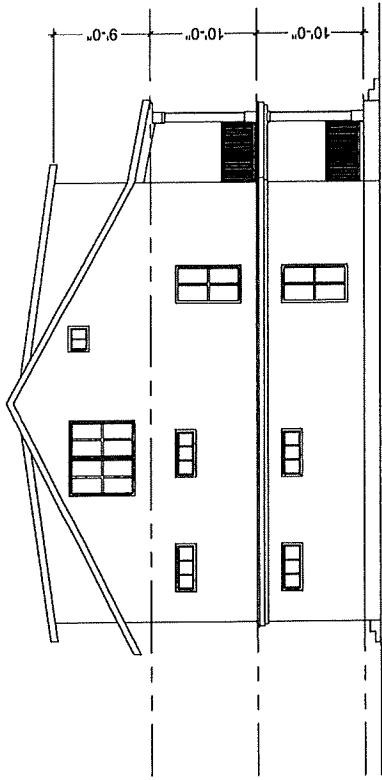
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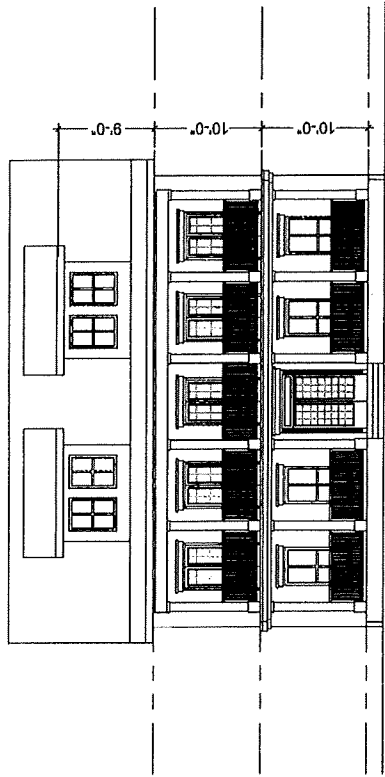
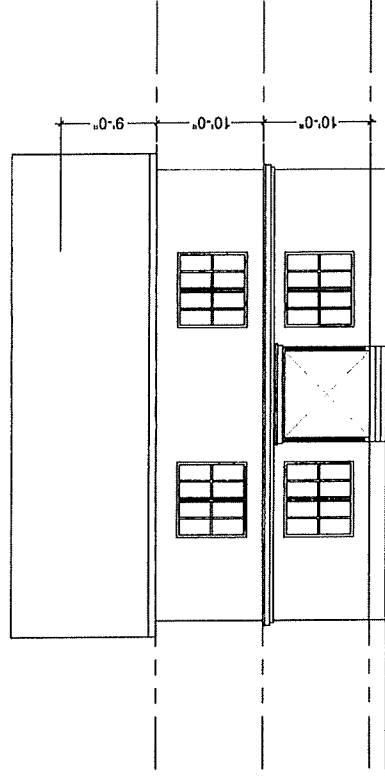
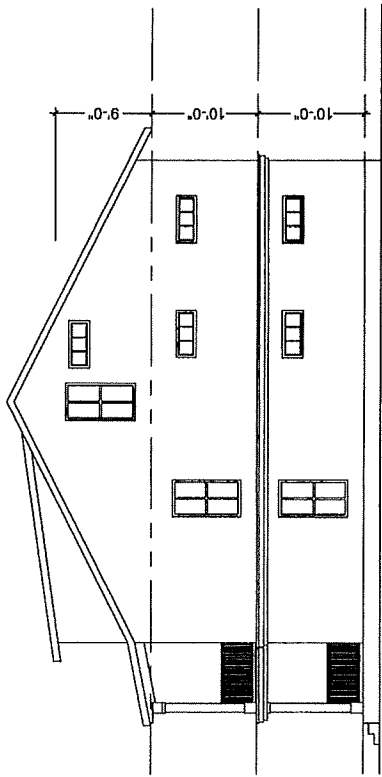
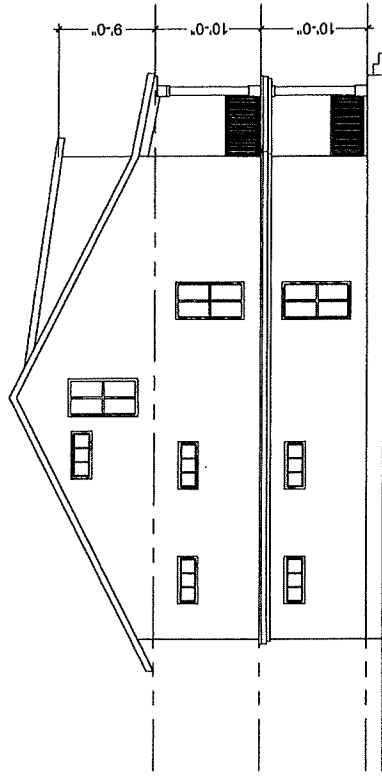
BUILDING C



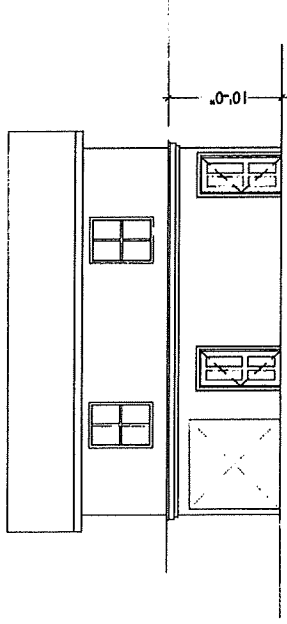
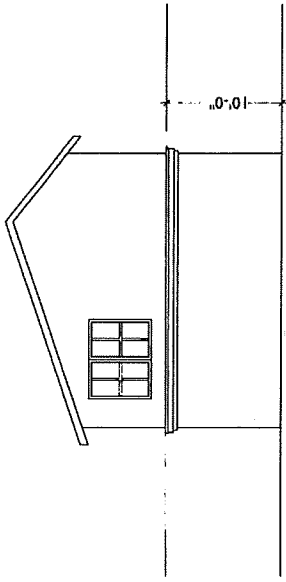
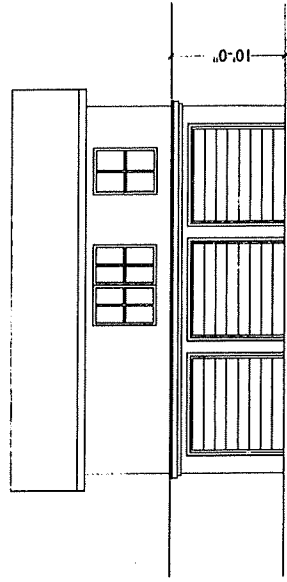
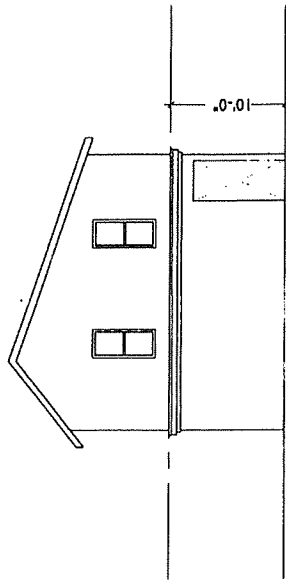
BUILDING A - ELEVATIONS



**BUILDING B - ELEVATIONS**



BUILDING C - ELEVATIONS



CARRIAGE HOUSE - ELEVATIONS

**EXHIBIT**

**“B”**

**ZONE CHANGE/CONCEPT PLAN COMMITTEE REPORT**

**TO:** Boone County Planning Commission

**FROM:** Corrin Gulick, Chair

**DATE:** October 16, 2024

**RE:** Request of **Charter Commercial, LLC (applicant)** for **Delaware Crossing, LLC (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

**REMARKS:**

1. We, the Committee Members were present at the Committee Meeting and voted on the above request or else were absent from voting. Further, based upon the vote, the Committee directs the Staff to draft the finding of fact to complete the Committee Report.
2. We, the Committee, recommend disapproval of the above referenced Zoning Map Amendment based on the following findings of fact:

**FINDINGS OF FACT:**

1. The proposed Zoning Map Amendment is not consistent with the 2040 Future Land Use Map of the comprehensive plan, Our Boone County Plan 2040.

The northern portion of the site is identified for Rural Density Residential, which is described as low density residential uses of up to one dwelling unit per acre. The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, would allow residential uses at a density of approximately 6 units per acre.

The southern portion of the site is identified for Suburban Density Residential, which is described as single-family housing of up to four units per acre. The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, would allow multi-family residential uses at a density of approximately 6 units per acre.

2. The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, is not consistent with the following Goals and Objectives contained in the comprehensive plan, Our Boone County Plan 2040:

- a. Land uses and zoning decisions shall strive to balance the rights of landowners with the rights of neighbors and the community (Overall Goal A, Objective 4).

Areas surrounding the site in question have been developed with detached single-family residential dwellings. The proposed development would introduce

attached single-family and multi-family residential dwelling units into the area, contrary to the established development pattern.

- b. Residential developments shall be evaluated for impact on infrastructure, environment, health and safety considerations, buffering, and design with secondary consideration given to dwelling unit type and density (Demographics Goal B, Objective 4).

Insufficient information has been submitted to determine what impact the proposed development will have on the existing street system and the 100-year floodplain of Gunpowder Creek.

- c. Mixed use, higher density neighborhoods shall be encouraged near existing or proposed transportation nodes and shall be designed to establish neighborhoods with a mix of retail, public, and recreational uses (Demographics Goal B, Objective 6).

The proposed development is not located within a reasonable walking distance of nearby commercial activity, which is located along Burlington Pike and US 42.

- d. Residential developments shall be encouraged to plan and build in a manner compatible with the general housing density and design planned for the area, with the existing conditions of the site, and significant site features (Demographics Goal B, Objective 6).

The proposed development will provide for a housing type and density which is not found within the vicinity of the site in question.

- e. Priority shall be given towards maintaining, protecting, and improving the capacity and safety of the existing road system across jurisdictions (Transportation Goal A, Objective 1).

Holbrook Lane, Watson Lane, and Lariat Way are local streets. The proposed development will generate additional traffic that the existing street system was not designed and built to accommodate.

- f. New land development shall be closely coordinated with the local transportation system to assure efficiency and continuity within Boone County and with the regional multi-modal transportation system (Transportation Goal B, Objective 9).

Insufficient information has been submitted to determine if the proposed development will have a negative impact on the capacity of the existing street system.

3. The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, is not consistent with the following Planned Development criteria, as outlined in Section 1506 of the zoning regulations:

- a. In general, Planned Developments shall have a pedestrian orientation, where it is possible to live, work, shop, and play in the same immediate vicinity without a required dependence on the automobile. This may be accomplished through the use of comprehensive pedestrian circulation networks including multipurpose paths and walks along main routes and open spaces such as stream corridors, between major destinations within the development and adjoining areas, secondary walk connections to the multi-purpose paths, the creation of a designed pedestrian environment including street trees in addition to other required landscaping, decorative street lights and other street furniture, and seating areas, and the use of integral curb walks where appropriate along streets. Disruptions in major paths due to street and drive intersections shall be minimal. Additionally in commercial areas, pedestrian orientation can be accomplished by placing buildings in close proximity to the street with parking areas to the side and rear of buildings, mixing uses within the same multi-story buildings, building entrances directly facing streets with reduced setbacks, architectural design which employs display windows, projecting signs, and awnings at street level, and designed outdoor seating and gathering spaces at the street level. Each development proposal must demonstrate in detail how the project will be made walkable throughout.

Except for sidewalks along the internal street system and a trail from Lariat Way to Gunpowder Creek, the proposed development does not provide for any innovative pedestrian movement systems.

- b. **Compatibility of Uses:** Measures shall be taken to assure compatibility of land uses within a Planned Development itself and adjacent sites. Such measures may include the provisions of buffer zones, common open space areas and landscape features, transitional land uses, or a mixed-use development in which no specific type of land use is dominate. When applicable, the design methods recommended in the "Development Layout, Lot Sizes, and Setbacks" section of the Comprehensive Plan's Land Use Element shall be employed. Compatibility measures/mitigation measures shall exceed the usual minimum standards of these regulations when needed to address impacts of the proposed development.

Shaker Run Subdivision has been developed at an intensity of 4.55 units per acre. The currently platted area of Valley Creek Farms Subdivision has been developed at an intensity of 3.85 units per acre. The proposed development, at a maximum intensity of 6.00 units per acre will be incompatible with the development of the adjacent neighborhoods.

- c. Landscaping: Substantial landscaping shall be provided in a Planned Development with emphasis given to street scape areas, buffer zones, and the provision of significant landscaping (in terms of size of landscape areas, and quantity and quality of landscape materials) within the developed portions of the site. The use of landscape design guidelines is required for multi-phased projects. The retention of existing healthy, substantial trees should occur wherever possible. Properly designed street tree plantings may be permitted to fulfill some landscaping requirements as part of an overall amenity package.

Insufficient information has been submitted to determine conformity with this criteria item.

- d. Transportation Connections and Entry Points: The provision of transportation connections (street connections, pedestrian paths, multi-purpose trails, sidewalks, and bicycle facilities) shall be provided in all planned development unless physically unfeasible or unsafe. This shall include connections to adjoining properties and developments, and inter-connectivity within the development itself, and contain minimal use of cul-de-sacs or other dead-end types of streets only when necessary. Transportation connections shall account for the County's adopted Transportation Plan and any adopted bikeway and/or pedestrian plans. In addition, the various entry points (streets, paths, etc.) into a Planned Development shall be marked or otherwise defined through the use of landscaping, low-key signage on retaining walls, architectural or sculptural elements, archways, markers, etc. Any structures used to demarcate entry points shall visually correlate to the planned architectural theme by the use of consistent design details, materials, and colors.

Holbrook Lane, if constructed between Oakbrook Road and Camp Ernst Road, would act as a collector street. However, Holbrook Lane does not extend to Camp Ernst Road. Additionally, there is no timeline to construct the extension. Therefore, as proposed, the development will have to use the existing local street system for access. Until Holbrook Lane is extended to Camp Ernst Road, development of the site in question for a higher density than what is recommended by the comprehensive plan is premature.

4. The existing SR-1 zoning of the site in question is appropriate and the proposed SR-1/PD zoning for the site is inappropriate.

The existing SR-1 district, which permits detached single-family residential dwellings at a maximum intensity of four (4) units per acre, is consistent with the 2040 Future Land Use Map of the comprehensive plan, Our Boone County Plan 2040.

The existing SR-1 district provides for a land use and intensity which is compatible with the developed areas adjacent to the site in question.

The proposed SR-1/PD district, along with the submitted Concept Development Plan, would allow the development of attached single-family and multi-family residential dwellings at a maximum intensity of six (6) units per acre, representing an increase of fifty (50) percent over the recommended intensity as outlined in the comprehensive plan.

Lariat Way and Cannondale Drive have been designed and built as local streets, which are defined as a type of street to serve less than one hundred (100) residential lots or units. The proposed SR-1/PD district, along with the submitted Concept Development Plan, would create a situation where these streets would serve more than one hundred (100) residential units.

5. On April 27, 2018, a Preliminary Plat was approved for the development of the Valley Creek Farms subdivision showing the development of 181 detached single-family residential lots. To date, seventy-eight (78) of those lots have been platted. The remaining one hundred three (103) lots are encompassed within the site in question.

It seems reasonable to assume that the owners of the existing lots, and those that are considering purchase of the existing lots, would have done their due diligence and found that the area to the north and west of the existing development was going to be an extension of the subdivision.

The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, significantly changes the character of what those existing/potential homeowners expected.

6. The Committee concluded that there have been no major changes of an economic, physical, or social nature within the area involved which were not anticipated in the adopted comprehensive plan and which have substantially altered the basic character of such area.

A copy of the Public Hearing minutes accompanies the findings and recommendation serving as a summary of the evidence and testimony presented by the proponents and opponents of this request. Attached is the signature page for the Zone Change/Concept Development Plan Committee Vote.

**ZONE CHANGE/CONCEPT PLAN COMMITTEE VOTE**

TO: Boone County Planning Commission

FROM: Corrin Gulick, Chairwoman

DATE: September 18, 2024

**REMARKS:**

We, the Committee Members were present at the Committee Meeting and voted on the above request or else were absent from voting. Further, based upon the vote, the Committee directs the Staff to draft the findings of fact and conditions if deemed necessary in order to complete the Committee Report.

**ZONING MAP AMENDMENT, Corrin Gulick, Chairwoman, Michael Schwartz, Staff**

4. Request of **Charter Commercial, LLC (applicant)** for **Delaware Crossing, LLC (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

~~LDC MULTIFAMILY/ROBINSON FOUNDATION~~  
SEPTEMBER 18, 2024 CHAPTER / DELAWARE  
CROSSING

Janet Kegley  
Janet Kegley

For Project \_\_\_ Absent \_\_\_  
Against Project  \_\_\_  
Abstain \_\_\_ Deferred \_\_\_

Corrin Gulick  
Corrin Gulick

For Project \_\_\_ Absent \_\_\_  
Against Project  \_\_\_  
Abstain \_\_\_ Deferred \_\_\_

Rick Lunnemann  
Rick Lunnemann (Chairman)

For Project \_\_\_ Absent \_\_\_  
Against Project  \_\_\_  
Abstain \_\_\_ Deferred \_\_\_

Kathy Clark (Alternate)  
Kathy Clark (Alternate)

For Project \_\_\_ Absent \_\_\_  
Against Project \_\_\_  
Abstain \_\_\_ Deferred \_\_\_

Steve Harper  
Steve Harper

For Project \_\_\_ Absent \_\_\_  
Against Project  \_\_\_  
Abstain \_\_\_ Deferred \_\_\_

Steve Turner (Alternate)  
Steve Turner (Alternate)

For Project \_\_\_ Absent \_\_\_  
Against Project \_\_\_  
Abstain \_\_\_ Deferred \_\_\_

David Hincks  
David Hincks

For Project \_\_\_ Absent \_\_\_  
Against Project  \_\_\_  
Abstain \_\_\_ Deferred \_\_\_

Jackie Steele (Alternate)  
Jackie Steele (Alternate)

For Project \_\_\_ Absent \_\_\_  
Against Project \_\_\_  
Abstain \_\_\_

TOTAL:    \_\_\_ - DEFERRED    \_\_\_ - FOR PROJECT    \_\_\_ - ABSENT  
              \_\_\_ 5 - AGAINST PROJECT    \_\_\_ - ABSTAIN

**COMMISSION MEMBERS PRESENT:**

Mr. Randy Bessler  
Mrs. Kathy Clark  
Mrs. Pamela Goetting  
Ms. Corrin Gulick, Vice Chairwoman  
Mr. Steve Harper, Temporary Presiding Officer  
Mr. Rick Lunnemann  
Mr. Charlie Rolfsen, Chairman  
Mr. Bob Schwenke  
Mr. Kenny Vaught

**COMMISSION MEMBERS NOT PRESENT:**

Mr. David Hincks  
Mrs. Janet Kegley  
Mr. Eric Richardson  
Mrs. Jackie Steele, Secretary/Treasurer  
Mr. Tom Szurlinski  
Mr. Steve Turner

**LEGAL COUNSEL PRESENT:**

Mr. Dale Wilson

**STAFF MEMBERS PRESENT:**

Mr. Kevin P. Costello, AICP, Executive Director  
Mr. Michael D. Schwartz, Director, Zoning Services

Chairman Rolfsen introduced the first item on the Agenda at 7:36 p.m.

**ZONING MAP AMENDMENT, Michael Schwartz, Staff**

1. Request of **Charter Commercial, LLC (applicant)** for **Delaware Crossing, LLC (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

Staff member, Michael Schwartz, referred to his PowerPoint presentation (see Staff Report). The request is to rezone the 44 acre site from SR-1 to SR-1/PD. The site has 2 distinct portions. The northern portion contains 19.97 acres. The southern portion contains 24.4 acres. Access to the site is from Holbrook Lane, Watson Lane and Lariat Way. Valley Creek Farms dates back to April, 2018 as the Planning Commission approved a Preliminary Plat. It had 97 single-family detached lots. The northern portion of the site is currently occupied by a single-story single-family residential dwelling with 3 accessory barn structures. The southern portion of the site is currently vacant and is heavily wooded. Gunpowder Creek is located along the eastern edge of the site. The property surrounding the site is zoned SR-1. Oakbrook Subdivision is zoned SR-1/PD. Pages 1-5 of the Staff Report identify the applicable regulations to the request. Topographically, a valley bisects the site with slopes having a grade change of 70 feet. The eastern edge of the site is located within the 100 year floodplain of Gunpowder Creek. The 2040 Future Land Use Map designates the site for Rural Density Residential (RD) and Suburban Density Residential (SD). Due to the hillsides, a portion of the site is designated as Developmentally Sensitive (DS). Holbrook Lane is a County maintained local street with no sidewalks. Watson Lane is a County maintained local street with sidewalks on both sides of the street. Lariat Way is also a County maintained local street with sidewalks being installed with lot development. Pages 6-11 include the portions of the Comprehensive Plan that are relevant. The western portion of the street is located within a 600 foot wide buffer of an underground pipeline. Mr. Schwartz showed photographs of the site and adjoining properties.

The submitted Concept Development Plan is divided into 2 parts. The northern part is planned to have 139 attached single-family residential dwellings in 24 single-story buildings. The southern portion will have 157 multi-family residential dwellings in 24, 3 story buildings and 13, 2 story garages. There will be a dwelling unit on top of each garage. Public streets will be built in the northern portion of the development. Street connections will be made via Watson Lane, Holbrook Lane and Lariat Way. A private drive will serve the multi-family area in the south portion of the site. There will be stormwater retention areas, amenity space and sidewalks. There will be a walking trail from Lariat Way to Gunpowder Creek. Public utilities will be provided. The developer has provided some building elevations and floor plans. Mr. Schwartz noted that he offered a summary of building intensity of the project as well as neighboring developments on Page 11 of the Staff Report. The applicant has provided a project narrative, a traffic assessment (peak hour traffic generation and trip distribution but no intersection analyses or capacity analysis). Staff has reviewed the submitted plans against the zoning and subdivision regulations. Those findings can be found on Pages 12-14 of the Staff Report. Most notably, the maximum intensity of an SR-1/PD district is 6 units per acre and the applicant is proposing 6.67 units per acre. Comments from outside agencies can be found on Pages 14-15 of the Staff Report. Staff concerns about the project are listed on Pages 15-16 of the Staff Report. It includes a list of requirements that are being requested to be waived or modified using the PD regulations. Staff has found that there is a discrepancy between the number of units listed in the project narrative and the number of units shown on the submitted plans. Staff feels that there needs to be more amenities given the size of the development. Staff also feels that there should be a discussion of the extension of Holbrook Lane to Camp Ernst Road. The Plan only shows the applicant extending it just past the

applicant's entrance. The applicant has also requested some off-site signage for the proposed development. Staff has offered some suggestions.

Chairman Rolfsen asked if the applicant was present and wanted to proceed with their presentation?

Mr. Pat Manger, Loveland, Ohio, represents the applicant and stated that the northern part of the site will be single-story patio homes (2 bedrooms). The southern parcel will include 1 bedroom multi-story townhome buildings (2 and 3 stories high). He offered to comply with Concerns #2, #3, #4, #5 and #6. He stated that he won't exceed the 12% grade for streets. In terms of amenities, Mr. Manger noted that he is working with the County to connect their trail to Oakbrook Park. They are attempting to preserve as much green space as possible. He is working with the County to connect Oakbrook Road to KY 237 via Holbrook Lane. They would be responsible for their portion of Holbrook Lane as it affects their property. He is talking with the Valley Creek Farms HOA about the proposed sign and is in the process of working it out with the HOA. They are agreeable to extend Holbrook Lane to their western property line. They are in agreement with sidewalks. They are willing to comply with Item #5 and connect the southern area to the walking trail. In conclusion, they want to build one and two bedroom units in order to meet the demand as stated in the NKADD Study. There is a housing shortage. There is a need for 6,000 homes (2,000 units for rental) in the next 5 years.

Chairman Rolfsen asked if anyone in the audience would like to speak in favor or against the request or had any questions?

Mr. Michael Koseruba, 1959 Cliffview Lane, stated that no one from his neighborhood was notified. The proposal doesn't add up. There is a bonus in which 266 units becomes 296 units because of the PD. Don't be fooled. It is an apartment unit bomb in the middle of single-family residences and semi-rural residences. The roads, parks and schools can't take it. We are looking at a piece of property that doesn't have a road to it. There is no traffic coming out of a vacant piece of land. What about the traffic elsewhere in the area? Traffic will come from the west and the south. We are opposed to the project as we prefer single-family housing. Don't let the developer touch Gunpowder Creek. Oakbrook Park is not large enough for the project.

Mr. Charlie Schneckenberger, 6516 Cannondale Lane, read his comments for the record (see Exhibit A). He stated that the zone change request should be denied. The applicant is requesting a 67% increase above the SR-1 zone (4 units per acre). The applicant didn't even follow the requirements under the PD. Over 60% of the land in the subdivision is being rezoned including the open space. If approved, the current subdivision will be reduced to 18 acres. The 89 residential lots will have a density of 5 units per acre. The zone change will adversely affect his property density and will violate his SR-1 zoning.

Mr. Schwartz showed the current and proposed density of the subdivision and project using a map for illustrative purposes.

Mr. Mike Parker, realtor, stated that the NKADD study was wrong. There is not a sufficient amount of housing inventory in Northern Kentucky. They are 60% short. There is no need for one bedroom apartments. The project will impact the schools and roads. It will change the character of the area. It will affect the quality of life. There is a lack of community support. The Board's ultimate decision on the project will tell Boone County residents whether you believe in single-family and home ownership or whether you believe in putting as many units as possible. If this change occurs, there will be other developers that will come forward to change single-family to multi-family. In this week of 2018, there were 322 active homes for sale for an average of \$247,000. In a normal market, it is 700 homes in Boone County. In 2021, there were 82 homes active with an average sales price of \$323,000. This week there are 181 active homes with an average sales price of \$393,000. Don't change the SR-1.

Chairman Rolfsen asked the audience how many people think traffic is the issue? Over 100 residents raised their hand.

Ms. Stacy Blevins, 2968 Lariat Way, stated that her house is in front of the zone change sign. She just built a half million dollar home. She thought it would be a quiet neighborhood and not full of apartments. She doesn't feel good about having her kids play in the area. Her street is a hill. It will be the main access for the new residents. It won't be safe to cross the street with 200 more residents. If the project goes, they will sell their home.

Mr. Jeff Schaefer, 6501 Cannondale Drive, stated that he was the first person to build in Valley Creek Farms Subdivision. He signed a contract with Brookstone Homes. The builder has neglected the neighborhood for the last 4 years. The lots are in terrible shape. The reason the subdivision has not sold out in the last 7.5 years is due to Brookstone Homes. The apartments will trash their home values. There is only one way in and out. It is through Lariat Way. There are no stop signs in the subdivision. They don't have complete sidewalks after 7.5 years. Drees and Fischer Home Subdivisions have them. If the developer would have sold the lots, they would not need the apartments. There was no model home or salesperson. Brookstone has gotten out of the single-family development. Maronda Homes has bought the remaining lots. The applicant's traffic study showed 2,000 trips per day in and out of the subdivision and no stop signs. The number of units proposed will put stress on the schools. They are overwhelmed both in the school and on the buses. The original subdivision only had 250 people. With the proposed change, it will increase to 734 people. The infrastructure in place now only accommodates 250 people. There is only one way in and out of the subdivision for emergency response. Are there plenty of apartment complexes planned for the area? Chairman Rolfsen replied yes. There was an approved project north of the site on KY 237 for 324 apartments. Why put apartments in the middle of single-family? Just build the 324 units already approved. Chairman Rolfsen stated that project may not be built if the traffic signal is not built. Mr. Schaefer stated that he bought his house because he thought the subdivision was only for single-family homes.

Mr. Bob Lussmyer, 6520 Cannondale Drive, stated it is tough to turn left from Cannondale Drive between 7:00 and 9:00 a.m. and 4:00 and 6:00 p.m. There is too much traffic to make a left turn without a signal. It is dangerous. The grade for Lariat Way is very steep. The traffic study is extremely misplaced. There are a lot of people coming up Camp Ernst and Pleasant Valley. Their subdivision will be a great short cut. In addition, there is a feeder creek that runs to Gunpowder Creek. Will it be impacted? It is supposed to be a green area.

Mr. Dennis Stock, 6505 Cannondale Drive, stated that the roads in the subdivision don't have the final pavement. They still have the construction grade. They have been that way for 8 years. Mr. Costello explained the bond process for street completion. It is up to the County to cash in the bond to complete the paving work if the developer can't. If a traffic signal is installed on KY 237 and Cannondale Drive, it may spoil the flow of traffic at the roundabout. The current zoning of SR-1 is still appropriate. The apartments are inappropriate. The single-family homes are okay for the area. They just have to be marketed and built. The economic value is still valid. Maronda Homes will do a better job marketing.

Ms. Tangerine Pike, stated that she owns the property on the other side of Holbrook Lane next to the subject site. She explained that her property rights will be significantly harmed because of the apartments (rental property). It is a revolving door of residents. They don't care.

Ms. Danielly Bruseth, 6589 Watson Lane, stated that her concern is privacy. There is also a blind curve.

Chairman Rolfsen inquired about connectivity. Mr. Schwartz replied that there was a stub at Watson Lane and Trevino Lane.

Mr. Shawn Gross, 6505 Watson Lane, stated that he moved from Price Hill. He moved because of density and transients. All the streets in Shaker Run Subdivision that connect to Holbrook Lane are at a decline. He is concerned about speed from additional traffic using Watson Lane to get to Holbrook Lane. There are a lot of kids in the neighborhood. Parking is on one side of the street.

Ms. Susan Schwendemann, 1911 Cliffview Lane, stated that the building design looks cheap and like tenement housing. Who needs one bedroom housing?

Mr. Russell Alexander, 1629 Hogan Lane, stated that he has been in the pest control business for 25 years. One bedroom means 5 people and drug dealers. It is hard to stop on Holbrook Lane due to its slope. He is concerned about safety and property values.

Mr. Jeff Rice, 1805 Promontory Drive, asked how much will the apartments be rented for each month? We need rent control based on income. That is why people are bundling up in units. It should be affordable. What about SD1? Can sanitary sewer service be provided? Chairman Rolfsen asked if the Staff received a letter from SD1? Mr. Schwartz replied no. He asked if the County had a plan for future development?

Mr. Costello replied that there is a plan in place currently. The plan is being updated. He explained the process and the areas for growth and preservation. He advised Mr. Rice and others to participate in the process and visit the Planning Commission's website for more information. Mr. Rice inquired about the new stormwater from the project and its affect on Gunpowder Creek. Chairman Rolfsen explained the new requirements of stormwater management and requirements for development. Water has to be retained on-site for a period of time and then released slowly and naturally from the site.

Mr. Brendon Aragon, 6500 Cannondale Drive, stated that he spoke to SD1 officials and they offered some of the site had erosion control measures in place and some have not been put in place because it hasn't been reviewed yet. Chairman Rolfsen explained that it would be the next step unless the project is not approved.

Mr. Koseruba asked why can't the Board or Staff not accept the application if it is a bad project or plan? Mr. Dale Wilson explained that KRS Chapter 100 allows any property owner to apply for a zone change for their own property. That individual has a right to apply. The denial of that right is a denial of due process.

Ms. Julie Haley, Cliffs section of Oakbrook stated that Oakbrook Road needs repaving. The County should take care of existing roads first before building new ones. The patio homes look the same. It will deeply lower the value of their homes.

Ms. Veronica Huffman, 6647 Sarazen Court, stated that she bought her home 19 years ago. The school buses are overcrowded and late. People run the stop signs in her subdivision. Would you want this type of development in your backyard? Stephens Elementary School is overpopulated. There is no lighting in Oakbrook Subdivision.

Ms. Judy Forbes, 6604 Trapside, asked if the Board thought about the American dream or owning a home. Rental is not the American dream. We need more homes in the County.

Ms. Cassie Ames, 6509 Cannondale Drive, explained the double bus run procedure. She asked who makes sure the stormwater is directed to the pond because the developer doesn't have a good track record with water. Lighting is an issue on Lariat Way. It should be added. Chairman Rolfsen explained that the Planning Commission reviews the stormwater plans to make sure it is designed properly and built.

Mr. T.J. Roberts, 8062 East Bend Road, stated that Boone County is ripe for home ownership. America is losing homeowners. The Board must keep the single-family home alive as well as home ownership.

Mr. Costello noted that in the early 2000s, Boone County was generating about 1,500 single-family homes per year. In the last five years that number has been reduced to 500 homes per year. Part of the problem is the fact that we only have 3 main home builders. There needs to be

more builders. The small homebuilders left the market in 2008 with the recession. No one is building starter homes. There are over 4,000 lots throughout the County where homes can be built. It makes sense to have a good supply of homes at various income levels. The Planning Commission has a strong history of supporting single-family residential development.

Mr. Steve Rawlings, State Representative, District #66, noted that he lives in Burlington. When he was campaigning in Florence, the biggest issue was in rental homes. They are owned by corporations that are buying up these properties and renting them out. Please maintain the integrity of Boone County and he asked the Board to respectfully deny the rule change and keep the zoning the way it is now.

Mr. Michael Ames, 6509 Cannondale Drive, stated that he informed the neighborhood about tonight's Public Hearing. The builder for the proposed project is the same builder of the subdivision. For 7 long years, he only built out half of it. He mismanaged it. Why would you let him continue? Is Fischer or Drees having problems selling homes? The builder should have a decent product.

Chairman Rolfsen asked if the Commissioners had any questions or comments?

Mr. Schwenke asked the Staff to give the Committee more history about the density of both projects. He expressed a concern about factoring in the open space permanently. He also had a concern about the grade of the streets for fire trucks, etc. Can the grades be reduced?

Ms. Gulick had a concern about the upkeep of the existing development. What is going on with completing the work? She asked Staff to research it.

Mr. Rolfsen asked if it was the same builder or did he sell it? Mr. Schwartz doesn't know for sure.

Mrs. Goetting asked about the proposed 139 attached single-family dwellings. Are those to be purchased or rented?

Mr. Manger responded that all 296 units are rentals.

Mr. Costello stated there were questions about the owner of the original subdivision? Is that person or company still around or is there a new entity? Is the previous builder associated with the new entity?

Mr. Manger replied that he was unfamiliar with it. He is only present to answer the questions about the zone change. He is not prepared to provide that information tonight. He said he would later.

Mr. Costello explained that the Board wants to know how the proposal fits in with the existing development and what was planned and who is doing the work? Is it the same company?

Chairman Rolfsen asked if it is affordable housing in terms of monthly rents? Mr. Manger responded that they don't have that information available at this time. There is a lot that goes into those figures – cost of on-site and off-site improvements. This is the first part of the process. Additional information is needed like the traffic study.

Dr. Clark asked if the applicant had additional information of how the project impacts the schools? Any numbers? Mr. Manger explained that the demographic they are targeting are baby boomers and empty nesters. Dr. Clark stated that those people could have kids or grandkids.

Mr. Lunnemann asked for more information of what the multi-family will look like in terms of architecture and materials? Due to the proposed number of units, what about amenities? A pool, playground, pickleball courts, etc.? Where will it be located?

Ms. Gulick stated that the submitted traffic study does tell her a lot. She asked for ADT information on the existing roadways and whether the developer will have a more detailed traffic study?

Chairman Rolfsen asked if the developer will have this information in time for the September 4<sup>th</sup> Committee Meeting? Mr. Manger replied that he would have to confirm and verify.

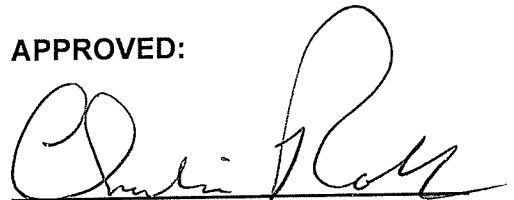
Mr. Vaught asked for more information about the impact of the project on Gunpowder Creek?

Mrs. Goetting asked if someone wanted to build more single-family homes, is the property already zoned for it? Mr. Schwartz replied yes as 97 homes could be built on the remaining lots. Brookstone Builders committed to build the lots from the beginning. Mr. Schwartz will verify that information.

Chairman Rolfsen explained the next steps of the process with the Committee Meeting and action by the full Board.

**Seeing no further questions or comments, Chairman Rolfsen announced that the Committee Meeting for this item will be on September 4, 2024 at 5:00 P.M. This item will be on the Agenda for the Business Meeting on September 18, 2024 at 7:00 p.m. in the Fiscal Courtroom. Chairman Rolfsen closed the Public Hearing at 9:35 p.m.**

APPROVED:



Charlie Rolfsen  
Chairman

Attest:



Kevin P. Costello, AICP  
Executive Director

Exhibit A – Packet from Charlie Schneckenberger  
Exhibit B – Packet from Mike Parker

Charlie Schneckenberger  
6516 Cannondale Dr.  
Burlington, KY 41005  
(Valley Creek Farms)

Why I think the zone request should be denied:

1. Zoning code Article 15 section 1501.A.2 states “intensity of use in a Planned Development District may exceed the maximum intensity permitted in the underlying zoning district by up to 50%. Zone SR-1 allows for 4 units per acre and the request is asking for 6.69 units per acre, which is above the zoning code. Delaware Crossing LLC didn’t even bother to follow the zoning code.
2. According to Article 15 section 1503.A “uses proposed for a Planned Development District Shall not adversely affect adjacent property.” Looking at the maps that I provided, you will see Valley Creek Farms in an approved cluster subdivision. All of parcel 050.00-00-001.05 was originally part of the Valley Creek Farms cluster subdivision zoned SR-1. This Parcel of land was used to make lot sizes in Valley Creek Farms smaller than .25 acres. Boone county subdivision regulation, Section 401.A open space and residential subdivisions, states: “in Cluster residential subdivisions the remnant land not designated as building lots is required to be undeveloped....” By rezoning parcel 050.00-00-001.05 and putting it in the Planned Development District, 60% of our subdivision will be rezoned including all the free space. If this rezoning request is approved, Valley Creek Farms will be reduced to approximately 18 acres. The 89 residential lots will have a density of 5 units per acre. This zoning change will **adversely affect my property density and will violate the SR-1 zoning code for my property.** Delaware Crossing LLC appears to hope the planning board doesn’t notice the change to Valley Creek Farms cluster subdivision density.

Respectfully,

Charlie Schneckenberger.





**Boone County**  
**Planning Commission**  
**8/21/24**

**Mike Parker – 38 Year Vet of Boone County**  
**Real Estate**

Request of Charter Commercial, LLC (applicant) for Delaware Crossing, LLC (owner) for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

**Question – 139 attached Single Family residential dwellings – what are they?**  
**157 Multi – are what**

**Cons - Has been zoned SR1**

**Emergency Traffic Police, Fire, Life Squad**

**Apartments brings more Police Calls just because of the density**

**Schools are crowded already, the density has the possibility of more students than if it was Single Family!**

**It will affect Property Values, period!**

**LOW INVENTORY Single Family**

**in Boone County**

- **Impact on Infrastructure:** Emphasize the potential strain on existing infrastructure, including roads, water, sewer systems, and public services. Discuss how the increase in density from 139 attached single-family homes and 157 multi-family units might overwhelm these systems, leading to traffic congestion, overburdened schools, and inadequate emergency services.
- **Character of the Neighborhood:** Argue that the proposed zoning change and the subsequent development are inconsistent with the existing character of the neighborhood. The shift from Suburban Residential One (SR-1) to a Planned Development (PD) might lead to higher density housing, which could alter the suburban feel and reduce property values for current residents.
- **Environmental Concerns:** Raise concerns about the environmental impact of the proposed development. This could include potential damage to local wildlife habitats, increased stormwater runoff

leading to flooding, or the loss of green space that contributes to the community's quality of life.

- **Traffic and Safety Issues:** Point out the likely increase in traffic, which could lead to safety concerns, especially on roads like Holbrook Lane and Lariat Way. Discuss how the added volume of cars could contribute to accidents and make it difficult for residents to move safely within their neighborhood.
- **Lack of Compatibility with the Comprehensive Plan:** If applicable, argue that the proposed development is not in alignment with the county's comprehensive plan, which might prioritize low-density residential development in this area. Highlight any goals or policies from the plan that the proposal contradicts.
- **Quality of Life:** Stress how the proposed development could negatively impact the quality of life for current residents. Discuss concerns about noise, pollution, overcrowding, and the potential loss of the community's suburban lifestyle.

- **Lack of Community Support:** If you have evidence or anecdotes showing that a significant portion of the community opposes this development, bring that to the commission's attention. Public opinion can be a powerful factor in planning decisions.

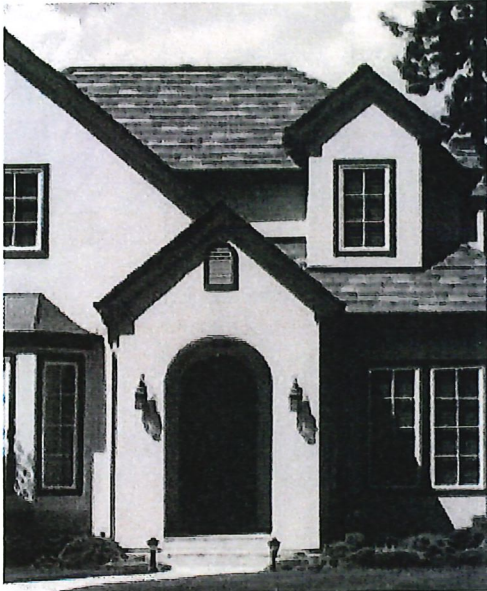
- **Potential for Overdevelopment:** Argue that the proposed development could lead to overdevelopment of the area, which might not be sustainable in the long term. Discuss the risks of creating a densely populated area without the necessary infrastructure or services to support it.

Here is the main reason you turn this down, what your decision tonight or when you vote on it will tell Boone County Residents whether you believe in Single Family Homeownership, the American Dream or you believe in as many units as possible on ever square inch of land left in Boone County!

Because if you approve this, trust me many more developers will start looking at other zoning parcels like this and want to build more multi family like this!

Please PROTECT BOONE COUNTY SINGLE FAMILY HOMES!

# BOONE COUNTY SINGLE FAMILY



Su	M	Tu	W	Th	F	Sa
				1	2	3
4	5	6	7	8	9	10
11	12	13	14	15	16	17
18	19	20	21	22	23	24
25	26	27	28	29	30	31

Sunday, August 18

# 2024

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Market Summary Table .....	Page 3
Glossary .....	Page 4

### Parker Real Estate Group

HUFF Realty  
 60 Cavalier Blvd.  
 Florence, KY 41042  
 NKYHomes.com  
 SearchNKYHomes.com  
 Mike@MikeParker.com  
 859-647-0700 Office

*ACTIVE  
 181  
 \$395,192*



# BOONE COUNTY SINGLE FAMILY

Sunday, August 18, 2024

Parker Real Estate Group  
 HUFF Realty  
 Mike@MikeParker.com  
 859-647-0700 Office

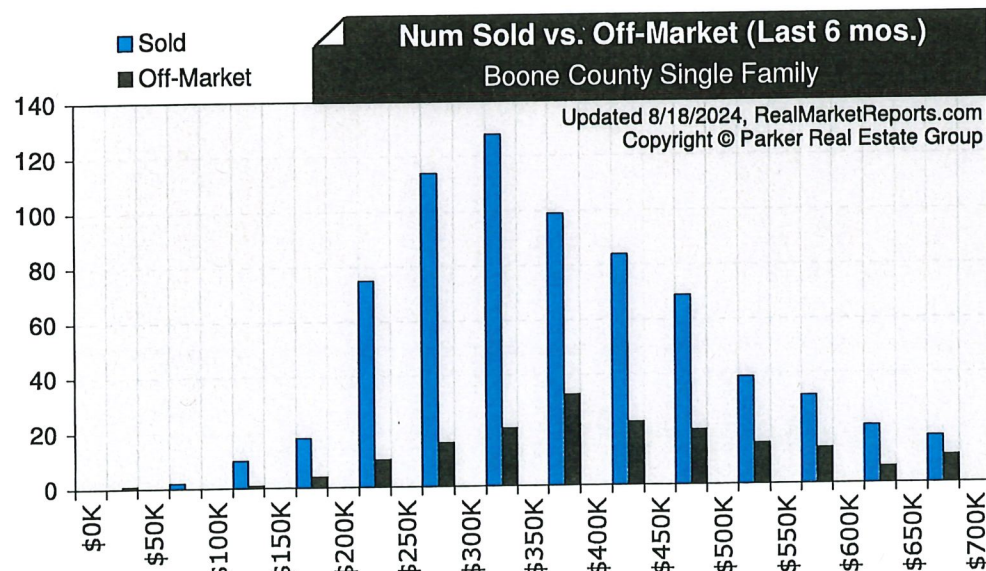
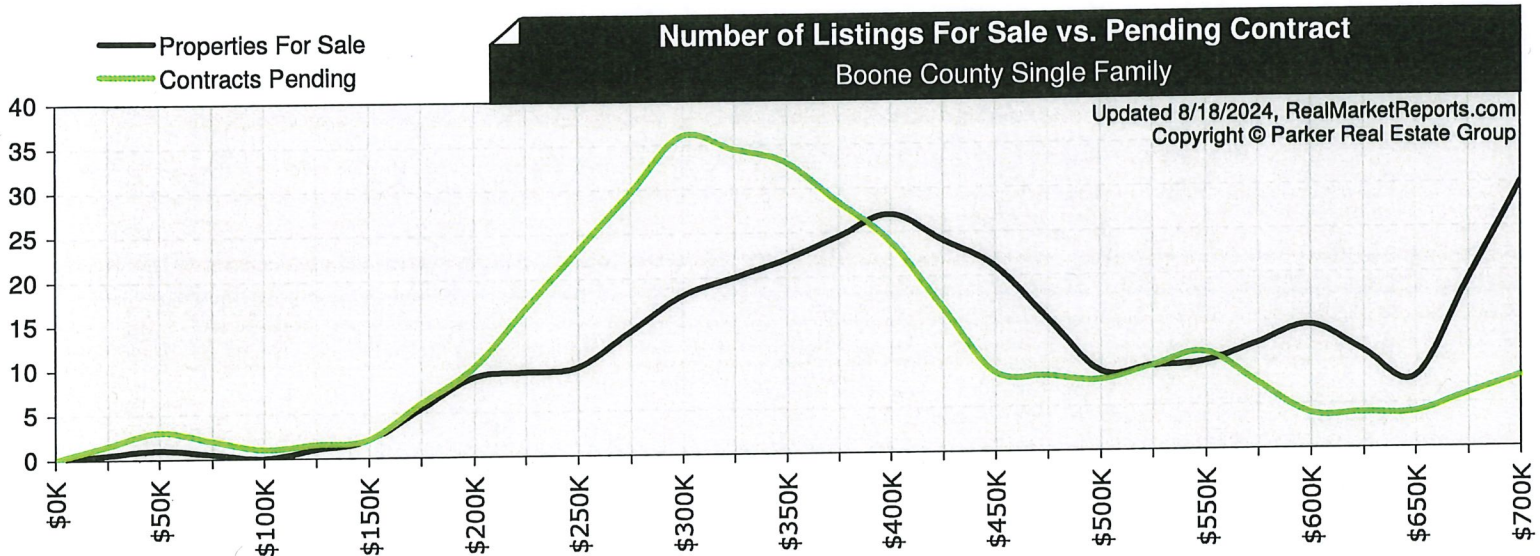
*Let's take a look* at the real estate market. Currently there are 176 sales pending in the market overall, leaving 181 listings still for sale. The resulting pending ratio is 49.3% (176 divided by 357). So you might be asking yourself, that's great... but what exactly does it mean? I'm glad you asked!

The pending ratio indicates the supply & demand of the market. Specifically, a high ratio means that listings are in demand and quickly going to contract. Alternatively, a low ratio means there are not enough qualified buyers for the existing supply.

*"Current inventory is described as active."*

Taking a closer look, we notice that the \$300K - \$350K price range has a relatively large number of contracts pending sale.

We also notice that the \$700K + price range has a relatively large inventory of properties for sale at 30 listings. The median list price (or asking price) for all properties in this market is \$455,000.



A total of 743 contracts have closed in the last 6 months with a median sold price of \$360,000. Breaking it down, we notice that the \$300K - \$350K price range contains the highest number of sold listings.

Alternatively, a total of 196 listings have failed to sell in that same period of time. Listings may fail to sell for many reasons such as being priced too high, having been inadequately marketed, the property was in poor condition, or perhaps the owner had second thoughts about selling at this particular time. The \$350K - \$400K price range has the highest number of off-market listings at 33 properties.

# BOONE COUNTY SINGLE FAMILY

Sunday, August 18, 2024

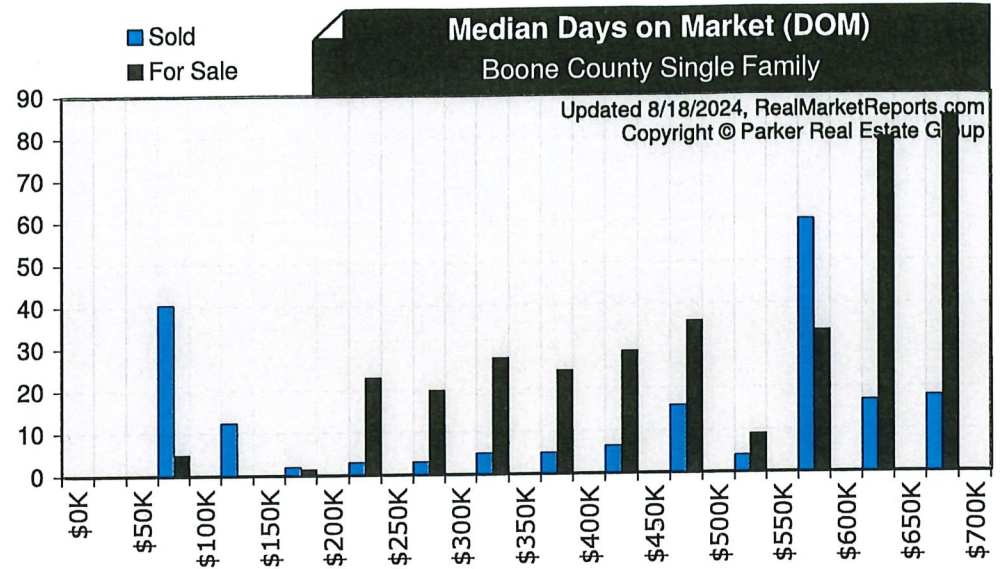
Parker Real Estate Group  
 HUFF Realty  
 Mike@MikeParker.com  
 859-647-0700 Office

Looking at the chart to the right, you might be wondering why average days on market (DOM) is important. This is a useful measurement because it can help us to determine whether we are in a buyer's market (indicated by high DOM), or a seller's market (indicated by low DOM). Active listings (properties for sale) have been on the market for a median time of 31 days.

Analysis of sold properties for the last six months reveals a median sold price of \$360,000 and 5 days on market. Notice that properties in the \$150K - \$200K price range have sold quickest over the last six months.

The recent history of sales can be seen in the two charts below. The median sold price for the last 30 days was \$391,250 with a DOM of 5 days.

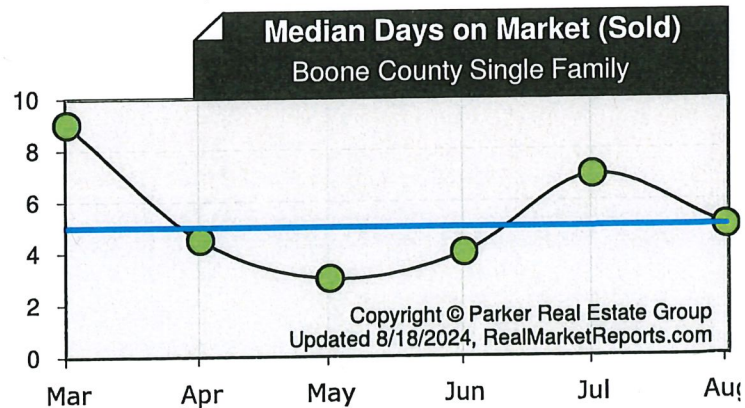
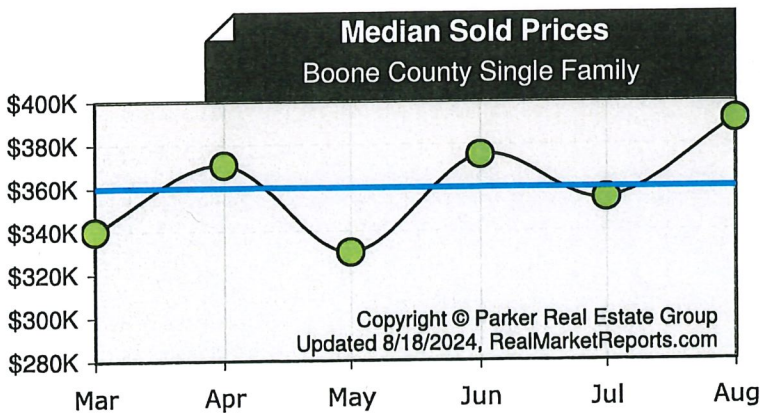
Since the recent DOM is greater than the median DOM for the last 6 months, it is a negative indicator for demand. It is always important to realize that real estate markets can fluctuate due to many factors, including shifting interest rates, the economy, or seasonal changes.



**"The median list-to-sales ratio for this area is 100.0%."**

Ratios are simple ways to express the difference between two values such as list price and sold price. In our case, we typically use the list-to-sale ratio to determine the percentage of the final list price that the buyer ultimately paid. It is a very common method to help buyers decide how much to offer on a property.

Analysis of the absorption rate indicates an inventory of 1.5 months based on the last 6 months of sales. This estimate is often used to determine how long it would take to sell off the current inventory of properties if all conditions remained the same. It is significant to mention that this estimate does not take into consideration any additional properties that will come onto the market in the future.



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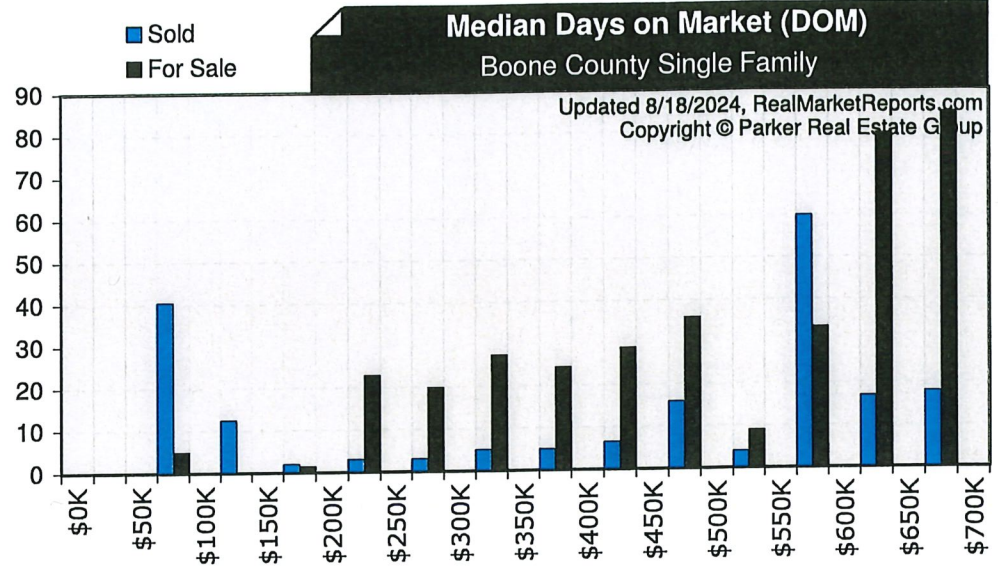
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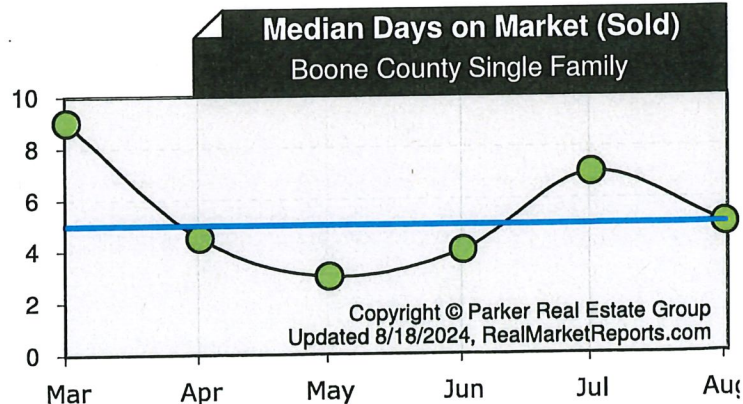
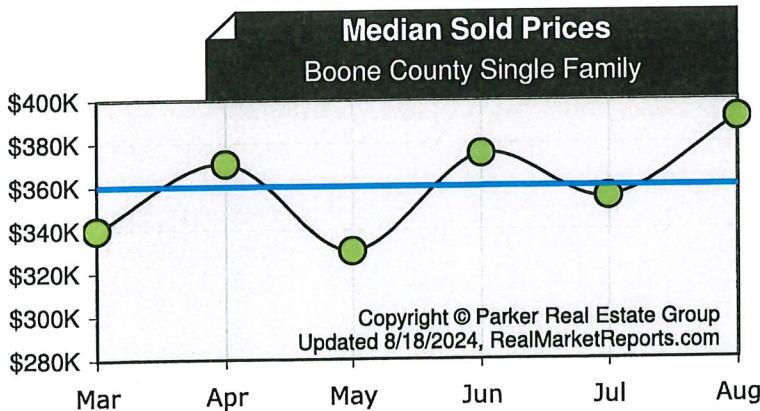
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# BOONE COUNTY SINGLE FAMILY

Sunday, August 18, 2024

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MARKET SUMMARY TABLE		176 PENDING <sup>[2]</sup>				196 OFF-MARKET (last 6 mos) <sup>[3]</sup>				743 SOLD/CLOSED (last 6 months) <sup>[4]</sup>				ABSORPTION RATE (months of inventory)	
Price Range		Num #	Days on Market	Current List Price	Num #	Pend Ratio	Num #	Num #	Days on Market	Original List Price	Final List Price	Sold/Closed Price	List-Sale Ratio	Est. Mos.	
Low	High														
\$0	\$49,999	0			0		1	0							
\$50,000	\$99,999	1	A 5 M 5	\$ 79,900 \$ 79,900	3	75%	0	2	A 41 M 41	\$ 101,750 \$ 101,750	\$ 101,750 \$ 101,750	\$ 82,500 \$ 82,500	81% 81%	3	
\$100,000	\$149,999	0			1	N/A	1	10	A 23 M 13	\$ 107,990 \$ 102,500	\$ 118,495 \$ 105,000	\$ 115,995 \$ 109,950	98% 105%	N/A	
\$150,000	\$199,999	2	A 2 M 2	\$ 179,950 \$ 179,950	2	50%	4	18	A 7 M 2	\$ 175,972 \$ 179,450	\$ 173,194 \$ 179,400	\$ 177,589 \$ 175,000	103% 98%	0.7	
\$200,000	\$249,999	9	A 29 M 23	\$ 223,089 \$ 219,000	10	53%	10	75	A 14 M 3	\$ 230,100 \$ 230,000	\$ 227,553 \$ 229,900	\$ 229,206 \$ 231,000	101% 100%	0.7	
\$250,000	\$299,999	10	A 36 M 20	\$ 281,880 \$ 287,000	23	70%	16	114	A 10 M 3	\$ 272,974 \$ 275,000	\$ 270,846 \$ 274,900	\$ 272,759 \$ 274,450	101% 100%	0.5	
\$300,000	\$349,999	18	A 41 M 28	\$ 331,228 \$ 330,000	36	67%	21	128	A 18 M 5	\$ 324,538 \$ 325,000	\$ 323,075 \$ 324,900	\$ 322,366 \$ 324,900	100% 100%	0.8	
\$350,000	\$399,999	22	A 56 M 25	\$ 383,586 \$ 391,088	33	60%	33	99	A 17 M 5	\$ 380,110 \$ 376,015	\$ 376,580 \$ 375,000	\$ 373,436 \$ 374,900	99% 100%	1.3	
\$400,000	\$449,999	27	A 50 M 29	\$ 420,201 \$ 419,900	24	47%	23	84	A 33 M 7	\$ 428,717 \$ 426,000	\$ 424,131 \$ 424,900	\$ 422,483 \$ 421,498	100% 99%	1.9	
\$450,000	\$499,999	21	A 44 M 36	\$ 478,968 \$ 480,738	9	30%	20	69	A 47 M 16	\$ 488,277 \$ 479,900	\$ 478,408 \$ 475,000	\$ 474,099 \$ 474,258	99% 100%	1.8	
\$500,000	\$549,999	9	A 14 M 9	\$ 520,989 \$ 515,000	8	47%	15	39	A 39 M 4	\$ 538,877 \$ 540,000	\$ 532,566 \$ 530,000	\$ 526,064 \$ 525,000	99% 99%	1.4	
\$550,000	\$599,999	10	A 67 M 34	\$ 578,823 \$ 579,950	11	52%	13	32	A 65 M 60	\$ 591,315 \$ 599,617	\$ 581,099 \$ 581,897	\$ 576,946 \$ 575,000	99% 99%	1.9	
\$600,000	\$649,999	14	A 73 M 80	\$ 631,885 \$ 628,950	4	22%	6	21	A 51 M 17	\$ 641,044 \$ 639,000	\$ 629,254 \$ 629,900	\$ 624,971 \$ 621,500	99% 99%	4	
\$650,000	\$699,999	8	A 88 M 85	\$ 672,113 \$ 664,900	4	33%	10	17	A 42 M 18	\$ 708,412 \$ 699,900	\$ 690,224 \$ 699,000	\$ 677,833 \$ 680,000	98% 97%	2.8	
\$700,000	+	30	A 75 M 52	\$ 894,026 \$ 809,949	8	21%	23	35	A 57 M 18	\$ 908,618 \$ 811,346	\$ 887,039 \$ 799,900	\$ 878,881 \$ 799,900	99% 100%	5.1	
Market Totals		181			176	49%	196	743						1.5	
Market Averages			53	\$ 511,560					27	\$ 402,352	\$ 397,148	\$ 395,192	100%		
Market Medians			31	\$ 455,000					5	\$ 365,000	\$ 360,000	\$ 360,000	100%		

Date Range (Off-Market & Sold) = 02/18/2024 to 08/18/2024

Data believed to be accurate but not guaranteed.

Status = [1] A; [2] P; [3] E, L, W; [4] C

Area = K09, K10, K11

Property Sub Type = SF

Favors Sellers Favors Buyers

Sunday, August 18, 2024

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Mike@MikeParker.com

859-647-0700 Office

**1. PROPERTIES FOR SALE (ACTIVE)**

- a. **Number Active:** The number of listings for sale which are currently being marketed but do not yet have a purchase agreement.
- b. **Days on Market (DOM):** The marketing period of currently active listings. This does not account for some listings which have had a previous listing period, but were re-entered as a new listing.
- c. **Current List Price:** The price that a property seller is currently asking.

**2. CONTRACTS PENDING**

- a. **Number Pending:** The number of current listings for which a contract has been signed but has not yet closed.
- b. **Pending Ratio:** Sometimes called a "list-to-pending ratio". This is a measure of how fast properties are going under contract vs. how fast they are being listed.

$$\text{Pending Ratio} = \frac{P \text{ (Number of Pending Listings)}}{A+P \text{ (Number of Active + Pending)}}$$

(P) represents properties that buyers have chosen  
 (A+P) represents the entire pool from which they have chosen

**3. OFF-MARKET**

- a. **Number Off-Market:** The number of listing agreements that have failed to close in the last 6 months. Some owners may choose to re-offer their property for sale.

**4. PROPERTIES SOLD (CLOSED CONTRACT)**

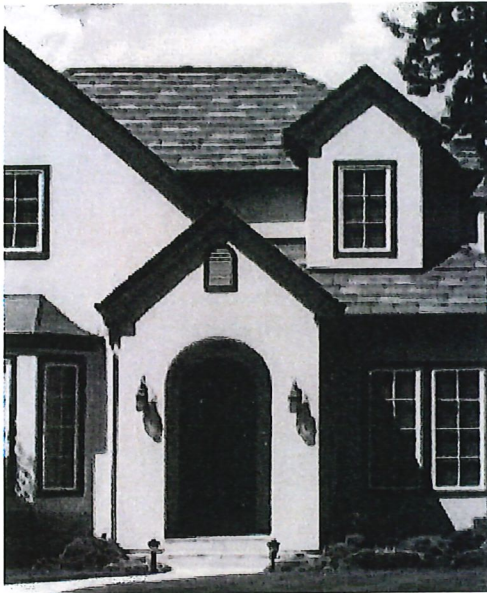
- a. **Number Sold:** The number of properties that have gone to a closing in the last 6 months.
- b. **Days on Market (DOM):** The marketing time it has taken properties to sell in the last 6 months.
- c. **Original List Price:** The price at which a sold property was originally marketed.
- d. **Final List Price:** The price at which a sold property was marketed just prior to selling.
- e. **Sold/Closed Price:** The price for which a property sold.
- f. **List to Sales Ratio:** The percentage of the list price that the buyer ultimately paid for the property.

$$\text{List to Sales Ratio} = \frac{\text{Sold Price}}{\text{Final List Price}}$$

**5. ABSORPTION RATE / MONTHS OF INVENTORY**

- a. **Absorption Rate / Months of Inventory:** An estimate of how fast listings are currently selling measured in months. For example, if 100 properties sell per month and there are 800 properties for sale - there is an 8 month supply of inventory before counting the additional properties that will come on the market.

# BOONE COUNTY SINGLE FAMILY



Su	M	Tu	W	Th	F	Sa
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Sunday, August 22

# 2021

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### Mike Parker Team

HUFF Realty  
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[www.MikeParker.com](http://www.MikeParker.com)  
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 859-647-0700

*82 ACTIVE  
 ASP \$323,000*

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-  [mikeparkercrs](#)

# BOONE COUNTY SINGLE FAMILY

Sunday, August 22, 2021

Mike Parker Team

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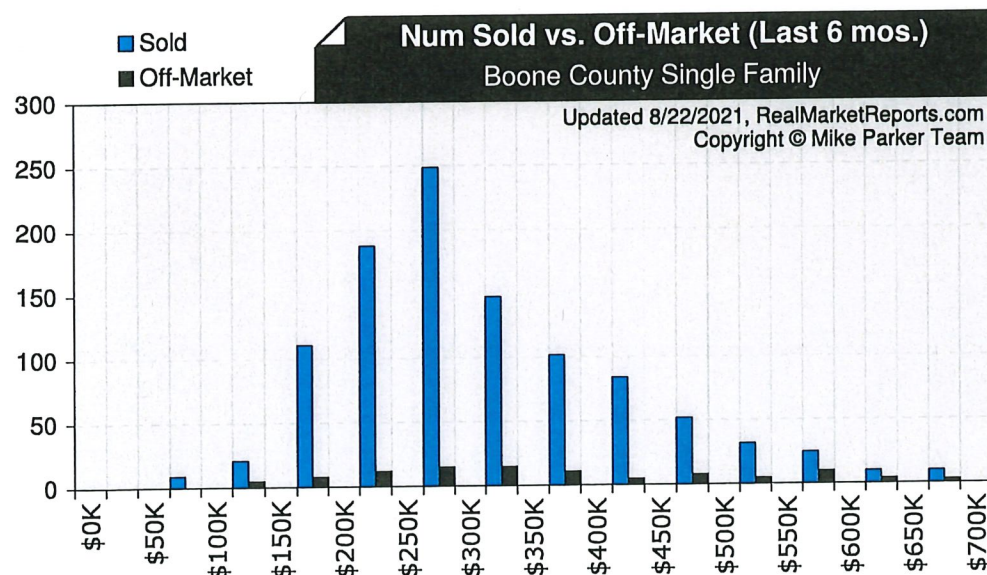
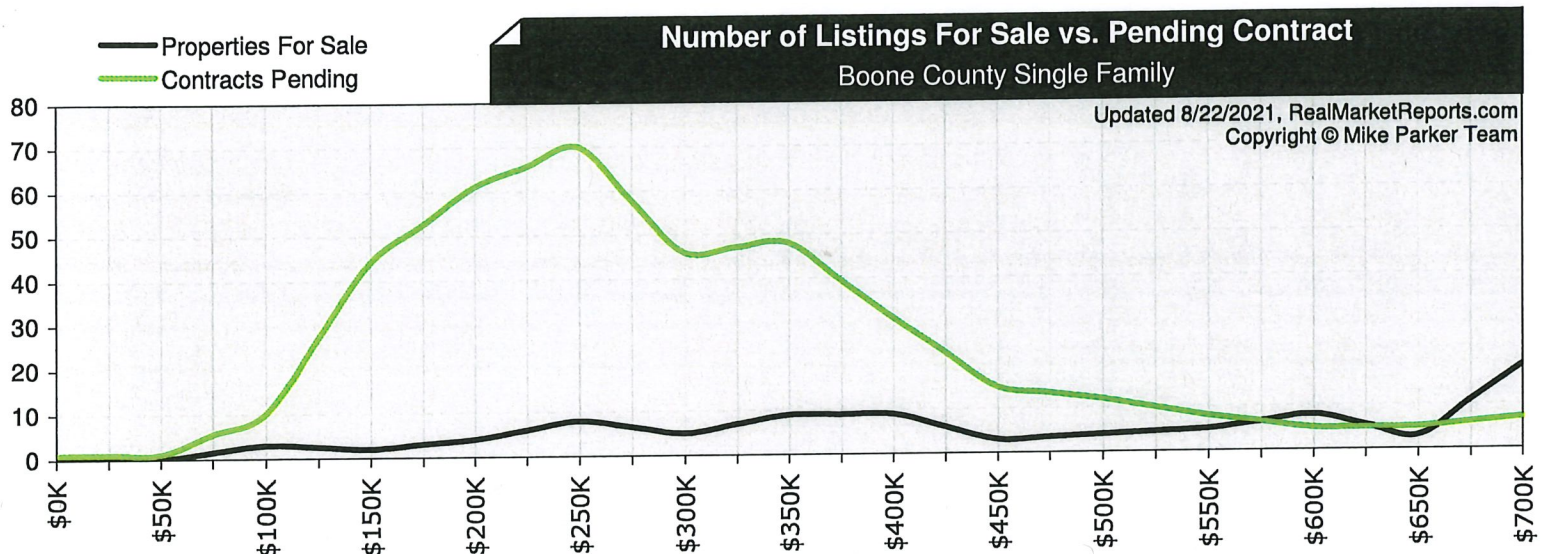
*Let's take a look* at the real estate market. Currently there are 364 sales pending in the market overall, leaving 82 listings still for sale. The resulting pending ratio is 81.6% (364 divided by 446). So you might be asking yourself, that's great... but what exactly does it mean? I'm glad you asked!

The pending ratio indicates the supply & demand of the market. Specifically, a high ratio means that listings are in demand and quickly going to contract. Alternatively, a low ratio means there are not enough qualified buyers for the existing supply.

*"Current inventory is described as very active."*

Taking a closer look, we notice that the \$250K - \$300K price range has a relatively large number of contracts pending sale.

We also notice that the \$700K + price range has a relatively large inventory of properties for sale at 19 listings. The median list price (or asking price) for all properties in this market is \$497,450.



A total of 1065 contracts have closed in the last 6 months with a median sold price of \$287,000. Breaking it down, we notice that the \$250K - \$300K price range contains the highest number of sold listings.

Alternatively, a total of 119 listings have failed to sell in that same period of time. Listings may fail to sell for many reasons such as being priced too high, having been inadequately marketed, the property was in poor condition, or perhaps the owner had second thoughts about selling at this particular time. The \$700K + price range has the highest number of off-market listings at 18 properties.

# BOONE COUNTY SINGLE FAMILY

Sunday, August 22, 2021

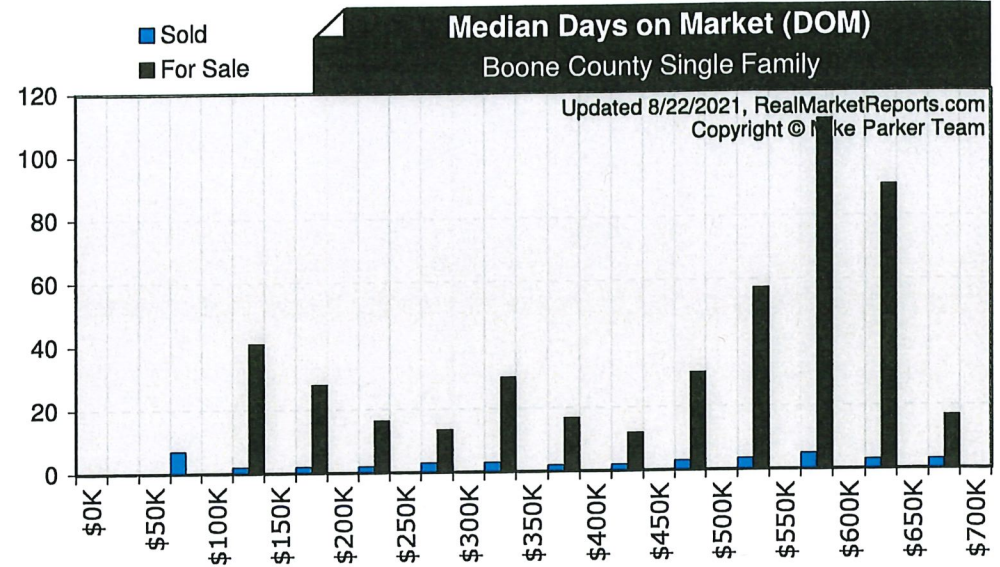
Mike Parker Team  
 HUFF Realty  
 mike@mikeparker.com  
 859-647-0700

Looking at the chart to the right, you might be wondering why average days on market (DOM) is important. This is a useful measurement because it can help us to determine whether we are in a buyer's market (indicated by high DOM), or a seller's market (indicated by low DOM). Active listings (properties for sale) have been on the market for a median time of 45 days.

Analysis of sold properties for the last six months reveals a median sold price of \$287,000 and 2 days on market. Notice that properties in the \$200K - \$250K price range have sold quickest over the last six months.

The recent history of sales can be seen in the two charts below. The median sold price for the last 30 days was \$285,000 with a DOM of 2 days.

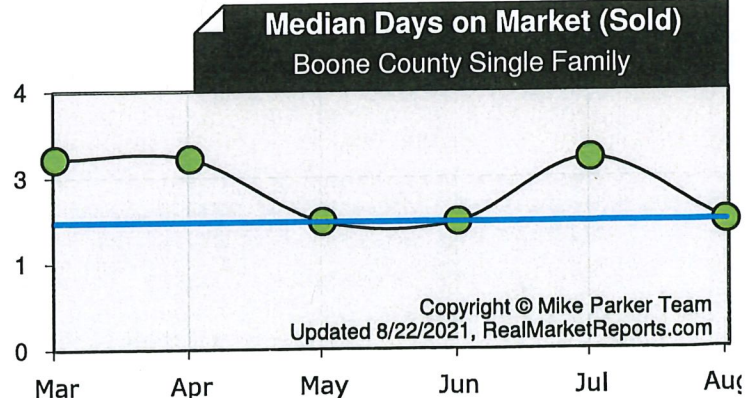
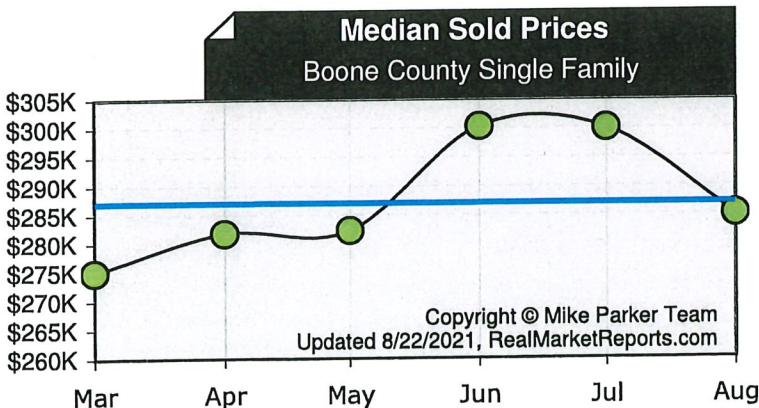
Since the recent DOM is greater than the median DOM for the last 6 months, it is a negative indicator for demand. It is always important to realize that real estate markets can fluctuate due to many factors, including shifting interest rates, the economy, or seasonal changes.



**"The median list-to-sales ratio for this area is 100.7%."**

Ratios are simple ways to express the difference between two values such as list price and sold price. In our case, we typically use the list-to-sale ratio to determine the percentage of the final list price that the buyer ultimately paid. It is a very common method to help buyers decide how much to offer on a property.

Analysis of the absorption rate indicates an inventory of 0.5 months based on the last 6 months of sales. This estimate is often used to determine how long it would take to sell off the current inventory of properties if all conditions remained the same. It is significant to mention that this estimate does not take into consideration any additional properties that will come onto the market in the future.



# BOONE COUNTY SINGLE FAMILY

Sunday, August 22, 2021

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MARKET SUMMARY TABLE		364 PENDING <sup>[2]</sup>			119 OFF-MARKET (last 6 mos) <sup>[3]</sup>			1065 SOLD/CLOSED (last 6 months) <sup>[4]</sup>			ABSORPTION RATE (months of inventory)		Est. Mos.	
Price Range		Num #	Days on Market	Current List Price	Num #	Pend Ratio	Num #	Num #	Days on Market	Original List Price	Final List Price	Sold/Closed Price	List-Sale Ratio	Est. Mos.
Low	High													
\$0	\$49,999	0			1	N/A	0	0						
\$50,000	\$99,999	0			1	N/A	0	9	A 18 M 7	\$ 91,856 \$ 90,000	\$ 87,300 \$ 85,000	\$ 76,222 \$ 72,000	87% 85%	N/A
\$100,000	\$149,999	3	A 45 M 41	\$ 129,633 \$ 139,000	10	77%	5	21	A 11 M 2	\$ 124,519 \$ 125,000	\$ 130,895 \$ 129,900	\$ 131,586 \$ 130,000	101% 100%	0.9
\$150,000	\$199,999	2	A 28 M 28	\$ 182,450 \$ 182,450	44	96%	8	111	A 8 M 2	\$ 175,192 \$ 174,900	\$ 175,232 \$ 174,900	\$ 176,222 \$ 175,000	101% 100%	0.1
\$200,000	\$249,999	4	A 25 M 17	\$ 220,200 \$ 217,450	61	94%	12	188	A 8 M 2	\$ 218,473 \$ 218,750	\$ 218,476 \$ 219,450	\$ 224,596 \$ 225,000	103% 103%	0.1
\$250,000	\$299,999	8	A 88 M 14	\$ 277,188 \$ 274,000	70	90%	15	249	A 12 M 3	\$ 268,094 \$ 265,000	\$ 268,132 \$ 265,000	\$ 272,540 \$ 270,614	102% 102%	0.2
\$300,000	\$349,999	5	A 76 M 30	\$ 328,180 \$ 325,000	46	90%	15	148	A 16 M 3	\$ 319,289 \$ 319,000	\$ 318,955 \$ 319,000	\$ 321,987 \$ 321,375	101% 101%	0.2
\$350,000	\$399,999	9	A 61 M 17	\$ 381,252 \$ 391,388	48	84%	11	102	A 9 M 2	\$ 371,184 \$ 370,000	\$ 369,060 \$ 369,900	\$ 371,362 \$ 370,000	101% 100%	0.5
\$400,000	\$449,999	9	A 19 M 12	\$ 420,355 \$ 420,000	31	78%	5	84	A 15 M 2	\$ 424,932 \$ 425,000	\$ 424,563 \$ 425,000	\$ 425,597 \$ 427,545	100% 101%	0.6
\$450,000	\$499,999	3	A 33 M 31	\$ 498,267 \$ 499,900	15	83%	8	52	A 29 M 3	\$ 476,107 \$ 478,784	\$ 475,097 \$ 475,000	\$ 471,224 \$ 474,900	99% 100%	0.3
\$500,000	\$549,999	4	A 63 M 58	\$ 531,840 \$ 532,580	12	75%	5	32	A 14 M 4	\$ 521,669 \$ 531,950	\$ 514,863 \$ 525,000	\$ 523,091 \$ 525,000	102% 100%	0.8
\$550,000	\$599,999	5	A 124 M 111	\$ 581,016 \$ 580,243	8	62%	10	25	A 17 M 5	\$ 577,298 \$ 575,000	\$ 574,102 \$ 575,000	\$ 569,057 \$ 569,000	99% 99%	1.2
\$600,000	\$649,999	8	A 92 M 90	\$ 621,616 \$ 624,535	5	38%	4	10	A 12 M 3	\$ 615,401 \$ 617,983	\$ 615,664 \$ 619,000	\$ 619,023 \$ 615,194	101% 99%	4.8
\$650,000	\$699,999	3	A 43 M 17	\$ 674,687 \$ 675,000	5	63%	3	10	A 14 M 3	\$ 683,860 \$ 684,450	\$ 681,370 \$ 677,000	\$ 669,650 \$ 672,500	98% 99%	1.8
\$700,000	+	19	A 89 M 89	\$ 1,055,282 \$ 875,000	7	27%	18	24	A 81 M 6	\$ 956,379 \$ 897,000	\$ 926,250 \$ 887,000	\$ 886,877 \$ 823,500	96% 93%	4.8
Market Totals		82			364	82%	119	1065						0.5
Market Averages		68		\$ 564,415				14		\$ 322,399	\$ 321,192	\$ 323,024	101%	
Market Medians		45		\$ 497,450				2		\$ 285,000	\$ 285,000	\$ 287,000	101%	

Date Range (Off-Market & Sold) = 02/21/2021 to 08/22/2021

Data believed to be accurate but not guaranteed.

Status = [1] Active; [2] Pending; [3] Cancelled, Expired; [4] Sold

Area = K09, K10, K11

Property Sub Type = Single Family

Favors Sellers Favors Buyers

Sunday, August 22, 2021

Mike Parker Team

HUFF Realty  
mike@mikeparker.com  
859-647-0700**1. PROPERTIES FOR SALE (ACTIVE)**

- a. **Number Active:** The number of listings for sale which are currently being marketed but do not yet have a purchase agreement.
- b. **Days on Market (DOM):** The marketing period of currently active listings. This does not account for some listings which have had a previous listing period, but were re-entered as a new listing.
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- b. **Pending Ratio:** Sometimes called a "list-to-pending ratio". This is a measure of how fast properties are going under contract vs. how fast they are being listed.

$$\text{Pending Ratio} = \frac{P \text{ (Number of Pending Listings)}}{A+P \text{ (Number of Active + Pending)}}$$

(P) represents properties that buyers have chosen  
(A+P) represents the entire pool from which they have chosen

**3. OFF-MARKET**

- a. **Number Off-Market:** The number of listing agreements that have failed to close in the last 6 months. Some owners may choose to re-offer their property for sale.

**4. PROPERTIES SOLD (CLOSED CONTRACT)**

- a. **Number Sold:** The number of properties that have gone to a closing in the last 6 months.
- b. **Days on Market (DOM):** The marketing time it has taken properties to sell in the last 6 months.
- c. **Original List Price:** The price at which a sold property was originally marketed.
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**5. ABSORPTION RATE / MONTHS OF INVENTORY**

- a. **Absorption Rate / Months of Inventory:** An estimate of how fast listings are currently selling measured in months. For example, if 100 properties sell per month and there are 800 properties for sale - there is an 8 month supply of inventory before counting the additional properties that will come on the market.

# BOONE COUNTY SINGLE FAMILY



Su	M	Tu	W	Th	F	Sa
			1	2	3	4
5	6	7	8	9	10	11
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Sunday, August 19

# 2018

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*322 ACTIVE  
 \$247,578*

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# BOONE COUNTY SINGLE FAMILY

Sunday, August 19, 2018

**Mike Parker Team**  
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 mike@mikeparker.com  
 859-647-0700

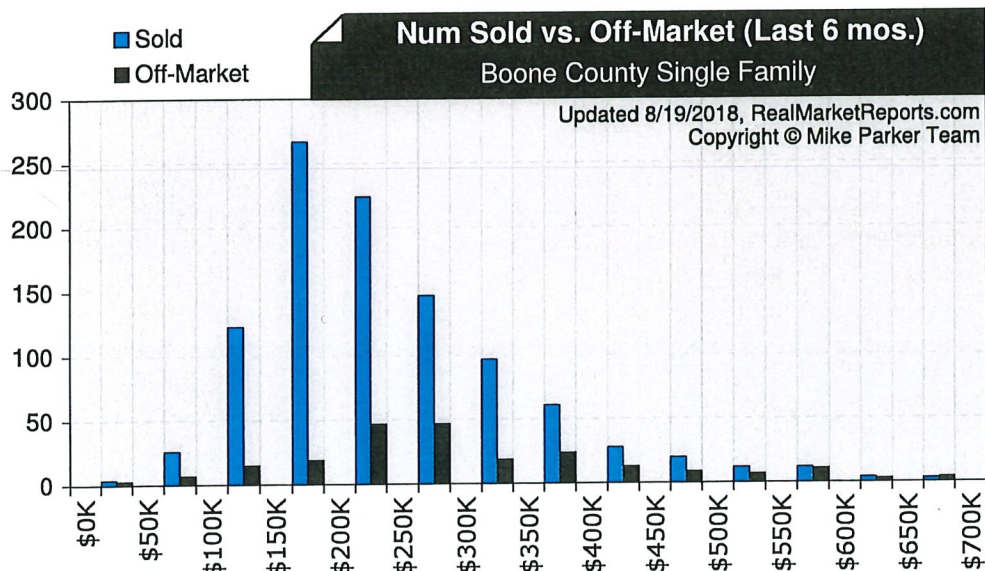
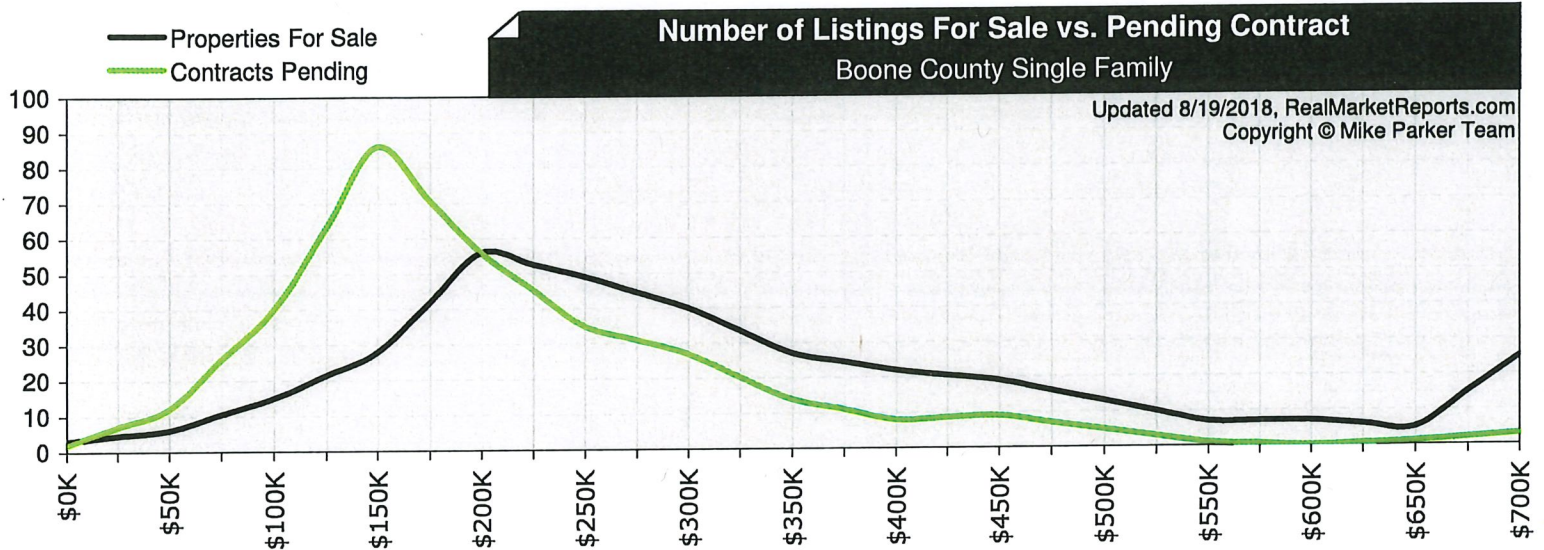
*Let's take a look* at the real estate market. Currently there are 299 sales pending in the market overall, leaving 322 listings still for sale. The resulting pending ratio is 48.1% (299 divided by 621). So you might be asking yourself, that's great... but what exactly does it mean? I'm glad you asked!

The pending ratio indicates the supply & demand of the market. Specifically, a high ratio means that listings are in demand and quickly going to contract. Alternatively, a low ratio means there are not enough qualified buyers for the existing supply.

*"Current inventory is described as active."*

Taking a closer look, we notice that the \$150K - \$200K price range has a relatively large number of contracts pending sale.

We also notice that the \$200K - \$250K price range has a relatively large inventory of properties for sale at 56 listings. The median list price (or asking price) for all properties in this market is \$308,250.



A total of 1038 contracts have closed in the last 6 months with a median sold price of \$222,250. Breaking it down, we notice that the \$150K - \$200K price range contains the highest number of sold listings.

Alternatively, a total of 241 listings have failed to sell in that same period of time. Listings may fail to sell for many reasons such as being priced too high, having been inadequately marketed, the property was in poor condition, or perhaps the owner had second thoughts about selling at this particular time. The \$200K - \$250K price range has a high number of off-market listings at 47 properties.

# BOONE COUNTY SINGLE FAMILY

Sunday, August 19, 2018

Mike Parker Team

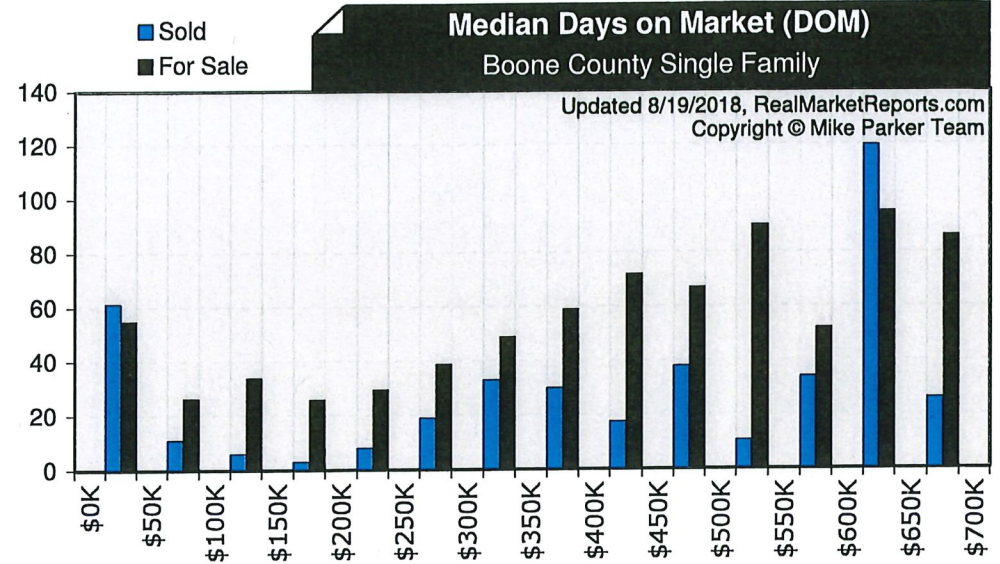
HUFF Realty  
mike@mikeparker.com  
859-647-0700

Looking at the chart to the right, you might be wondering why average days on market (DOM) is important. This is a useful measurement because it can help us to determine whether we are in a buyer's market (indicated by high DOM), or a seller's market (indicated by low DOM). Active listings (properties for sale) have been on the market for a median time of 48 days.

Analysis of sold properties for the last six months reveals a median sold price of \$222,250 and 8 days on market. Notice that properties in the \$150K - \$200K price range have sold quickest over the last six months.

The recent history of sales can be seen in the two charts below. The median sold price for the last 30 days was \$220,000 with a DOM of 5 days.

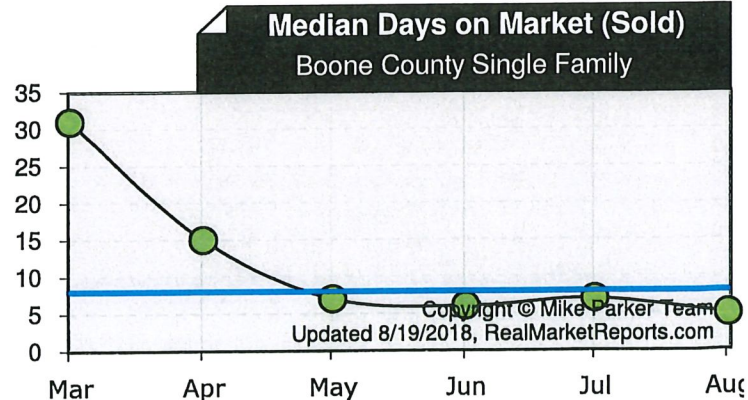
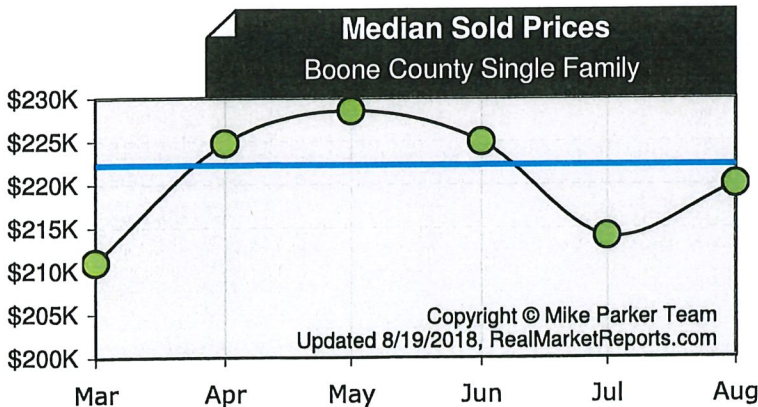
Since the recent DOM is less than the median DOM for the last 6 months, it is a positive indicator for demand. It is always important to realize that real estate markets can fluctuate due to many factors, including shifting interest rates, the economy, or seasonal changes.



**"The median list-to-sales ratio for this area is 98.8%."**

Ratios are simple ways to express the difference between two values such as list price and sold price. In our case, we typically use the list-to-sale ratio to determine the percentage of the final list price that the buyer ultimately paid. It is a very common method to help buyers decide how much to offer on a property.

Analysis of the absorption rate indicates an inventory of 1.9 months based on the last 6 months of sales. This estimate is often used to determine how long it would take to sell off the current inventory of properties if all conditions remained the same. It is significant to mention that this estimate does not take into consideration any additional properties that will come onto the market in the future.



# BOONE COUNTY SINGLE FAMILY

Sunday, August 19, 2018

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MARKET SUMMARY TABLE		299 PENDING <sup>[2]</sup>				241 OFF-MARKET (last 6 mos) <sup>[3]</sup>				1038 SOLD/CLOSED (last 6 months) <sup>[4]</sup>		ABSORPTION RATE (months of inventory)		
Price Range		Num #	Days on Market	Current List Price	Num #	Pend Ratio	Num #	Num #	Days on Market	Original List Price	Final List Price	Sold/Closed Price	List-Sale Ratio	Est. Mos.
Low	High													
\$0	\$49,999	3	A 66 M 55	\$ 29,900 \$ 29,900	2	40%	3	4	A 89 M 62	\$ 52,425 \$ 54,900	\$ 46,175 \$ 52,400	\$ 42,125 \$ 42,000	91% 80%	4.5
\$50,000	\$99,999	6	A 38 M 27	\$ 86,450 \$ 85,000	12	67%	7	26	A 25 M 11	\$ 86,928 \$ 85,000	\$ 84,286 \$ 84,950	\$ 81,085 \$ 84,500	96% 99%	1.4
\$100,000	\$149,999	15	A 55 M 34	\$ 133,087 \$ 139,500	40	73%	15	123	A 23 M 6	\$ 133,682 \$ 136,500	\$ 132,632 \$ 135,000	\$ 131,183 \$ 135,000	99% 100%	0.7
\$150,000	\$199,999	28	A 45 M 26	\$ 178,146 \$ 184,950	86	75%	19	267	A 17 M 3	\$ 176,761 \$ 175,000	\$ 175,716 \$ 175,000	\$ 175,191 \$ 175,000	100% 100%	0.6
\$200,000	\$249,999	56	A 48 M 30	\$ 226,479 \$ 228,950	56	50%	47	224	A 23 M 8	\$ 230,045 \$ 229,900	\$ 227,470 \$ 227,500	\$ 224,685 \$ 224,950	99% 99%	1.5
\$250,000	\$299,999	49	A 78 M 39	\$ 274,092 \$ 272,000	35	42%	47	147	A 43 M 19	\$ 277,739 \$ 275,000	\$ 273,318 \$ 269,900	\$ 268,190 \$ 265,000	98% 98%	2
\$300,000	\$349,999	40	A 92 M 49	\$ 327,398 \$ 333,915	27	40%	19	97	A 61 M 33	\$ 337,530 \$ 338,500	\$ 331,458 \$ 332,900	\$ 324,039 \$ 325,000	98% 98%	2.5
\$350,000	\$399,999	27	A 85 M 59	\$ 379,169 \$ 379,900	14	34%	24	61	A 58 M 30	\$ 386,846 \$ 387,950	\$ 379,171 \$ 379,900	\$ 371,948 \$ 370,000	98% 97%	2.7
\$400,000	\$449,999	22	A 96 M 72	\$ 422,048 \$ 422,358	8	27%	13	28	A 55 M 18	\$ 438,880 \$ 439,900	\$ 432,830 \$ 434,445	\$ 422,971 \$ 422,500	98% 97%	4.7
\$450,000	\$499,999	19	A 71 M 67	\$ 476,343 \$ 475,000	9	32%	9	20	A 77 M 38	\$ 502,859 \$ 504,376	\$ 487,364 \$ 482,500	\$ 472,052 \$ 474,000	97% 98%	5.7
\$500,000	\$549,999	13	A 85 M 90	\$ 532,365 \$ 534,900	5	28%	7	12	A 54 M 11	\$ 555,132 \$ 542,450	\$ 543,007 \$ 533,950	\$ 524,707 \$ 526,250	97% 99%	6.5
\$550,000	\$599,999	7	A 73 M 52	\$ 580,100 \$ 575,000	1	13%	11	12	A 75 M 34	\$ 603,693 \$ 587,500	\$ 593,610 \$ 587,500	\$ 571,792 \$ 572,500	96% 97%	3.5
\$600,000	\$649,999	7	A 72 M 95	\$ 621,329 \$ 617,500	0		3	4	A 111 M 120	\$ 691,475 \$ 685,950	\$ 656,200 \$ 654,950	\$ 619,500 \$ 617,500	94% 94%	10.5
\$650,000	\$699,999	5	A 130 M 86	\$ 678,390 \$ 675,000	1	17%	4	3	A 32 M 26	\$ 724,100 \$ 697,500	\$ 718,233 \$ 715,000	\$ 682,633 \$ 679,500	95% 95%	10
\$700,000	+	25	A 163 M 117	\$ 974,505 \$ 849,900	3	11%	13	10	A 197 M 197	\$ 929,770 \$ 822,450	\$ 910,769 \$ 796,950	\$ 889,305 \$ 817,500	98% 103%	15
Market Totals		322			299	48%	241	1038						1.9
Market Averages		79		\$ 367,890				35	\$ 255,579	\$ 251,841	\$ 247,518	98%		
Market Medians		48		\$ 308,250				8	\$ 227,250	\$ 224,950	\$ 222,250	99%		

Date Range (Off-Market & Sold) = 02/18/2018 to 08/19/2018  
 Data believed to be accurate but not guaranteed.  
 Status = [1] Active; [2] Pending; [3] Cancelled, Expired; [4] Sold  
 Area = K09, K10, K11  
 Property Sub Type = Single Family

Favors Sellers Favors Buyers

Sunday, August 19, 2018

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 mike@mikeparker.com  
 859-647-0700

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**5. ABSORPTION RATE / MONTHS OF INVENTORY**

- a. **Absorption Rate / Months of Inventory:** An estimate of how fast listings are currently selling measured in months. For example, if 100 properties sell per month and there are 800 properties for sale - there is an 8 month supply of inventory before counting the additional properties that will come on the market.

**BOONE COUNTY PLANNING COMMISSION  
BOONE COUNTY FISCAL COURTROOM  
BURLINGTON, KENTUCKY  
BUSINESS MEETING  
OCTOBER 16, 2024  
7:00 P.M.**

---

Chairman Rolfsen opened the Business Meeting at 7:02 p.m. and welcomed the audience to the Planning Commission's October 16, 2024 Business Meeting. Chairman Rolfsen then invited everyone to join him in reciting the Pledge of Allegiance.

**COMMISSION MEMBERS PRESENT:**

Mr. Randy Bessler  
Mrs. Pamela Goetting  
Ms. Corrin Gulick, Vice Chairwoman  
Mr. Steve Harper, Temporary Presiding Officer  
Mr. David Hincks  
Mrs. Janet Kegley  
Mr. Rick Lunnemann  
Mr. Eric Richardson  
Mr. Charlie Rolfsen, Chairman  
Mr. Bob Schwenke  
Mrs. Jackie Steele, Secretary/Treasurer  
Mr. Tom Szurlinski  
Mr. Steve Turner

**COMMISSION MEMBERS NOT PRESENT:**

Mrs. Kathy Clark  
Mr. Kenny Vaught

**LEGAL COUNSEL PRESENT:**

Mr. Dale Wilson

**STAFF MEMBERS PRESENT:**

Mr. Kevin P. Costello, AICP, Executive Director  
Mr. Michael Schwartz, Director, Zoning Services  
Mr. Steve Lilly, GISP, Planner  
Mr. Steve Gay, GISP, Director, GIS Services

**APPROVAL OF THE MINUTES:**

Chairman Rolfsen stated that the Commissioners received copies of the Minutes from the September 18, 2024 Business Meeting and the October 2, 2024 Public Hearings. He asked if there were any comments or corrections?

**Mr. Szurlinski moved to approve the Minutes as presented. Mr. Hincks seconded the motion and it carried unanimously.**

**BILLS:**

Mr. Kevin P. Costello, AICP, Executive Director, advised the Commissioners of the **Bills to be Paid**, which included a list of **Expenses** and **Salaries and Benefits** for a time period between September 5, 2024 and October 2, 2024.

**EXPENSES:**

Accounting Fees	942.00
Attorney Fees	4,800.00
Consultant/Professional Svcs Fees	11,460.00
Filing Fees	900.00
Legal Ads/Recruitment	469.83
Miscellaneous Expense	155.00
Office & Board Meeting Supplies	1,724.75
Office Equipment / Expense	331.39
Postage Expense	502.25
Printing/Pub/Dues/Subscriptions	100.00
Professional Development	<u>397.32</u>

**TOTAL: \$ 21,782.54**

**SALARIES AND BENEFITS:**

FICA-BCPC Portion	\$ 7,023.34
Health/Dental/Life/LTD	17,872.93
Retirement – BCPC Portion	18,726.86
Salaries – Staff Expenses	96,479.09
Salaries – BCPC & BOA	<u>1,540.00</u>

**TOTAL: \$ 141,642.22**

**GRAND TOTAL: \$ 163,424.76**

Ms. Gulick moved that the bills (expenses and salaries and benefits) be approved and paid as presented and indicated approval of those items, which have been previously paid. Mrs. Goetting seconded the motion and it carried unanimously.

**ACTION ON PLAN REVIEWS:**

**ZONING MAP AMENDMENT - Rick Lunnemann, Chairman, Michael Schwartz, Staff**

1. Request of **LDG Multifamily LLC (applicant)** for **Joyce Robinson Foundation, Inc. (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Urban Residential Two (UR-2) for an approximate 19 acre area located at 10637 Dixie Highway, Boone County, Kentucky. The request is for a zone change to allow for the development of a 312 unit multi-family residential development with on-site amenities and a Variance increasing the height of a fence along the rear property line from six (6) feet to eight (8) feet.

Staff Member, Michael Schwartz, read the Committee Report, which recommended approval based upon Findings of Facts and Conditions for the Zoning Map Amendment and Variance. The applicant/owner has signed the Condition Letter agreeing to the six conditions. The Committee vote was unanimous by the five Committee Members.

Chairman Rolfsen asked if there was anyone in the audience who wanted to speak in favor or against the request?

Mr. David Johnson, attorney for the applicant, stated that he agreed with the Committee's recommendation. He is available to answer any questions.

Seeing no further comment, **Mr. Lunnemann moved, by Resolution to the Boone County Fiscal Court, to recommend approval of the Zoning Map Amendment and Variance based upon the Committee Report and Findings of Fact and Conditions. Mr. Schwenke seconded the motion.**

Chairman Rolfsen asked the applicant how he was going to tie into the public sanitary sewer? Ms. Michelle Bollman, Viox & Viox, Inc. replied that she has been working with the Kentucky Transportation Cabinet on an easement to connect to the existing public sewer owned by SD1. That is the preferred route. Also, they are working with the adjoiner to the north. If that doesn't work out, they have spoken to SD1 to install a public pump station on the south side.

Mr. Bob Schwenke expressed a concern about lower income housing at that location with children that don't have adequate transportation. There is no public transportation by the site. What about TANK providing service? There are no sidewalks on U.S. 25. There is no lighting along Dixie Highway. He has trouble voting for the request. There are a lot of warehouses and truck traffic along Dixie Highway.

Chairman Rolfsen inquired about amenities like a playground for kids? Mr. Brett Budd from LDG Communities, stated they will have amenities such as a playground and club house/fitness center along with a resident service room. Mr. Budd replied that it is affordable housing and limited to 60% income. Most of their residents have transportation because it is considered a rural area.

Chairman Rolfsen stated that it was shocking that the School Board did not respond or comment on the proposal considering it had 3 bedroom units. Those types of units usually have more than 2.1 kids per unit in lower income housing. There was no opposition to the project and it backs up to a rest area. Mr. Johnson noted that a bus Park & Ride is located at Mt. Zion Road. Mr. Schwenke thought the project needs a sidewalk along Dixie Highway.

Chairman Rolfsen stated that he has been hearing about the widening of Dixie Highway for decades. People will walk towards Mt. Zion Road and Richwood Road and there is no sidewalk.

**Seeing no further discussion, Chairman Rolfsen asked for a vote on the original motion made by Mr. Lunnemann and seconded by Mr. Schwenke. The vote found all by Mr. Schwenke voting in favor of the request. The motion passed by a vote of 12 in favor and 1 against (Mr. Schwenke).**

**ZONING MAP AMENDMENT - Steve Harper, Chairman, Steve Lilly, Staff**

2. Request of **Sinkula Investments (applicant)** for **Mt. Zion MHC LP (owner)** for a Zoning Map Amendment from Mobile Home Park (MHP) to Commercial Four (C-4), including the following variances: (1) reducing the Buffer Yard A width from ten (10) feet to two (2) feet along Lakeside Drive; and (2) reducing the Buffer Yard A width from ten (10) feet to two (2) feet along the west property line, for an approximate 1.2 acre area located at the southwest corner of the intersection of Mt Zion Road and Lakeside Drive, including the properties at 10013, 10017, 10021, 10025, 10029, and 10033 Lakeside Drive, Boone County, Kentucky. The request is for a Zoning Map Amendment to allow the development of an eating and drinking establishment with drive-through service.

**Mrs. Kegley moved to defer the request until the December 4, 2024 Business Meeting. Mr. Lunnemann seconded the motion and it passed unanimously.**

**CHANGE OF CONCEPT DEVELOPMENT PLAN - David Hincks, Chairman, Steve Lilly, Staff**

3. Request of **Steve Berling (applicant)** for **BS NKY Properties (owner)** for a Change of Concept Development Plan in a Commercial Services (C-3) district, for an approximate 2.06 acre area located at 3005 Verona-Mudlick Road, Boone County, Kentucky. The request is for: (1) a Change of Concept Development Plan to allow for an expanded outdoor storage area for the existing farm equipment business; and (2) Variances reducing the buffer yard width along the rear property line from sixty (60) feet, with a thirty (30) foot option, to ten (10) feet, reducing the buffer yard width along the side property line from sixty (60) feet, with a thirty (30) foot option, to twenty (20) feet, and reducing the buffer yard width along the front property line from ten (10) feet to three (3) feet.

Staff Member, Steve Lilly, read the Committee Report, which recommended approval based upon Findings of Facts and Conditions for the Change in Concept Development Plan and Variances. The owner has signed the Condition Letter. Mr. Lilly reminded the Board that the Planning Commission takes final action on the Variances.

Chairman Rolfsen asked if there was anyone in the audience who wanted to speak in favor or against the request?

Mr. Steve Berling, applicant, stated that he was present to answer any questions.

Seeing no further comment, **Mr. Hincks moved, by Resolution to the Boone County Fiscal Court, to recommend approval of the Change in Concept Development Plan and Variances based upon the Committee Report and Findings of Fact and Conditions. He noted that the Committee addressed the neighbor's concern about an adequate buffer. Mrs. Kegley seconded the motion and it passed unanimously.**

**ZONING MAP AMENDMENT - Corrin Gulick, Chairwoman, Michael Schwartz, Staff**

4. Request of **Charter Commercial, LLC (applicant)** for **Delaware Crossing, LLC (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

Staff Member, Michael Schwartz, read the Committee Report, which recommended denial based upon Findings of Facts of the Zoning Map Amendment.

Chairman Rolfsen asked if there was anyone in the audience who wanted to speak in favor or against the request?

Mr. Pat Manger, applicant, referred to his PowerPoint presentation. He showed color renderings of two types of products they planned to build on the site – patio homes (two bedroom ranch type) and a multi-family development. The overall plan is to connect sidewalks to local parks. The Map Amendment complies with the overall Goals and Objectives (Items #1,3,4,5,6 and 7) as well as Goal A of the Demographic Element. It is also consistent with the Environmental Section, Natural and Cultural Resources Element, Economy, Public Facilities and Transportation. The major changes in the area are the Airhub, DHL and Mazak. In regard to the social side, there is the recent NKADD Housing Study and discussion about a housing shortage. The traffic study has been completed. The conclusion was that there could be a slight impact on the existing road network. No improvements are required at the intersection of Cannondale and Camp Ernst Road. The connection to Lariat Way, etc. were previously approved as well as with Holbrook and Trevino. It is nothing new. It can happen today.

Mr. Jeff Schafer, 6501 Cannondale Drive, stated that concerns over this development are numerous. It doesn't meet the criteria in evaluating the Zoning Change request to putting 300 rental units with an established single family community. The applicant has completely disregarded the residents. There will be 2,100 trips daily plus delivery trucks, etc. and will cause major safety concerns. Instead of 25 single family homes, the applicant wanted to put in 157 one bedroom apartments. There is no traffic light at Camp Ernst Road. There can't be one because it will result in a useless roundabout. What about the additional strain on schools? Baby Boomers are not looking at one bedroom apartments. The project would result in 700 people in an area that is approved only for SR-1. The residential streets were built for only 200 people. What is the environmental impact of the increased density? The proposed project will impact their property values. In March, 2017, he paid a lot premium to Brookstone. If he would have known about future apartments, he would not have purchased his lot. He would not be here tonight if it wasn't for the neglect of the community by the developer. The community should have been finished. For the past 4 years, they have been no salesman or model home even though other homes were

being built in the area. There has not been a finished street or sidewalks in the past 7 years. There has not been an HOA meeting in the last 3 years. There is a pattern of mismanagement - Brookstone Homes, Madeira Place, LLC, Delaware Crossing, LLC and Charter Companies. In 2022, there was a conveyance plat done and it is stamped that the southern 24 acres of this lot in and of itself is non-buildable. Is that true? Mr. Schwartz replied yes, unless it is attached to another parcel that has road frontage or a public street is built on the property.

Seeing no further comment, **Ms. Gulick moved, by Resolution to the Boone County Fiscal Court, to recommend denial of the Zoning Map Amendment based upon the Committee Report and Findings of Fact. Mrs. Kegley seconded the motion.**

Ms. Gulick stated that she voted no on the request because she has a serious concern about the connection to KY 237. If this was approved previously and she voted for it, she was wrong because the proposal takes streets that were local and makes them operate as a collector. That can create driver expectations and safety concerns. The Traffic Impact Study does take into consideration traffic that uses the route as a cut through. Additional traffic from KY 237 and Oakbrook will use these streets in the subdivision. Only 3 years out, there is a significant decrease in the level of service turning left. It is already a "D" level service. A driver will accept a small gap to turn into traffic. It creates a safety concern. A traffic signal will cause more problems for the roundabout traffic. Because of this, she will vote no.

Chairman Rolfsen stated that the only thing that has changed in Boone County is the phase "affordable housing." What does it mean? Developers continue to use it to save Boone County. Why aren't Campbell and Kenton counties doing anything about it?

**Chairman Rolfsen asked for a vote on the original motion made by Ms. Gulick to deny the request and seconded by Mrs. Kegley. The motion to deny the request passed unanimously.**

**ZONING TEXT AMENDMENTS - Rick Lunnemann, Chairman, Michael Schwartz, Staff**

5. Request of the **Boone County Planning Commission Technical/Design Review Committee** to consider a Zoning Text Amendment modifying the definitions of "Electric Vehicle Charging Station, Public" and "Gasoline Filling Station".

Staff Member, Michael Schwartz, read the Committee Report, which recommended approval based upon Findings of Facts for the Zoning Text Amendment.

Seeing no further comment, **Mrs. Goetting moved, by Resolution to the Boone County Fiscal Court, City of Florence, City of Union and City of Walton to recommend approval of the Zoning Text Amendment based upon the Committee Report and Findings of Fact. Mr. Turner seconded the motion and it passed unanimously.**

**TECHNICAL/DESIGN REVIEW – Rick Lunnemann, Chairman, Steve Lilly, Staff**

6. Microtel Inn & Suites – 7490 Woodspoint Drive

Mr. Steve Lilly presented the request. The applicant wants to repaint the building. He showed what the building looks like now and with the paint work completed. The Committee met prior to

the Business Meeting and recommended approval with one condition – the yellow stripe will be removed. It is LED lighting that is related to signage.

**Mr. Lunnemann moved to approve the request with one condition. Mr. Hincks seconded the motion and it passed unanimously.**

**G. EXECUTIVE DIRECTOR'S REPORT:**

Mr. Kevin P. Costello, AICP, explained that it was time to renew the subscription for aerial photography. He referred to his September 30, 2024 memo to the Board. The total cost for 2 flights, etc., is \$32,591 for FY 2024-2025. This amount is in the current Planning Commission budget and a map shows the flight area. Mr. Steve Gay explained the coverage area. **Mrs. Steele moved to authorize Mr. Costello to sign a contract with NearMap not to exceed \$32,591 according to the terms outlined in the memo. Mrs. Goetting seconded the motion and it passed unanimously.**

**H. COMMITTEE REPORTS:**

- Airport (Randy Bessler)  
No Report
- Administrative/Personnel (Tom Szurlinski)  
No Report
- Enforcement (Tom Szurlinski)  
No Report
- Long-Range Planning/Comprehensive Plan (Bob Schwenke)  
No Report
- Technical/Design Review (Rick Lunnemann)  
No Report
- Executive (Charlie Rolfsen)  
No Report

**I. CHAIRMAN'S REPORT: (Charlie Rolfsen)**

Chairman Rolfsen announced the birth of this third grandchild.

**J. OKI REPORT: (Randy Bessler)  
No Report**

**K. ADJOURNMENT:**

There being no further business to come before the Planning Commission, **Mr. Bessler moved to adjourn the meeting. Mr. Harper seconded the motion and it passed unanimously. The meeting was adjourned at 8:00 P.M.**

APPROVED



Charlie Rolfsen  
Chairman

Attest:



Kevin P. Costello, AICP  
Executive Director

# SUPPORTING INFORMATION

final report

July 1, 2024

## Traffic Assessment

Hickory Burlington & Poplar Pointe Apartments  
Burlington, KY

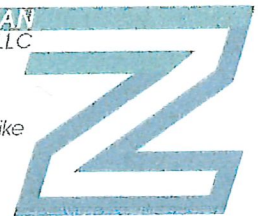
Prepared for

**Boone County Planning Commission**



**DIANE B. ZIMMERMAN**  
Traffic Engineering, LLC

12803 High Meadows Pike  
Prospect, KY 40059  
502.648.1858  
dianeبزim@att.net



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## INTRODUCTION

The preliminary site plan for apartments on Holbrook Lane in Burlington, KY shows 157 apartment units and 139 rental townhouses. **Figure 1** displays a map of the site. Access to the development will be from Holbrook Lane, Watson Lane, and Lariat Way. The purpose of this study is to identify the traffic characteristics of this development and the adjacent road.

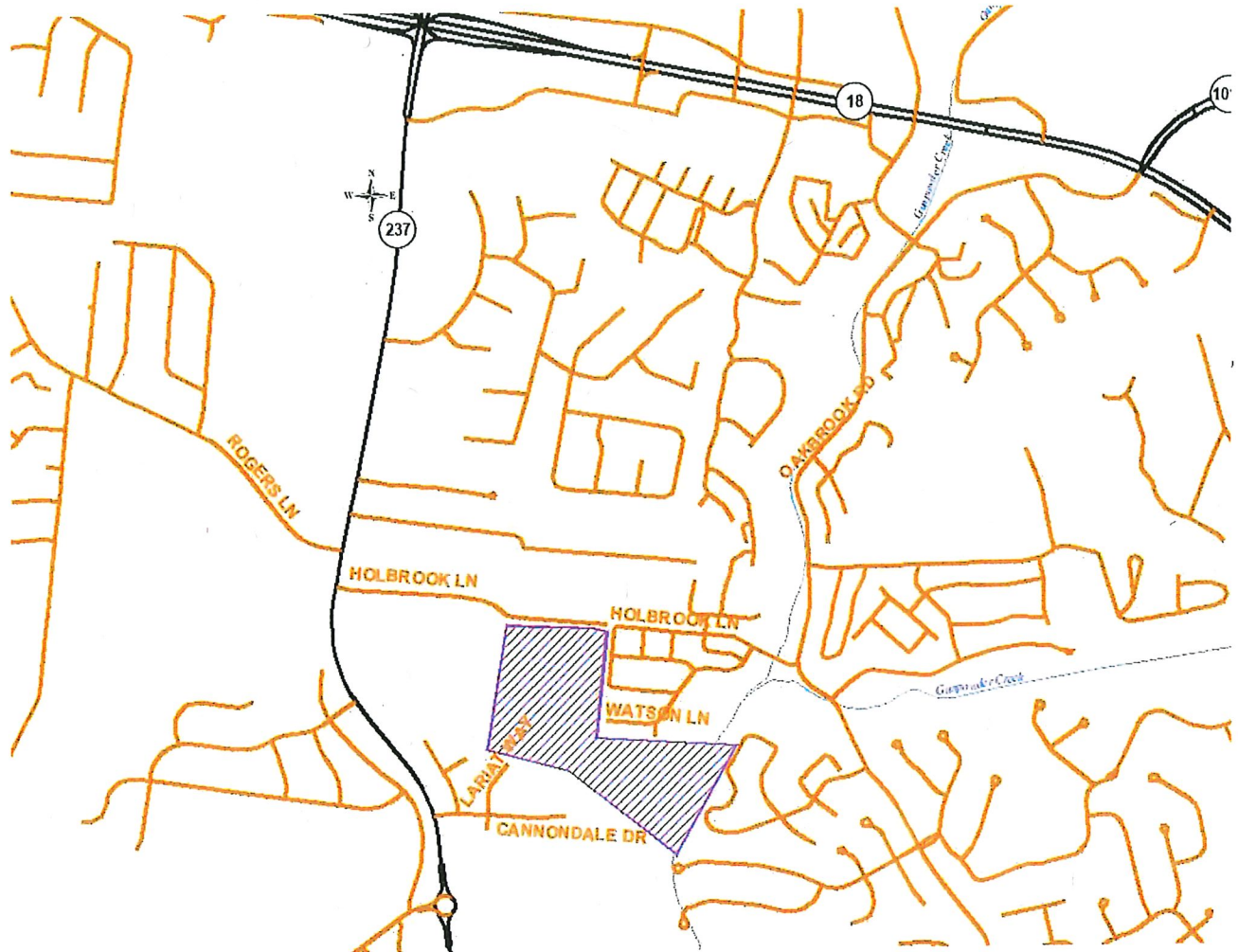


Figure 1. Site Map

## EXISTING CONDITIONS

Holbrook Lane is maintained by Boone County and is part of Shaker Run subdivision. The road has two eleven-foot lanes with curb and gutter, and an eight-foot multi-use path along the south side. The road is classified as a local road. The speed limit is 25 mph. The intersection with Oakwood Road is controlled with a stop sign on Holbrook Lane. There is a right and left turn lane on Holbrook Lane.

Hickory Burlington & Poplar Pointe Apartments  
Traffic Assessment

Lariat Way is maintained by Boone County and is part of Valley Creek Farms subdivision. The road has two eleven-foot lanes with curb and gutter, and sidewalks. The road is classified as a local road. The speed limit is 25 mph. The intersection with Cannondale Drive is controlled with a stop sign on Cannondale Drive. There are no turn lanes.

## TRIP GENERATION

The Institute of Transportation Engineers Trip Generation Manual, 11<sup>th</sup> Edition contains trip generation rates for a wide range of developments. The land uses of “Single-Family Attached (215)” and “Multifamily Housing (Low-Rise) (220)” were reviewed and determined to be the best match. The trip generation results are listed in **Table 1**. This distribution is based upon the existing traffic counts on KY 237 and Oakwood Road. **Figure 2** shows the trip distribution throughout the road network during the peak hours.

**Table 1. Peak Hour Trips Generated by Site**

Land Use	A.M. Peak Hour			P.M. Peak Hour			Daily
	Trips	In	Out	Trips	In	Out	Trips
Single Family Attached (139 units)	67	17	50	79	47	32	1,009
Multifamily Low Rise (157 units)	72	17	55	88	55	33	1,082
Total	139	34	105	167	102	65	2,091

Hickory Burlington & Poplar Pointe Apartments  
Traffic Assessment

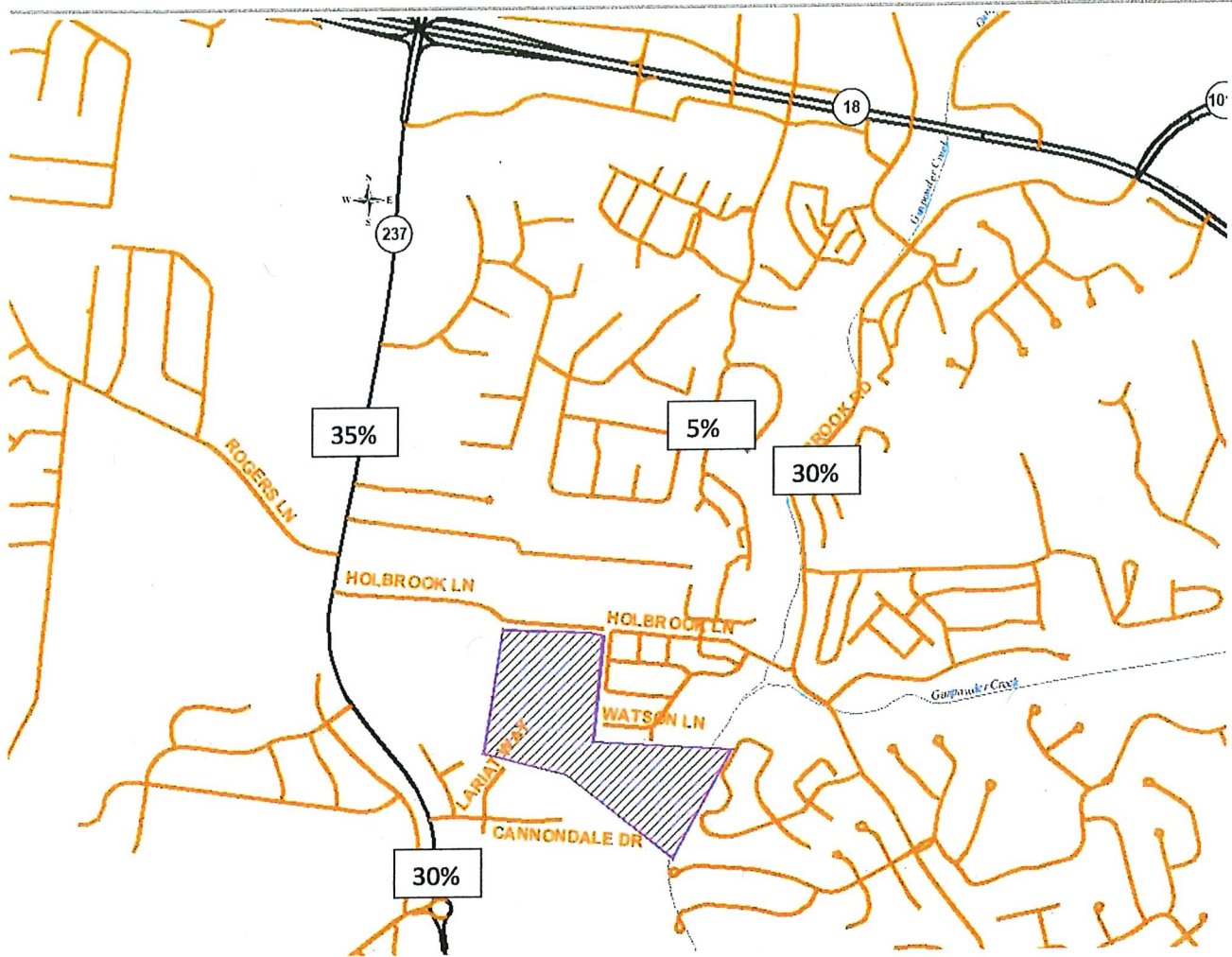
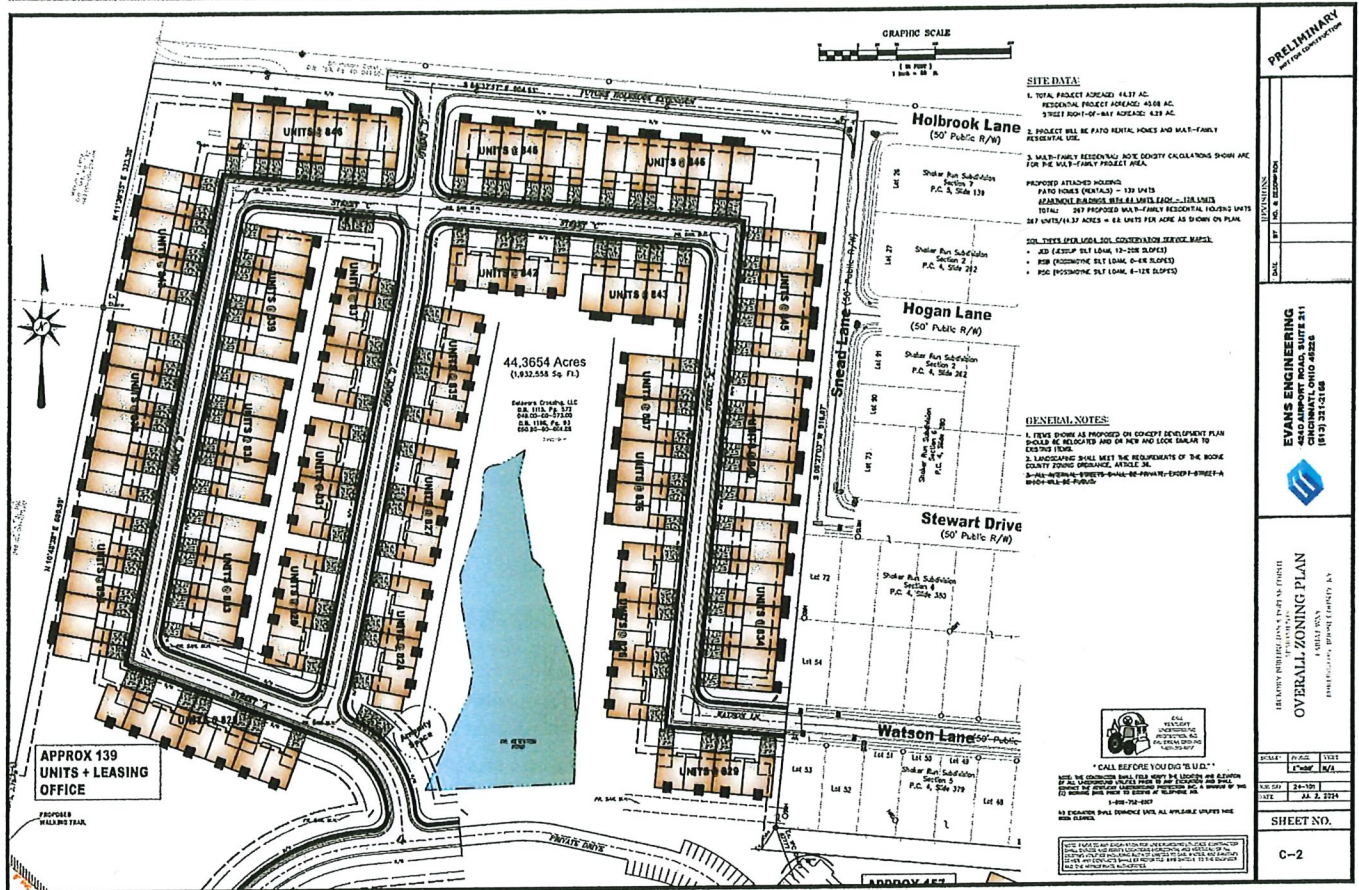
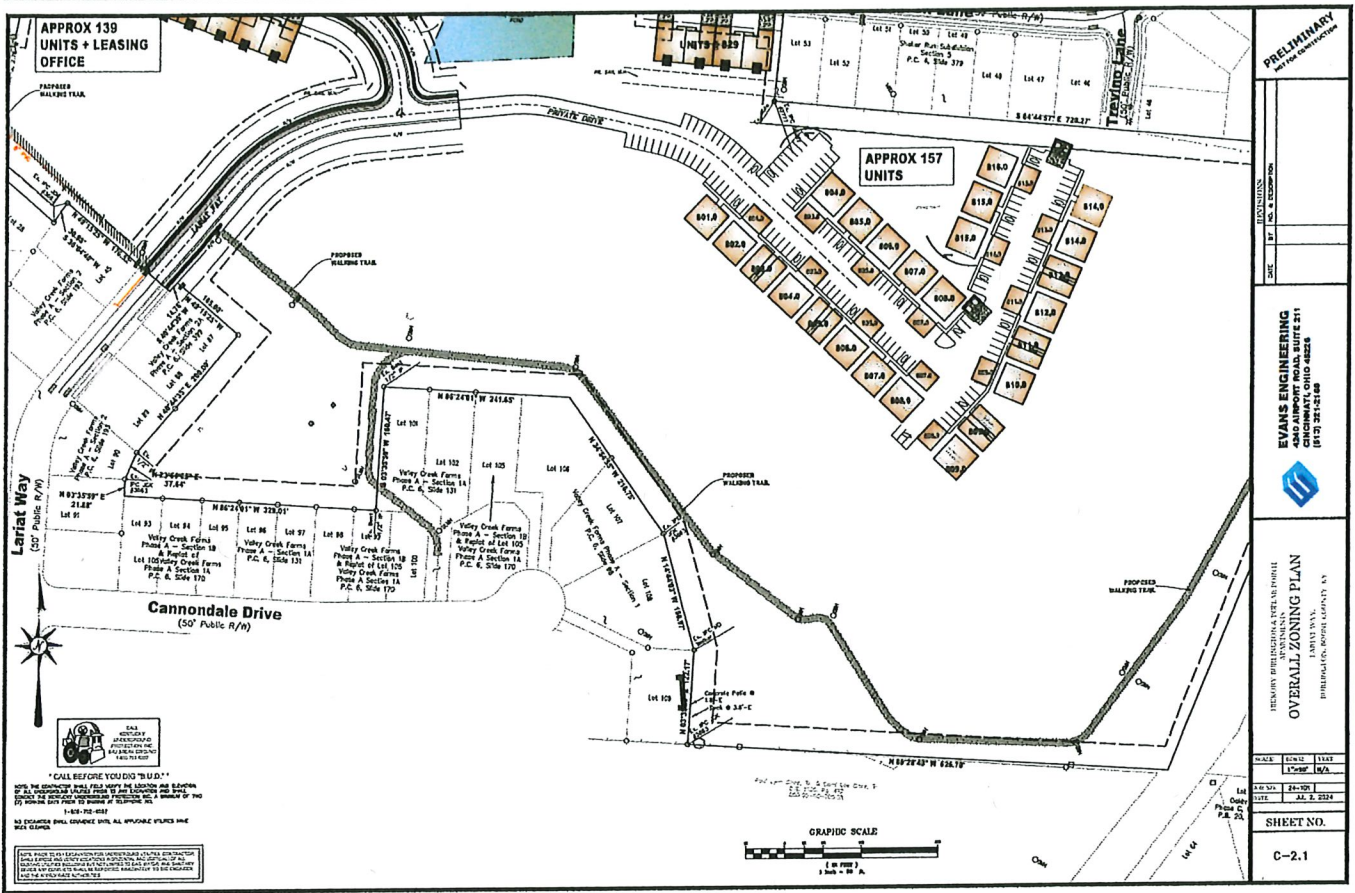


Figure 2. Peak Hour Trips Distribution

# Hickory Burlington & Poplar Pointe Apartments Traffic Assessment



# Hickory Burlington & Poplar Pointe Apartments Traffic Assessment



**PRELIMINARY**  
NOT FOR CONSTRUCTION

DATE: 07/17/2024 BY: J. ZIMMERMAN

**EVANS ENGINEERING**  
4840 AIRPORT ROAD, SUITE 211  
DUBLIN, VA 22028  
(703) 261-2100

**OVERALL ZONING PLAN**  
HICKORY BURLINGTON & POPLAR POINTE APARTMENTS  
LARIAT WAY  
DUBLIN, VA 22028

DATE	BY	REVISION
07/17/2024	J. ZIMMERMAN	ISSUE FOR REVIEW

DATE	BY	REVISION
07/17/2024	J. ZIMMERMAN	ISSUE FOR REVIEW

**SHEET NO.**  
C-2.1

final report

September 10, 2024

## Traffic Impact Study

Hickory Burlington & Poplar Pointe Apartments  
Burlington, KY

Prepared for

Boone County Planning Commission



DIANE B. ZIMMERMAN  
Traffic Engineering, LLC

12803 High Meadows Pike  
Prospect, KY 40059  
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## INTRODUCTION

The preliminary site plan for apartments on Holbrook Lane in Burlington, KY shows 157 apartment units and 139 rental townhouses. **Figure 1** displays a map of the site. Access to the development will be from Holbrook Lane, Watson Lane, and Lariat Way. The purpose of this study is to examine the traffic impacts of the development upon the adjacent highway system. For this study, the impact area was defined to be the intersections of KY 237 at Cannondale Drive, and Holbrook Lane at Watson Lane and Oakbrook Road.

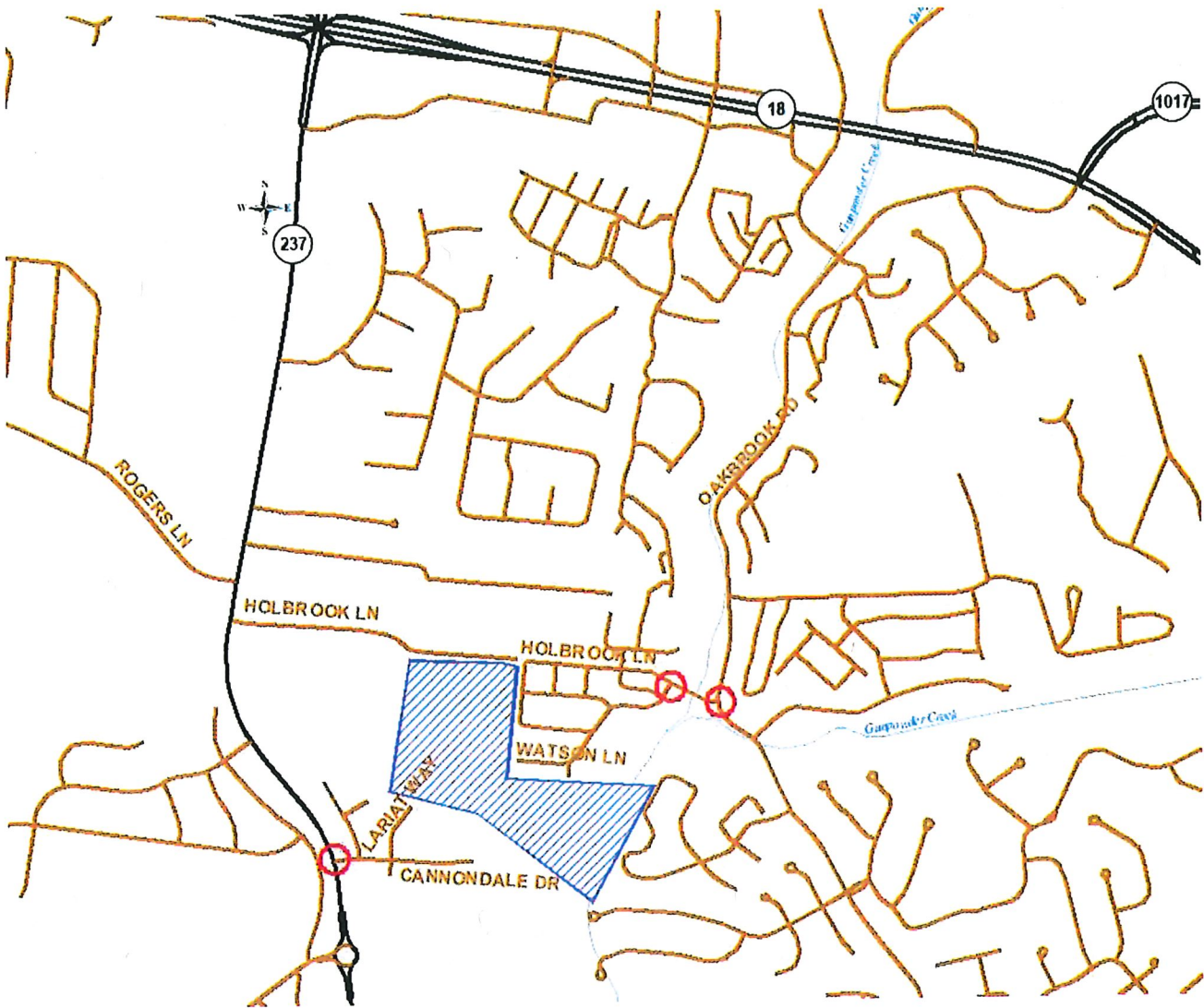


Figure 1. Site Map

## EXISTING CONDITIONS

Holbrook Lane is maintained by Boone County and is part of Shaker Run subdivision. The road has two eleven-foot lanes with curb and gutter, and an eight-foot multi-use path along the south side. The road is classified as a local road.

Hickory Burlington & Poplar Pointe Apartments  
 Traffic Impact Study

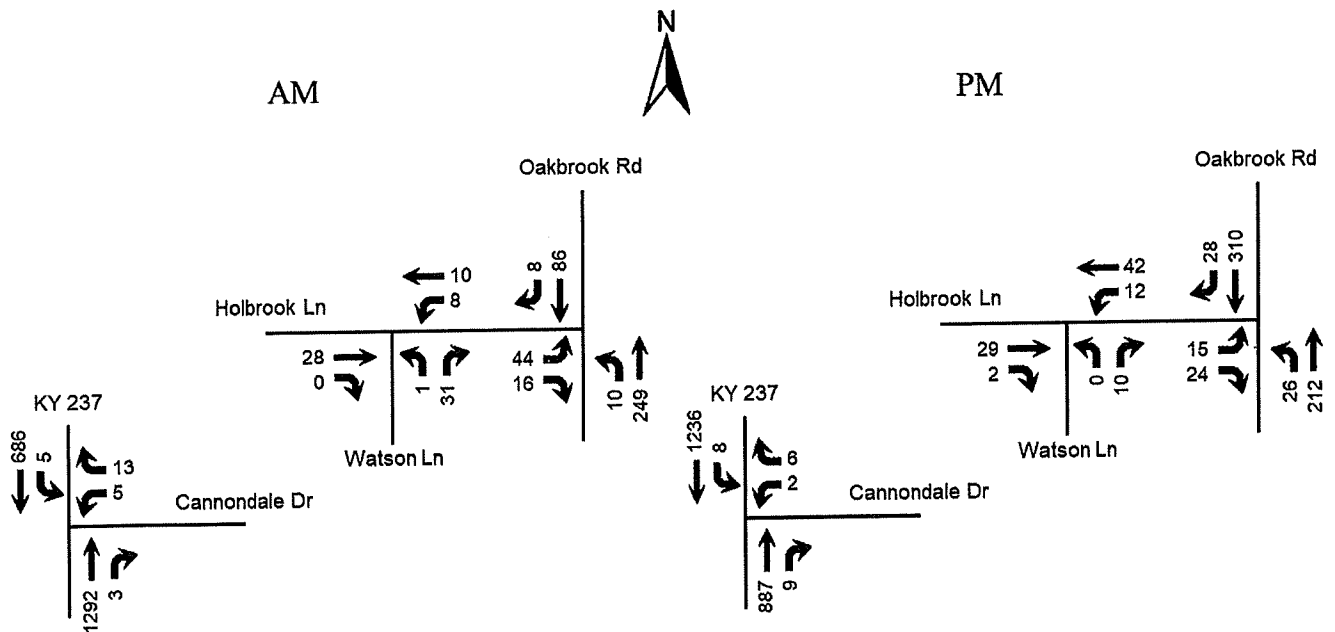
The speed limit is 25 mph. The intersection with Oakbrook Road is controlled with a stop sign on Holbrook Lane. There is a right and left turn lane on Holbrook Lane.

Lariat Way is maintained by Boone County and is part of Valley Creek Farms subdivision. The road has two eleven-foot lanes with curb and gutter, and sidewalks. The road is classified as a local road. The speed limit is 25 mph. The intersection with Cannondale Drive is controlled with a stop sign on Cannondale Drive. There are no marked turn lanes.

KY 237, Camp Ernst Road, is a state-maintained road with an estimated 2024 Average Daily Traffic (ADT) of 19,500 vehicles per day between Camp Ernst Road and KY 18, as estimated from the Kentucky Transportation Cabinet (KYTC) 2023 count at station G89. The road has four eleven-foot lanes, curb and gutter, and a 13-foot two-way left-turn lane through the study area. The speed limit is 45 mph. There are sidewalks on both sides.

Oakbrook Road, is a locally maintained road with an estimated 2024 Average Daily Traffic (ADT) of 4,900 vehicles per day between KY 18 and KY 237, as estimated from the Kentucky Transportation Cabinet (KYTC) 2023 count at station M08. The road has two eleven-foot lanes, with a stabilized shoulder, through the study area. The speed limit is 35 mph. There are sidewalks on the west side.

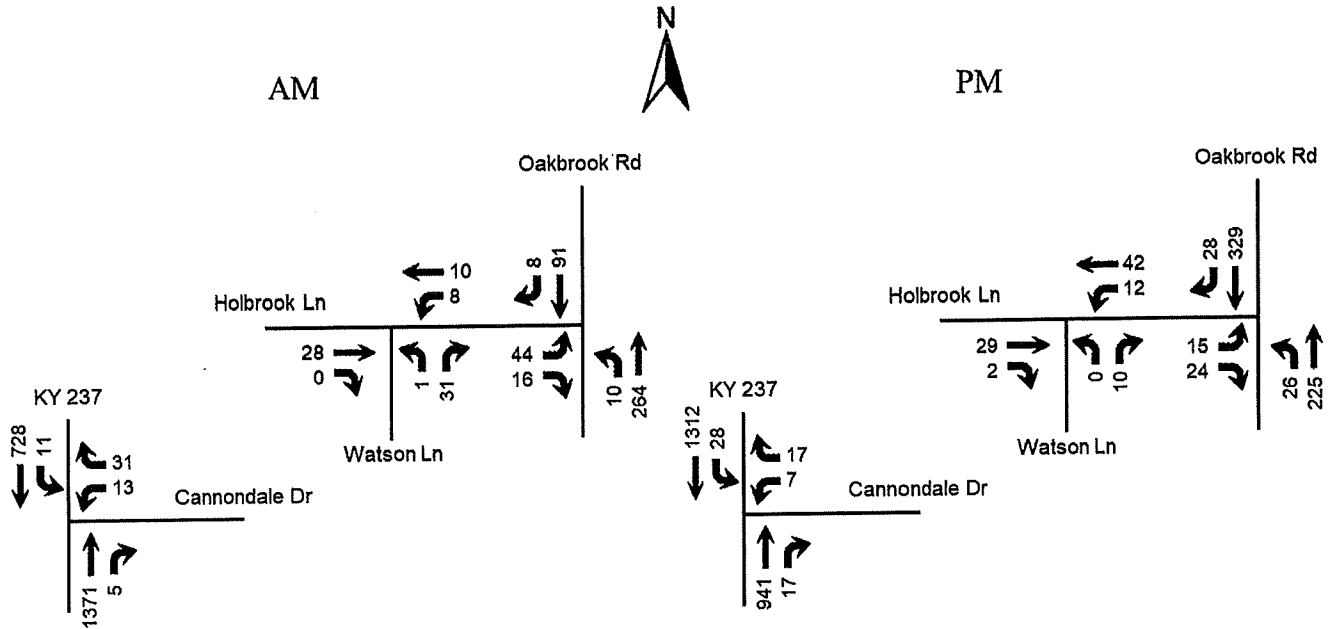
Peak hour traffic counts for the intersections were obtained on Tuesday, August 27, 2024. The a.m. peak hour occurred between 7:00 and 8:00, and the p.m. peak hour varied. **Figure 2** illustrates the 2024 a.m. and p.m. peak hour traffic volumes. The Appendix contains the full count data.



**Figure 2. Existing Peak Hour Volumes**

### FUTURE CONDITIONS

The project completion date is 2027. An annual growth rate of 2.0 percent was applied to the 2024 thru volumes on KY 237 and Oakbrook Road. This was determined by the historical growth at KYTC stations G89 and B91. Trip generation for the remaining 42 lots with access to Cannondale Drive was included. **Figure 3** displays the 2027 No Build peak hour volumes.



**Figure 3. 2027 No Build Peak Hour Volumes**

### TRIP GENERATION

The Institute of Transportation Engineers Trip Generation Manual, 11<sup>th</sup> Edition contains trip generation rates for a wide range of developments. The land uses of “Single-Family Attached (215)” and “Multifamily Housing (Low-Rise) (220)” were reviewed and determined to be the best match. The trip generation results are listed in **Table 1**. The trips were assigned to the highway network with the percentages shown in **Figure 4**. The study assumes 18 units will utilize the Watson Lane. The distribution is based upon the existing traffic counts on KY 237 and Oakwood Road. **Figure 5** shows the trips generated by this development and distributed throughout the road network during the peak hours. **Figure 6** displays the individual turning movements for the peak hours when the development is completed.

**Table 1. Peak Hour Trips Generated by Site**

Land Use	A.M. Peak Hour			P.M. Peak Hour			Daily Trips
	Trips	In	Out	Trips	In	Out	
Single Family Attached (139 units)	67	17	50	79	47	32	1,009
Multifamily Low Rise (157 units)	72	17	55	88	55	33	1,082
<b>Total</b>	<b>139</b>	<b>34</b>	<b>105</b>	<b>167</b>	<b>102</b>	<b>65</b>	<b>2,091</b>



**Figure 4. Peak Hour Trips Distribution**

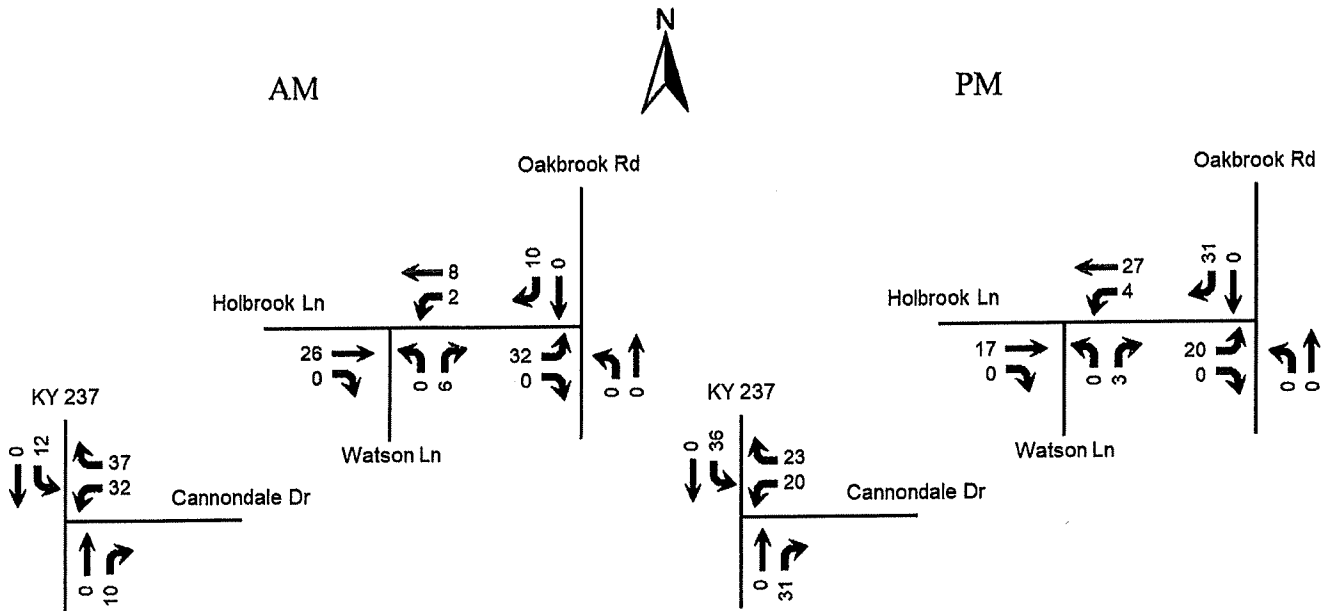


Figure 5. Peak Hour Trips Generated by Site

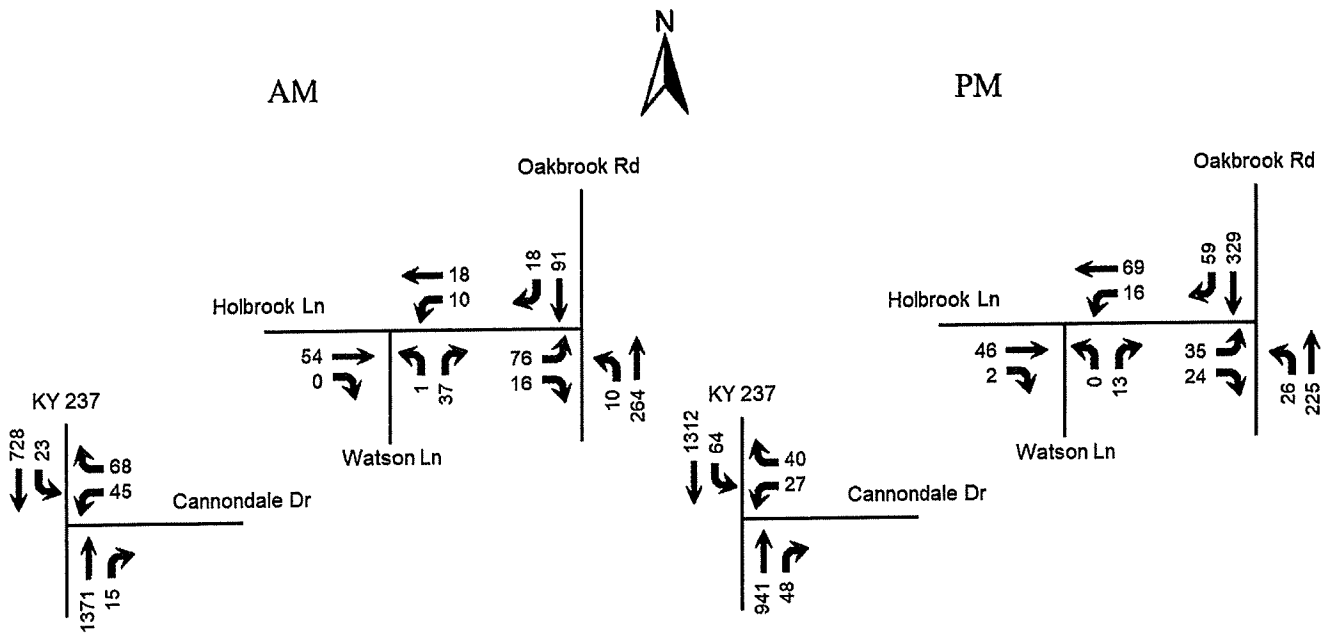


Figure 6. 2027 Build Peak Hour Volumes

## ANALYSIS

The qualitative measure of operation for a roadway facility or intersection is evaluated by assigning a “Level of Service”. Level of Service is a ranking scale from A through F, “A” is the best operating condition and “F” is the worst. Level of Service results depend upon the facility that is analyzed. In this case, the Level of Service is based upon the total delay experienced for lanes at stop-controlled intersections.

To evaluate the impact of the proposed development, the vehicle delays at the intersections were determined using procedures detailed in the Highway Capacity Manual, 7<sup>th</sup> edition. Future delays and Level of Service were determined for the intersections using the HCS Two-Way Stop Controlled (version 2024) software. The delays and Level of Service are summarized in **Table 2**.

**Table 2. Peak Hour Level of Service**

Approach	A.M.			P.M.		
	2024 Existing	2027 No Build	2027 Build	2024 Existing	2027 No Build	2027 Build
<b>KY 237 at Cannondale Drive</b>						
Cannondale Drive Westbound	C 21.0	C 23.4	D 34.9	B 14.6	C 16.4	C 20.9
KY 237 Southbound (left)	B 12.0	B 12.7	B 13.0	A 9.8	B 10.3	B 10.7
<b>Oakbrook Road at Holbrook Lane</b>						
Holbrook Lane Eastbound	B 10.7	B 10.9	B 11.6	B 11.5	B 11.8	B 13.0
Oakbrook Road Northbound (left)	A 7.4	A 7.4	A 7.4	A 8.0	A 8.1	A 8.2
<b>Holbrook Lane at Watson Lane</b>						
Holbrook Lane Westbound (left)	A 7.4	A 7.4	A 7.5	A 7.3	A 7.3	A 7.3
Watson Lane Northbound	A 8.7	A 8.7	A 8.9	A 8.5	A 8.5	A 8.6

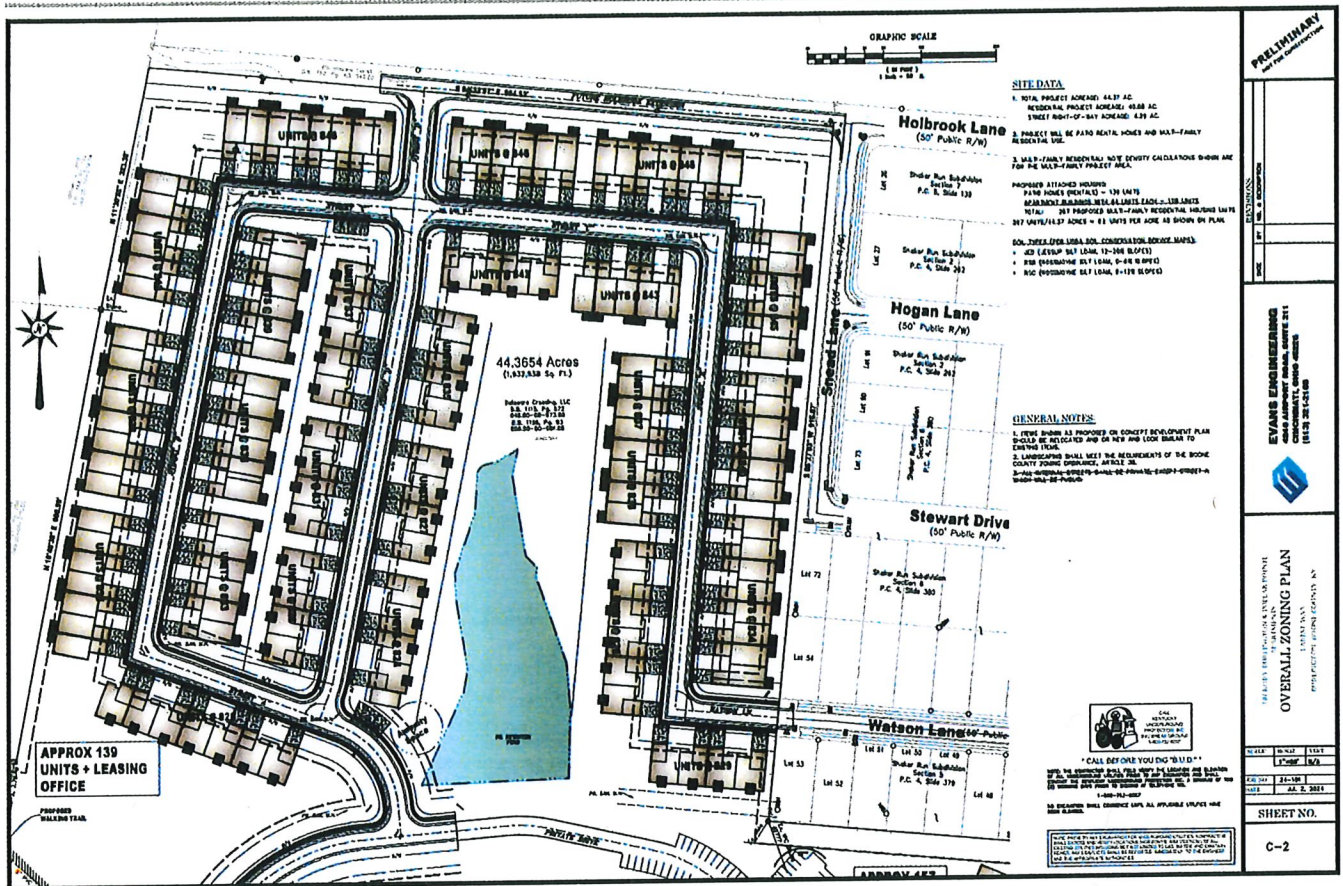
*Key: Level of Service, Delay in seconds per vehicle*

## CONCLUSIONS

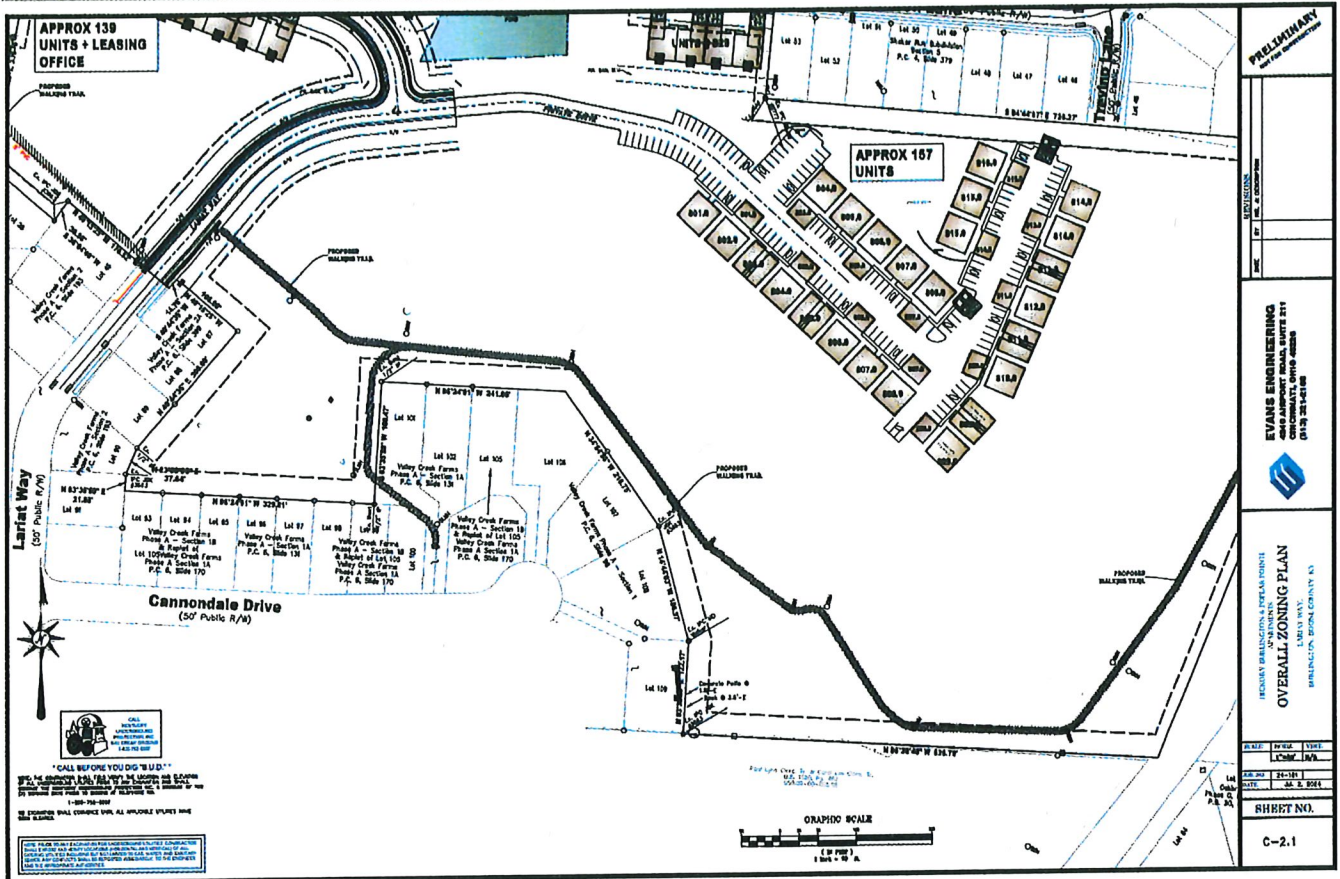
Based upon the volume of traffic generated by the development and the amount of traffic forecasted for the year 2027, there will be a slight impact to the existing highway network. No improvements are required at the intersections to maintain an acceptable level of service.

## APPENDIX

# Hickory Burlington & Poplar Pointe Apartments Traffic Impact Study



Hickory Burlington & Poplar Point Apartments  
Traffic Impact Study



**PRELIMINARY**

**EVANS ENGINEERING**  
4000 AMPORT ROAD, SUITE 211  
LUMMA WAY, WASHINGTON, DC 20746  
(301) 251-1418

**OVERALL ZONING PLAN**  
LUMMA WAY  
WASHINGTON, DISTRICT OF COLUMBIA, D.C.

DATE	BY	REVISED
11/11/11	EVANS	

**SHEET NO.**  
C-2.1

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

TRAFFIC COUNTS

Classified Turn Movement Count | All vehicles



Burlington, KY

Site 1  
KY-237 Camp Ernst Rd (South)  
KY-237 Camp Ernst Rd (North)

Date  
Tuesday, August 27, 2024

Weather  
Fair  
83°F

Lat/Long  
39.002805°, -84.701596°  
[Click here for Map](#)

[Click here for Detailed Weather](#)

Cannondale Dr



0700 - 0900 (Weekday 2h Session) (08-27-2024)  
All vehicles

TIME	Northbound				Southbound			
	KY-237 Camp Ernst Rd (South)		KY-237 Camp Ernst Rd (North)		KY-237 Camp Ernst Rd (South)		KY-237 Camp Ernst Rd (North)	
	Thru 1.1	Right 1.2	U-Turn 1.3	App Total	Left 1.4	Thru 1.5	U-Turn 1.6	App Total
0700 - 0715	296	1	0	297	2	214	0	216
0715 - 0730	339	0	0	339	1	174	0	175
0730 - 0745	346	1	0	347	0	166	0	166
0745 - 0800	311	1	0	312	3	132	0	135
Hourly Total	1292	3	0	1295	6	686	0	692
0800 - 0815	246	0	0	246	0	102	0	102
0815 - 0830	255	0	0	255	4	107	0	111
0830 - 0845	214	1	0	215	2	133	0	135
0845 - 0900	193	0	0	193	3	135	0	138
Hourly Total	908	1	0	909	9	477	0	486
Grand Total	2200	4	0	2204	15	1163	0	1178
Approach %	99.82	0.18	0.00	-	1.27	98.73	0.00	-
Intersection %	64.37	0.12	0.00	64.48	0.44	34.03	0.00	34.46
Heavy Vehicle %	5	25	-	5	13	5	-	5
PHF	0.93	0.75	0.00	0.93	0.50	0.80	0.00	0.80

Westbound Cannondale Dr				
Left 1.7	Right 1.8	U-Turn 1.9	App Total	Int Total
3	6	0	9	522
1	2	0	3	517
0	4	0	4	517
1	1	0	2	449
5	13	0	18	2005
0	2	0	2	350
2	6	0	8	374
2	5	0	7	357
0	1	0	1	332
4	14	0	18	1413
9	27	0	36	3418
25.00	75.00	0.00	-	
0.26	0.79	0.00	1.05	
11	7	-	8	5
0.42	0.54	0.00	0.50	0.96

1600 - 1800 (Weekday 2h Session) (08-27-2024)  
All vehicles

TIME	Northbound				Southbound			
	KY-237 Camp Ernst Rd (South)		KY-237 Camp Ernst Rd (North)		KY-237 Camp Ernst Rd (South)		KY-237 Camp Ernst Rd (North)	
	Thru 1.1	Right 1.2	U-Turn 1.3	App Total	Left 1.4	Thru 1.5	U-Turn 1.6	App Total
1600 - 1615	260	0	0	260	1	278	0	279
1615 - 1630	232	2	0	234	1	279	0	280
1630 - 1645	200	0	0	200	2	299	0	301
1645 - 1700	202	0	0	202	1	328	0	329
Hourly Total	894	2	0	896	5	1184	0	1189
1700 - 1715	239	2	0	241	4	274	0	278
1715 - 1730	227	5	0	232	1	316	0	317
1730 - 1745	219	2	0	221	2	318	0	320
1745 - 1800	209	0	0	209	3	230	0	233
Hourly Total	894	9	0	903	10	1138	0	1148
Grand Total	1788	11	0	1799	15	2322	0	2337
Approach %	99.39	0.61	0.00	-	0.64	99.36	0.00	-
Intersection %	43.07	0.26	0.00	43.34	0.36	55.94	0.00	56.30
Heavy Vehicle %	4	0	-	4	0	2	-	2
PHF	0.93	0.45	0.00	0.93	0.50	0.94	0.00	0.95

Westbound Cannondale Dr				
Left 1.7	Right 1.8	U-Turn 1.9	App Total	Int Total
0	1	0	1	540
1	1	0	2	516
1	2	0	3	504
0	3	0	3	534
2	7	0	9	2094
2	0	0	2	521
0	1	0	1	550
0	2	0	2	543
0	1	0	1	443
2	4	0	6	2057
4	11	0	15	4151
26.67	73.33	0.00	-	
0.10	0.26	0.00	0.36	
0	0	-	0	3
0.25	0.50	0.00	0.67	0.98

# Hickory Burlington & Poplar Pointe Apartments Traffic Impact Study

## Classified Turn Movement Count || All vehicles



Burlington, KY

Site 2  
Watson Ln  
  
Holbrook Ln (West)  
Holbrook Ln (East)

Date  
Tuesday, August 27, 2024  
  
Lat/Long  
39.006616°, -84.692065°  
[Click here for Map](#)

Weather  
Fair  
83°F  
[Click here for Detailed Weather](#)

0700 - 0900 (Weekday 2h Session) (08-27-2024)  
All vehicles

TIME	Northbound Watson Ln			
	Left 2.1	Right 2.2	U-Turn 2.3	App Total
0700 - 0715	0	12	0	12
0715 - 0730	0	10	0	10
0730 - 0745	0	2	0	2
0745 - 0800	1	7	0	8
Hourly Total	1	31	0	32
0800 - 0815	0	0	0	0
0815 - 0830	1	7	0	8
0830 - 0845	0	3	0	3
0845 - 0900	0	2	0	2
Hourly Total	1	12	0	13
Grand Total	2	43	0	45
Approach %	4.44	95.56	0.00	-
Intersection %	1.59	34.13	0.00	35.71
Heavy Vehicle %	0	2	-	2
PHF	0.25	0.65	0.00	0.67

Eastbound Holbrook Ln (West)				Westbound Holbrook Ln (East)				U-Turn 2.9	App Total	Int Total
Thru 2.4	Right 2.5	U-Turn 2.6	App Total	Left 2.7	Thru 2.8	U-Turn 2.9	App Total			
8	0	0	8	1	5	0	6	26		
10	0	0	10	1	3	0	4	24		
5	0	0	5	3	1	0	4	11		
5	0	0	5	3	1	0	4	17		
28	0	0	28	8	10	0	18	78		
4	1	0	5	4	4	0	8	13		
5	0	0	5	2	3	0	5	18		
4	0	0	4	3	0	0	3	10		
4	0	0	4	0	1	0	1	7		
17	1	0	18	9	8	0	17	48		
45	1	0	46	17	18	0	35	126		
97.83	2.17	0.00	-	48.57	51.43	0.00	-	-		
35.71	0.79	0.00	36.51	13.49	14.29	0.00	27.78	-		
0	0	-	0	12	0	-	6	2		
0.70	0.00	0.00	0.70	0.67	0.50	0.00	0.75	0.75		

1600 - 1800 (Weekday 2h Session) (08-27-2024)  
All vehicles

TIME	Northbound Watson Ln			
	Left 2.1	Right 2.2	U-Turn 2.3	App Total
1600 - 1615	0	5	0	5
1615 - 1630	0	2	0	2
1630 - 1645	0	3	0	3
1645 - 1700	0	4	0	4
Hourly Total	0	14	0	14
1700 - 1715	0	1	0	1
1715 - 1730	0	2	0	2
1730 - 1745	0	1	0	1
1745 - 1800	0	5	0	5
Hourly Total	0	9	0	9
Grand Total	0	23	0	23
Approach %	0.00	100.00	0.00	-
Intersection %	0.00	12.23	0.00	12.23
Heavy Vehicle %	-	9	-	9
PHF	0.00	0.63	0.00	0.63

Eastbound Holbrook Ln (West)				Westbound Holbrook Ln (East)				U-Turn 2.9	App Total	Int Total
Thru 2.4	Right 2.5	U-Turn 2.6	App Total	Left 2.7	Thru 2.8	U-Turn 2.9	App Total			
9	0	0	9	5	12	1	18	32		
7	0	0	7	2	9	0	11	20		
9	0	0	9	2	11	0	13	25		
7	2	0	9	4	8	0	12	25		
32	2	0	34	13	40	1	54	102		
5	0	0	5	4	9	0	13	19		
8	0	0	8	2	14	0	16	26		
5	1	0	6	8	6	1	15	22		
6	1	0	7	3	4	0	7	19		
24	2	0	26	17	33	1	51	86		
56	4	0	60	30	73	2	105	188		
93.33	6.67	0.00	-	28.57	69.52	1.90	-	-		
29.79	2.13	0.00	31.91	15.96	38.83	1.06	55.85	-		
0	0	-	0	0	0	0	0	1		
0.81	0.25	0.00	0.86	0.75	0.75	0.00	0.84	0.91		

# Hickory Burlington & Poplar Pointe Apartments Traffic Impact Study



## Classified Turn Movement Count || All vehicles

Burlington, KY

Site 3  
Oakbrook Dr (South)  
Oakbrook Dr (North)  
Holbrook Ln



Date  
Tuesday, August 27, 2024

Weather

Fair  
83°F

[Click here for Detailed Weather](#)

Lat/Long  
39.006195°, -84.690733°  
[Click here for Map](#)



### 0700 - 0900 (Weekday 2h Session) (08-27-2024)

All vehicles

TIME	Northbound				Southbound				Eastbound				Int Total
	Oakbrook Dr (South)		Oakbrook Dr (North)		Oakbrook Dr (North)		Oakbrook Dr (North)		Holbrook Ln		Holbrook Ln		
	Left 3.1	Thru 3.2	U-Turn 3.3	App Total	Thru 3.4	Right 3.5	U-Turn 3.6	App Total	Left 3.7	Right 3.8	U-Turn 3.9	App Total	
0700 - 0715	3	61	0	64	17	3	0	20	15	4	0	19	103
0715 - 0730	3	67	0	70	16	1	0	17	15	6	0	21	108
0730 - 0745	3	55	0	58	23	1	0	24	6	2	0	8	90
0745 - 0800	1	66	0	67	30	3	0	33	8	4	0	12	112
Hourly Total	10	249	0	259	86	8	0	94	44	16	0	60	413
0800 - 0815	1	57	0	58	14	6	0	20	2	2	0	4	82
0815 - 0830	1	52	0	53	21	4	0	25	7	5	0	12	90
0830 - 0845	0	49	0	49	28	3	0	31	5	2	0	7	87
0845 - 0900	1	44	0	45	20	0	0	20	3	3	0	6	71
Hourly Total	3	202	0	205	83	13	0	96	17	12	0	29	330
Grand Total	13	451	0	464	169	21	0	190	61	28	0	89	743
Approach %	2.80	97.20	0.00	-	88.95	11.05	0.00	-	68.54	31.46	0.00	-	-
Intersection %	1.75	60.70	0.00	62.45	22.75	2.83	0.00	25.57	8.21	3.77	0.00	11.98	2
Heavy Vehicle %	0	1	-	1	5	10	-	5	2	0	-	1	2
PHF	0.83	0.93	0.00	0.93	0.72	0.67	0.00	0.71	0.73	0.67	0.00	0.71	0.92

### 1600 - 1800 (Weekday 2h Session) (08-27-2024)

All vehicles

TIME	Northbound				Southbound				Eastbound				Int Total
	Oakbrook Dr (South)		Oakbrook Dr (North)		Oakbrook Dr (North)		Oakbrook Dr (North)		Holbrook Ln		Holbrook Ln		
	Left 3.1	Thru 3.2	U-Turn 3.3	App Total	Thru 3.4	Right 3.5	U-Turn 3.6	App Total	Left 3.7	Right 3.8	U-Turn 3.9	App Total	
1600 - 1615	5	42	0	47	60	11	0	71	8	6	0	14	132
1615 - 1630	4	61	0	65	65	7	0	72	2	7	0	9	146
1630 - 1645	8	64	0	72	70	6	0	76	3	9	0	12	160
1645 - 1700	5	41	0	46	84	5	0	89	4	6	1	11	146
Hourly Total	22	208	0	230	279	29	0	308	17	28	1	46	584
1700 - 1715	5	53	0	58	72	9	0	81	3	3	0	6	145
1715 - 1730	8	54	0	62	84	8	0	92	4	6	0	10	164
1730 - 1745	6	52	0	58	75	9	0	84	3	4	0	7	149
1745 - 1800	6	47	0	53	69	1	0	70	6	5	0	11	134
Hourly Total	25	206	0	231	300	27	0	327	16	18	0	34	592
Grand Total	47	414	0	461	579	56	0	635	33	46	1	80	1176
Approach %	10.20	89.80	0.00	-	91.18	8.82	0.00	-	41.25	57.50	1.25	-	-
Intersection %	4.00	35.20	0.00	39.20	49.23	4.76	0.00	54.00	2.81	3.91	0.09	6.80	1
Heavy Vehicle %	0	0	-	0	1	0	-	0	6	0	0	3	3
PHF	0.81	0.83	0.00	0.83	0.92	0.78	0.00	0.92	0.88	0.67	0.25	0.81	0.94

 **TIS Simplified Traffic Forecast**

Count Year	2024
Opening Year	2027
Design Year	2037
Years Back	15

Number of Counts 13

Growth Rate 0.21%

**KYTC Traffic Count Station #1**

KYTC Traffic Count Station #1	
STA ID	008G89
Paste Count Data Here	
2024	
2023	19128
2022	
2021	
2020	16527
2019	
2018	
2017	18491
2016	
2015	
2014	13135
2013	
2012	
2011	15300
2010	
2009	
2008	13300
2007	
2006	
2005	12700
2004	
2003	
2002	13600
2001	
2000	
1999	10200

**KYTC Traffic Count Station #2**

KYTC Traffic Count Station #2	
STA ID	008M08
Paste Count Data Here	
2024	
2023	4854
2022	
2021	
2020	4700
2019	
2018	
2017	5656
2016	
2015	
2014	
2013	
2012	
2011	
2010	
2009	
2008	
2007	
2006	
2005	
2004	
2003	
2002	
2001	
2000	
1999	

**KYTC Traffic Count Station #3**

KYTC Traffic Count Station #3	
STA ID	008B91
Paste Count Data Here	
2024	
2023	17804
2022	
2021	
2020	11519
2019	
2018	
2017	12257
2016	
2015	
2014	13831
2013	
2012	
2011	13200
2010	
2009	
2008	15100
2007	
2006	
2005	13600
2004	
2003	
2002	11300
2001	
2000	
1999	8740

Without MO8

**TIS Simplified Traffic Forecast**

Count Year	2024	Number of Counts	10
Opening Year	2027	Growth Rate	1.97%
Design Year	2037		
Years Back	15		

**KYTC Traffic Count Station #1**

STA ID	008G89
Paste Count Data Here	
2024	
2023	19128
2022	
2021	
2020	16527
2019	
2018	
2017	18491
2016	
2015	
2014	13135
2013	
2012	
2011	15300
2010	
2009	
2008	13300
2007	
2006	
2005	12700
2004	
2003	
2002	13600
2001	
2000	
1999	10200
1998	
1997	
1996	9830

**KYTC Traffic Count Station #2**

STA ID	008M08
Paste Count Data Here	
2024	
2023	
2022	
2021	
2020	
2019	
2018	
2017	
2016	
2015	
2014	
2013	
2012	
2011	
2010	
2009	
2008	
2007	
2006	
2005	
2004	
2003	
2002	
2001	
2000	
1999	
1998	
1997	
1996	

**KYTC Traffic Count Station #3**

STA ID	008B91
Paste Count Data Here	
2024	
2023	17804
2022	
2021	
2020	11519
2019	
2018	
2017	12257
2016	
2015	
2014	13831
2013	
2012	
2011	13200
2010	
2009	
2008	15100
2007	
2006	
2005	13600
2004	
2003	
2002	11300
2001	
2000	
1999	8740
1998	
1997	
1996	10700

HCS REPORTS

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	KY 237 at Cannondale Dr							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Cannondale Dr							
Analysis Year	2024							North/South Street	KY 237							
Time Analyzed	AM Peak							Peak Hour Factor	0.95							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
<b>Lanes</b>																
<b>Vehicle Volumes and Adjustments</b>																
Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement																
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		1	0	1	0	0	2	0	0	1	2	0
Configuration						L		R			T	TR		L	T	TR
Volume (veh/h)						5		13			1292	3	0	5	686	0
Percent Heavy Vehicles (%)						20		0					3	0		
Proportion Time Blocked																
Percent Grade (%)							0									
Right Turn Channelized							No									
Median Type   Storage						Left Only										1
<b>Critical and Follow-up Headways</b>																
Base Critical Headway (sec)						7.5		6.9							4.1	
Critical Headway (sec)						7.90		6.90							4.10	
Base Follow-Up Headway (sec)						3.5		3.3							2.2	
Follow-Up Headway (sec)						3.70		3.30							2.20	
<b>Delay, Queue Length, and Level of Service</b>																
Flow Rate, v (veh/h)						5		14							5	
Capacity, c (veh/h)						112		402							517	
v/c Ratio						0.05		0.03							0.01	
95% Queue Length, Q <sub>95</sub> (veh)						0.1		0.1							0.0	
95% Queue Length, Q <sub>95</sub> (ft)						2.9		2.5							0.0	
Control Delay (s/veh)						38.6		14.3							12.0	
Level of Service (LOS)						E		B							B	
Approach Delay (s/veh)						21.0								0.1		
Approach LOS						C								A		

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	KY 237 at Cannondale Dr							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Cannondale Dr							
Analysis Year	2027							North/South Street	KY 237							
Time Analyzed	AM Peak No Build							Peak Hour Factor	0.96							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
Lanes																
<p>Major Street North-South</p>																
Vehicle Volumes and Adjustments																
Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		1	0	1	0	0	2	0	0	1	2	0
Configuration						L		R			T	TR		L	T	TR
Volume (veh/h)						13		31			1371	5	0	11	728	0
Percent Heavy Vehicles (%)						8		0					3	0		
Proportion Time Blocked																
Percent Grade (%)								0								
Right Turn Channelized								No								
Median Type   Storage						Left Only										1
Critical and Follow-up Headways																
Base Critical Headway (sec)						7.5		6.9								4.1
Critical Headway (sec)						7.66		6.90								4.10
Base Follow-Up Headway (sec)						3.5		3.3								2.2
Follow-Up Headway (sec)						3.58		3.30								2.20
Delay, Queue Length, and Level of Service																
Flow Rate, v (veh/h)						14		32								11
Capacity, c (veh/h)						110		377								480
v/c Ratio						0.12		0.09								0.02
95% Queue Length, Q <sub>95</sub> (veh)						0.4		0.3								0.1
95% Queue Length, Q <sub>95</sub> (ft)						10.6		7.5								2.5
Control Delay (s/veh)						42.4		15.4								12.7
Level of Service (LOS)						E		C								B
Approach Delay (s/veh)						23.4										0.2
Approach LOS						C										A

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Cannondale AM 27 NB.xtw

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Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	KY 237 at Cannondale Dr								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Cannondale Dr								
Analysis Year	2027							North/South Street	KY 237								
Time Analyzed	AM Peak Build							Peak Hour Factor	0.96								
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
Lanes																	
<p>Major Street North-South</p>																	
Vehicle Volumes and Adjustments																	
Approach	Eastbound				Westbound				Northbound				Southbound				
Movement	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Priority		10	11	12		7	8	9	10	1	2	3	4	4	5	6	
Number of Lanes		0	0	0		1	0	1	0	0	2	0	0	1	2	0	
Configuration						L		R			T	TR		L	T	TR	
Volume (veh/h)						45		68			1371	15		0	23	728	0
Percent Heavy Vehicles (%)						8		0						3	0		
Proportion Time Blocked																	
Percent Grade (%)								0									
Right Turn Channelized								No									
Median Type   Storage						Left Only											1
Critical and Follow-up Headways																	
Base Critical Headway (sec)						7.5		6.9								4.1	
Critical Headway (sec)						7.66		6.90								4.10	
Base Follow-Up Headway (sec)						3.5		3.3								2.2	
Follow-Up Headway (sec)						3.58		3.30								2.20	
Delay, Queue Length, and Level of Service																	
Flow Rate, v (veh/h)						47		71								24	
Capacity, c (veh/h)						108		374								476	
v/c Ratio						0.44		0.19								0.05	
95% Queue Length, Q <sub>95</sub> (veh)						1.9		0.7								0.2	
95% Queue Length, Q <sub>95</sub> (ft)						50.5		17.5								5.0	
Control Delay (s/veh)						62.1		16.9								13.0	
Level of Service (LOS)						F		C								B	
Approach Delay (s/veh)						34.9										0.4	
Approach LOS						D										A	

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	KY 237 at Cannondale Dr							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Cannondale Dr							
Analysis Year	2024							North/South Street	KY 237							
Time Analyzed	PM Peak							Peak Hour Factor	0.98							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
Lanes																
Vehicle Volumes and Adjustments																
Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		1	0	1	0	0	2	0	0	1	2	0
Configuration						L		R			T	TR		L	T	TR
Volume (veh/h)						2		6			887	9	0	8	1236	0
Percent Heavy Vehicles (%)						0		0					0	0		
Proportion Time Blocked																
Percent Grade (%)								0								
Right Turn Channelized								No								
Median Type   Storage						Left Only										1
Critical and Follow-up Headways																
Base Critical Headway (sec)						7.5		6.9								4.1
Critical Headway (sec)						7.50		6.90								4.10
Base Follow-Up Headway (sec)						3.5		3.3								2.2
Follow-Up Headway (sec)						3.50		3.30								2.20
Delay, Queue Length, and Level of Service																
Flow Rate, v (veh/h)						2		6								8
Capacity, c (veh/h)						195		556								754
v/c Ratio						0.01		0.01								0.01
95% Queue Length, Q <sub>95</sub> (veh)						0.0		0.0								0.0
95% Queue Length, Q <sub>95</sub> (ft)						0.0		0.0								0.0
Control Delay (s/veh)						23.7		11.5								9.8
Level of Service (LOS)						C		B								A
Approach Delay (s/veh)						14.6										0.1
Approach LOS						B										A

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	KY 237 at Cannondale Dr								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Cannondale Dr								
Analysis Year	2027							North/South Street	KY 237								
Time Analyzed	PM Peak No Build							Peak Hour Factor	0.98								
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
<b>Lanes</b>																	
<p style="text-align: center;">Major Street North-South</p>																	
<b>Vehicle Volumes and Adjustments</b>																	
Approach	Eastbound				Westbound				Northbound				Southbound				
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Movement									1U	1	2	3	4U	4	5	6	
Priority		10	11	12		7	8	9	0	0	2	0	0	1	2	0	
Number of Lanes		0	0	0		1	0	1			2	0		1	2	0	
Configuration						L		R			T	TR		L	T	TR	
Volume (veh/h)						7		17			941	17	0	28	1312	0	
Percent Heavy Vehicles (%)						0		0					0	0			
Proportion Time Blocked																	
Percent Grade (%)								0									
Right Turn Channelized								No									
Median Type   Storage						Left Only							1				
<b>Critical and Follow-up Headways</b>																	
Base Critical Headway (sec)						7.5		6.9							4.1		
Critical Headway (sec)						7.50		6.90							4.10		
Base Follow-Up Headway (sec)						3.5		3.3							2.2		
Follow-Up Headway (sec)						3.50		3.30							2.20		
<b>Delay, Queue Length, and Level of Service</b>																	
Flow Rate, v (veh/h)						7		17							29		
Capacity, c (veh/h)						171		530							714		
v/c Ratio						0.04		0.03							0.04		
95% Queue Length, Q <sub>95</sub> (veh)						0.1		0.1							0.1		
95% Queue Length, Q <sub>95</sub> (ft)						2.5		2.5							2.5		
Control Delay (s/veh)						27.0		12.0							10.3		
Level of Service (LOS)						D		B							B		
Approach Delay (s/veh)						16.4								0.2			
Approach LOS						C								A			

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	KY 237 at Cannondale Dr							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Cannondale Dr							
Analysis Year	2027							North/South Street	KY 237							
Time Analyzed	PM Peak Bulld							Peak Hour Factor	0.98							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
<b>Lanes</b>																
<p>Major Street North-South</p>																
<b>Vehicle Volumes and Adjustments</b>																
Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		1	0	1	0	0	2	0	0	1	2	0
Configuration						L		R			T	TR		L	T	TR
Volume (veh/h)						27		40			941	48	0	64	1312	0
Percent Heavy Vehicles (%)						0		0					0	0		
Proportion Time Blocked																
Percent Grade (%)								0								
Right Turn Channelized								No								
Median Type   Storage						Left Only										1
<b>Critical and Follow-up Headways</b>																
Base Critical Headway (sec)						7.5		6.9								4.1
Critical Headway (sec)						7.50		6.90								4.10
Base Follow-Up Headway (sec)						3.5		3.3								2.2
Follow-Up Headway (sec)						3.50		3.30								2.20
<b>Delay, Queue Length, and Level of Service</b>																
Flow Rate, v (veh/h)						28		41								65
Capacity, c (veh/h)						155		518								695
v/c Ratio						0.18		0.08								0.09
95% Queue Length, Q <sub>95</sub> (veh)						0.6		0.3								0.3
95% Queue Length, Q <sub>95</sub> (ft)						15.0		7.5								7.5
Control Delay (s/veh)						33.3		12.5								10.7
Level of Service (LOS)						D		B								B
Approach Delay (s/veh)						20.9										0.5
Approach LOS						C										A

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	Oakbrook Rd at Holbrook Ln								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Holbrook Ln								
Analysis Year	2024							North/South Street	Oakbrook Dr								
Time Analyzed	AM Peak							Peak Hour Factor	0.92								
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
<b>Lanes</b>																	
<p style="text-align: center;">Mago Street North-South</p>																	
<b>Vehicle Volumes and Adjustments</b>																	
Approach	Eastbound				Westbound				Northbound				Southbound				
Movement	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6	
Number of Lanes		1	0	1		0	0	0	0	0	1	0	0	0	1	0	
Configuration		L		R						LT						TR	
Volume (veh/h)		44		16						10	249				86	8	
Percent Heavy Vehicles (%)		2		0						0							
Proportion Time Blocked																	
Percent Grade (%)	0																
Right Turn Channelized	No																
Median Type   Storage	Undivided																
<b>Critical and Follow-up Headways</b>																	
Base Critical Headway (sec)		7.1		6.2						4.1							
Critical Headway (sec)		6.42		6.20						4.10							
Base Follow-Up Headway (sec)		3.5		3.3						2.2							
Follow-Up Headway (sec)		3.52		3.30						2.20							
<b>Delay, Queue Length, and Level of Service</b>																	
Flow Rate, v (veh/h)		48		17						11							
Capacity, c (veh/h)		608		964						1503							
v/c Ratio		0.08		0.02						0.01							
95% Queue Length, Q <sub>95</sub> (veh)		0.3		0.1						0.0							
95% Queue Length, Q <sub>95</sub> (ft)		7.6		2.5						0.0							
Control Delay (s/veh)		11.4		8.8						7.4	0.1						
Level of Service (LOS)		B		A						A	A						
Approach Delay (s/veh)		10.7								0.3							
Approach LOS		B								A							

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	Oakbrook Rd at Holbrook Ln							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Holbrook Ln							
Analysis Year	2027							North/South Street	Oakbrook Dr							
Time Analyzed	AM Peak No Build							Peak Hour Factor	0.92							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
Lanes																
Vehicle Volumes and Adjustments																
Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Priority																
Number of Lanes		1	0	1		0	0	0	0	0	1	0	0	0	1	0
Configuration		L		R						LT						TR
Volume (veh/h)		44		16						10	264				91	8
Percent Heavy Vehicles (%)		2		0						0						
Proportion Time Blocked																
Percent Grade (%)		0														
Right Turn Channelized		No														
Median Type   Storage		Undivided														
Critical and Follow-up Headways																
Base Critical Headway (sec)		7.1		6.2						4.1						
Critical Headway (sec)		6.42		6.20						4.10						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.52		3.30						2.20						
Delay, Queue Length, and Level of Service																
Flow Rate, v (veh/h)		48		17						11						
Capacity, c (veh/h)		591		957						1496						
v/c Ratio		0.08		0.02						0.01						
95% Queue Length, Q <sub>95</sub> (veh)		0.3		0.1						0.0						
95% Queue Length, Q <sub>95</sub> (ft)		7.6		2.5						0.0						
Control Delay (s/veh)		11.6		8.8						7.4	0.1					
Level of Service (LOS)		B		A						A	A					
Approach Delay (s/veh)		10.9								0.3						
Approach LOS		B								A						

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	Oakbrook Rd at Holbrook Ln							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Holbrook Ln							
Analysis Year	2027							North/South Street	Oakbrook Dr							
Time Analyzed	AM Peak Build							Peak Hour Factor	0.92							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
<b>Lanes</b>																
<b>Vehicle Volumes and Adjustments</b>																
Approach	Eastbound				Westbound				Northbound				Southbound			
Movement	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		1	0	1		0	0	0	0	0	1	0	0	0	1	0
Configuration		L		R						LT						TR
Volume (veh/h)		78		16						10	264				91	18
Percent Heavy Vehicles (%)		2		0						0						
Proportion Time Blocked																
Percent Grade (%)		0														
Right Turn Channelized		No														
Median Type   Storage		Undivided														
<b>Critical and Follow-up Headways</b>																
Base Critical Headway (sec)		7.1		6.2						4.1						
Critical Headway (sec)		6.42		6.20						4.10						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.52		3.30						2.20						
<b>Delay, Queue Length, and Level of Service</b>																
Flow Rate, v (veh/h)		83		17						11						
Capacity, c (veh/h)		587		950						1482						
v/c Ratio		0.14		0.02						0.01						
95% Queue Length, Q <sub>95</sub> (veh)		0.5		0.1						0.0						
95% Queue Length, Q <sub>95</sub> (ft)		12.7		2.5						0.0						
Control Delay (s/veh)		12.1		8.9						7.4	0.1					
Level of Service (LOS)		B		A						A	A					
Approach Delay (s/veh)		11.6								0.3						
Approach LOS		B								A						

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	Oakbrook Rd at Holbrook Ln								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Holbrook Ln								
Analysis Year	2024							North/South Street	Oakbrook Dr								
Time Analyzed	PM Peak							Peak Hour Factor	0.94								
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
<b>Lanes</b>																	
<b>Vehicle Volumes and Adjustments</b>																	
Approach	Eastbound				Westbound				Northbound				Southbound				
Movement	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Priority		10	11	12		7	8	9	10	1	2	3	4	5	6		
Number of Lanes		1	0	1		0	0	0	0	0	1	0	0	0	1	0	
Configuration		L		R						LT						TR	
Volume (veh/h)		15		24						26	212				310	28	
Percent Heavy Vehicles (%)		0		0						0							
Proportion Time Blocked																	
Percent Grade (%)	0																
Right Turn Channelized	No																
Median Type   Storage	Undivided																
<b>Critical and Follow-up Headways</b>																	
Base Critical Headway (sec)		7.1		6.2						4.1							
Critical Headway (sec)		6.40		6.20						4.10							
Base Follow-Up Headway (sec)		3.5		3.3						2.2							
Follow-Up Headway (sec)		3.50		3.30						2.20							
<b>Delay, Queue Length, and Level of Service</b>																	
Flow Rate, v (veh/h)		16		26						28							
Capacity, c (veh/h)		440		703						1210							
v/c Ratio		0.04		0.04						0.02							
95% Queue Length, Q <sub>95</sub> (veh)		0.1		0.1						0.1							
95% Queue Length, Q <sub>95</sub> (ft)		2.5		2.5						2.5							
Control Delay (s/veh)		13.5		10.3						8.0	0.2						
Level of Service (LOS)		B		B						A	A						
Approach Delay (s/veh)		11.5								1.1							
Approach LOS		B								A							

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	Oakbrook Rd at Holbrook Ln							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Holbrook Ln							
Analysis Year	2027							North/South Street	Oakbrook Dr							
Time Analyzed	PM Peak No Build							Peak Hour Factor	0.94							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
Lanes																
Vehicle Volumes and Adjustments																
Approach	Eastbound				Westbound				Northbound				Southbound			
Movement	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		1	0	1		0	0	0	0	0	1	0	0	0	1	0
Configuration		L		R						LT						TR
Volume (veh/h)		15		24						26	225				329	28
Percent Heavy Vehicles (%)		0		0						0						
Proportion Time Blocked																
Percent Grade (%)		0														
Right Turn Channelized		No														
Median Type   Storage		Undivided														
Critical and Follow-up Headways																
Base Critical Headway (sec)		7.1		6.2						4.1						
Critical Headway (sec)		6.40		6.20						4.10						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.50		3.30						2.20						
Delay, Queue Length, and Level of Service																
Flow Rate, v (veh/h)		16		26						28						
Capacity, c (veh/h)		420		685						1190						
v/c Ratio		0.04		0.04						0.02						
95% Queue Length, Q <sub>95</sub> (veh)		0.1		0.1						0.1						
95% Queue Length, Q <sub>95</sub> (ft)		2.5		2.5						2.5						
Control Delay (s/veh)		13.9		10.5						8.1	0.2					
Level of Service (LOS)		B		B						A	A					
Approach Delay (s/veh)		11.8								1.0						
Approach LOS		B								A						

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	Oakbrook Rd at Holbrook Ln							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Holbrook Ln							
Analysis Year	2027							North/South Street	Oakbrook Dr							
Time Analyzed	PM Peak Build							Peak Hour Factor	0.94							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
Lanes																
Vehicle Volumes and Adjustments																
Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		1	0	1		0	0	0	0	0	1	0	0	0	1	0
Configuration		L		R						LT						TR
Volume (veh/h)		35		24						26	225				329	59
Percent Heavy Vehicles (%)		0		0						0						
Proportion Time Blocked																
Percent Grade (%)		0														
Right Turn Channelized		No														
Median Type   Storage		Undivided														
Critical and Follow-up Headways																
Base Critical Headway (sec)		7.1		6.2									4.1			
Critical Headway (sec)		6.40		6.20									4.10			
Base Follow-Up Headway (sec)		3.5		3.3									2.2			
Follow-Up Headway (sec)		3.50		3.30									2.20			
Delay, Queue Length, and Level of Service																
Flow Rate, v (veh/h)		37		26									28			
Capacity, c (veh/h)		410		670									1157			
v/c Ratio		0.09		0.04									0.02			
95% Queue Length, Q <sub>95</sub> (veh)		0.3		0.1									0.1			
95% Queue Length, Q <sub>95</sub> (ft)		7.5		2.5									2.5			
Control Delay (s/veh)		14.6		10.6									8.2	0.2		
Level of Service (LOS)		B		B									A	A		
Approach Delay (s/veh)		13.0											1.1			
Approach LOS		B											A			

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	Holbrook Ln at Watson Ln							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Holbrook Ln							
Analysis Year	2027							North/South Street	Watson Ln							
Time Analyzed	AM Peak No Build							Peak Hour Factor	0.75							
Intersection Orientation	East-West							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
Lanes																
Vehicle Volumes and Adjustments																
Approach	Eastbound				Westbound				Northbound				Southbound			
Movement	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority	1U	1	2	3	4U	4	5	6	7	8	9		10	11	12	
Number of Lanes	0	0	1	0	0	0	1	0	0	1	0		0	0	0	
Configuration				TR		LT					LR					
Volume (veh/h)			28	0		8	10			1		31				
Percent Heavy Vehicles (%)						12				0		3				
Proportion Time Blocked																
Percent Grade (%)										0						
Right Turn Channelized																
Median Type   Storage	Undivided															
Critical and Follow-up Headways																
Base Critical Headway (sec)						4.1				7.1		6.2				
Critical Headway (sec)						4.22				6.40		6.23				
Base Follow-Up Headway (sec)						2.2				3.5		3.3				
Follow-Up Headway (sec)						2.31				3.50		3.33				
Delay, Queue Length, and Level of Service																
Flow Rate, v (veh/h)						11				43						
Capacity, c (veh/h)						1511				1028						
v/c Ratio						0.01				0.04						
95% Queue Length, Q <sub>95</sub> (veh)						0.0				0.1						
95% Queue Length, Q <sub>95</sub> (ft)						0.0				2.6						
Control Delay (s/veh)						7.4	0.1			8.7						
Level of Service (LOS)						A	A			A						
Approach Delay (s/veh)					3.3				8.7							
Approach LOS					A				A							

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	Holbrook Ln at Watson Ln							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Holbrook Ln							
Analysis Year	2027							North/South Street	Watson Ln							
Time Analyzed	AM Peak Build							Peak Hour Factor	0.75							
Intersection Orientation	East-West							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
Lanes																
Vehicle Volumes and Adjustments																
Approach	Eastbound				Westbound				Northbound				Southbound			
Movement	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			54	0		10	18			1		37				
Percent Heavy Vehicles (%)						12				0		3				
Proportion Time Blocked																
Percent Grade (%)										0						
Right Turn Channelized																
Median Type   Storage					Undivided											
Critical and Follow-up Headways																
Base Critical Headway (sec)						4.1				7.1		6.2				
Critical Headway (sec)						4.22				6.40		6.23				
Base Follow-Up Headway (sec)						2.2				3.5		3.3				
Follow-Up Headway (sec)						2.31				3.50		3.33				
Delay, Queue Length, and Level of Service																
Flow Rate, v (veh/h)						13					51					
Capacity, c (veh/h)						1467					984					
v/c Ratio						0.01					0.05					
95% Queue Length, Q <sub>95</sub> (veh)						0.0					0.2					
95% Queue Length, Q <sub>95</sub> (ft)						0.0					5.1					
Control Delay (s/veh)						7.5	0.1				8.9					
Level of Service (LOS)						A	A				A					
Approach Delay (s/veh)						2.7				8.9						
Approach LOS						A				A						

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																		
General Information								Site Information										
Analyst	DBZ							Intersection	Holbrook Ln at Watson Ln									
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction										
Date Performed	9/11/2024							East/West Street	Holbrook Ln									
Analysis Year	2024							North/South Street	Watson Ln									
Time Analyzed	PM Peak							Peak Hour Factor	0.91									
Intersection Orientation	East-West							Analysis Time Period (hrs)	0.25									
Project Description	Hickory																	
Lanes																		
Vehicle Volumes and Adjustments																		
Approach	Eastbound				Westbound				Northbound				Southbound					
Movement	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R		
Priority	1U	1	2	3	4U	4	5	6			7	8	9			10	11	12
Number of Lanes	0	0	1	0	0	0	1	0			0	1	0			0	0	0
Configuration				TR			LT					LR						
Volume (veh/h)			29	2			12	42			0		10					
Percent Heavy Vehicles (%)							0				0		0					
Proportion Time Blocked																		
Percent Grade (%)											0							
Right Turn Channelized																		
Median Type   Storage					Undivided													
Critical and Follow-up Headways																		
Base Critical Headway (sec)							4.1				7.1		6.2					
Critical Headway (sec)							4.10				6.40		6.20					
Base Follow-Up Headway (sec)							2.2				3.5		3.3					
Follow-Up Headway (sec)							2.20				3.50		3.30					
Delay, Queue Length, and Level of Service																		
Flow Rate, v (veh/h)							13						11					
Capacity, c (veh/h)							1591						1046					
v/c Ratio							0.01						0.01					
95% Queue Length, Q <sub>95</sub> (veh)							0.0						0.0					
95% Queue Length, Q <sub>95</sub> (ft)							0.0						0.0					
Control Delay (s/veh)							7.3	0.1					8.5					
Level of Service (LOS)							A	A					A					
Approach Delay (s/veh)							1.7						8.5					
Approach LOS							A						A					

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Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	Holbrook Ln at Watson Ln							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Holbrook Ln							
Analysis Year	2027							North/South Street	Watson Ln							
Time Analyzed	PM Peak No Build							Peak Hour Factor	0.91							
Intersection Orientation	East-West							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
Lanes																
Vehicle Volumes and Adjustments																
Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume (veh/h)			29	2		12	42			0		10				
Percent Heavy Vehicles (%)						0				0		0				
Proportion Time Blocked																
Percent Grade (%)										0						
Right Turn Channelized																
Median Type   Storage					Undivided											
Critical and Follow-up Headways																
Base Critical Headway (sec)						4.1				7.1		6.2				
Critical Headway (sec)						4.10				6.40		6.20				
Base Follow-Up Headway (sec)						2.2				3.5		3.3				
Follow-Up Headway (sec)						2.20				3.50		3.30				
Delay, Queue Length, and Level of Service																
Flow Rate, v (veh/h)						13					11					
Capacity, c (veh/h)						1591					1046					
v/c Ratio						0.01					0.01					
95% Queue Length, Q <sub>95</sub> (veh)						0.0					0.0					
95% Queue Length, Q <sub>95</sub> (ft)						0.0					0.0					
Control Delay (s/veh)						7.3	0.1				8.5					
Level of Service (LOS)						A	A				A					
Approach Delay (s/veh)						1.7				8.5						
Approach LOS						A				A						

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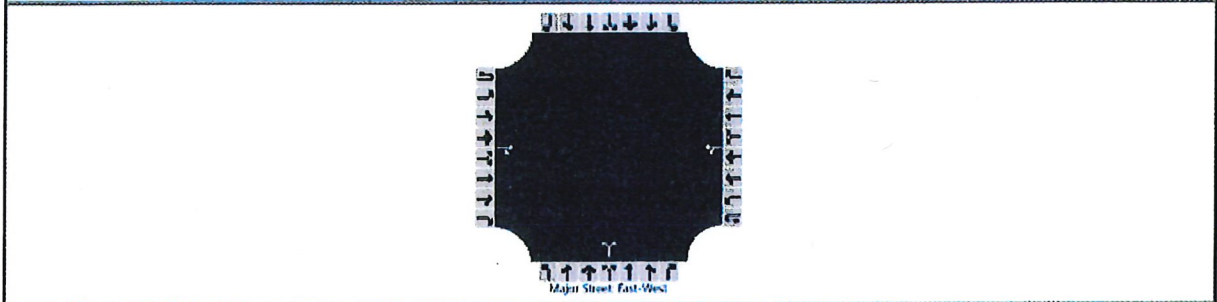
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Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

### HCS Two-Way Stop-Control Report

General Information		Site Information	
Analyst	DBZ	Intersection	Holbrook Ln at Watson Ln
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC	Jurisdiction	
Date Performed	9/11/2024	East/West Street	Holbrook Ln
Analysis Year	2027	North/South Street	Watson Ln
Time Analyzed	PM Peak Build	Peak Hour Factor	0.91
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	Hickory		

#### Lanes



#### Vehicle Volumes and Adjustments

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6	7	8	9		10	11	12	
Priority	1U	1	2	3	4U	4	5	6	7	8	9		10	11	12	
Number of Lanes	0	0	1	0	0	0	1	0	0	1	0		0	0	0	
Configuration				TR			LT				LR					
Volume (veh/h)			46	2			16	69			0				13	
Percent Heavy Vehicles (%)							0				0					
Proportion Time Blocked																
Percent Grade (%)											0					
Right Turn Channelized																
Median Type   Storage							Undivided									

#### Critical and Follow-up Headways

Base Critical Headway (sec)							4.1				7.1				6.2	
Critical Headway (sec)							4.10				6.40				6.20	
Base Follow-Up Headway (sec)							2.2				3.5				3.3	
Follow-Up Headway (sec)							2.20				3.50				3.30	




#### Delay, Queue Length, and Level of Service

Flow Rate, v (veh/h)							18				14					
Capacity, c (veh/h)							1566				1022					
v/c Ratio							0.01				0.01					
95% Queue Length, Q <sub>95</sub> (veh)							0.0				0.0					
95% Queue Length, Q <sub>95</sub> (ft)							0.0				0.0					
Control Delay (s/veh)							7.3	0.1			8.6					
Level of Service (LOS)							A	A			A					
Approach Delay (s/veh)							1.4				8.6					
Approach LOS							A				A					

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

I, Diane Bridwell Zimmerman, certify that this Traffic Impact Study has been prepared under my direct supervision, that I am a Professional Engineer registered in the State of Kentucky and have successfully completed the Traffic Impact Study Requirements training course required by KYTC. Furthermore, I certify that this study has been completed in accordance with the KYTC Traffic Impact Study Requirements and in accordance with engineering standards of practice. The results presented have been determined to be accurate representations of existing and anticipated conditions based on the assumptions and methodologies presented in this report.

Diane Bridwell Zimmerman, Professional Engineer License #16462

 <b>College of Engineering</b> <i>Kentucky Transportation Center</i>	 <b>TECHNOLOGY TRANSFER PROGRAM</b>
<b>TRAFFIC IMPACT STUDY COURSE Certificate of Completion (3.5 PDH)</b>	
<b>Diane Zimmerman</b> KY PE License No. 16462	<b>TIM THARPE</b> _____ Tim Tharpe, KYTC Director of Traffic Operations
Completed: 02/18/2022 Expires: 02/18/2026 Company: University of Kentucky	 _____ Adam Kirk, Instructor
<b>The official status of this certificate can be verified with the KYTC Division of Traffic Operations</b>	

# **Resolution Of The Boone County Fiscal Court**

## **Resolution No. 2024 - 320**

**A RESOLUTION RELATING TO THE BOONE COUNTY FISCAL COURT APPROVING THE PLANNING COMMISSIONS RECOMMENDATION FOR DENIAL FOR A REQUEST OF CHARTER COMMERCIAL, LLC (APPLICANT) FOR DELAWARE CROSSING, LLC (OWNER) FOR A ZONING MAP AMENDMENT FROM SUBURBAN RESIDENTIAL ONE (SR-1) TO SUBURBAN RESIDENTIAL ONE/PLANNED DEVELOPMENT (SR-1/PD) FOR AN APPROXIMATE 44 ACRE AREA LOCATED AT 1841 HOLBROOK LANE AND AT THE TERMINUS OF LARIAT WAY, INCLUDING THE PROPERTY HAVING A PARCEL IDENTIFICATION NUMBER (PIDN) OF 050.00-00-001.05, BOONE COUNTY, KENTUCKY.**

**WHEREAS**, the Boone County Planning Commission received a request for a Zoning Map Amendment to the Boone County Zoning Map, and such Zoning Map Amendment being a zone change from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky, which is more particularly described below; and

**WHEREAS**, the Boone County Planning Commission as the planning unit for the unincorporated areas of Boone County, Kentucky, was requested to and has conducted a Public Hearing serving as a due process trial-type hearing, and made Findings of Fact recommending denial for the Zoning Map Amendment;

**NOW, THEREFORE, BE IT RESOLVED BY THE FISCAL COURT OF COUNTY OF BOONE, COMMONWEALTH OF KENTUCKY:**

### **SECTION I**

That the request for a Zoning Map Amendment for the real estate which is more particularly described below is hereby denied, this Zoning Map Amendment being a zone change from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The real

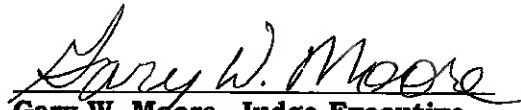
estate which is the subject of this request for a Zoning Map Amendment in a Suburban Residential One (SR-1) zone is more particularly described in DEED BOOKS/PAGE NOS: 1115/572 and 1196/93 (as supplied by the applicant), as recorded in the Boone County Clerk's office.

**SECTION II**

That as a basis for denial for a Zoning Map Amendment request are the Findings of Fact of the Boone County Planning Commission as set forth in its minutes and official records for this request, which shall be and are hereby incorporated by reference as if fully set out in this Resolution and marked as "Exhibit 1."

**SECTION III**

That this Resolution is hereby approved and adopted in Open Session of the Boone County Fiscal Court this 19<sup>th</sup> day of November, 2024.

  
**Gary W. Moore, Judge-Executive  
Boone County Fiscal Court**

**Attest:**

  
\_\_\_\_\_  
**Shona Schulkers,  
Fiscal Court Clerk**

**BOONE COUNTY PLANNING COMMISSION****EXHIBIT 1**

[www.boonecountyky.org/pc](http://www.boonecountyky.org/pc)  
[www.boonecountygis.com](http://www.boonecountygis.com)

Boone County Planning Commission  
2950 Washington Street, Room 317  
P.O. Box 958  
Burlington, KY 41005

Phone (859) 334-2196; Fax (859) 334-2264  
[plancom@boonecountyky.org](mailto:plancom@boonecountyky.org)



November 6, 2024

The Honorable Gary W. Moore  
County Judge-Executive  
Boone County Fiscal Court  
P.O. Box 900  
Burlington, KY 41005

Dear Judge Moore:

This letter is written to advise you of the action by the Boone County Planning Commission in regard to the request of **Charter Commercial, LLC (applicant)** for **Delaware Crossing, LLC (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

At their meeting of October 16, 2024, the Boone County Planning Commission voted unanimously to recommend denial, by **Resolution R-24-028-D**. This action was taken upon hearing the recommendation of the Committee and based on the Findings of Fact attached to the enclosed Resolution. The deadline for action to be taken by the Boone County Fiscal Court is **Tuesday, January 14, 2025**.

The approved minutes of the August 21, 2024 Public Hearing, along with the pertinent information regarding this request are enclosed. If you need any further information, please do not hesitate to call the Planning Commission office.

With Kindest Regards,

Charlie Rolfsen  
Chairman

CR/tlb

Enclosures

**RESOLUTION R-24-028-D**

**A RESOLUTION OF THE BOONE COUNTY PLANNING COMMISSION RECOMMENDING DENIAL FOR A REQUEST OF CHARTER COMMERCIAL, LLC (APPLICANT) FOR DELAWARE CROSSING, LLC (OWNER) FOR A ZONING MAP AMENDMENT FROM SUBURBAN RESIDENTIAL ONE (SR-1) TO SUBURBAN RESIDENTIAL ONE/PLANNED DEVELOPMENT (SR-1/PD) FOR AN APPROXIMATE 44 ACRE AREA LOCATED AT 1841 HOLBROOK LANE AND AT THE TERMINUS OF LARIAT WAY, INCLUDING THE PROPERTY HAVING A PARCEL IDENTIFICATION NUMBER (PIDN) OF 050.00-00-001.05, BOONE COUNTY, KENTUCKY, AND PROVIDING THE RECOMMENDATION BE FORWARDED TO THE BOONE COUNTY FISCAL COURT, BURLINGTON, KENTUCKY.**

**WHEREAS**, the Boone County Planning Commission received a request for a Zoning Map Amendment to the Boone County Zoning Map and such Zoning Map Amendment being a zone change from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky, which is more particularly described below; and,

**WHEREAS**, the Boone County Planning Commission as the planning unit for the unincorporated areas of Boone County, Kentucky, was requested to and has conducted a Public Hearing serving as a due process trial-type hearing and made Findings of Fact recommending denial for the Zoning Map Amendment.

**NOW, THEREFORE, BE IT RESOLVED BY THE BOONE COUNTY PLANNING COMMISSION AS FOLLOWS:**

**SECTION I**

That the request for a Zoning Map Amendment for the real estate which is more particularly described below shall be and is hereby recommended denial, this Zoning Map Amendment being a zone change from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The real estate which is the subject of this request for a Zoning Map Amendment in a Suburban Residential One (SR-1) zone is more particularly described in DEED BOOKS/PAGE NOS: 1115/572 and 1196/93 (as supplied by the applicant) as recorded in the Boone County Clerk's office.

**SECTION II**

That as a basis for the recommendation of denial for a Zoning Map Amendment request are the Findings of Fact of the Boone County Planning Commission as set forth in its minutes and official records for this request shall be and are hereby incorporated by reference as if fully set out in this Resolution and marked as "Exhibit A."

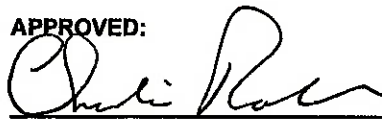
The Committee recommended denial for this request based on the Findings of Fact as set forth in the Committee Report and marked as "Exhibit B."

**SECTION III**

That a copy of this Resolution recommending denial for a Zoning Map Amendment for a zone change from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky, having jurisdiction over the property for its action on the recommendation of the Boone County Planning Commission.

**PASSED AND APPROVED ON THIS 6<sup>TH</sup> DAY OF NOVEMBER, 2024.**

**APPROVED:**



**CHARLIE ROLFSEN  
CHAIRMAN**

**ATTEST:**



**TREVA L. BEAGLE  
MANAGER, ADMINISTRATIVE SERVICES**

CR/tlb

# EXHIBIT

“A”

## STAFF REPORT

#1

Request of **Charter Commercial, LLC (applicant)** for **Delaware Crossing, LLC (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

August 21, 2024

### REQUEST

- A. The request is to rezone an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way from SR-1 to SR-1/PD to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

### SITE HISTORY

- 1969-1981 Based on information contained in the Boone County GIS, the buildings located at 1841 Holbrook Lane were constructed.
- 2018 On April 27, 2018, the Boone County planning Commission approved a Preliminary Plat for Valley Creek Farms subdivision.
- 2022 On January 31, 2022, the Boone County Planning Commission approved a Conveyance Plat creating the southern lot of the site in question as a non-buildable lot.

### APPLICABLE REGULATIONS

- A. Section 308 of the Boone County Zoning Regulations states that before any map amendment is granted, the Planning Commission and legislative body shall use the following criteria:
1. The map amendment is in agreement with the adopted comprehensive plan and any specific study designed to further detail the Boone County Comprehensive Plan for the location in question; or
  2. The existing zoning classification is inappropriate and that the proposed zoning classification is appropriate; or
  3. There have been major changes of an economic, physical, or social nature not anticipated in the adopted comprehensive plan that substantially alter the area's character.
- B. Section 930 of the Boone County Zoning Regulations states that "the purpose of the Suburban Residential One district is to provide a low density, residential environment whose dwelling types and densities are typical of a suburban character. It is also to provide limited or passive and active recreational uses that are appropriate to the permitted uses in the district. Suburban Residential One districts will be located on lands

within established urban areas where adequate infrastructure facilities and services are available or proposed.”

- C. Section 931 of the Boone County Zoning Regulations does not identify attached single-family dwellings or multi-family dwellings as a principally permitted use in the SR-1 district.
- D. Section 1506 of the Boone County Zoning Regulations provides for the following planned development criteria:
1. **Mixed Use Development and Pedestrian Orientation:** Planned Developments shall generally have a mixed use orientation (combination of differing types of residential, commercial, public or civic, and/or industrial uses) both within the development itself and relative to the relationship between the proposed planned development and adjacent sites. The additional intensity allowed in a Planned Development by Section 1501 shall only be permitted when a true mixed use and/or an amenity - oriented development with community and recreation facilities as described in this standard is provided.
  2. In general, Planned Developments shall have a pedestrian orientation, where it is possible to live, work, shop, and play in the same immediate vicinity without a required dependence on the automobile. This may be accomplished through the use of comprehensive pedestrian circulation networks including multipurpose paths and walks along main routes and open spaces such as stream corridors, between major destinations within the development and adjoining areas, secondary walk connections to the multi-purpose paths, the creation of a designed pedestrian environment including street trees in addition to other required landscaping, decorative street lights and other street furniture, and seating areas, and the use of integral curb walks where appropriate along streets. Disruptions in major paths due to street and drive intersections shall be minimal. Additionally in commercial areas, pedestrian orientation can be accomplished by placing buildings in close proximity to the street with parking areas to the side and rear of buildings, mixing uses within the same multi-story buildings, building entrances directly facing streets with reduced setbacks, architectural design which employs display windows, projecting signs, and awnings at street level, and designed outdoor seating and gathering spaces at the street level. Each development proposal must demonstrate in detail how the project will be made walkable throughout.
  3. **Compatibility of Uses:** Measures shall be taken to assure compatibility of land uses within a Planned Development itself and adjacent sites. Such measures may include the provisions of buffer zones, common open space areas and landscape features, transitional land uses, or a mixed-use development in which no specific type of land use is dominate. When applicable, the design methods recommended in the “Development Layout, Lot Sizes, and Setbacks” section of the Comprehensive Plan’s Land Use Element shall be employed. Compatibility measures/mitigation measures shall exceed the usual minimum standards of these regulations when needed to address impacts of the proposed development.
  4. **Open Space:** Useable open space(s), in an amount over and above setback

and buffer yard areas and open areas required by the underlying district, shall be provided. These spaces may be provided in the form of parks, plazas, arcades, commons, trails, sports courts or other athletic and recreational areas, outdoor areas for the display of sculptural elements, etc. Land reservations for community facilities may be considered in lieu of useable open space. The use of single loaded streets to provide multi-purpose paths, park areas, or to protect stream corridors, may be proposed for this purpose. Open Space areas are encouraged to have street frontage and visibility. Any site proposed to be publicly dedicated or donated for park or open space purposes shall comply with the appropriate legislative body's requirements for acceptance of such dedications or donations.

5. **Multi-Modal Transportation System:** Planned Developments shall incorporate multi-modal transportation elements through the development, depending on the foreseeable needs of future residents and users of the site, and the relationship of the project site to the community at large. Such multi-modal elements may include provisions for mass transit stops or stations, car pooling lots, pedestrian and bicycle paths and lanes, bicycle parking areas, etc. Multi-modal facilities are encouraged to be combined with the pedestrian systems and open spaces described in Sections 1 and 4. Multi-modal facilities should connect to existing and future facilities that lie outside of the site.
6. **Preservation of Existing Site Features:** Existing topography, significant tree cover, tree lines along property lines, cemeteries, and water courses and water bodies shall be largely preserved and incorporated into the project design, where appropriate and consistent with the remainder of this article. The retention of such features may also fulfill portions of the requirements in Section 4 "Open Space" and Section 7 "Landscaping."
7. **Landscaping:** Substantial landscaping shall be provided in a Planned Development with emphasis given to street scape areas, buffer zones, and the provision of significant landscaping (in terms of size of landscape areas, and quantity and quality of landscape materials) within the developed portions of the site. The use of landscape design guidelines is required for multi-phased projects. The retention of existing healthy, substantial trees should occur wherever possible. Properly designed street tree plantings may be permitted to fulfill some landscaping requirements as part of an overall amenity package.
8. **Architecture:** Sites which are subject to architectural requirements through adopted overlay districts or land use studies shall follow said requirements. For all other sites, a consistent architectural theme shall be provided in planned developments. The theme shall largely use traditional, regionally influenced architectural forms and elements and shall allow variations within it. Traditional styles such as Georgian, Federal, I-House, Cape Cod, Craftsman, Tudor, Queen Anne, Italianate, early 20th century commercial structures, and local farm structures may be used as models. Contemporary or transitional styles of comparable quality may also be considered. The architectural theme shall also relate to existing structures on the project site and adjacent sites, especially if such existing structures are historic.

9. For attached or multi-family residential developments and commercial or office developments, the predominant building materials shall have a solid appearance, such as could be achieved with brick, stone, and architectural grade cast concrete products designed to replicate natural materials. Roof designs shall have a finished appearance through the use of three dimensional pitched roof forms with architectural grade roofing and/or the use of defined parapets with cornice lines. Long building facades and roof planes shall be interrupted through the use of three dimensional jogs in the building footprint and secondary roof forms such as hips, dormers, and gables. Such buildings shall include architectural detailing for cosmetic enhancement, largely use natural colors, and use a consistent design treatment on all facades. The use of architectural guidelines or building prototypes is required for all multi-phased projects. Developments should be mixed-use in character with multi-level buildings where commercial services are proposed. Walkability must be planned for when locating commercial and residential uses in proximity to each other. Office and residential uses are strongly advocated above commercial uses in business districts to decrease dependence on the automobile.
10. **Historic and Prehistoric Features:** Historic and prehistoric features on the project site shall be retained, utilized, and incorporated into the overall project design if physically and economically feasible.
11. **Signage:** A consistent signage theme shall be provided within a Planned Development. Building mounted signs shall be the predominate signage on the project site. Freestanding signs shall be monument style and of a limited size and height. Signage shall visually correlate to the planned architectural theme by the use of consistent design details, materials, and colors. The use of signage design guidelines is required for multi-phased projects.
12. **Transportation Connections and Entry Points:** The provision of transportation connections (street connections, pedestrian paths, multi-purpose trails, sidewalks, and bicycle facilities) shall be provided in all planned development unless physically unfeasible or unsafe. This shall include connections to adjoining properties and developments, and inter-connectivity within the development itself, and contain minimal use of cul-de-sacs or other dead-end types of streets only when necessary. Transportation connections shall account for the County's adopted Transportation Plan and any adopted bikeway and/or pedestrian plans. In addition, the various entry points (streets, paths, etc.) into a Planned Development shall be marked or otherwise defined through the use of landscaping, low-key signage on retaining walls, architectural or sculptural elements, archways, markers, etc. Any structures used to demarcate entry points shall visually correlate to the planned architectural theme by the use of consistent design details, materials, and colors.
13. **Conformance with Comprehensive Plan:** All Planned Developments shall conform to the provisions of the adopted Comprehensive Plan and take into account the limitations of existing or planned infrastructure.
14. Further, Concept Development Plan proposals within areas that are subject to

a specific land use or corridor study shall be evaluated against the criteria or requirements of such study as well as the criteria in this section.

15. A Planned Development and its uses, buildings or structures shall be minimally subject to the supplemental performance and development standards of this order, unless superseded by any special requirements, conditions, variances or other particulars imposed by the Planning Commission during the concept or preliminary application and hearing phases described in this article. Such special conditions may include provisions governing, common open space, lands or facilities, disposition of open land, infrastructure provisions including any physical design and/or any other requirement found to be necessary, appropriate or desirable for the purposes of this district.
- E. Section 1509 of the Boone County Zoning Regulations states that the Planning Commission shall require that a Concept Development Plan be submitted for the development or redevelopment of property located in a Planned Development district. If the Concept Development Plan involves a zone change, then the proposal is subject to the criteria then the proposal is subject to the criteria in Section 308 as well as the provisions of this article. If the Concept Development Plan does not involve a zone change, or only involves uses that are permitted in the underlying district (formerly called a "Utilization of an Underlying District Within a Planned Development"), then the proposal is subject only to the provisions of this article. The contents of the Concept Development Plan submittal shall include the items listed in Section 303.

#### SITE CHARACTERISTICS

- A. The approximate forty-four (44) acre area can be broken into two portions:
  1. The northern portion, containing approximately 19.97 acres; and
  2. The southern portion, containing approximately 24.40 acres.
- B. The northern portion of the site is currently occupied by a single-story detached single-family residential dwelling and three (3) accessory barn structures.
- C. The southern portion of the site is currently vacant and heavily wooded.
- D. Gunpowder Creek runs along the eastern edge of the site in question.
- E. The western edge of the site in question is located within the six hundred (600) foot buffer of an underground pipeline.
- F. Access is provided from the following street stubs: Holbrook Lane, Watson Lane, and Lariat Way.
- G. Topographically, a valley bisects the site with slopes having a grade change of seventy (70) feet.
- H. The eastern edge of the site in question is located within the 100-year floodplain of

Gunpowder Creek.

- I. Water service is available from 8" water lines within the Shaker Run and Valley Creek Farms subdivisions.
- J. Sanitary sewer service is available from 8" sewer lines within the Shaker Run and Valley Creek Farms subdivisions.

**ADJACENT LAND USES AND ZONES**

North: Detached single-family (SR-1)  
South: Detached single-family (Valley Creek Farms) and detached single-family/agricultural land (SR-1)  
East: Detached single-family (Shaker Run) (SR-1) and attached single-family (Oakbrook Cliffs (SR-1/PD)  
South: Detached single-family (Valley Creek Farms) and detached single-family/agricultural land (SR-1)

**RELATIONSHIP TO COMPREHENSIVE PLAN**

- A. The Our Boone County Plan 2040 Future Land Use Plan designates the site for "Rural Density Residential", "Suburban Density Residential", and "Developmentally Sensitive" uses.
  - 1. Rural Density Residential is described as low density residential uses of up to one dwelling unit per acre.
  - 2. Suburban Density Residential is described as single family housing of up to four units per acre.
  - 3. Developmentally Sensitive is described as areas that have an existing slope of twenty percent or greater for a height of 20 meters (67.6 feet), or have unique soil or flooding characteristics which limit the ability of an area to support urban development, or contain significant wooded areas, creeks, wildlife habitat, or other natural features that are important to a site's stability and visual character.
- B. The following Our Boone County – Plan 2040 Goals and Objectives apply to this application:
  - 1. Land uses and zoning decisions shall strive to balance the rights of landowners with the rights of neighbors and the community (Overall Goal A, Objective 4).
  - 2. Development policies shall not discriminate against any person (Demographics Goal A, Objective 1).
  - 3. Provide appropriate services, housing, employment, and shopping opportunities in order to meet the needs of the population in all geographic areas of the County (Demographics Goal A, Objective 4).
  - 4. A broad range of housing opportunities shall be provided to meet the needs and desires of all household types and shall be balanced with affordability, employment centers, commercial areas, and educational needs. [Unincorporated Boone County, City of Union, City of Walton only] (Demographics Goal B, Objective 1).
  - 5. A broad range of housing opportunities shall be provided to meet the needs and

- desires of all household types and shall be balanced with affordability, employment centers, commercial areas, and educational needs. The City of Florence acknowledges the need for affordable housing opportunities, but also desires an equal balance of housing options to maintain a diverse city. [City of Florence only] (Demographics Goal B, Objective 1).
6. Residential developments shall be evaluated for impact on infrastructure, environment, health and safety considerations, buffering, and design with secondary consideration given to dwelling unit type and density (Demographics Goal B, Objective 4).
  7. Mixed use, higher density neighborhoods shall be encouraged near existing or proposed transportation nodes and shall be designed to establish neighborhoods with a mix of retail, public, and recreational uses (Demographics Goal B, Objective 6).
  8. Residential developments shall be encouraged to plan and build in a manner compatible with the general housing density and design planned for the area, with the existing conditions of the site, and significant site features (Demographics Goal B, Objective 6).
  9. Development shall attempt to utilize existing topography and vegetation (including mature trees where possible) and preserve the existing character of the land where practical. [Unincorporated Boone County, City of Union, City of Walton only] (Environment Goal A, Objective 7).
  10. Development shall utilize existing topography and vegetation (including mature trees) and preserve the existing character of the land. [City of Florence only] (Environment Goal A, Objective 7).
  11. Priority shall be given towards maintaining, protecting, and improving the capacity and safety of the existing road system across jurisdictions (Transportation Goal A, Objective 1).
  12. New land development shall be closely coordinated with the local transportation system to assure efficiency and continuity within Boone County and with the regional multi-modal transportation system (Transportation Goal B, Objective 9).

C. The following are excerpts from Our Boone County - Plan 2040:

1. The main need of a population base is for safe, sound, and sanitary housing. Meeting this involves considering both housing supply and demand in order to determine what those future needs will be. It is important to forecast housing growth by areas in order to ensure that an equal balance of options exist to meet the needs of a diverse population. Furthermore, in order for Boone County to retain its youth, the county must provide housing choices that complement the educational opportunities, the current job market, and commercial attractions. Otherwise, an out-migration of the young population will occur. A variety of multi-family housing options should include apartments, townhouses, patio homes, condominiums, nursing homes, and retirement housing all ranging greatly in price per unit to accommodate a wide range of relative affordability. These types of units are increasing in demand beyond the established urban areas such as Florence and should be encouraged to occur in the unincorporated portions of Boone County as viable options for both the building community and the prospective resident, especially as the county's land values rise. Older housing stock that can be rehabilitated without undue effort or expense should be retained and in some

cases, incorporated into new subdivision design rather than being demolished. There are concentrations of older housing stock in need of restoration or rehabilitation in many of the older communities like Florence, Belleview-McVile, Burlington, Constance, Hebron, Petersburg, Stringtown, Taylorsport, Union, Verona, and Walton. Home ownership, maintenance, and rehabilitation assistance of older homes in these areas should be incentivized and encouraged through Federal or local grants. New residential development, regardless of the type, should be evaluated for its impact on the county's existing infrastructure and natural environment to ensure that it fits in with existing surrounding development through proper buffering and design. High density developments should be close to thoroughfares and urban services and be sensitively developed in terms of building height, setbacks, mass, and visual impact. A variety of housing opportunities within the established urban (and suburban) areas of Boone County would encourage mixing of different income levels. Most new construction of typical single-family housing should be encouraged to occur within undeveloped areas within the established suburban portion of the county in order to take advantage of existing infrastructure and services already in place and avoid placing strain on the more rural transportation network in the undeveloped portions of the county. Consideration must be given to existing conditions that may affect new development in established areas such as adjoining land uses and proximity to the Cincinnati-Northern Kentucky International Airport where commerce and airplane noise may be an issue.

Established single-family housing areas should not experience an influx of multi-family or duplex construction unless adequate buffering or proper development design can be provided to ease the transition. Duplexes are often more successful than multi-family housing in blending in with single-family residential surroundings. Therefore, when buffering is minimal between single-family housing areas and proposed multi-family units, an appropriate transitional area of duplexes may be designed. New subdivision development should include design considerations and gradation of lot sizes to lessen the impact on the character of the area. This is particularly true in areas of agricultural zoning and could include larger lots or green space along the main road(s), larger lots or a buffer along adjacent low density residential uses, strategic open space, and detailed attention to the orientation of housing units. The bottom line is that infrastructure partners must work together and correlate improvements in order to ensure capacity and be sensitive to the surrounding area. A progression of densities of residential uses from high (multi-family) to low (single family) shall be encouraged. Where traditional progressions of high to low net density are not possible through creative development design, an appropriate and attractive visual transition should be achieved. This could include existing vegetation or new landscaping and/or fencing. Existing vegetation (such as wooded fence rows) should be retained as much as possible to provide buffer strips and stormwater filters. The density of typical residential developments in Boone County over the past couple of decades has been consistent with the suburban residential densities reflected on the 2035 Future Land Use Map (up to 4 units per acre). However, there is a large segment of the population that desires to live in more rural areas while still having convenient access to centers of commercial, services, and employment activity. For residential development to occur in these outlying areas, adequate and proper

infrastructure must be provided, and impact on the natural environment must be minimized. If significant large lot residential subdivision development does occur in some of these rural areas, roads and other components of infrastructure will have to be improved. Existing and planned water, stormwater, and sanitary sewer services influence the locations of new residential construction. However, just because one form of public infrastructure exists or has been improved, does not automatically mean the area is ready for suburban development. Other forms of infrastructure may also be needed or improved first. Development phasing is an option to ensure that the timing of a new development corresponds with the provision of adequate infrastructure. In order to support the densities needed to sustain mass transit and to foster new affordable housing options, average minimum densities should be encouraged for mixed-use, planned developments. This would allow a variety of housing types but also enable a logical progression of intensities. The highest residential densities would be in the activity centers of these mixed-use developments and would include neighborhood commercial, employment, public uses, as well as transit stations or stops. (Demographics, Conclusions and Recommendations, Housing Types and Locations, pages 20-21).

2. The demand for more affordable residences, generated by the commercial and industrial employment growth centers, has resulted in the development of many new condominium and apartment complexes. However, attempts at more affordable new construction single-family or apartment developments are often met with localized resistance from the public. Creative site design and architecture can help overcome some of this opposition. Clustered housing or Open Space Subdivisions shall be promoted (via density bonuses) as they enable the development industry and local government to address density concerns and strengthen the sense of community, as well as meet the housing demands of a changing population. Mixed-use, planned developments that can provide a mix of housing options, commercial opportunities, offices, public uses, recreational uses, and multiple transportation options (i.e. transit, pedestrian, park-and-ride) could be one strategy for meeting this demand for higher density. Areas of employment growth in Boone County should experience development pressure for housing options relative in affordability to the jobs and shopping opportunities being created in the area. Manufactured (or modular) homes are an economical alternative to conventional homes and can lower the cost of construction to help offset rising land costs. These manufactured homes have historically been in demand in the rural, western half of the county. Most manufactured homes are permitted throughout all of Boone County and can occur separately or in a subdivision, unless regulated by private deed restrictions or restrictive covenants. Mobile homes, however, are strictly limited to the few remaining mobile home parks in Boone County or in the Agricultural (A-1) zoning district which is primarily located in the western portion of the county. However, A-1 zoning requires a minimum five acre lot size making this a less economical housing alternative with the increasing land prices. (Demographics, Conclusions and Recommendations, Affordability, page 21).
3. Developments in Boone County must recognize the potential impacts upon adjoining land uses and incorporate a transition of land uses, building setbacks, and/or landscaping to minimize these impacts. Potential impacts include visual,

noise or vibrations, odors, dust, smoke, and light. Buffering to mitigate these impacts should be an integral part of the design of proposed projects; where appropriate, existing site features should be used in meeting this guideline. Developments should provide buffering along public roadways, to soften the visual impact. Appropriate wooded areas and stream valleys should remain as open space within developments and between developments. Developments proposed adjacent to planned or established open spaces should provide pedestrian access where appropriate. Natural green space benefits the community as well as encourages developers to create innovative development designs through clustering of buildings and impermeable area. Typically, buffering is required and provided between both like and unlike land uses. However, as the development of Boone County fills in previously rural areas, different lot sizes and designs of residential development sometimes impact each other. Where an appropriate gradation of lot size and setbacks cannot be designed into a proposed residential subdivision development of a significantly higher density than existing adjacent residential uses, deliberate vegetation buffering may need to be incorporated into the design to help protect low density residential areas. Buffer areas should use and supplement existing site features where possible (Land Use, Future Land Use Development Guidelines, Buffering, pages 95-96).

4. Developments in Boone County must include landscaping to accompany the proposed project. Retention of existing healthy vegetation is considered a component of landscaping and is encouraged. This landscaping should be designed to improve the public view of a development, and should be incorporated into parking lots and other vehicle circulation areas, as well as within open spaces and around structures. Landscaping is intended to soften the visual impacts of the development from adjoining properties and roadways. The amount of heat absorbed by impervious cover from sun radiation is decreased by landscaping, which reduces energy costs. Landscaping helps purify the air of harmful pollutants, thus reducing health impacts. It also helps reduce the quantity and improve the quality of storm water runoff, including temperature. The use of bioretention islands (water filtering basins) and grass swales should be used where possible in place of raised islands as described in Northern Kentucky's Storm Water Best Management Practices Manual (2012). Native species are often heartier than nursery stock and should be used wherever possible to minimize pesticides and other high impact forms of maintenance. Developments along major roadways in Boone County must include landscaping between the development and the right-of-way in order to promote the aesthetic appearance from the roads and to facilitate the compatibility of differing land uses (Land Use, Future Land Use Development Guidelines, Landscaping, page 96).
5. The developing residential area south of KY 18, west of Oakbrook Drive and east of Camp Ernst Road must include street connections that enable traffic generated by the area to travel in all four directions. This should allow Oakbrook traffic to proceed to KY 237 in the Rogers Lane area, and Shaker Run traffic to access KY 18 without traveling on Camp Ernst Road or Oakbrook Road. Development in the area around Rogers Lane should be carefully planned. The remaining portions of this section of Boone County should develop in a Suburban Residential fashion, with the exception of planned High Suburban Density Residential uses between

Hickory Hill Subdivision and KY 18. Subdivision street connections are vital to this area around Burlington to give alternative routes for residents (Land Use, Description and Purpose of Land Use Maps, Future Land Use Geographic Areas, Conclusions and Recommendations, 11 – Burlington, page 120).

- D. Holbrook Lane is a county maintained local street providing for two way traffic within two driving lanes. A sidewalk exists along the south side of the roadway.
- E. Watson Lane is a county maintained local street providing for two way traffic within two driving lanes. Sidewalks exist along both sides of the roadway.
- F. Lariat Way is a county maintained local street providing for two way traffic within two driving lanes. Sidewalks are being provided as individual lots are developed.

**STAFF COMMENTS**

A. The submitted Concept Development Plan indicates the following:

- 1. Construction of 139 attached single-family residential dwellings in twenty-four (24) single-story buildings.
- 2. Construction of 157 multi-family residential dwellings in twenty-four (24) three-story buildings and thirteen (13) two-story garages).
- 3. Construction of public streets within the northern portion of the site.
- 4. Street connections at Watson Lane, Holbrook Lane, and Lariat Way.
- 5. Construction of street extensions for Holbrook Lane and Lariat Way.
- 6. Provision for stormwater retention.
- 7. Provision for amenity space.
- 8. Construction of sidewalks.
- 9. Construction of a private access drive within the southern portion of the site, extending from the terminus of Lariat Way.
- 10. Provision for a walking train in the southern portion of the site.
- 11. Provision for public utilities.

B. The following table provides a summary of intensity for the proposed development as well as for the existing adjacent areas:

	INTENSITY (du/ac)
Shaker Run Subdivision	4.55
Valley Creek Farms Subdivision	
Final Platted	4.12
Remaining Preliminary Plat	2.20
<b>Total</b>	<b>3.20</b>
Hickory Burlington	6.96
Poplar Point Apartments	6.43
<b>Total Development</b>	<b>6.67</b>

- C. The applicant has provide a Project Narrative and Traffic Assessment. The Traffic Assessment provides anticipated peak hour traffic generation numbers and trip distribution. However, the report does not include any intersection analyses or recommendations.
- D. Staff has reviewed the submitted Concept Development Plans against the Boone County Zoning and Subdivision Regulations, and offers the following comments (some of these requirements can be adjusted by the Planning Commission and Fiscal Court pursuant to the Planned Development (PD) regulations):

1. Section 505.2 of the zoning regulations prohibits townhouse and multi-family residential dwellings within the underlying SR-1 district.
2. Section 1504 of the zoning regulations state that the intensity of use in a Planned Development district may exceed the maximum intensity permitted in the underlying zoning district by up to fifty percent (50%).

Section 903.3 of the zoning regulations state that the maximum intensity of the underlying SR-1 district shall not exceed four (4) units per acre.

Therefore, the maximum intensity allowed in the proposed SR-1/PD district is six (6) units per acre.

The submitted Concept Development Plan indicates an overall development intensity of 6.67 units per acre.

3. Section 1506 of the Boone County Zoning Regulations includes a set of criteria that is to be used to evaluate the appropriateness of proposed developments. The following is an analysis of this criteria against the submitted plan:
  - a. Mixed Use Development and Pedestrian Orientation – The submitted plan includes two different residential types and includes open space areas.
  - b. Compatibility of Uses – The submitted plan indicates that development of the overall project will have an intensity of 6.67 units per acre. The adjacent Shaker Run Subdivision has an intensity of 4.55 units per acre and the Valley Creek Farms Subdivision has an intensity of 4.12 units per acre. The submitted plan provides for a minimum perimeter building setback of ten (10) feet adjacent to these subdivisions.
  - c. Open Space – Within the northern portion of the site, the open space is concentrated to the interior of the development, situated with and around the stormwater retention facility. Within the southern portion of the site, the open space is concentrated along the hillsides.
  - d. Multi-Modal Transportation System – The site will be accessed by passenger vehicles. The plan also includes sidewalks and walking trails.
  - e. Preservation of Existing Site Features – The development has been designed to preserve the hillsides located within the southern portion of the site.
  - f. Landscaping – The submitted plan does not provide for any specific

- g. landscaping.  
Architecture – The proposed townhomes will have masonry knee walls along the front and portions of the side elevations, paneled walls, pitched roofs, and architectural grade garage doors. Insufficient information has been provided to determine the building materials of the multi-family residential buildings.
  - h. Signage – Insufficient information has been submitted to determine if the proposed signage will visually correlate to the architectural theme of the proposed development. However, the submitted concept development plan does indicate that names of the two proposed distinct development areas are to be added to the existing signage for Valley Creek Farms, at Camp Ernst Road and that additional signage is to be installed at the entrances to the development at the Holbrook Lane extension, Watson Lane, and Lariat Way extension.
  - i. Transportation Connections and Entry Points – The submitted plan indicates that access will be provided from Watson Lane, an extension of Lariat Way, and a future extension of Holbrook Lane.
4. Section 3151 of the zoning regulations provides requirements for trash enclosure areas. Insufficient information has been submitted to determine compliance with these regulations.
  5. Section 3316 of the zoning regulations provides requirements for exterior lighting. Insufficient information has been submitted to determine compliance with these regulations.
  6. Section 3321 of the zoning regulations requires sidewalks that are immediately adjacent to off-street parking spaces to have a minimum width of six (6) feet. The submitted plan indicates that the sidewalks immediately adjacent to parking spaces within the southern portion of the site will have a width of four (4) feet.
  7. Section 3325 of the zoning regulations provide for minimum parking space requirements for residential uses, based on the number of bedrooms in each unit. This requirement will be verified as part of a Major Site Plan application.
  8. Section 3408.7.A of the zoning regulations states that in residential and residential planned development districts, entrance signs shall be permitted subject to the following restrictions:
    - a. Shall only be a monument sign.  
  
Insufficient information has been submitted to determine compliance with this requirement.
    - b. Shall only be permitted for a residential subdivision that involves the construction of a new street or for a multi-family development, or section of a development, that contains twenty (20) or more dwelling units.

The submitted proposal meets this requirement.

- c. Shall be limited to one (1) sign, up to a maximum sign area of one hundred (100) square feet, or two (2) signs, each having a maximum sign area of fifty (50) square feet.

Insufficient information has been submitted to determine compliance with this requirement.

- d. The maximum height of the sign shall not exceed ten (10) feet.

Insufficient information has been submitted to determine compliance with this requirement.

- e. Shall be located at the entrance into the residential development from either an arterial, collector, or subcollector street.

The submitted proposal indicates that the proposed signage will be at the entrances to the proposed development from local streets.

9. Section 402.1.1 of the subdivision regulations state that maximum grade of public streets shall not exceed twelve (12) percent.

The submitted plan indicates that the grade of proposed street "D", at the entrance from Holbrook Lane, will have a grade of approximately thirty-four (34) percent.

10. The subdivision regulations do not allow individual parking spaces to be accessed from a public street.

The submitted plan indicates the provision for individual parking spaces along proposed streets "A" and "C".

- E. Staff sent out an Agency Memo to the Boone County Building Department, Community Development Division, Boone County Public Works, Boone County Schools, Boone County Water District, Burlington Fire District, Florence Fire Department, the Kentucky Transportation Cabinet, and SD1.

1. Bridget Striker, Boone County Planning Commission Community Development Division, replied that there are neither structures nor sites of historic significance within the project area.
2. Randy Childress, Florence Fire Department, replied that he spoke with the Burlington Fire District and that they will be doing the plan review on this project.
3. Chad Eha, Burlington Fire Protection District, replied as follows: Just a few quick questions regarding the sight plan submitted for Hickory Burlington/Poplar Pointe. Would it be possible to cul-de-sac Lariat Way before the private drive to allow traffic not committing to the private drive for the new development a turn around especially larger trucks like our equipment. Also, would it be possible to connect Lariat Way into Trevino Lane allowing a second egress in the event of a larger scale incident. Residents could become stuck in the event of a large-scale incident once the fire department secures a water supply line from a hydrant blocking the

only proposed access point to the proposed development. Other than that, it looks like a very nice development for the area.

4. Mike Rouse, Boone County Water District, replied that the master meter should be supplied by Trevino Lane.
5. Rob Franxman, County Engineer, replied that a full traffic impact study should be completed and that KY 237 and Cannondale should be part of that study. He would request the intersection of Oakbrook and Holbrook be looked at.

### STAFF CONCERNS

- A. The following list provides a summary of those requirements that are being requested to be waived/altered using the PD district regulations:
  1. Allow townhouse and multi-family residential dwellings.
  2. Reduce the width of sidewalks immediately adjacent to off-street parking areas from six (6) feet to four (4) feet.
  3. Allow entrance signs along local streets rather from arterial, collector, or subcollector streets.
  4. Increase the maximum grade allowed for public streets from twelve (12) percent to thirty-four (34) percent.
  5. Allow individual parking spaces along public streets.
  6. Allow for off-site signage.
- B. The submitted plans provide notations stating that there will be approximately 139 townhome units and approximately 157 multi-family units. The project narrative states that there will 139 townhome units and 157 multi-family units. The applicant needs to address the discrepancy between the generalized notation on the plans and the specific numbers provided in the project narrative.
- C. Given the number of proposed dwelling units, the applicant should address what, if any, amenities will be provided as part of the project.
- D. The applicant should address the timing of the proposed extension of Holbrook Lane, both to the project entrance as well as further to the west.
- E. The applicant has indicated that they would like to have the names of the two proposed distinct development areas added to the existing signage for Valley Creek Farms, at Camp Ernst Road. The applicant should address the ownership of these signs and the owner's willingness to accommodate the request.
- F. The following modifications should be made to the proposed concept development plan:
  1. Holbrook Lane should be extended to the western boundary of the site.
  2. Sidewalks should be provided along both sides of the proposed Holbrook Lane extension.
  3. Sidewalks should be provided along both sides of the proposed Lariat Way extension.
  4. Sidewalks should be provided along both sides of the proposed private access drive, from Lariat Way to the multi-family dwelling area.

5. An additional walking trail should be provided from the multi-family development area to the proposed walking trail.
6. Additional perimeter buffer yard width should be provided within the northern portion of the site.

**CONCLUSION**

This request needs to be evaluated by the Boone County Planning Commission and Boone Fiscal Court in terms of Articles 3 and 15 of the Boone County Zoning Regulations, the appropriate planning documents noted herein, and the potential impacts on existing and planned uses in the area.

Respectfully submitted,



Michael D. Schwartz  
Director, Zoning Services

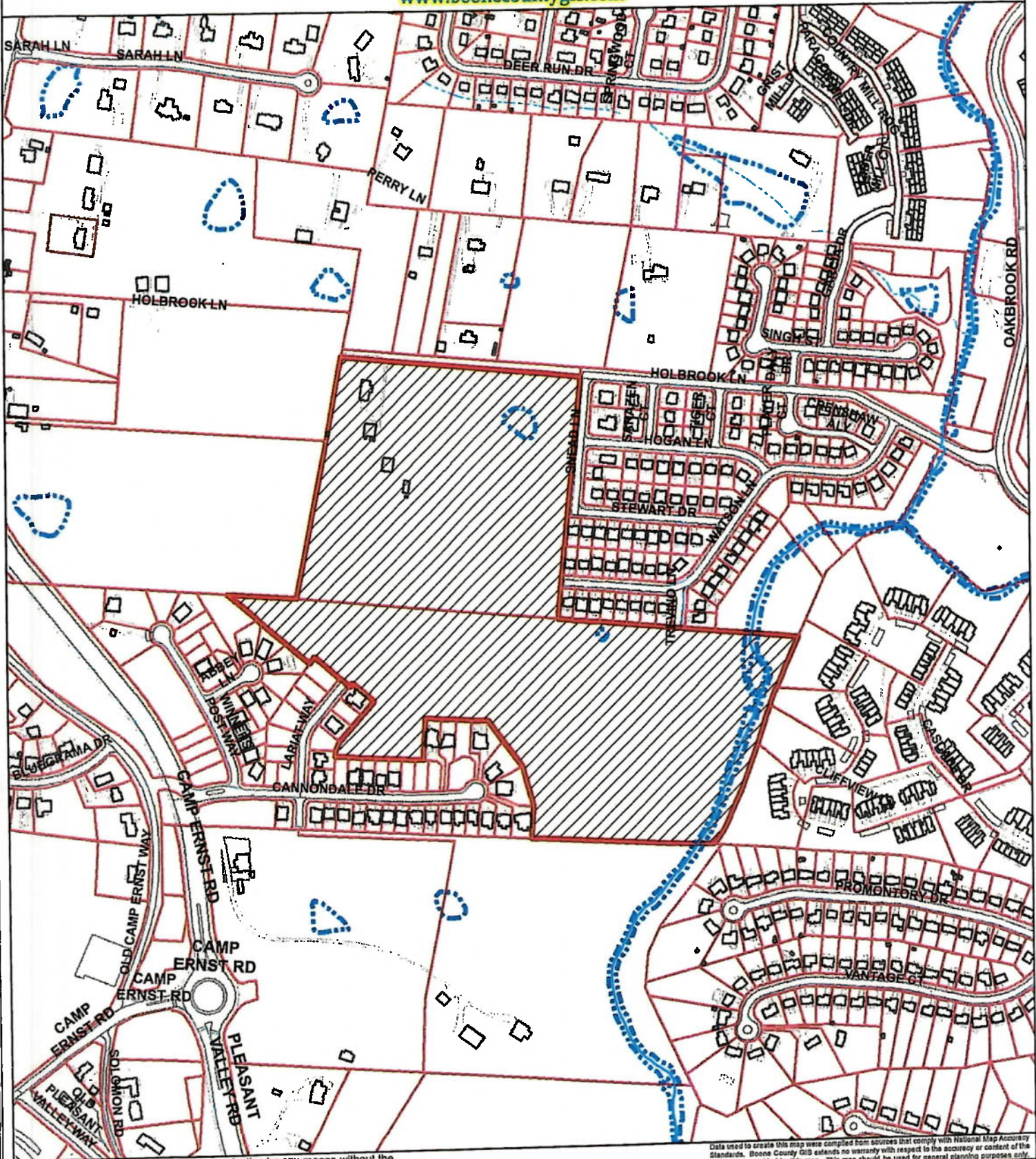
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**Attachments:**

- \*Vicinity Map
- \*Valley Creek Farms Preliminary Plat Map
- \*Aerial Map
- \*Topographical and Floodplain Map
- \*Zoning Map
- \*2040 Future Land Use Map
- \*Pipeline Buffer Map
- \*Concept Plan Map
- \*Application
- \*Project Narrative
- \*Traffic Assessment
- \*Concept Development Plan

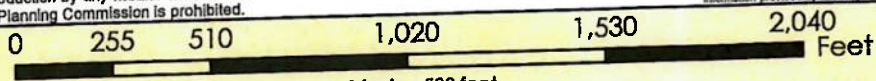
# Vicinity Map

[www.boonecountygis.com](http://www.boonecountygis.com)



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1 inch = 500 feet



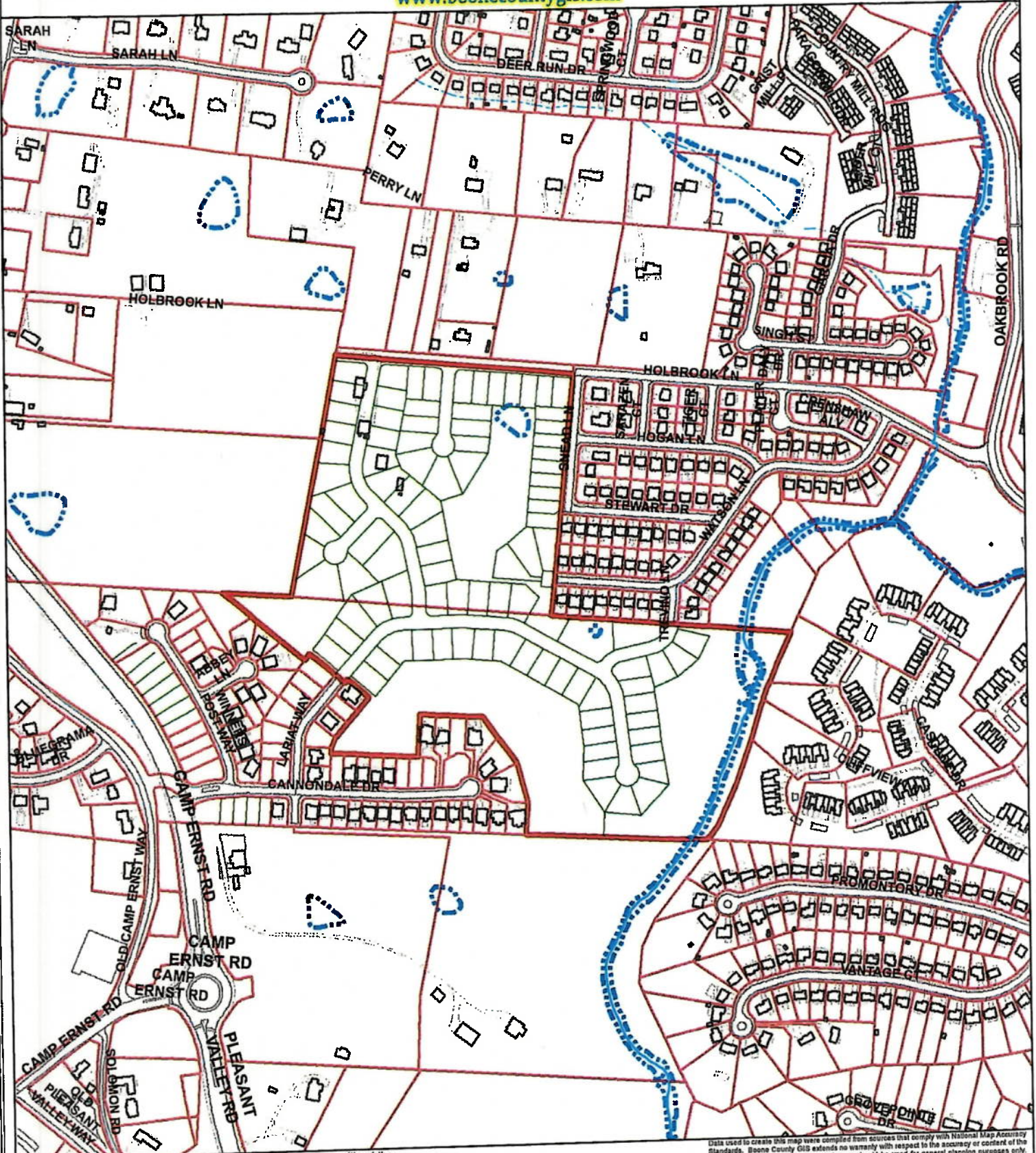
**Boone County GIS - Putting Northern Kentucky on the Map**



AcMap Document: \* .mxd

# Valley Creek Farms Preliminary Plat Map

[www.boonecountygis.com](http://www.boonecountygis.com)

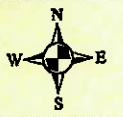


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**Boone**



Boone County GIS  
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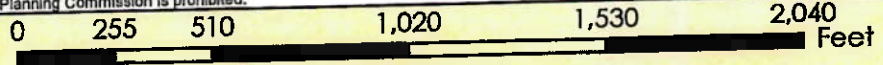
# Aerial Map

[www.boonecountygis.com](http://www.boonecountygis.com)



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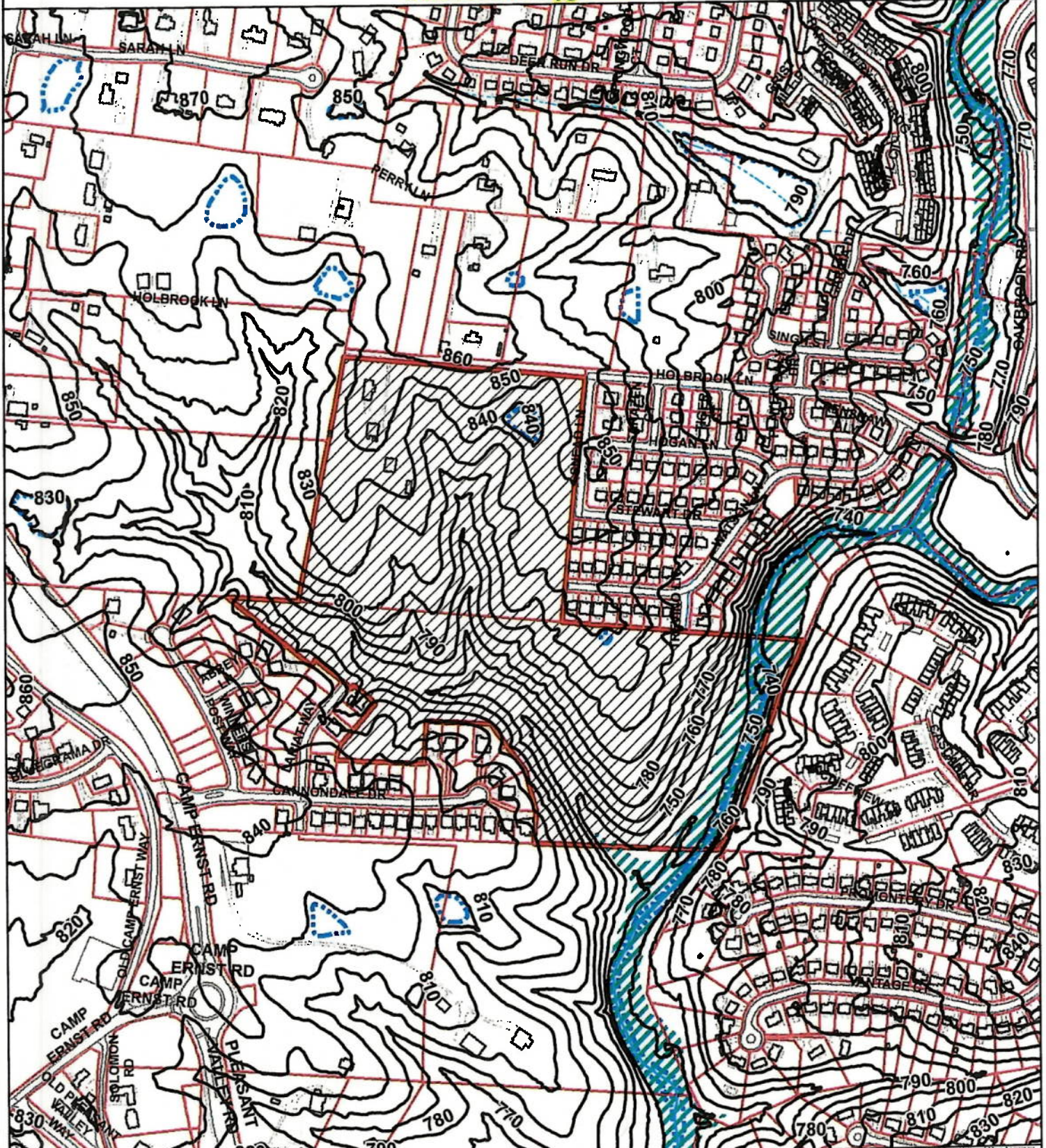
## Boone County GIS - Putting Northern Kentucky on the Map

Map Created: xx/xx/2022

North Arrow: Copyright © 2004 ESRI  
ArchMap Document: \*med

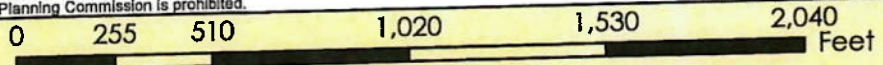
# Topographic and Floodplain Map

[www.boonecountygis.com](http://www.boonecountygis.com)



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**Boone County GIS - Putting Northern Kentucky on the Map**

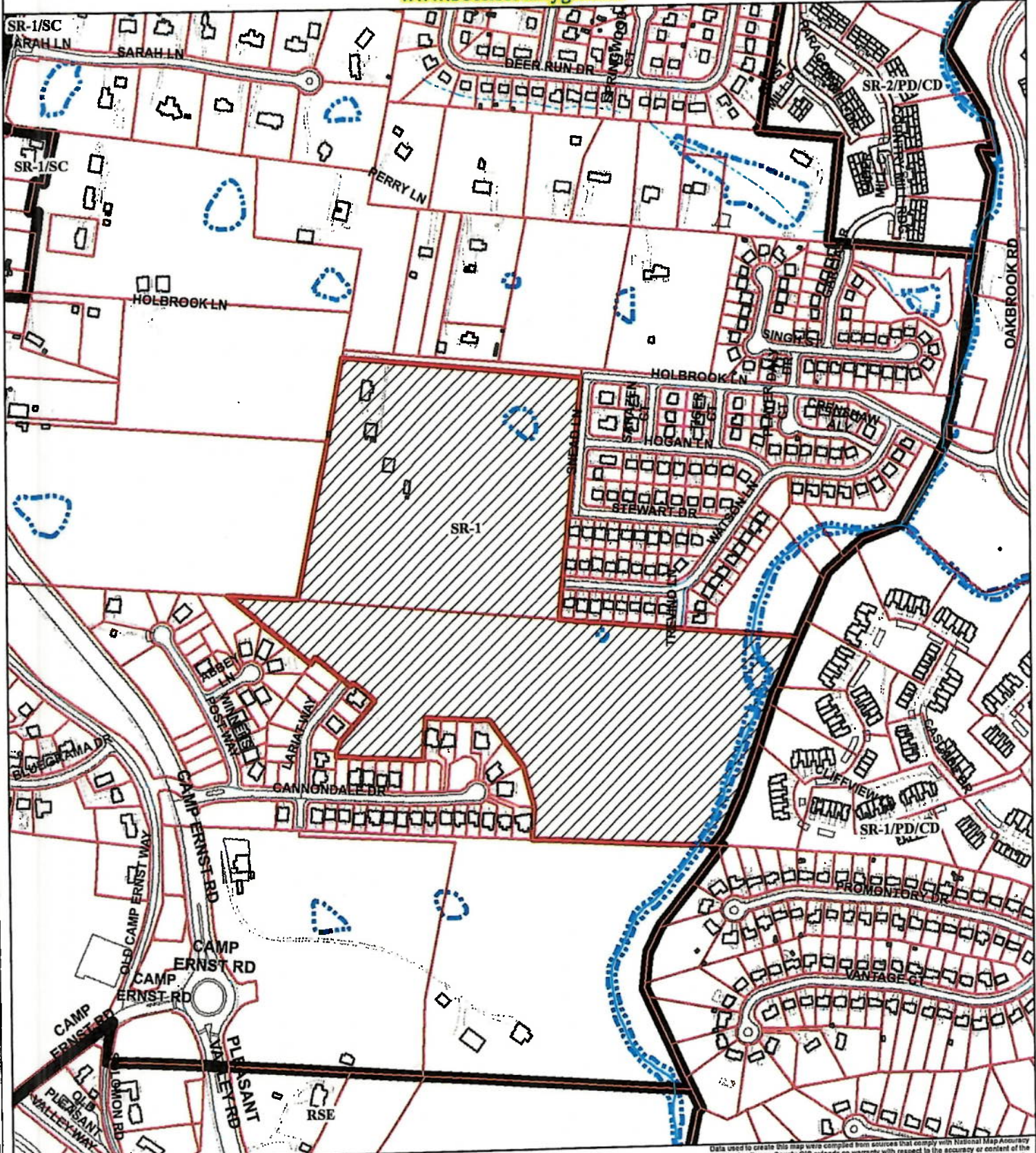


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Map Document: \* .mxd

# Zoning Map

[www.boonecountygis.com](http://www.boonecountygis.com)



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Map Date: 1/10/2022 9:58:12 AM  
ArcMap Document: \*mxd

# 2040 Future Land Use Map

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1 Inch = 500 feet



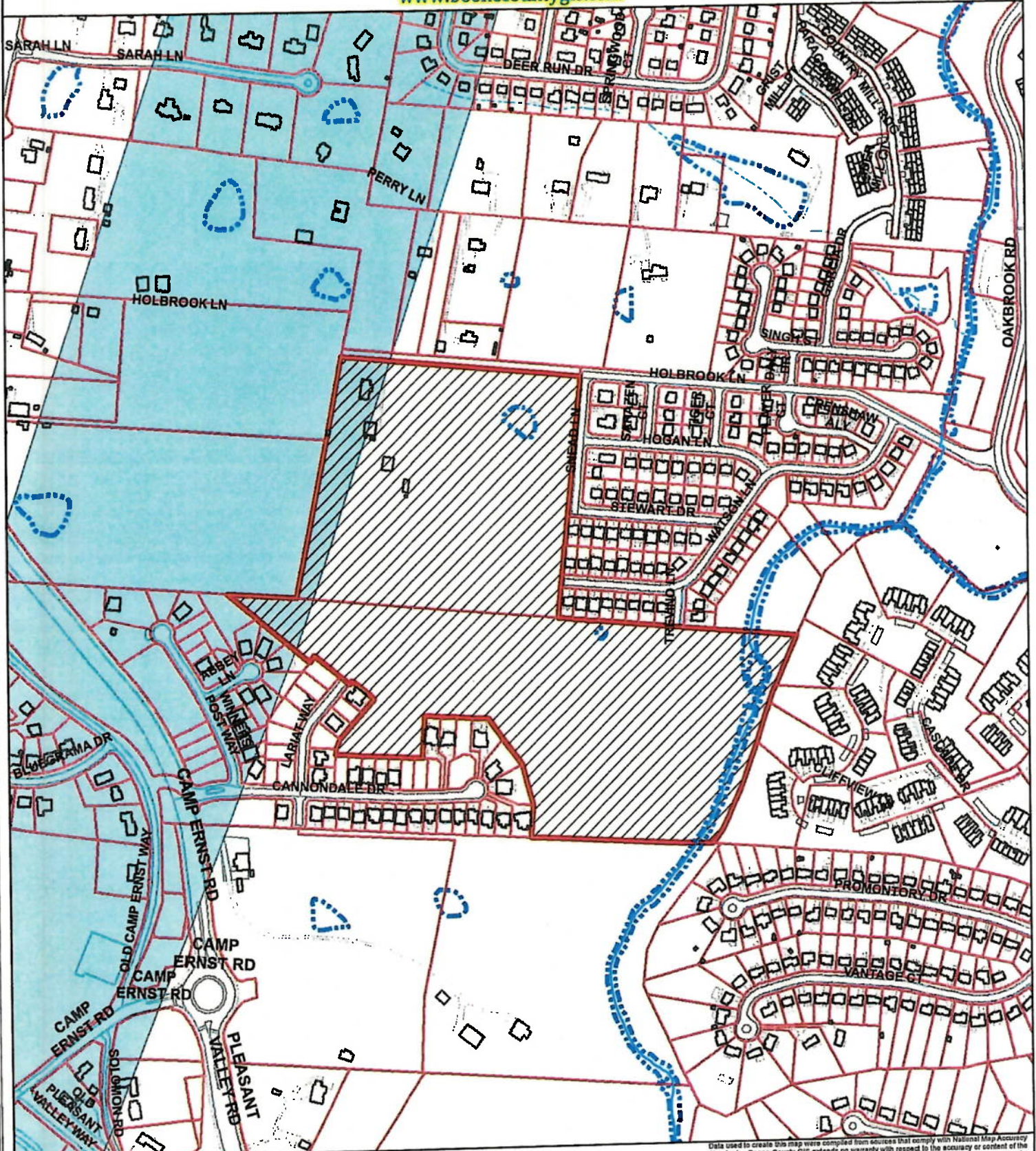
**Boone**

Map Created: x/x/2022

Boone County GIS  
ArcMap Document: \* mxd

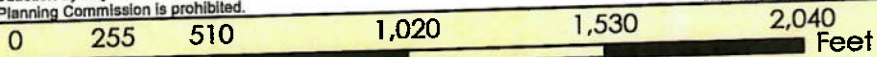
# Pipeline Buffer Map

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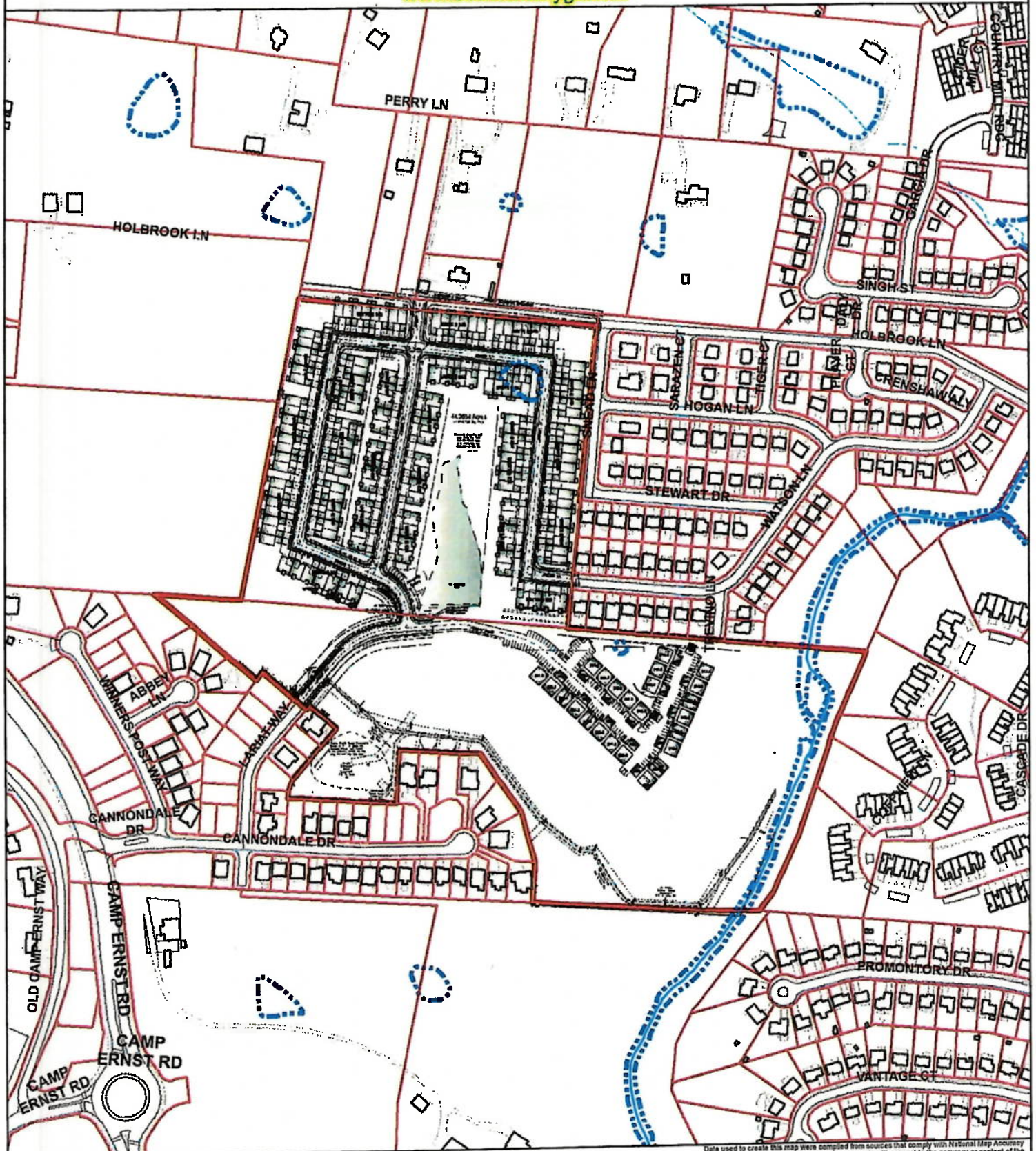


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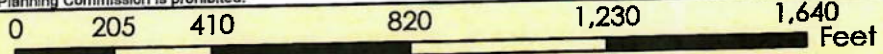
# Concept Plan Map

[www.boonecountygis.com](http://www.boonecountygis.com)



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Map Created: x/xx/2022

Map Document: \*mxd



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12. 1196 93 2031  
Deed Book Page Number Group Number

13. Have you had a pre-application meeting with the BCPC staff:  Yes  No

14. Have you submitted a Concept Development Plan:  Yes  No

15. Have you met or discussed your proposed development with any of the following organizations/agencies (check all that apply):

- |  |   |
|--|---|
| <input checked="" type="checkbox"/> Boone County Building Department   | <input type="checkbox"/> Local Fire District                          |
| <input type="checkbox"/> Boone County Public Works Department  | <input type="checkbox"/> Local School District                        |
| <input type="checkbox"/> Boone County Water District   | <input type="checkbox"/> Northern Kentucky Health Department          |
| <input type="checkbox"/> Cincinnati Bell   | <input type="checkbox"/> Owen Cooperative Electric, Inc.              |
| <input checked="" type="checkbox"/> Cincinnati/Northern Kentucky International Airport (Kentucky Airport Zoning Commission for height restrictions near the airport) | <input type="checkbox"/> Sanitation District No. 1                    |
| <input type="checkbox"/> Duke Energy   | <input type="checkbox"/> USDA NRCS/Boone County Conservation District |
| <input type="checkbox"/> Florence Public Services Department   | <input type="checkbox"/> Other: _____                                 |
| <input type="checkbox"/> Kentucky Division of Water  |   |
| <input type="checkbox"/> Kentucky Transportation Cabinet   |   |

16. Concept Development Plan Jurisdiction/Location (check all that apply):  
 Unincorporated Boone  Florence  Walton  Union

17. Waiver of 60 Day Time Requirement by Originator for Final Planning Commission Action:

In accordance with the provisions of KRS 100.211, the applicant(s) and property owner(s) or originator(s) hereby waive the 60 day time limit for the Boone County Planning Commission to take final action on my (our) Zoning Map Amendment/Concept Development Plan application. This time limit waiver is considered effective immediately upon receipt by the Boone County Planning Commission and expires on September 19, 2024

Property Owner's Signature: Paul May

Applicant's Signature: Paul May

**SECTION B:** (To be completed by Planning Commission staff)

1. Date Received: 7/2/24 Fee Received: \$3203.31 Receipt #: 90839

2. Number of Copies Received: \_\_\_\_\_

3. Has the following been submitted (check all that apply):

- Completed Application
- Concept Development Plan
- Legal Description
- Names and Mailing Addresses of Adjacent Property Owners

4. Date the application is Administratively Complete (as defined in KRS 100.211): \_\_\_\_\_

5. Staff Reviewer: \_\_\_\_\_

6. Committee Chairperson: \_\_\_\_\_

7. Scheduled Public Hearing Date: \_\_\_\_\_

8. Boone County Planning Commission Action: \_\_\_\_\_ Date of Action: \_\_\_\_\_

- \_\_\_\_\_ Approved
- \_\_\_\_\_ Approved with Conditions
- \_\_\_\_\_ Denial
- \_\_\_\_\_ Other

9. Resolution Number: \_\_\_\_\_

Boone County Planning Commission  
Boone County Administration Building  
2950 Washington Street, Room 317  
P.O. Box 958  
Burlington, Kentucky 41005  
Phone: 859-334-2196 Fax: 859-334-2264  
[plancom@boonecountyky.org](mailto:plancom@boonecountyky.org)  
[www.boonecountyky.org](http://www.boonecountyky.org)

# Valley Creek Farms—

## Hickory Burlington, and Poplar Pointe Apartments

### Project Overview:

Charter Commercial, LLC (applicant) is submitting a Concept Development Plan for a residential development in Boone County, east of Camp Ernst Rd. which will connect via an extension of Lariat Way in a previously approved subdivision known as Valley Creek Farms-Phase A-Section 2A. The applicant will extend Lariat Way approximately 450' feet to a new street which will be directed northward to a series of streets that would connect back in with the Shaker Run Subdivision to the east, at Watson Lane. We also would anticipate a future extension of Holbrook Lane which would potentially connect to the northwest to Camp Ernst Rd. The development will reside on Boone County Auditor Parcel Numbers 049.00-00-073.00 and 050.00-00-001.05, comprising approximately 44.37 acres. The development will be comprised of two product types, a patio home rental product, and a second area with small multi-family unit buildings. The Patio Home rental product will be referred to as "Hickory Burlington" and the multi-family portion of this project will be referred to as "Poplar Pointe Apartments". Overall, the proposed request is an appropriate transitional use to the existing residential and will provide the residents of Burlington with an alternative housing option amidst the national housing shortage.

The Hickory Burlington project is lower density than traditional multi-family and compatible with the surrounding single-family neighborhoods adjacent to its location. Hickory Burlington will consist of 139 single-story, ranch style, rental patio homes, each with two bedrooms, two baths, and private garages, in attached groupings of 4-8 units. The development will have a distinct neighborhood feel in a quiet setting with heavy landscaping, walking trails, and a retention pond which will be used as an amenity as well as stormwater management. While Hickory Burlington is not age restricted, it is anticipated that many renters will include baby boomers who are downsizing but want to remain in the community and are choosing the flexibility of a low-maintenance lifestyle.

The Poplar Pointe Apartments will be located further east of Valley Creek Farms, and south of Shaker Run Subdivision. It will be situated away from these subdivisions to provide distance between the multi-family buildings and the existing single-family residences. We anticipate the buildings to be located stepping into the topography while providing beautiful view corridors of the wooded area backing up to Gunpowder Creek to the east

and south. We are proposing twenty-four (24) small, multi-family buildings, each with one-bedroom unit apartments. We are proposing thirteen (13) detached, three-car garages (carriage houses), each with a one-bedroom apartment above the garage units. We are proposing 157 units which are broken down to be approximately one hundred thirty-seven (137) 1-Bedroom units. We are proposing approximately 178 parking spaces, including 39 garage spaces, providing approximately a ratio of 1.13 parking spaces per unit.

Anticipated is an overall density of approximately 6.69 units per acre, based upon 296 units on 44.37 acres. This would be reasonably compatible with the SR-1 density plus a 50% bonus for density. The development would provide approximately 75 % Open Space, walkability with the sidewalks on the proposed streets as well as the proposed walking trails that will interconnect with the trails proposed for Valley Creek Farms. Ultimately, we envision providing connectivity to Shaker Run Subdivision which will provide a pathway to get to Overland Park which is situated to the north and east of this development.

#### Community Facilities:

Boone County School District- Contact will be made to coordinate.

Fire Department: Contact will be made to coordinate.

Sanitary Sewer is present on the property to the south and east part. Connection could be made to the main in Watson Lane, and additionally mains to the south can serve this development. We anticipate a sanitary main extension to follow the new road network and will connect to the main.

Water is available via connections at Lariat Way and Watson Lane. Applicant's Engineer has been in contact with Boone County Water and understands that water capacity is available for this development.

Duke Energy has gas and electric facilities adjacent to this development which will provide electric and gas to the development.

#### Construction:

It is anticipated that the patio home portion will precede the apartment construction. Roadway construction would be anticipated in late 2024 and early 2025 and patio home construction would commence after mass grading and roadway and utility construction.

Apartment construction would be anticipated mid to late 2025 with completion in 2026.

#### Signage:

Applicant is proposing names to be added to the existing signage for Valley Creek Farms (at Camp Ernst Rd.) with lettering for Hickory Burlington and Poplar Pointe Apartments. Additional signage for Hickory Burlington is proposed at the intersection of Lariat Way and Street "A", as well as small monument sign at the entrance to the Hickory Burlington section at Watson Lane.

It would be anticipated to have signage at the end of Lariat Way as it leads to the private road servicing the Poplar Pointe Apartments.

Access:

Access to the development has been previously mentioned but formally, access to the project will come from Camp Ernst Road via Cannondale Dr., then Lariat Way. The new road network will connect the Hickory Burlington to Shaker Run Subdivision by Watson Lane. Future extension of Holbrook Lane to Camp Ernst Rd. is anticipated in the future as development occurs on those lots north and west of our proposed project.

Traffic Analysis has been performed by Diana Zimmerman and is attached as Exhibit "A"

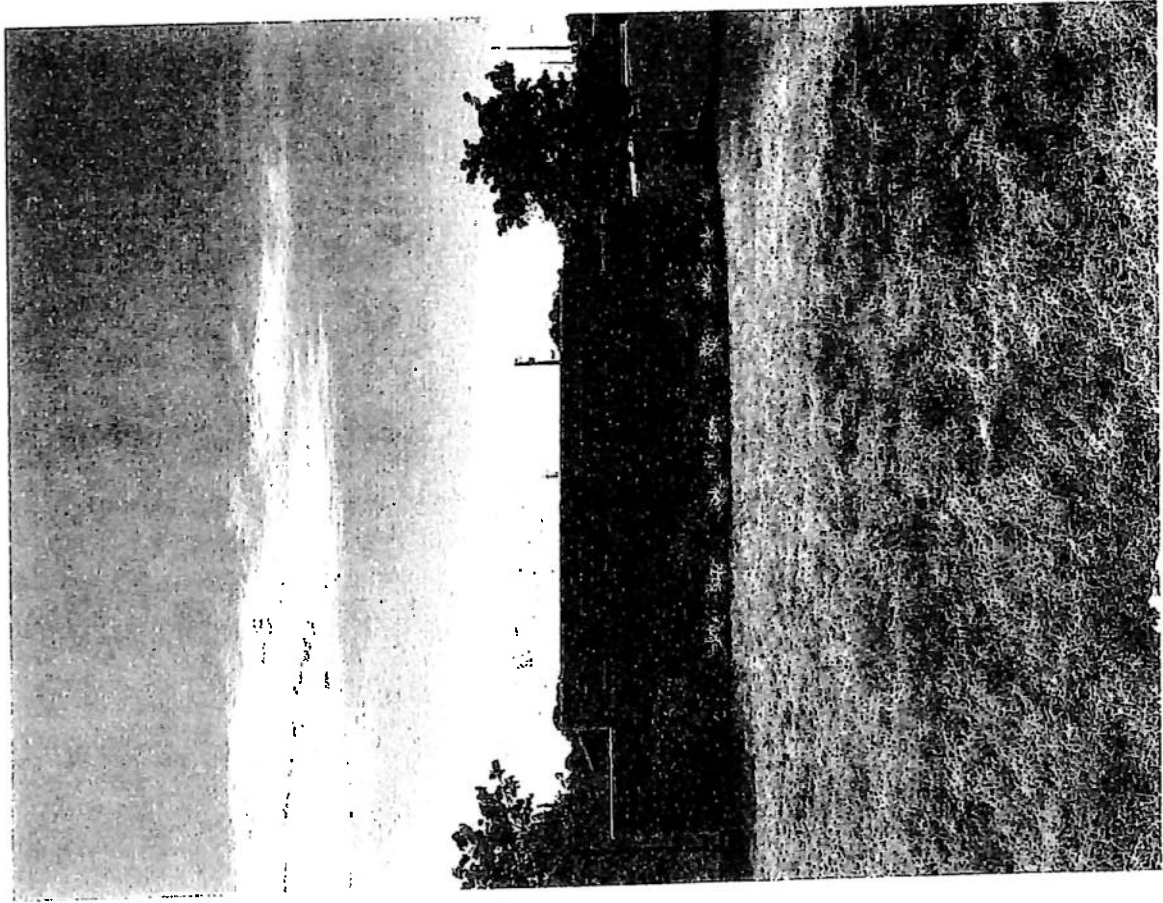
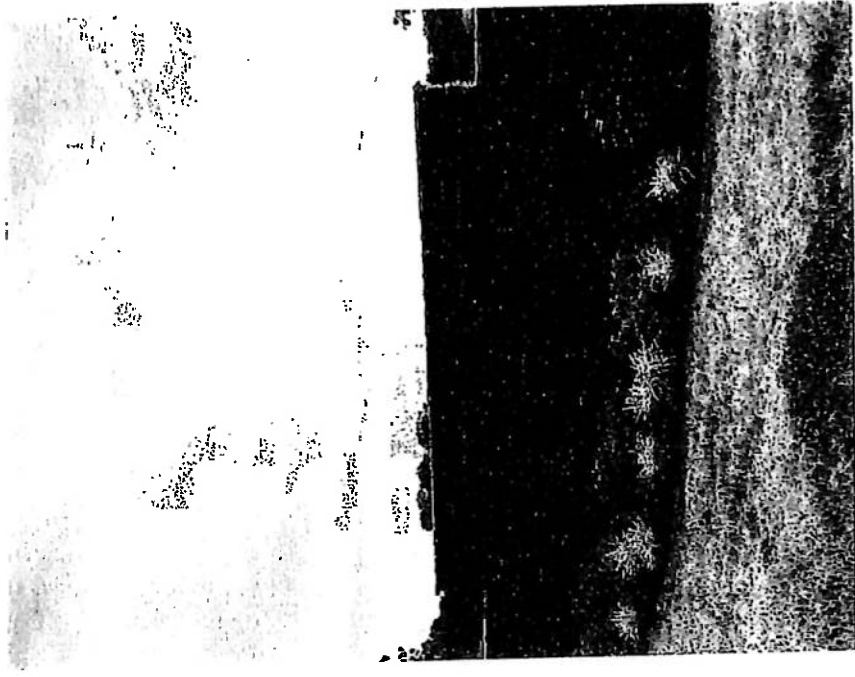
Open Space:

75% Open Space

Development Standards:

We are proposing to follow SR-1 PD Standards which would require setbacks as follows:

Front Setback=	30 ft, 20 ft proposed
Rear Setback=	30 ft, 20 ft proposed
Side Setback=	5', 15 ft total
	0 ft for attached units



final report

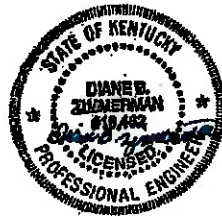
July 1, 2024

## Traffic Assessment

Hickory Burlington & Poplar Pointe Apartments  
Burlington, KY

Prepared for

Boone County Planning Commission



DIANE B. ZIMMERMAN  
Traffic Engineering, LLC

12803 High Meadows Pike  
Prospect, KY 40059  
502.648.1858  
dianeblzim@att.net

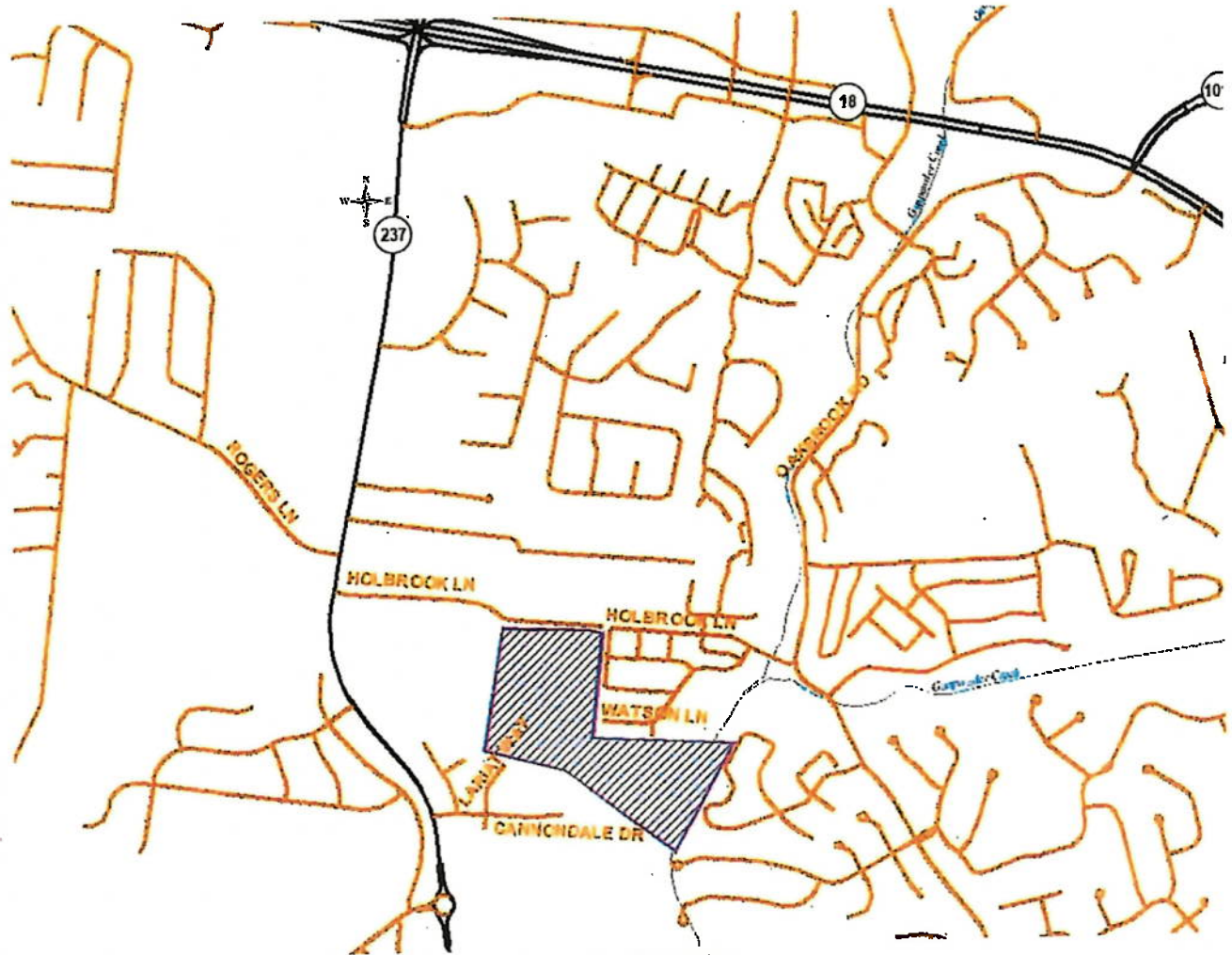


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EXISTING CONDITIONS .....	2
TRIP GENERATION .....	3
Table 1. Peak Hour Trips Generated by Site.....	3
Figure 2. Peak Hour Trips Distribution .....	4

## INTRODUCTION

The preliminary site plan for apartments on Holbrook Lane in Burlington, KY shows 157 apartment units and 139 rental townhouses. **Figure 1** displays a map of the site. Access to the development will be from Holbrook Lane, Watson Lane, and Lariat Way. The purpose of this study is to identify the traffic characteristics of this development and the adjacent road.



**Figure 1. Site Map**

## EXISTING CONDITIONS

Holbrook Lane is maintained by Boone County and is part of Shaker Run subdivision. The road has two eleven-foot lanes with curb and gutter, and an eight-foot multi-use path along the south side. The road is classified as a local road. The speed limit is 25 mph. The intersection with Oakwood Road is controlled with a stop sign on Holbrook Lane. There is a right and left turn lane on Holbrook Lane.

Lariat Way is maintained by Boone County and is part of Valley Creek Farms subdivision. The road has two eleven-foot lanes with curb and gutter, and sidewalks. The road is classified as a local road. The speed limit is 25 mph. The intersection with Cannondale Drive is controlled with a stop sign on Cannondale Drive. There are no turn lanes.

### TRIP GENERATION

The Institute of Transportation Engineers Trip Generation Manual, 11<sup>th</sup> Edition contains trip generation rates for a wide range of developments. The land uses of "Single-Family Attached (215)" and "Multifamily Housing (Low-Rise (220))" were reviewed and determined to be the best match. The trip generation results are listed in Table 1. This distribution is based upon the existing traffic counts on KY 237 and Oakwood Road. Figure 2 shows the trip distribution throughout the road network during the peak hours.

**Table 1. Peak Hour Trips Generated by Site**

Land Use	A.M. Peak Hour			P.M. Peak Hour			Daily
	Trips	In	Out	Trips	In	Out	Trips
Single Family Attached (139 units)	67	17	50	79	47	32	1,009
Multifamily Low Rise (157 units)	72	17	55	88	55	33	1,082
<b>Total</b>	<b>139</b>	<b>34</b>	<b>105</b>	<b>167</b>	<b>102</b>	<b>65</b>	<b>2,091</b>

Hickory Burlington & Poplar Pointe Apartments  
Traffic Assessment

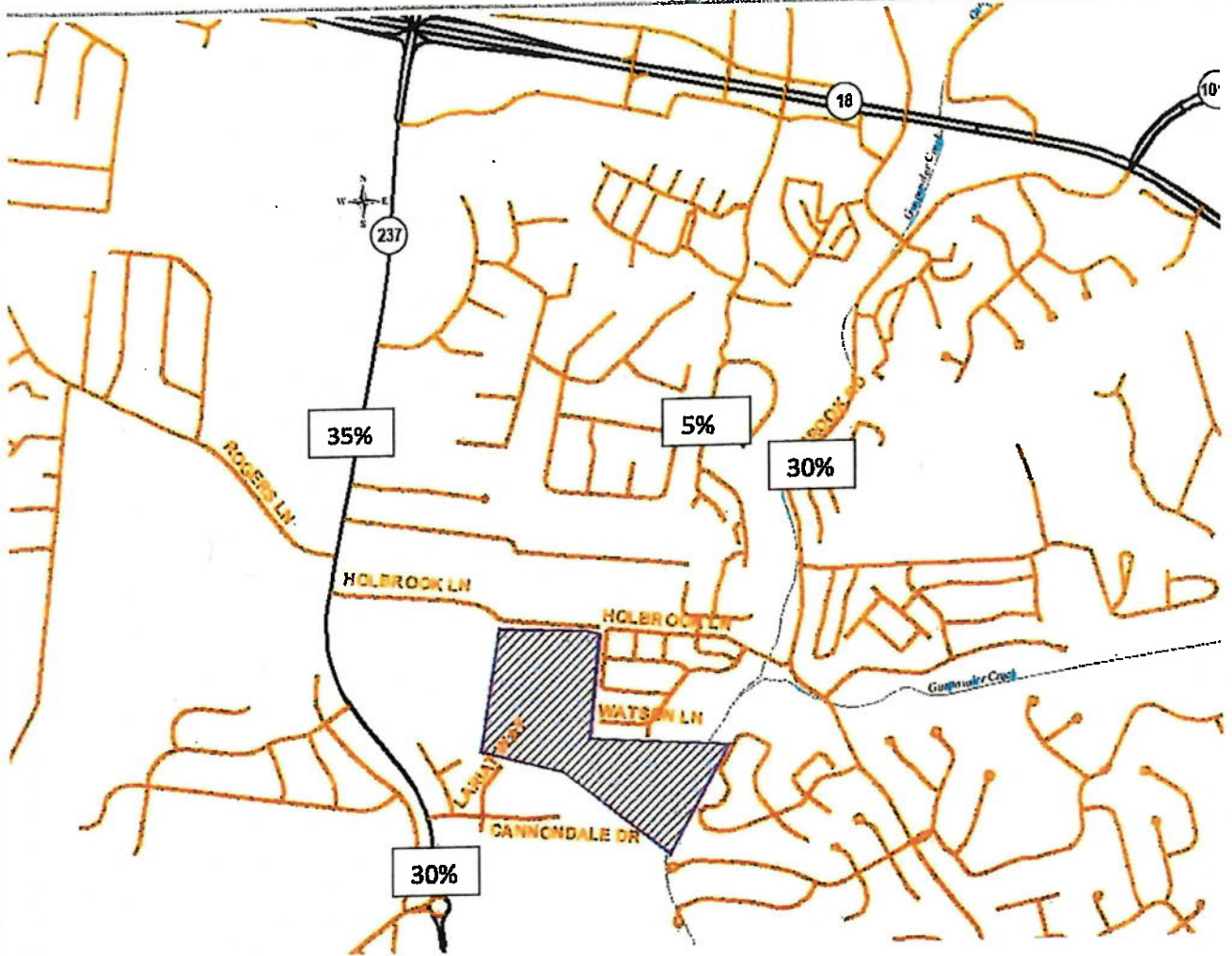
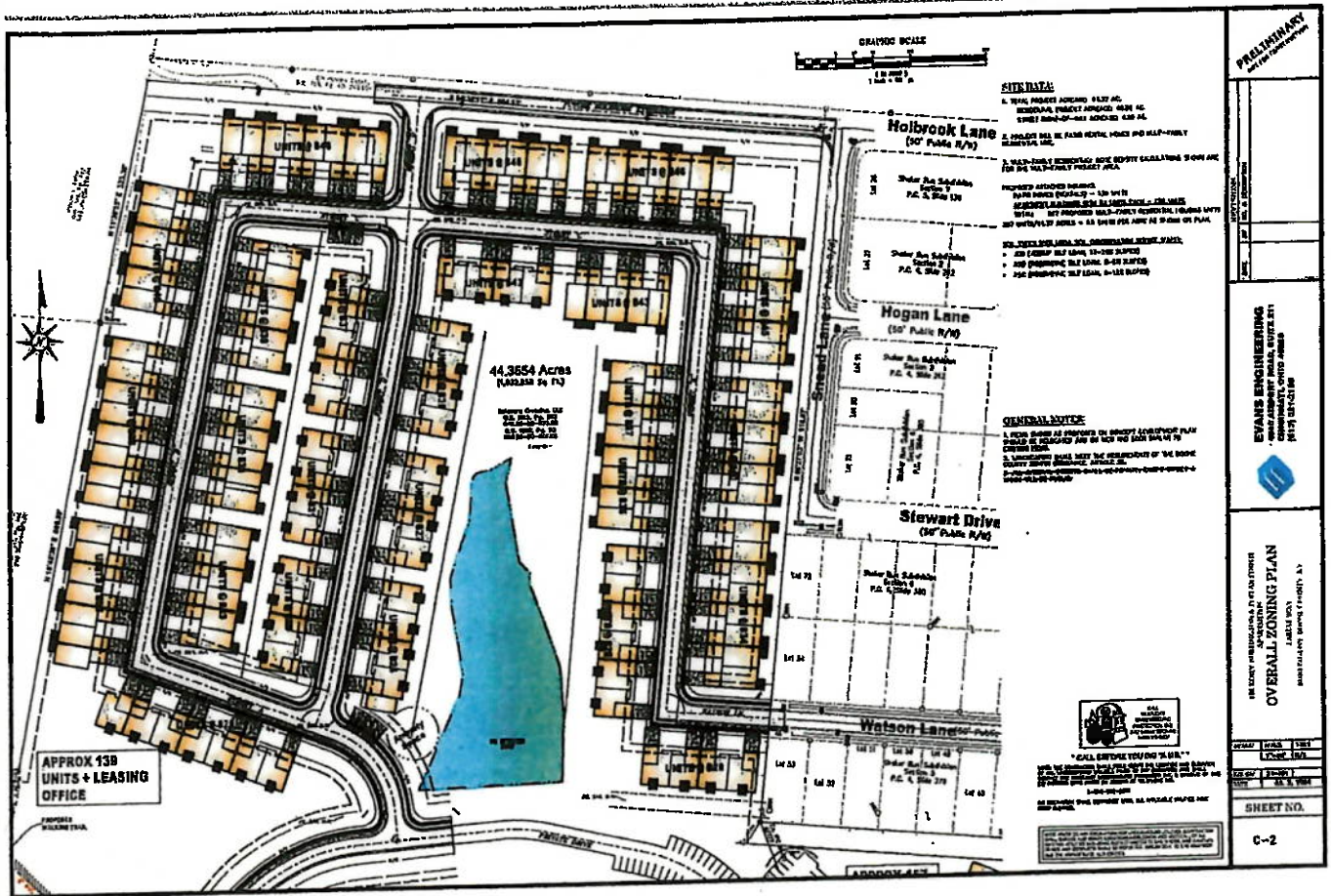
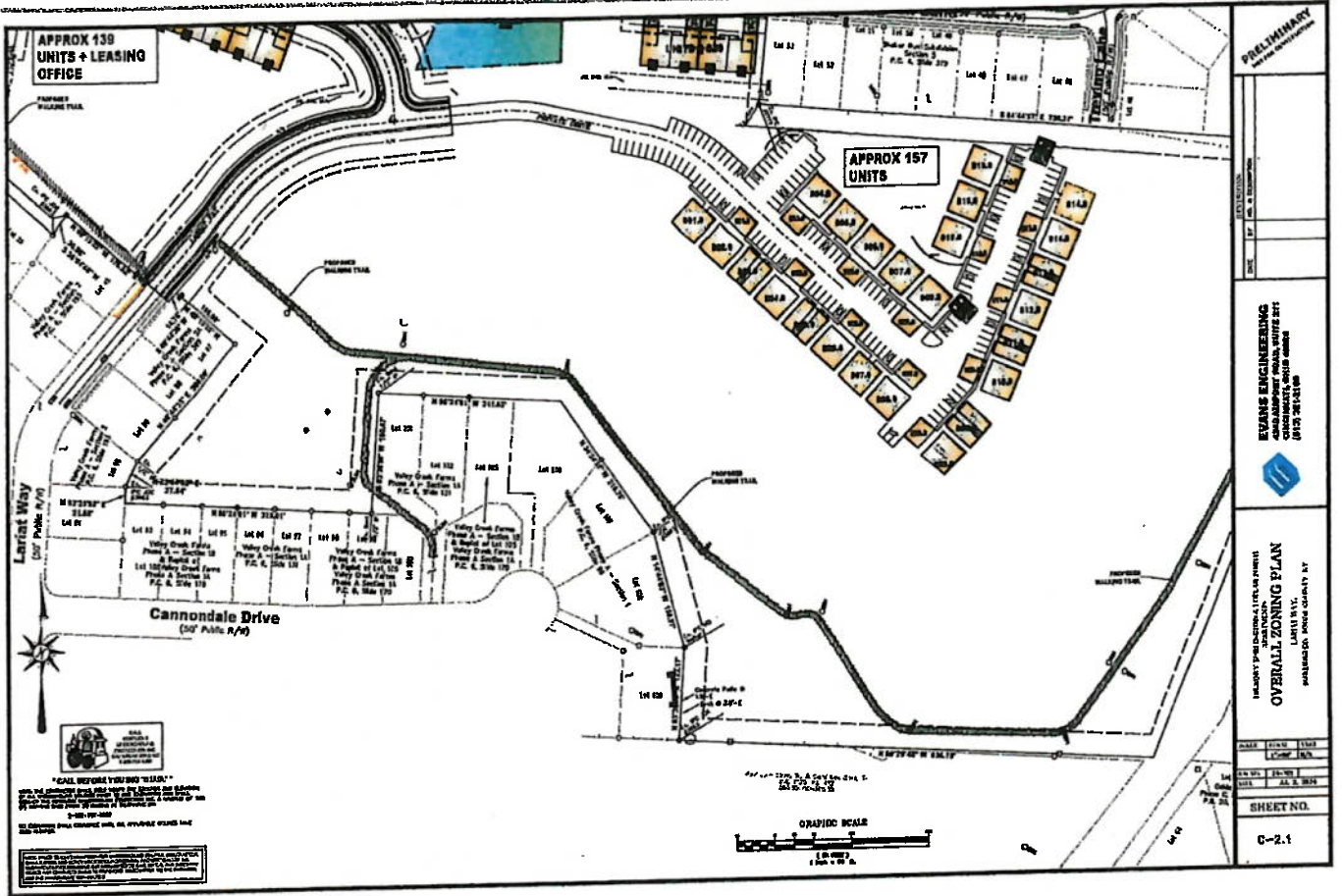


Figure 2. Peak Hour Trips Distribution

# Hickory Burlington & Poplar Pointe Apartments Traffic Assessment



# Hickory Burlington & Poplar Pointe Apartments Traffic Assessment



**PRELIMINARY**

EVANS ENGINEERING  
 4000 W. 10th Street, Suite 201  
 Larchmont, NY 10538-2612  
 (914) 261-1212

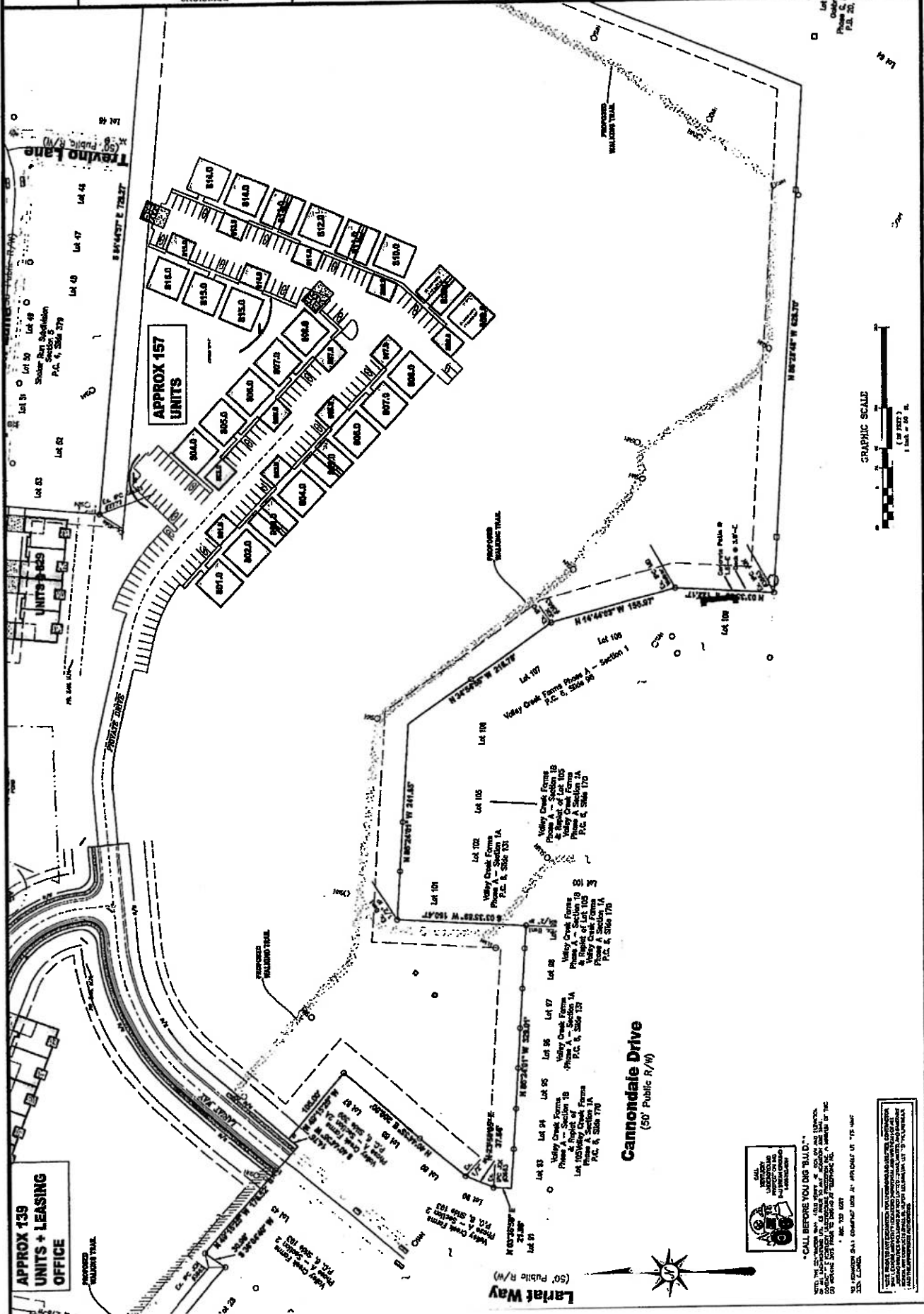
INDENTED TO SHOW PROPERTY BOUNDARIES  
**OVERALL ZONING PLAN**  
 LARCHMONT, NY  
 PREPARED BY: EVANS ENGINEERING

DATE	DESCRIPTION
08/11/10	ISSUED FOR PERMIT
08/11/10	ISSUED FOR PERMIT
08/11/10	ISSUED FOR PERMIT

SHEET NO.  
 C-2.1







**APPROX 139 UNITS + LEASING OFFICE**

**APPROX 157 UNITS**

**Cannondale Drive**  
 (50' Public R/W)



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 800-4-A-DIG IS THE NATIONAL TOLL-FREE NUMBER FOR THE  
 "CALL BEFORE YOU DIG" PROGRAM.

THIS PROJECT HAS BEEN REVIEWED BY THE OHIO DEPARTMENT OF PUBLIC SAFETY AND THE OHIO DEPARTMENT OF TRANSPORTATION. THE REVIEW IS LIMITED TO THE TECHNICAL ASPECTS OF THE PROJECT AND DOES NOT CONSTITUTE A GUARANTEE OF THE ACCURACY OF THE INFORMATION PROVIDED HEREON.

**PRELIMINARY**  
NOT FOR CONSTRUCTION

NO.	DATE	BY	REVISIONS

**EVANS ENGINEERING**  
4240 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45226  
(513) 261-2188



**EXISTING CONCEPT DEVELOPMENT PLAN**  
LICKON & JONAS TOWN  
LICKON WAY,  
BOWLING GREEN, OHIO 43402

DATE	BY	REVISION

SHEET NO.

C-3



**Holbrook Lane**  
(50' Public R/W)

Shaker Run Subdivision  
Section 7  
P.C. & S.M. 130

Shaker Run Subdivision  
Section 2  
P.C. & S.M. 282

**Hogan Lane**  
(50' Public R/W)

Shaker Run Subdivision  
Section 2  
P.C. & S.M. 282

Shaker Run Subdivision  
Section 2  
P.C. & S.M. 282

**Stewart Drive**  
(50' Public R/W)

Shaker Run Subdivision  
Section 2  
P.C. & S.M. 282

**Watson Lane** (50' Public R/W)

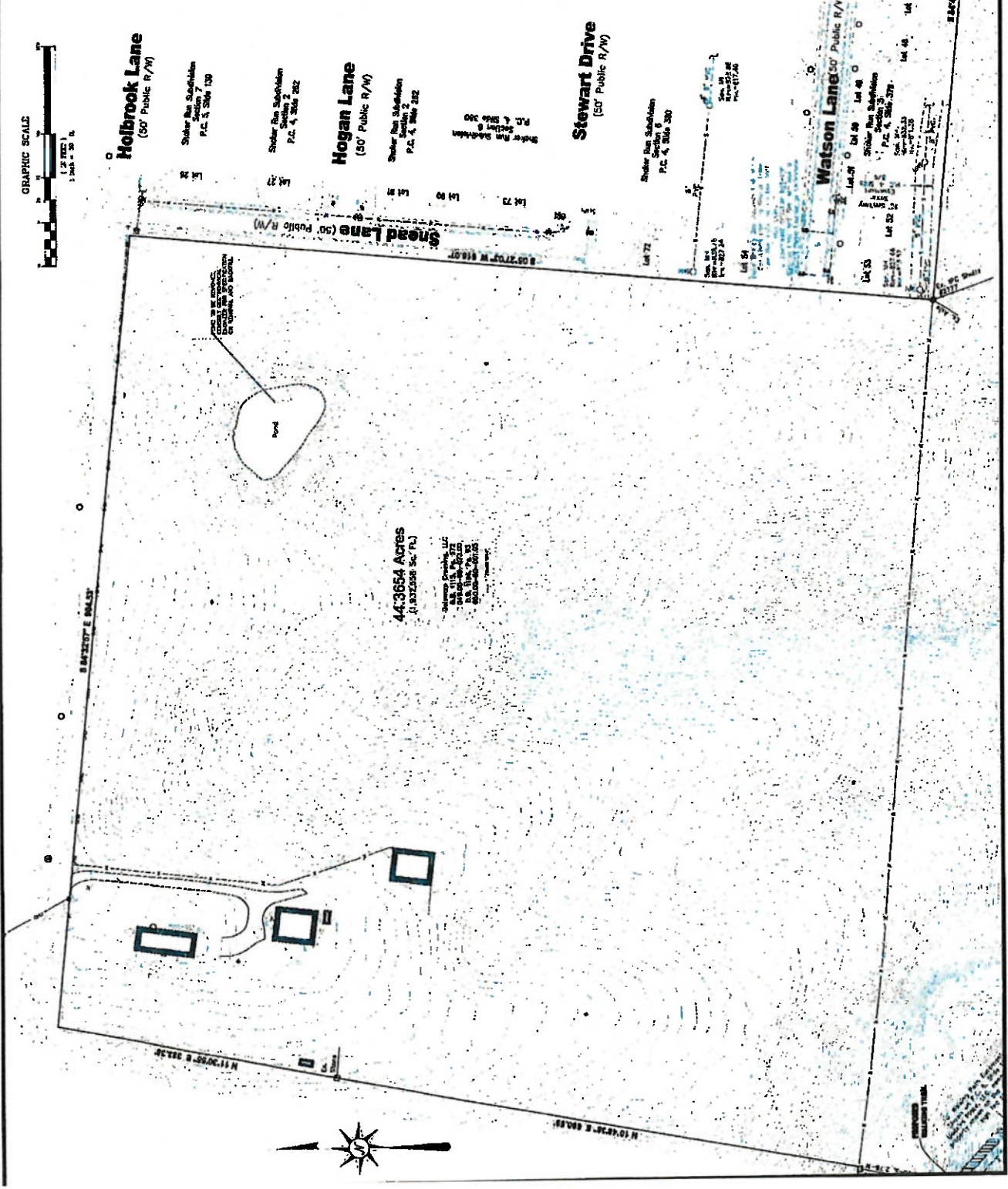
Shaker Run Subdivision  
Section 2  
P.C. & S.M. 282

- LEGEND**
- 1. MAJOR
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  - 3. EXISTING R/W
  - 4. EXISTING R/O
  - 5. EXISTING R/W
  - 6. EXISTING R/O
  - 7. WATER SHED
  - 8. WATER MAIN
  - 9. WATER VALVE
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PRELIMINARY  
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DATE	BY	NO. & REVISION

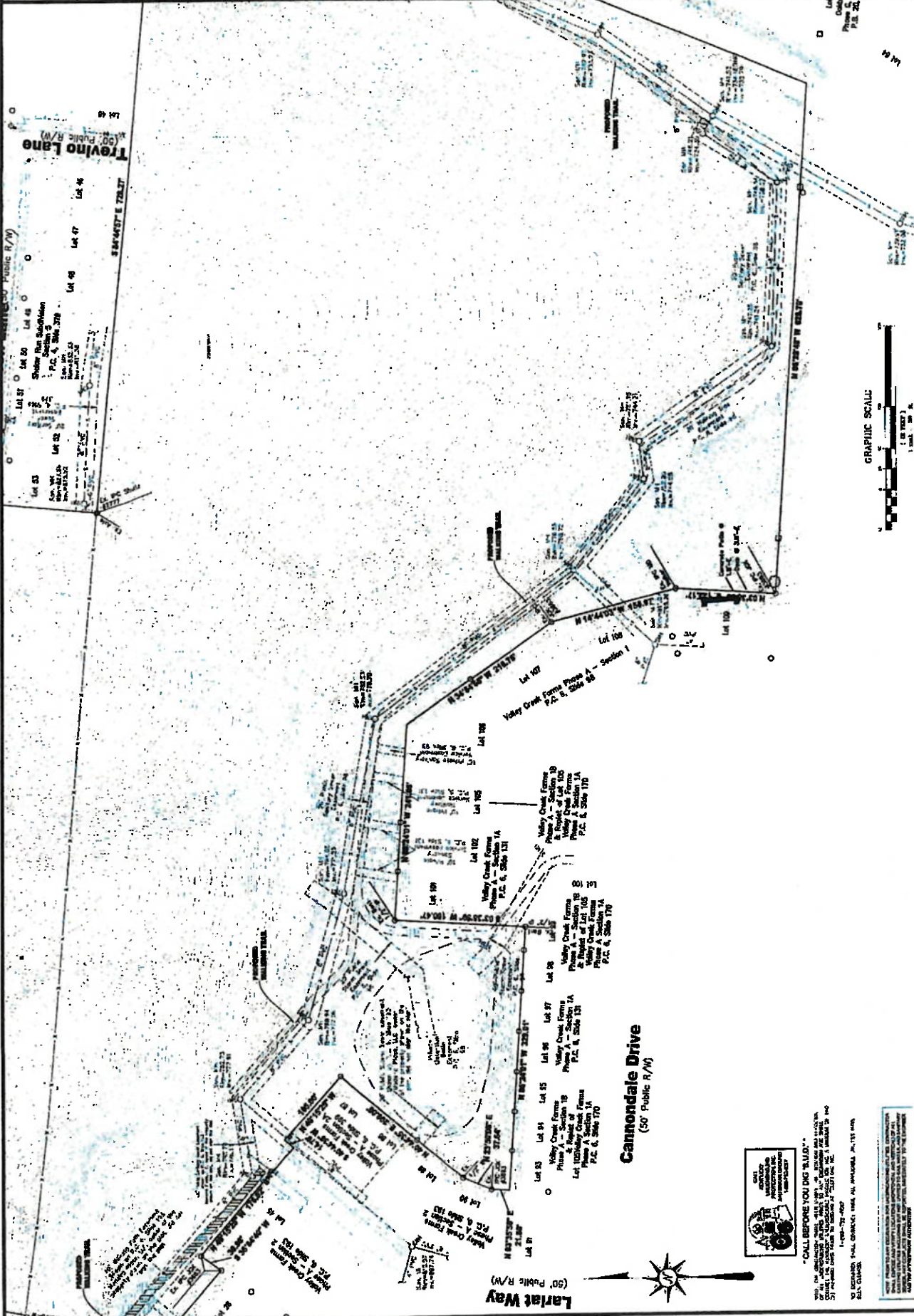
**EVANS ENGINEERING**  
4440 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45226  
(513) 241-2100



HECKROY ENGINEERING & ARCHITECTS  
A MAINTAIN  
DEVELOPING CONCEPT  
EXISTING PLAN  
LANNETT, OHIO  
MUNICIPALITY

SCALE	DATE	BY
1" = 50'	11-28-17	WVA
1" = 50'	11-28-17	WVA
1" = 50'	11-28-17	WVA

SHEET NO. C-3.1



**Cannondale Drive**  
(50' Public R/W)

**Larlet Way**  
(50' Public R/W)



"CALL BEFORE YOU DIG 811"  
The City of Cincinnati provides a free service to help you locate underground utilities before you dig. Call 311 or visit us online at [www.ci.cincinnati.oh.us](http://www.ci.cincinnati.oh.us).  
If you are a contractor, please call 311 or visit us online at [www.ci.cincinnati.oh.us](http://www.ci.cincinnati.oh.us) for more information.  
1-888-777-8887

NO RECORDS SHALL BE MAINTAINED OR PRESERVED FOR THE CITY OF CINCINNATI.  
THESE PLANS AND SPECIFICATIONS ARE THE PROPERTY OF THE CITY OF CINCINNATI AND ARE TO BE USED ONLY FOR THE PROJECT AND AT THE LOCATION SPECIFIED THEREON.  
NO PART OF THESE PLANS OR SPECIFICATIONS IS TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF THE CITY OF CINCINNATI.



**PRELIMINARY**  
 NOT FOR CONSTRUCTION

DATE	NO. & DESIGNATION

**EVANS ENGINEERING**  
 4840 AIRPORT ROAD, SUITE 211  
 CINCINNATI, OHIO 45228  
 (513) 251-2188

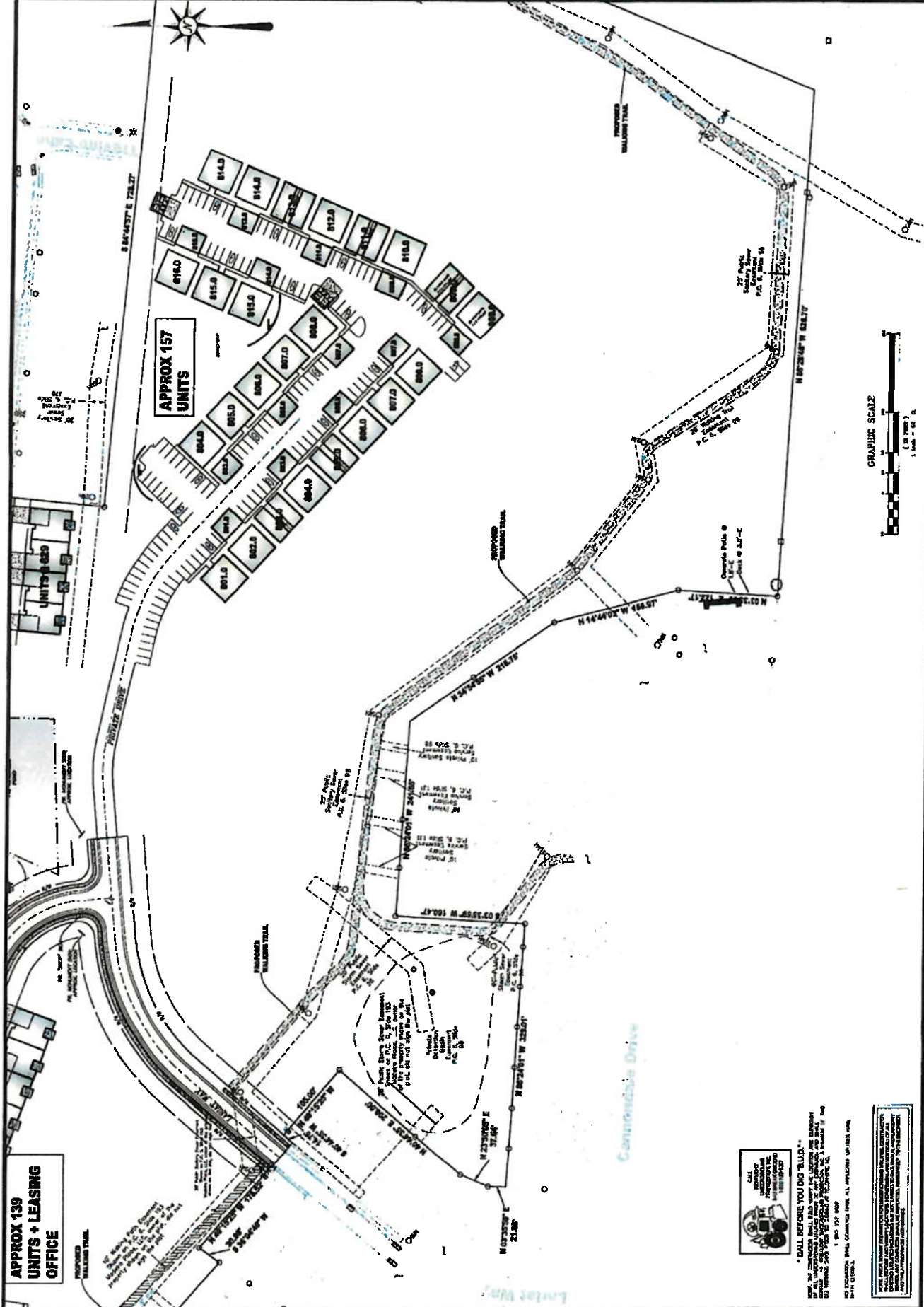


**PROPOSED CONCEPT DEVELOPMENT PLAN**  
 PICHONY BUILDINGS & APARTMENTS  
 (LAWRENCE WAY)  
 WASHINGTON, DC (D.C.)

SCALE	DATE	BY	CHKD
1" = 50'	11-29-07		

SHEET NO.

C-4.1



**APPROX 139 UNITS + LEASING OFFICE**

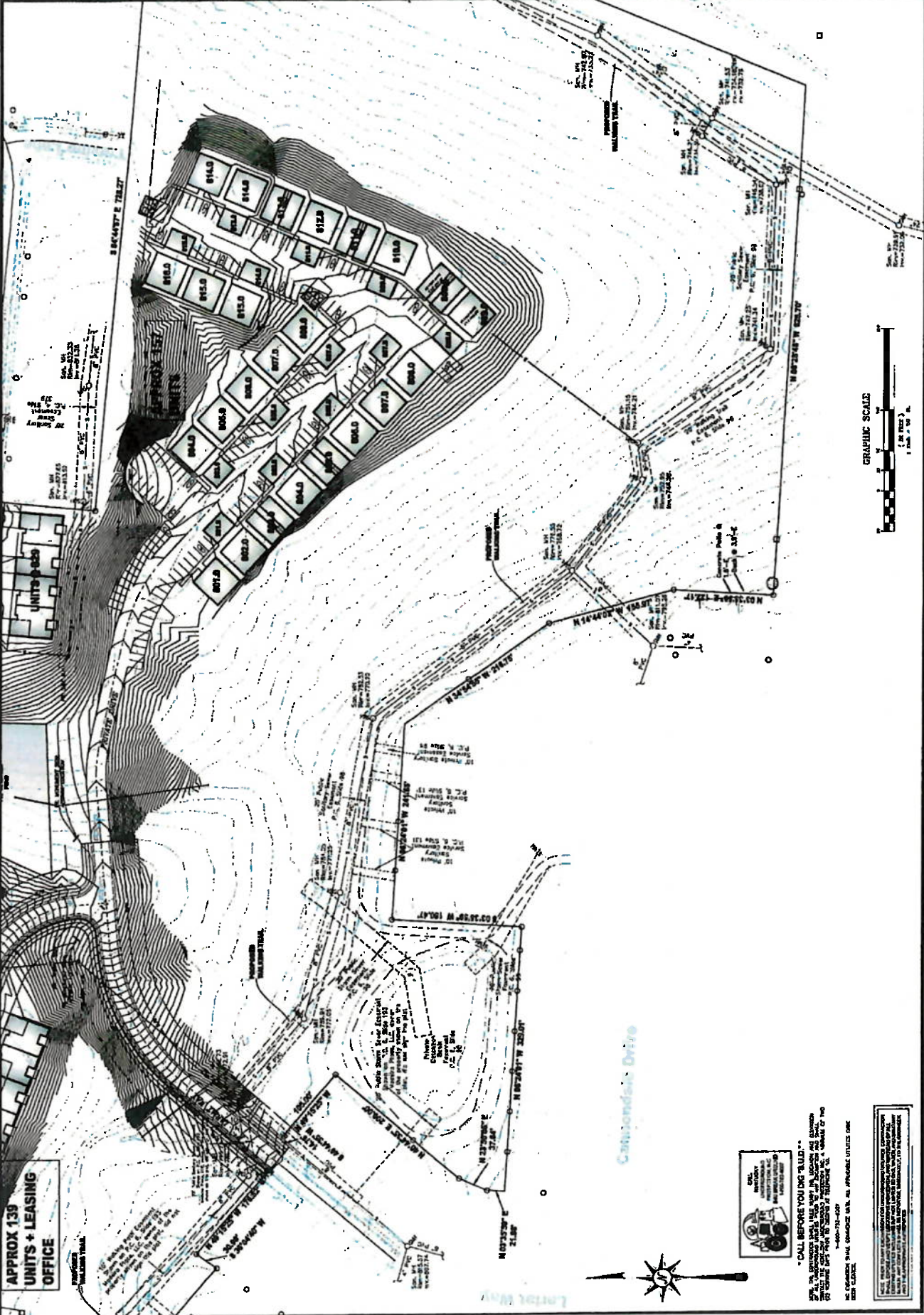
**APPROX 157 UNITS**



**\* CALL BEFORE YOU DIG \*  
 BEFORE ANY CONSTRUCTION SHALL BE UNDERTAKEN, THE LOCATION AND DEPTH OF ALL EXISTING UTILITIES SHALL BE DETERMINED BY A REGISTERED PROFESSIONAL ENGINEER OR SURVEYOR. THE LOCATION OF ALL UTILITIES SHALL BE SHOWN ON THE RECORD DRAWING. THE LOCATION OF ALL UTILITIES SHALL BE SHOWN ON THE RECORD DRAWING.**

**NO EXCAVATION SHALL BE UNDERTAKEN WITHOUT THE OBTAINING OF A PERMIT FROM THE CITY OF WASHINGTON. THE LOCATION OF ALL UTILITIES SHALL BE SHOWN ON THE RECORD DRAWING.**





**APPROX 139 UNITS + LEASING OFFICE**

**CALL BEFORE YOU DIG! 811**

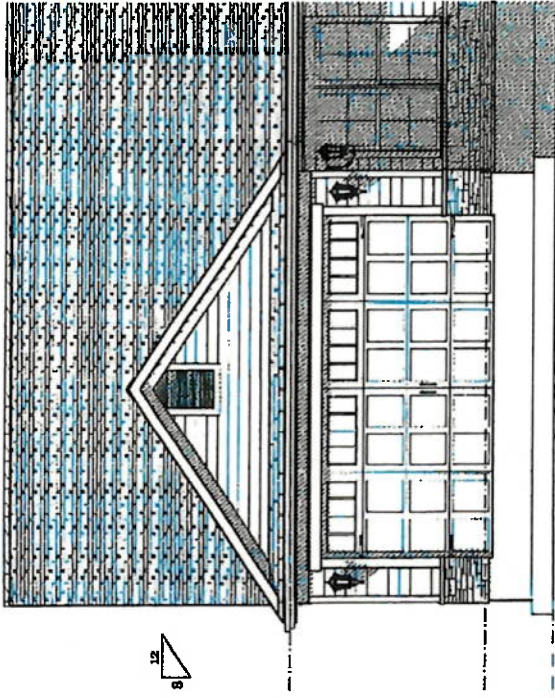
FOR A LIST OF UTILITIES TO CALL, VISIT [www.ohio811.com](http://www.ohio811.com) OR CALL 1-800-487-4877

OHIO 811 IS A SERVICE PROVIDED BY THE OHIO DEPARTMENT OF PUBLIC SAFETY, DIVISION OF EMERGENCY MANAGEMENT AND PREPAREDNESS, 100 EAST WASHINGTON AVENUE, COLUMBUS, OHIO 43260-1000

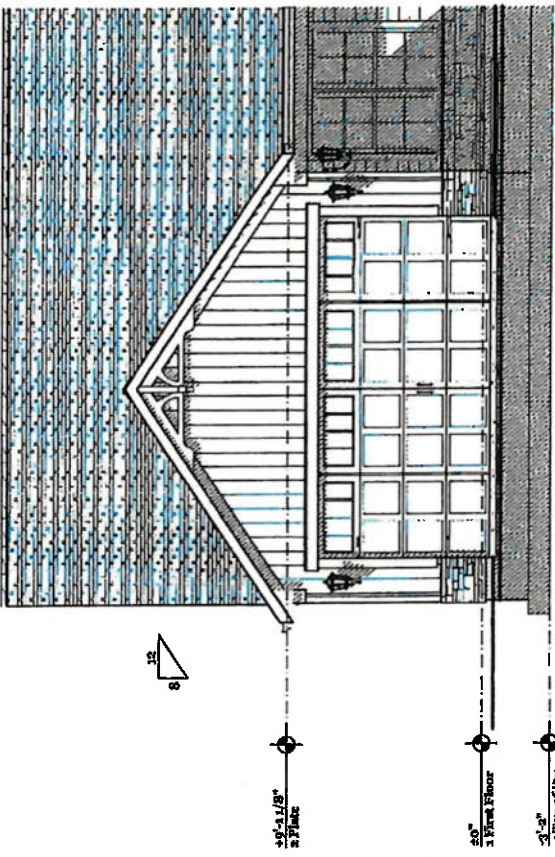
FOR A LIST OF UTILITIES TO CALL, VISIT [www.ohio811.com](http://www.ohio811.com) OR CALL 1-800-487-4877

OHIO 811 IS A SERVICE PROVIDED BY THE OHIO DEPARTMENT OF PUBLIC SAFETY, DIVISION OF EMERGENCY MANAGEMENT AND PREPAREDNESS, 100 EAST WASHINGTON AVENUE, COLUMBUS, OHIO 43260-1000





**Holbrook Ln. • 28' Unit • Front Elevation-B**  
3/16" = 1'-0"



**Holbrook Ln. • 28' Unit • Front Elevation-A**  
3/16" = 1'-0"

**CODE INFORMATION CONT.**

- Structural members shall be as shown.
- All framing shall be in accordance with IRC.
- All exterior walls shall be finished with 1/2" gypsum board.
- All exterior walls shall be finished with 1/2" gypsum board.
- All exterior walls shall be finished with 1/2" gypsum board.
- All exterior walls shall be finished with 1/2" gypsum board.
- All exterior walls shall be finished with 1/2" gypsum board.
- All exterior walls shall be finished with 1/2" gypsum board.

**Note:**  
All Downspouts connected to approved locations

**WALL BRACING METHOD**  
PER IRC 602.10.4  
CEILING CONTINUOUSLY SHEATHED  
WOOD STRUCTURAL PANEL, 5/8" MIN.  
FASTENERS (MINIMUM 16" ON CENTER)  
FASTENED TO ON EDGES AND 12" IN FIELD  
INTERIOR SHEATHING ON BRACED WALLS  
SHALL BE 5/8" MIN. GYPSUM BO. FASTENED  
IN ACCORDANCE WITH IRC TABLE 702.5.5

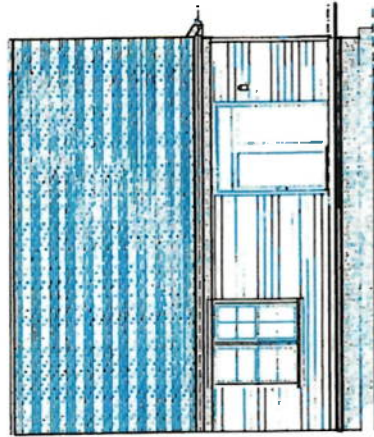
**EXHAUST FAN & SMOKE DETECTOR NOTE**

- 1. IDENTIFIED EXHAUST FAN LOCATIONS (SEE NOTE B, D FOR CODE INFO ON SHEET A-4)
- 2. IDENTIFIED SMOKE DETECTOR LOCATIONS (SEE NOTE E, F ON SHEET A-5 & THE NOTE TO THE LEFT FOR ALL SMOKE DETECTOR CODE INFO)
- 3. IDENTIFIED SMOKE/CO DETECTOR LOCATIONS (PROGRAMMABLE THERMOSTAT REQ'D FOR ALL FORCED AIR FURNACES)

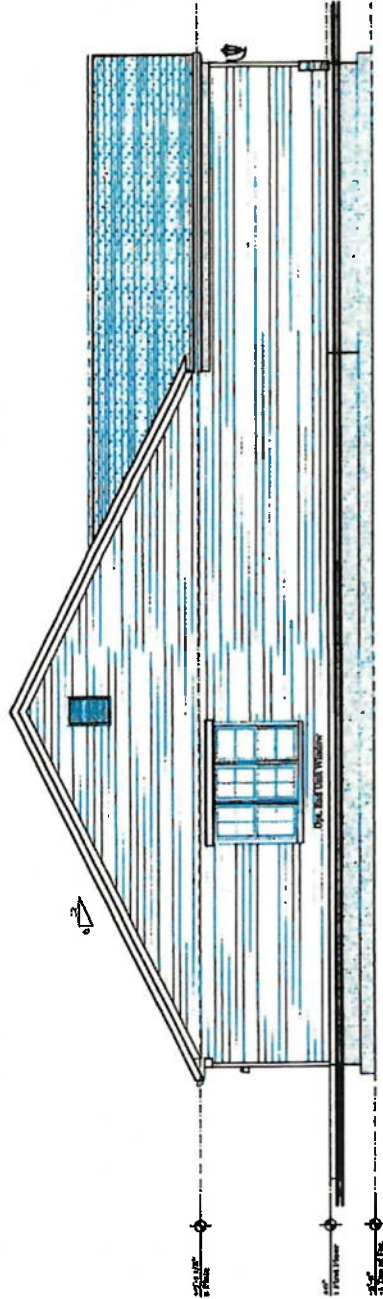
**ALL SMOKE ALARMS SHALL BE LISTED IN ACCORDANCE WITH UL-217 ON EACH LEVEL WITHIN EACH DWELLING UNIT. SMOKE ALARMS UTILIZING PHOTOELECTRIC AND OPTICAL TECHNOLOGY SHALL BE INSTALLED. BEDROOMS TO BE MONITORED BY PHOTOELECTRIC TYPE. SEPARATE OR DUAL-SENSING SMOKE ALARMS MAY BE USED**

**E1**

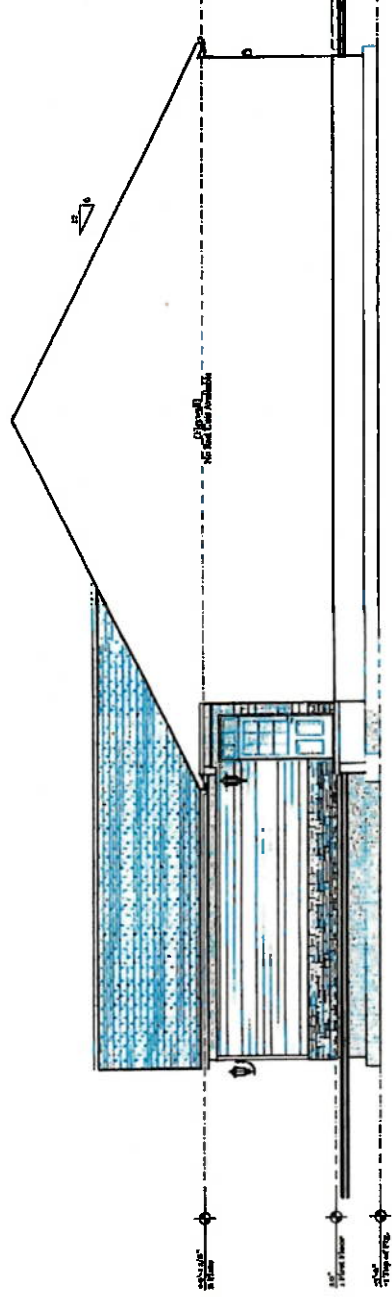
**E1**



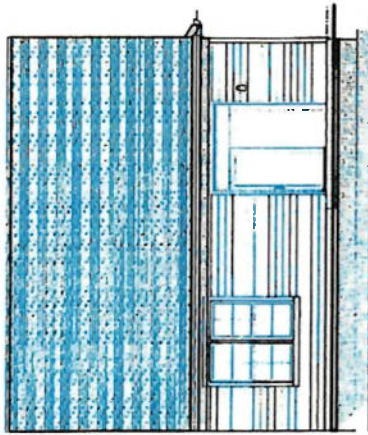
**E2** 28' Unit • Rear Elevation-A  
1/8" = 1'-0"



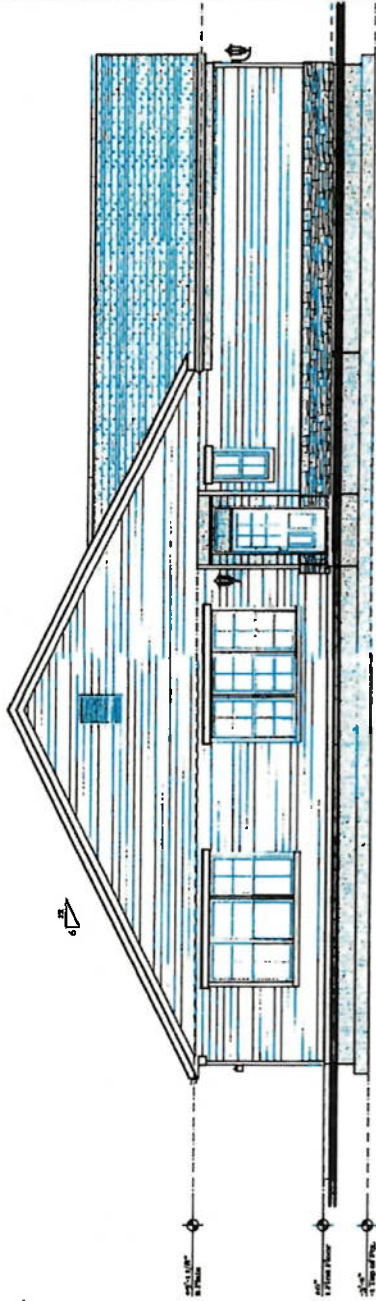
**E3** Holbrook Ln • 28' Unit • Left Elevation-A  
1/8" = 1'-0"



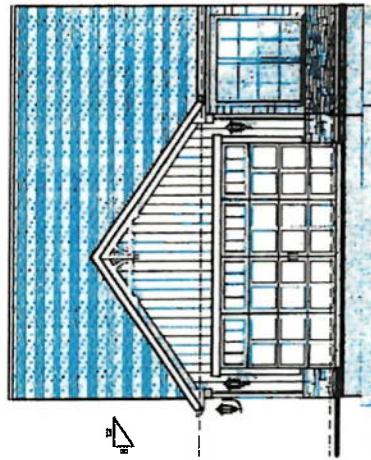
**E4** Holbrook Ln • 28' Unit • Right Elevation-A  
1/8" = 1'-0"



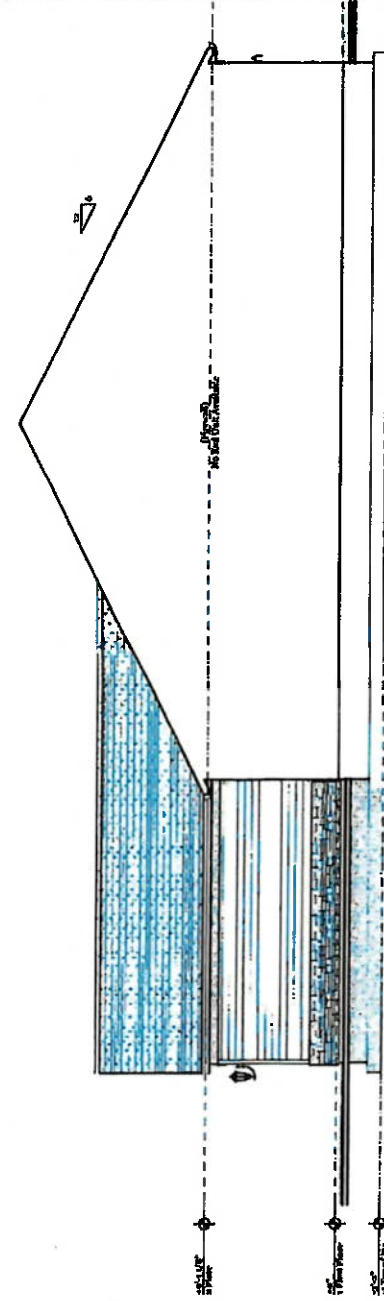
**E2** 28' Unit • Rear Elevation-A  
1/8" = 1'-0"



Holbrook Ln • 28' End Unit • Left Elevation-A  
1/8" = 1'-0"



**E1** 28' End Unit • Front Elevation-A  
1/8" = 1'-0"



Holbrook Ln • 28' End Unit • Right Elevation-A  
1/8" = 1'-0"

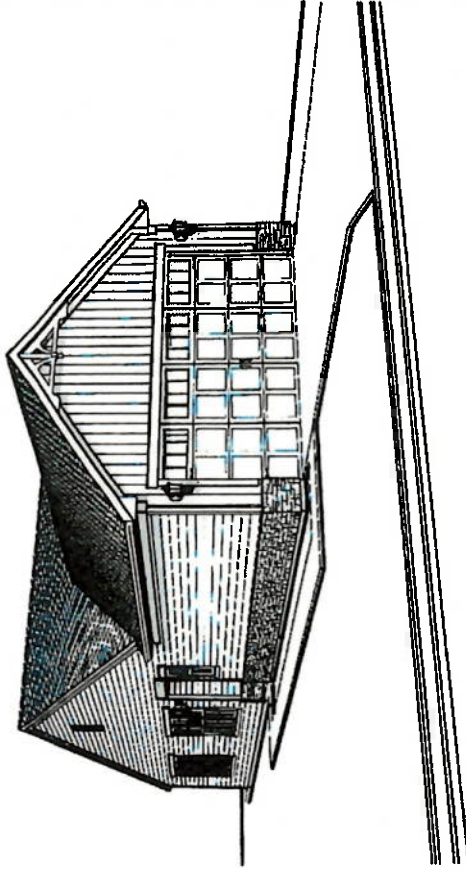
**HBOO** The Multi-Fam's  
 EIA • C. Varles  
 Fort Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
 7203 HOOSTER PIKE • CINCINNATI, OH 45227 • 513-751-1000

**BROOKSTONE HOMES, A.C.**  
 BROOKSTONE

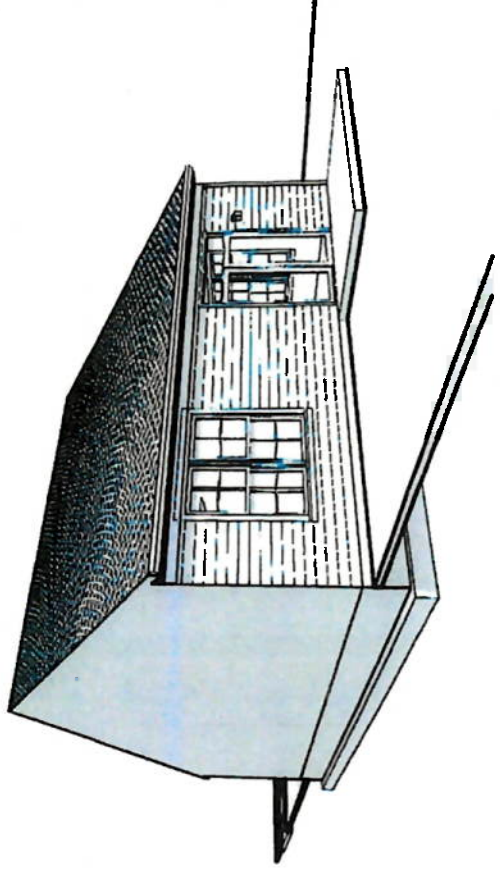
**Orland f/x**  
 11000 Research Rd. • Olive Springs • Rice Aik, Ohio 45424  
 Designed by: R. Fisher • (513) 393-0587  
 Email: orlandfx@icloud.com

**Vf/x**

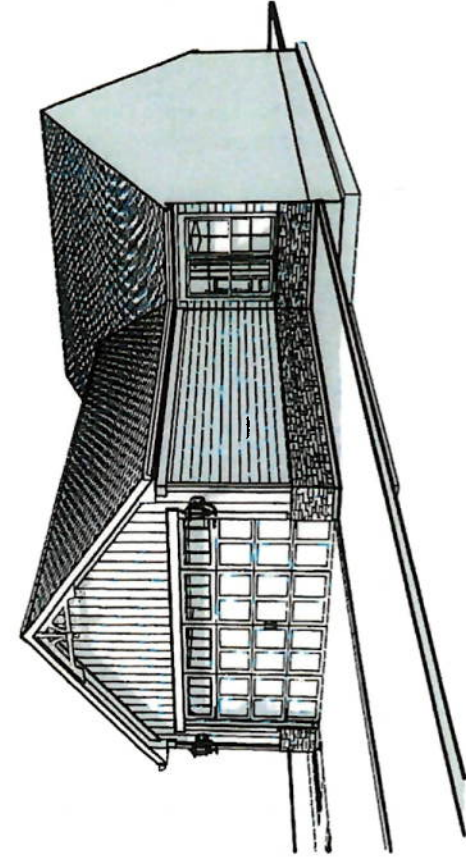




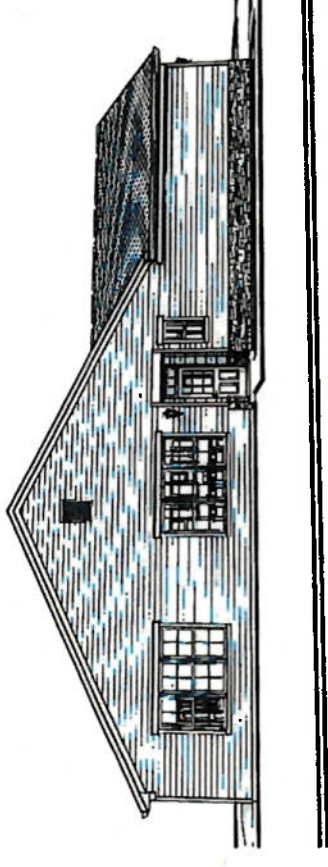
**Vfx2** Holbrook 28' End Ext • Vfx 3D Tour



**Vfx4** Holbrook 28' End Ext • Vfx 3D Tour

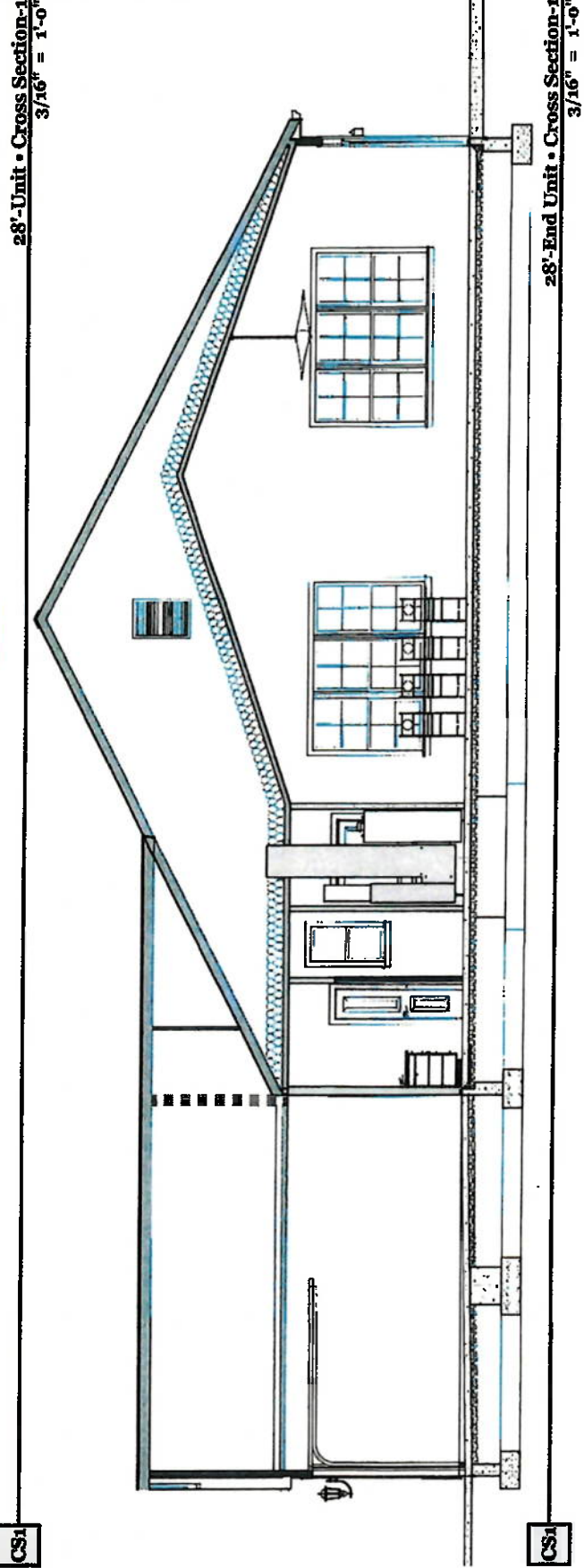
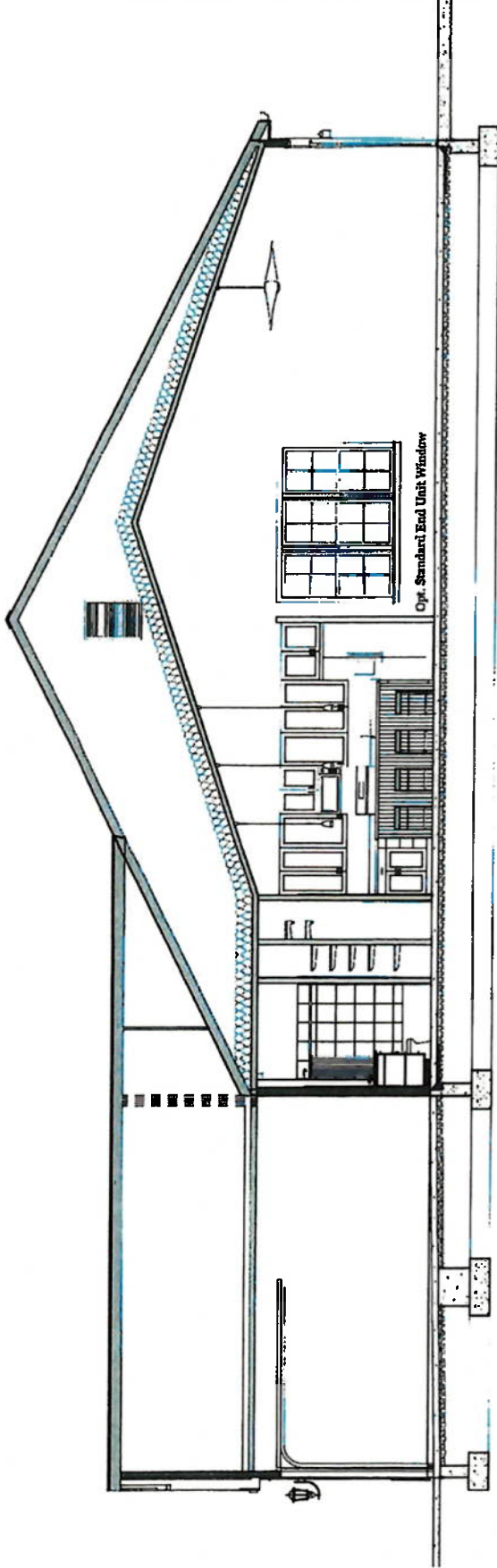


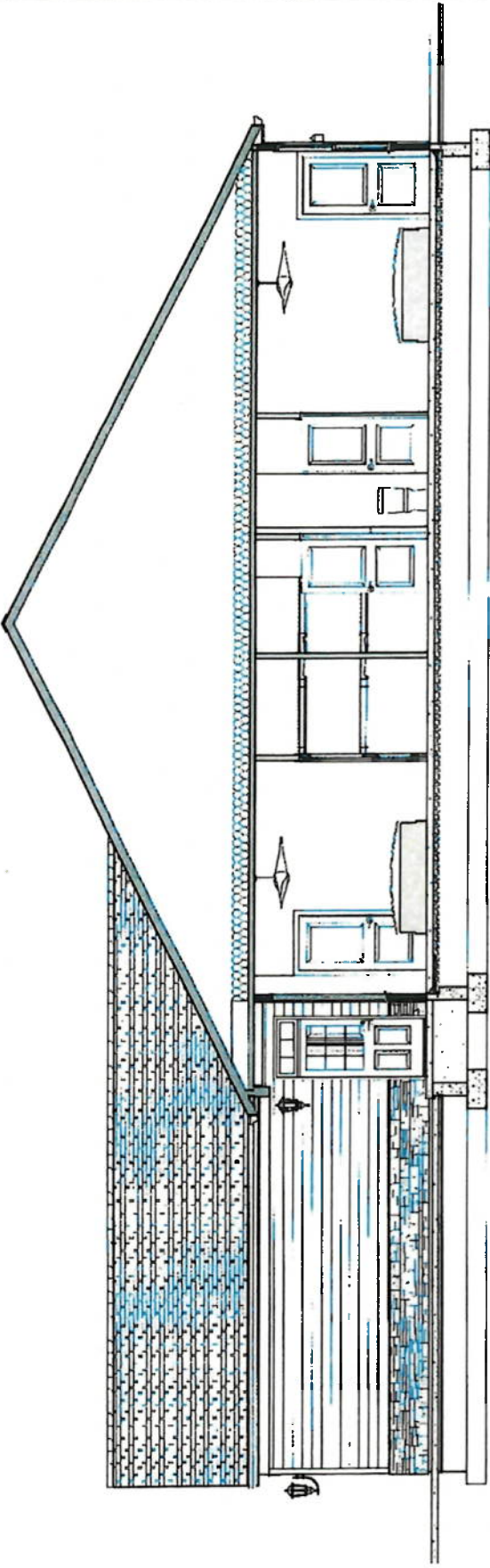
**Vfx1** Holbrook 28' End Ext • Vfx 3D Tour



**Vfx3** Holbrook 28' End Ext • Vfx 3D Tour







28' Unit • Cross Section-2  
3/16" = 1'-0"

CS2

**BROOKSTONE**

The Multi-Ram's  
EIA • G. Vartan  
For: Delaware Crossing, LLC  
1841 HUNTERDOME LANE • FROESCHKE, MARYLAND 41042  
BROOKSTONE HOMES, INC.  
7203 WOODWAY PARK • CHELSEA, OH 45227 • 513-200-7502

**BROOKSTONE**

Developed By: P. J. Fisher • (614) 581-0987  
www.brookstonehomes.com

**CD**  
C.2

SHEET NO. 1 of 2

Vf/x

28' End Unit • Cross Section-2  
3/16" = 1'-0"

CS2

CS2

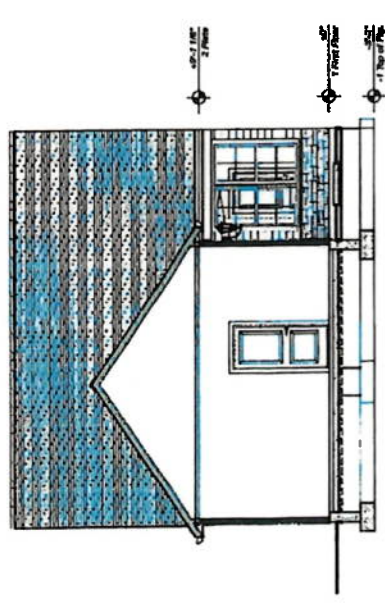
**HBOO The Multi-Fair's**  
 El-A-G-Yarles  
 For: Delaware Crossing, LLC  
 1841 Rockbrook Lane • Florence, Kentucky 41042  
 7203 Hodges Pike • Cincinnati, OH 45227-5120

**BROOKSTONE HOMES**  
 2022-0002

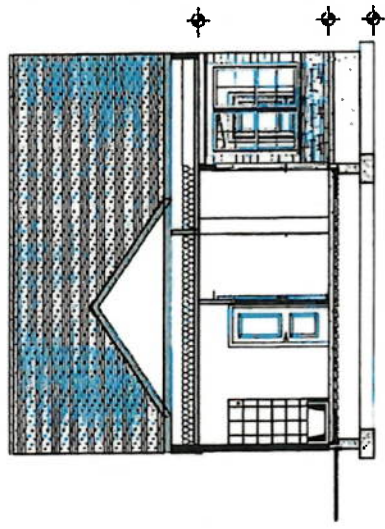
00000  
 04/27/24  
 04/27/24  
 04/27/24  
 04/27/24

1031 Kenned Rd. • Suite 2001 • New Ark, Ohio 43042  
 Designed By: J. Fisher • (614) 393-6872  
 Krlc@aol.com  
 Krlc@aol.com

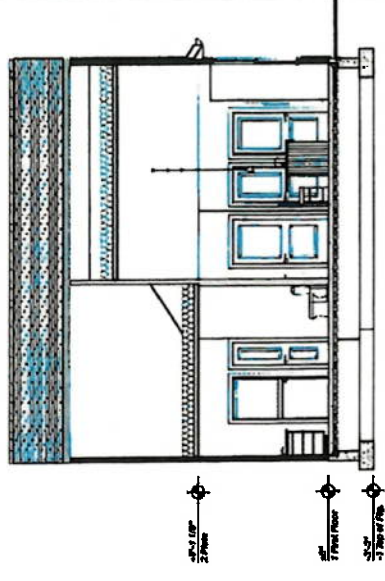
**CD**  
**C-3**  
 Sections  
 2024.10.22



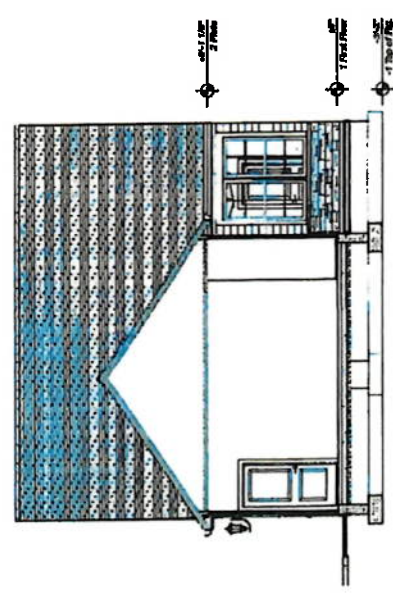
**BS1** 28'-Unit • Building Section-1  
 $1/8'' = 1'-0''$



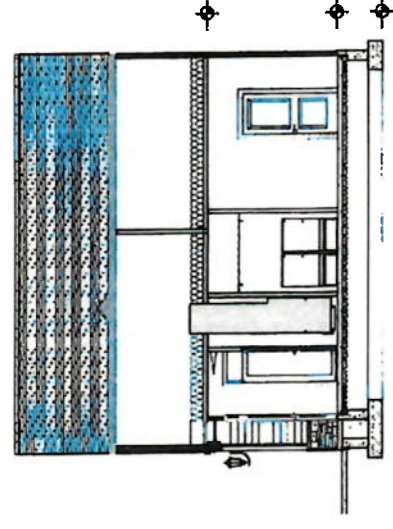
**BS2** 28'-Unit • Building Section-2  
 $1/8'' = 1'-0''$



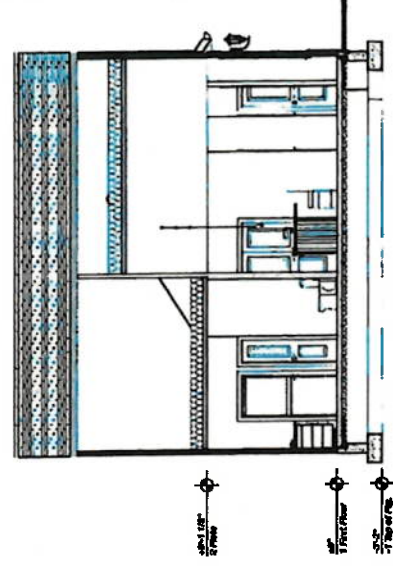
**BS3** 28'-Unit • Building Section-3  
 $1/8'' = 1'-0''$



**BS1** 28'-End Unit • Building Section-1  
 $1/8'' = 1'-0''$

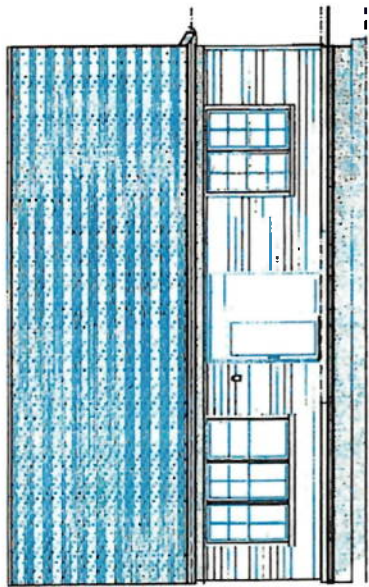


**BS2** 28'-End Unit • Building Section-2  
 $1/8'' = 1'-0''$

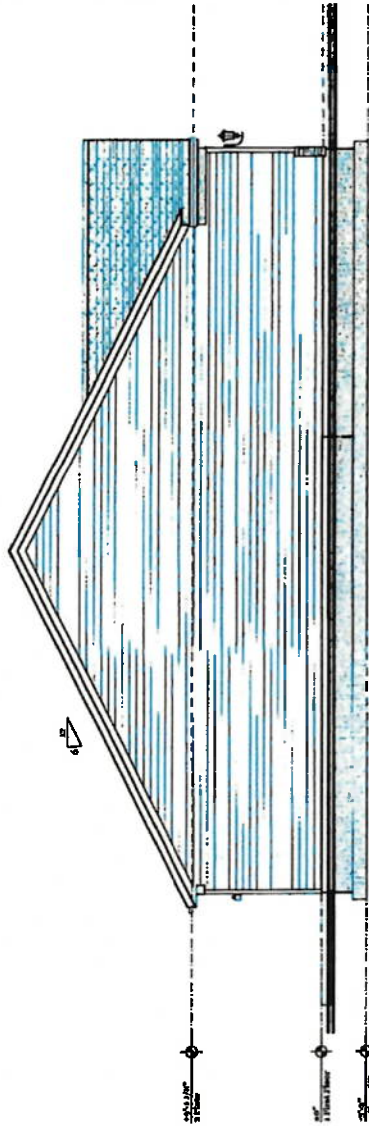


**BS3** 28'-End Unit • Building Section-3  
 $1/8'' = 1'-0''$

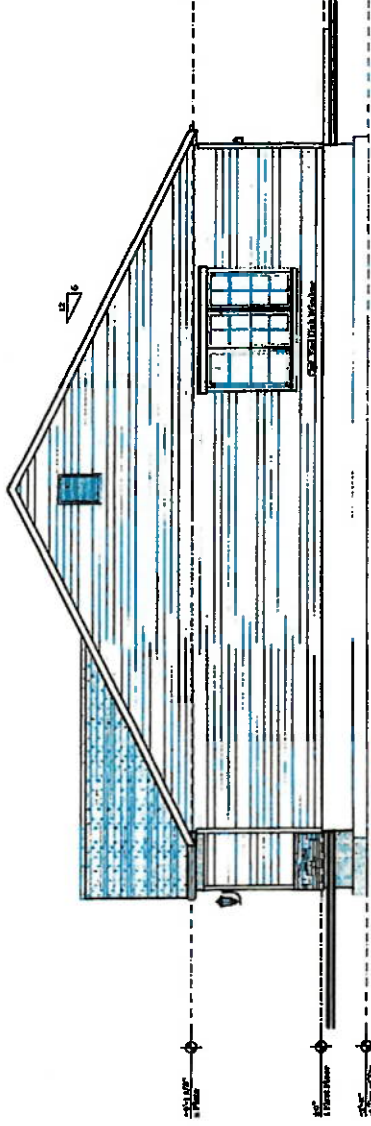




**E2** Holbrook Ln • 38' Unit • Rear Elevation-A  
1/8" = 1'-0"



**E3** Holbrook Ln • 38' Unit • Left Elevation-A  
1/8" = 1'-0"



**E4** Holbrook Ln • 38' Unit • Right Elevation-A  
1/8" = 1'-0"

**HBOO The Multi-Farm S**  
El-A • G. Varies  
For: Delaware Crossing, LLC  
1841 HOLBROOK LANE • HORNICK, KENTUCKY 41042  
C. J. C. BROOKSTONE HOMES  
7787 WOODBURN PIKE • CINCINNATI, OH 45227 • 513-202-1700

**BROOKSTONE**  
DESIGN GROUP  
44007246  
11111 Ash Road  
Cincinnati, OH 45244

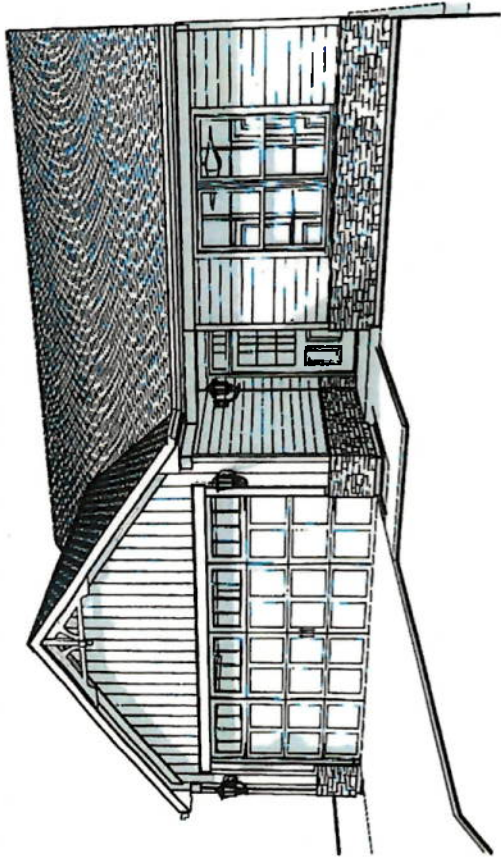
Design Group: 44/24/46

Right: Bernard L.L. • Suite 2001 • 210 Ash, Ohio 45424  
Designed By: P. J. Fisher • (615) 393-1877  
Email: [pjfisher@cloud.com](mailto:pjfisher@cloud.com)

*Original f/x*

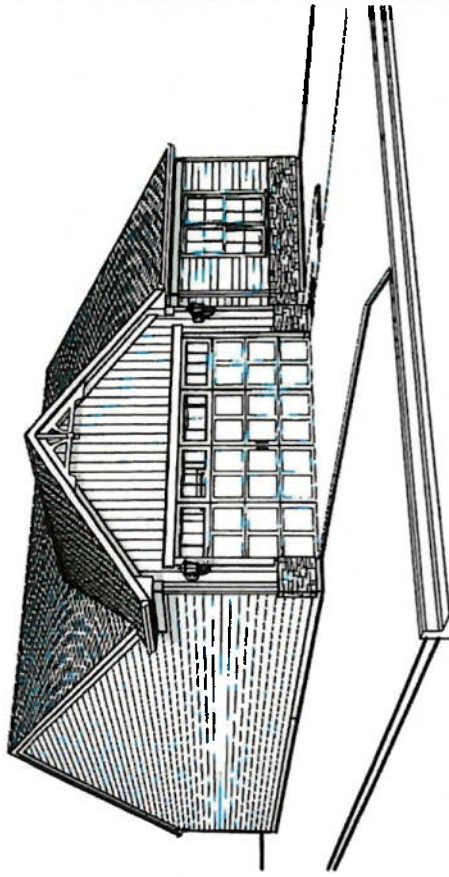
**Vf/x**

CD  
D.2  
CREATED: 4/2/17



Vfx1

Holbrook 38' Ext • Vfx 3D Tour



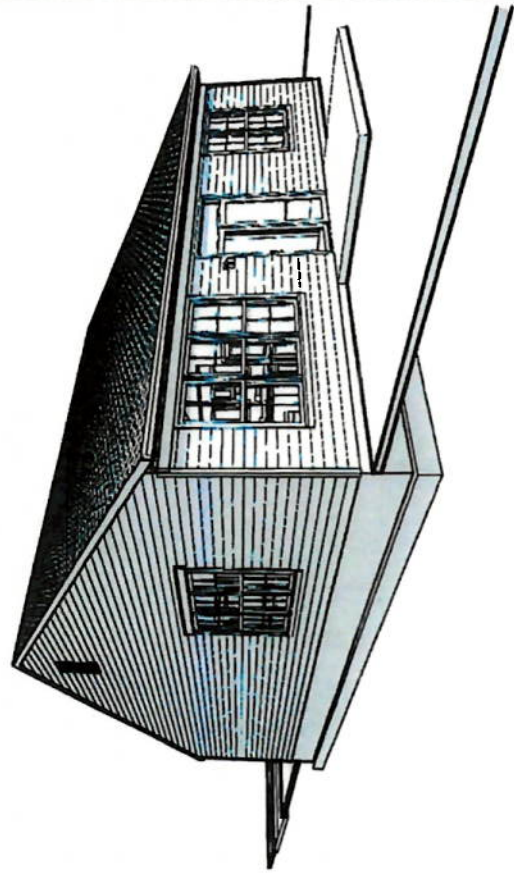
Vfx2

Holbrook 38' Ext • Vfx 3D Tour



Vfx3

Holbrook 38' Ext • Vfx 3D Tour



Vfx4

Holbrook 38' Ext • Vfx 3D Tour

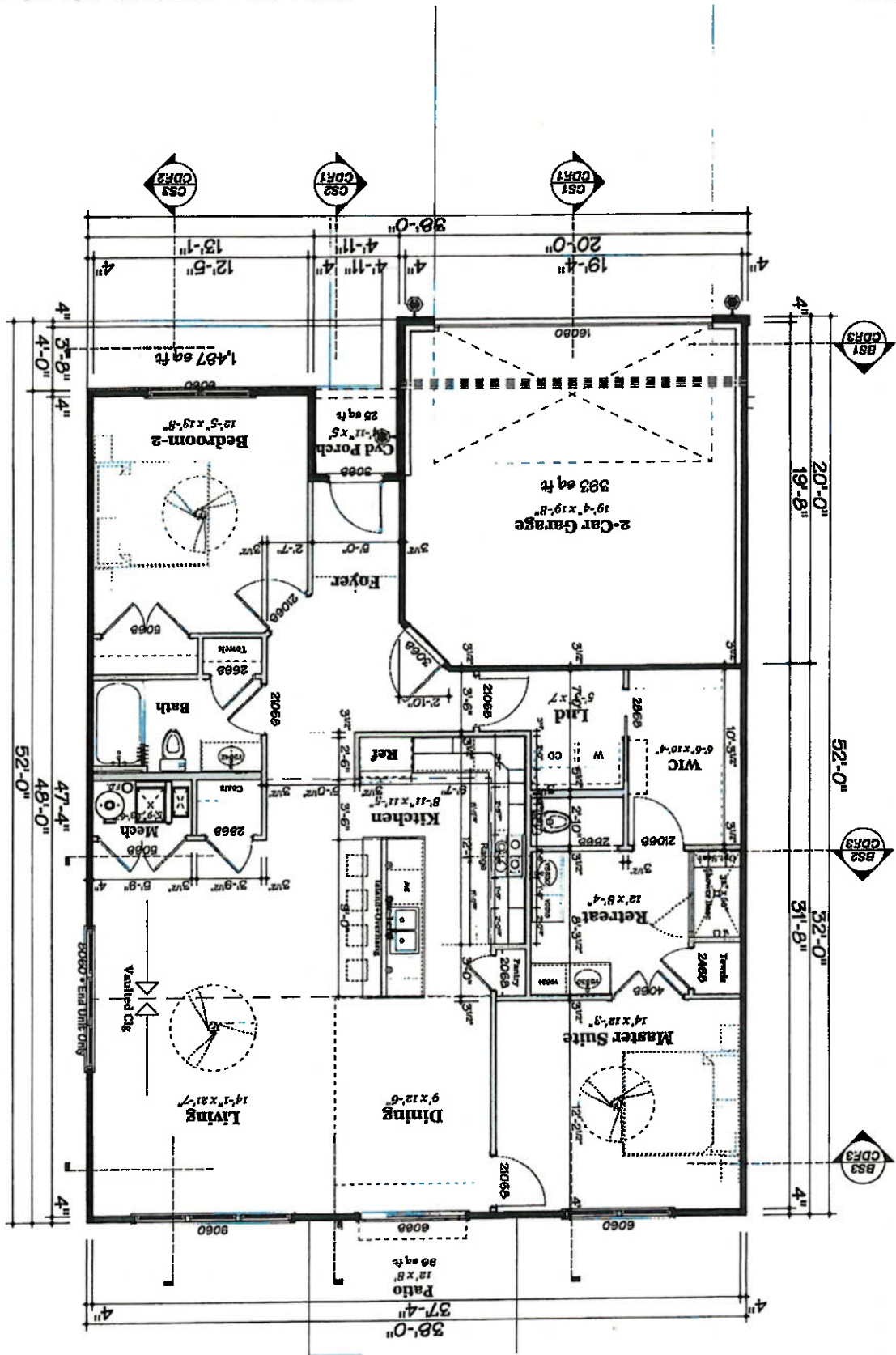
**HBOO** The Multi-Farm's  
 R.A. G. Varles  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LAKE • FLEMING, KENTUCKY 41042  
 BROOKSTONE HOMES, LLC  
 7722 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-763-7700

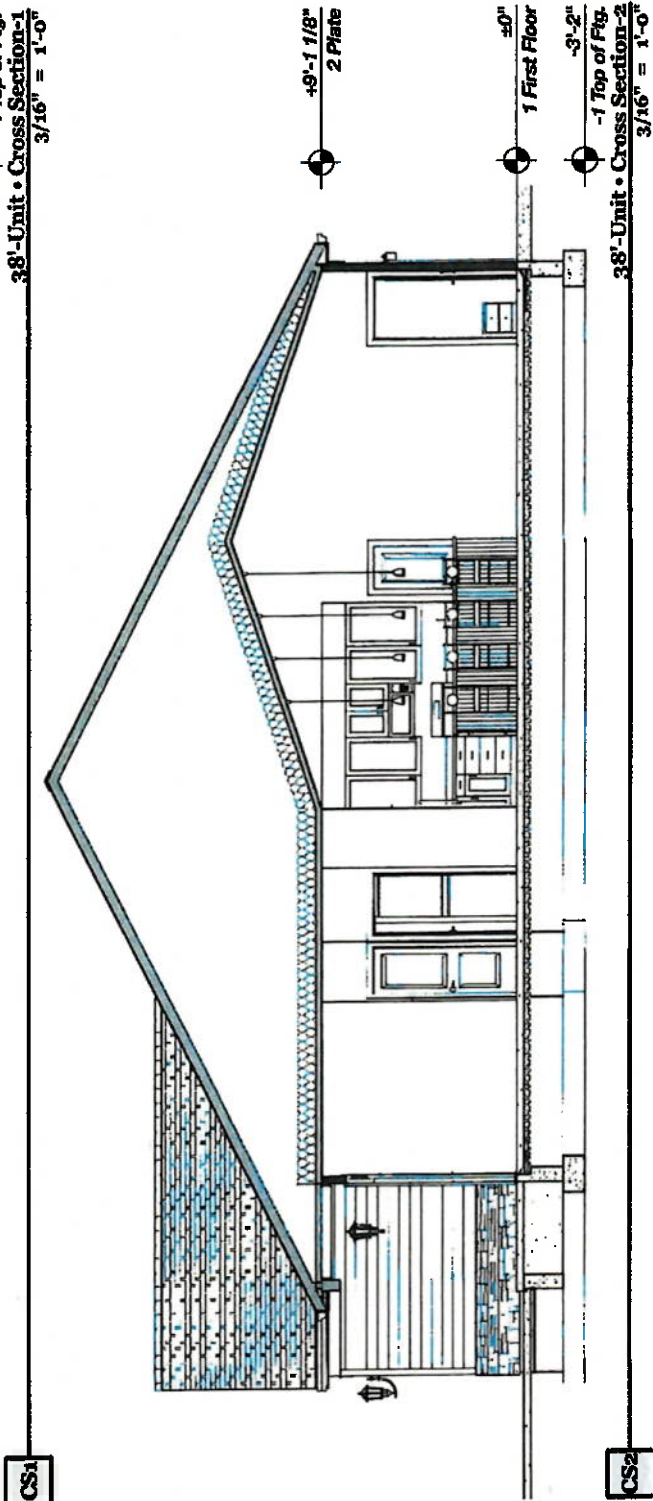
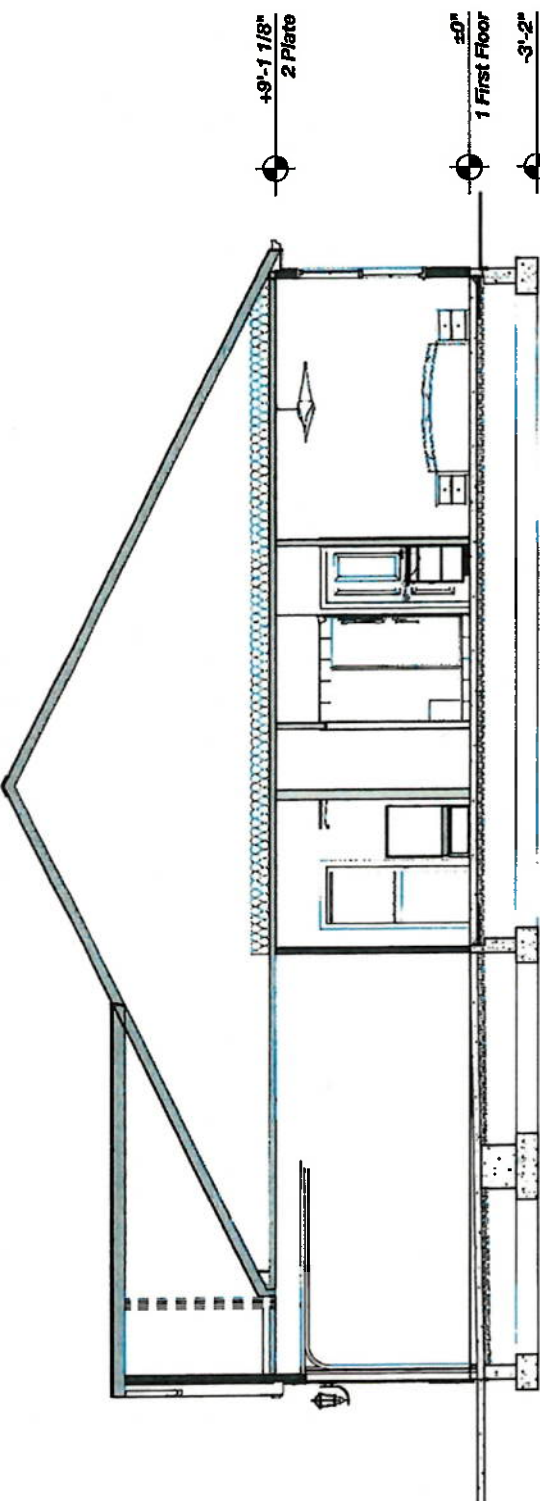
**BROOKSTONE**  
 ARCHITECTURE  
 10000 WOODBURN AVENUE  
 SUITE 100  
 CINCINNATI, OH 45244  
 513-763-7700  
 www.brookstonearch.com

Virtual f/x  
 10315 Rosewood Rd. • Suite 2001 • The Acks, Ohio 45424  
 Designed By: P. Fisher • (513) 993-0812  
 Virtual f/x  
 VirtualTour.com

CD  
 D-3  
 Vfx, Holbrook 38' Virtual Tour-A  
 COPYRIGHT © 2012

Holbrook Ln • 38' Unit • First Floor Plan  
 3/16" = 1'-0"



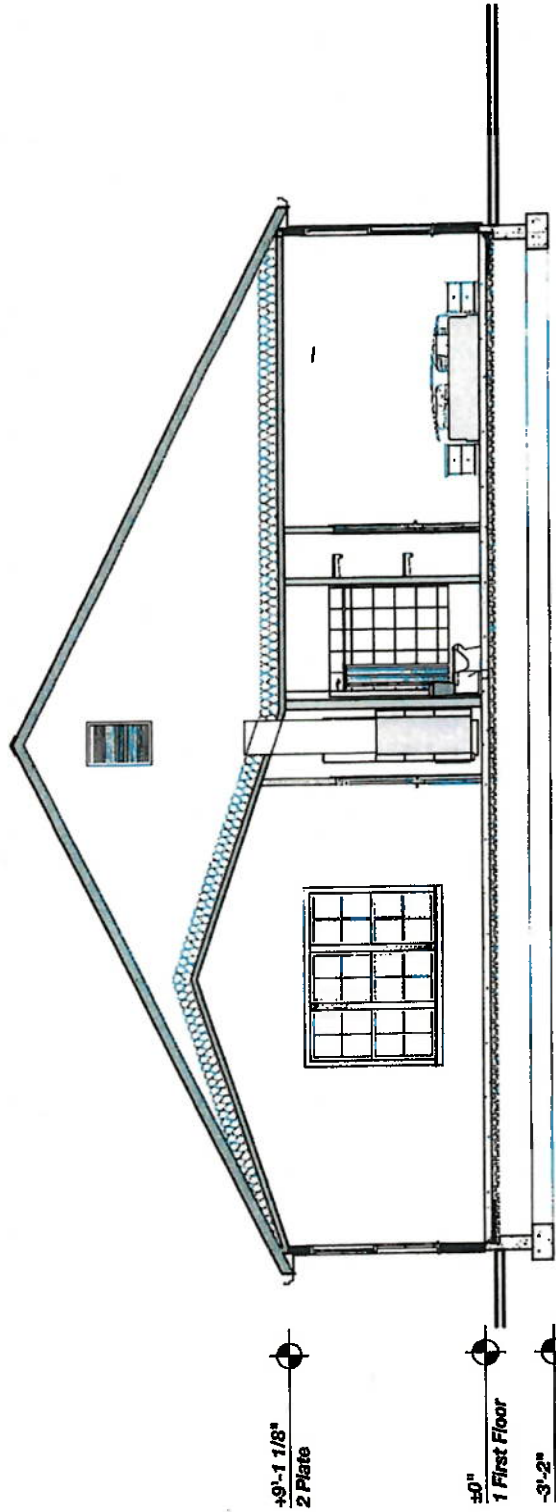


**Virtual f/x**  
 1831 Kenwood Rd. • Suite 2001 • Pitts Ahl, Ohio 45424  
 Designed By: P. Michael Fisher • (513) 393-0817  
 Permit: [cityofpitt.com](http://cityofpitt.com)

**Brookstone**  
 2021 FOUNTAIN  
 BROOKSTONE  
 100

**The Multi-Farm's**  
 R.A. • G. Varles  
 Fort: Delaware Crossing, LLC  
 1841 Rosebrook Lane • Florence, Kentucky 41042  
 BROOKSTONE HOMES, LLC  
 7203 Woodway Park • Cincinnati, OH 45227 • 513

**CDF**  
 22  
 CS1 - Cross Section-1  
 CS2 - Cross Section-2  
 .1  
 SHEET NO. 2 of 9



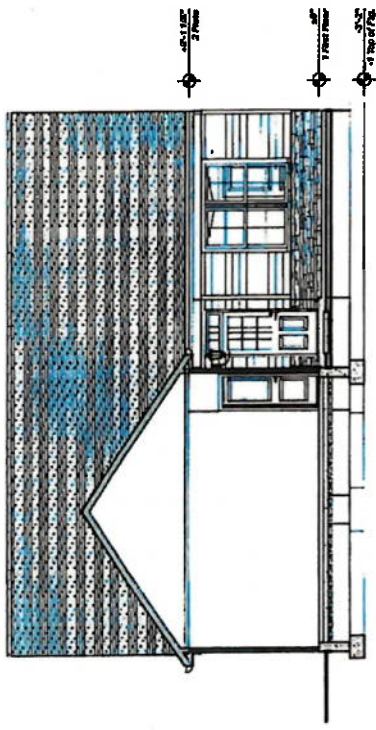
+9'-1 1/8"  
 2 Plate

+0"  
 1 First Floor

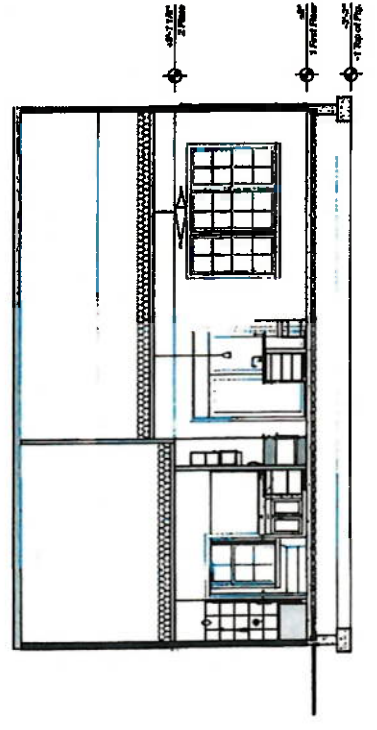
-3'-2"  
 -1 Top of Fig.

38'-Unit • Cross Section-3  
 3/16" = 1'-0"

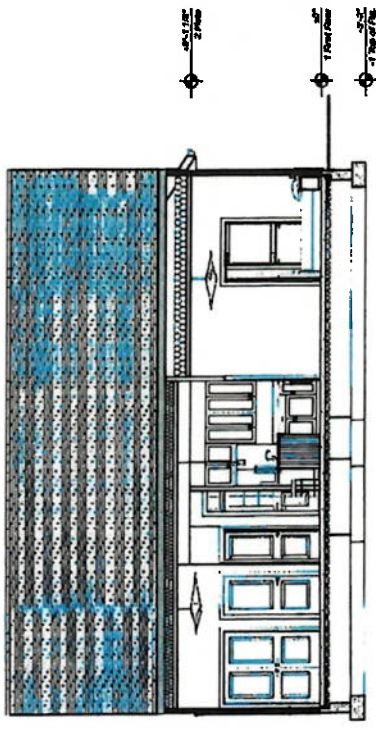
CS3



**BS1** — **38'-Unit • Building Section-1**  
 $1/8" = 1'-0"$

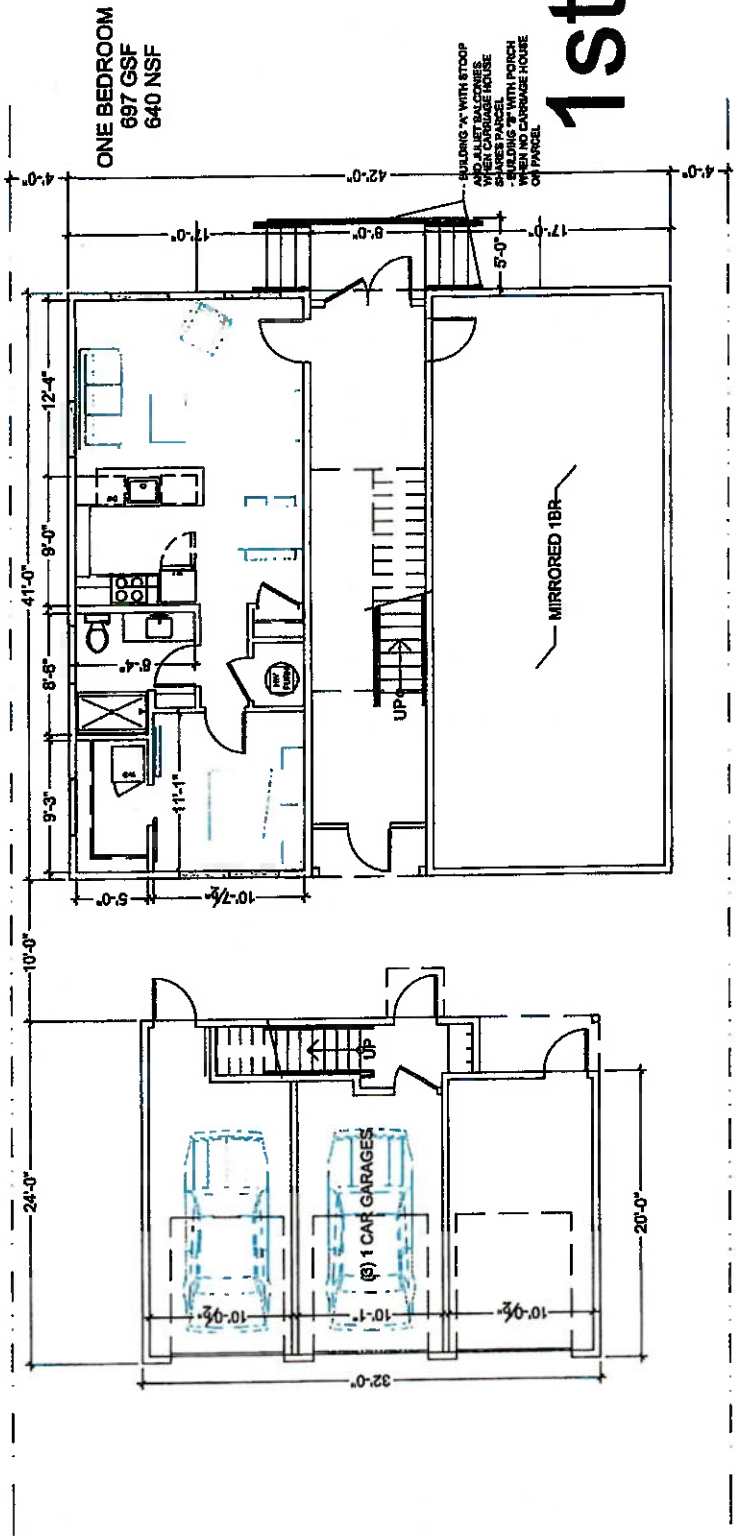


**BS2** — **38'-Unit • Building Section-2**  
 $1/8" = 1'-0"$

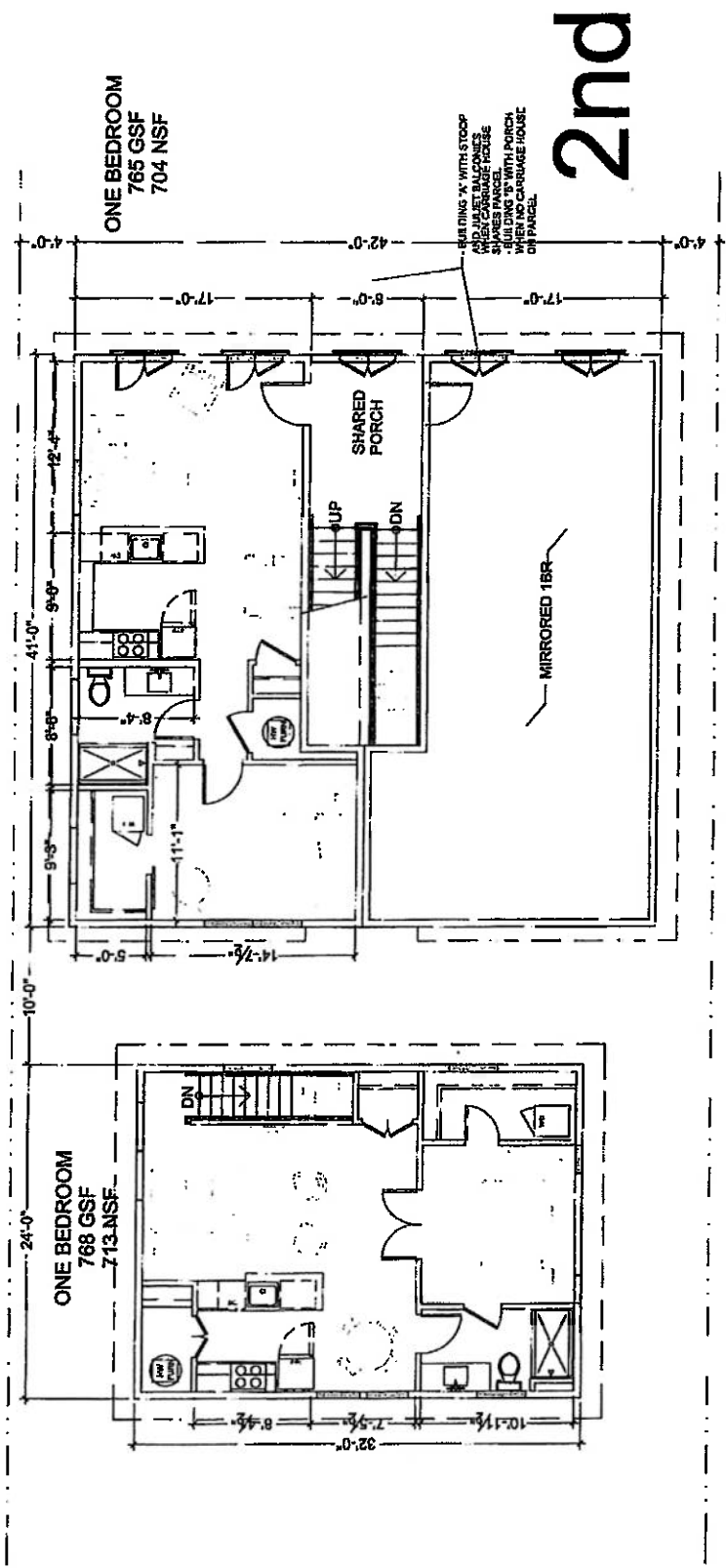


**BS3** — **38'-Unit • Building Section-3**

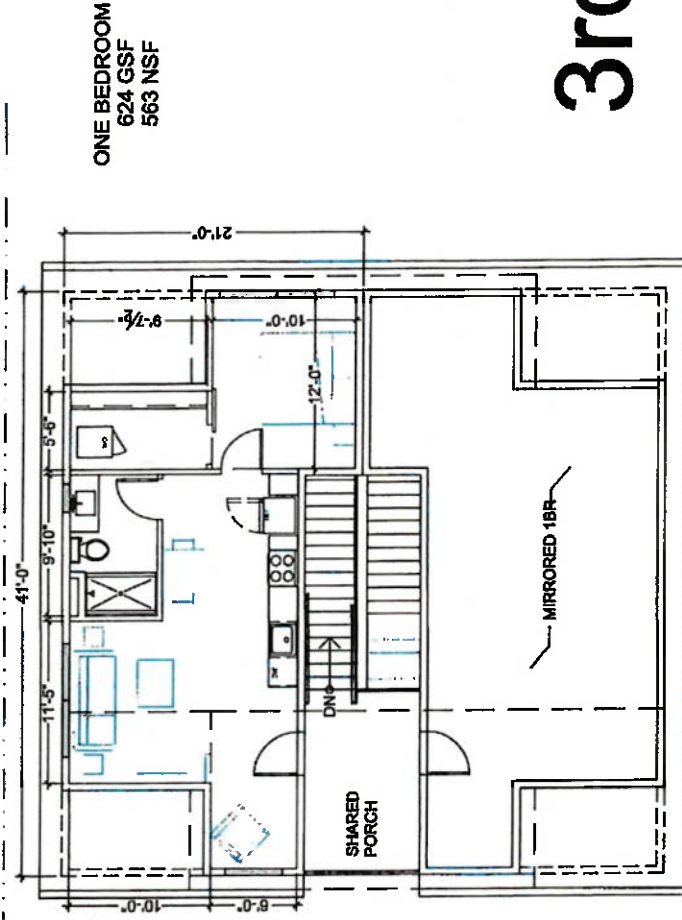
**POPLAR POINTE APARTMENTS  
SCHEMATIC DESIGN REVIEW SET**



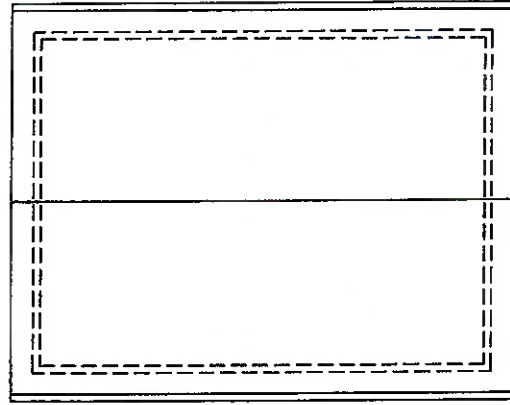
CARRIAGE HOUSE AND BUILDING A (BUILDING B SIMILAR)



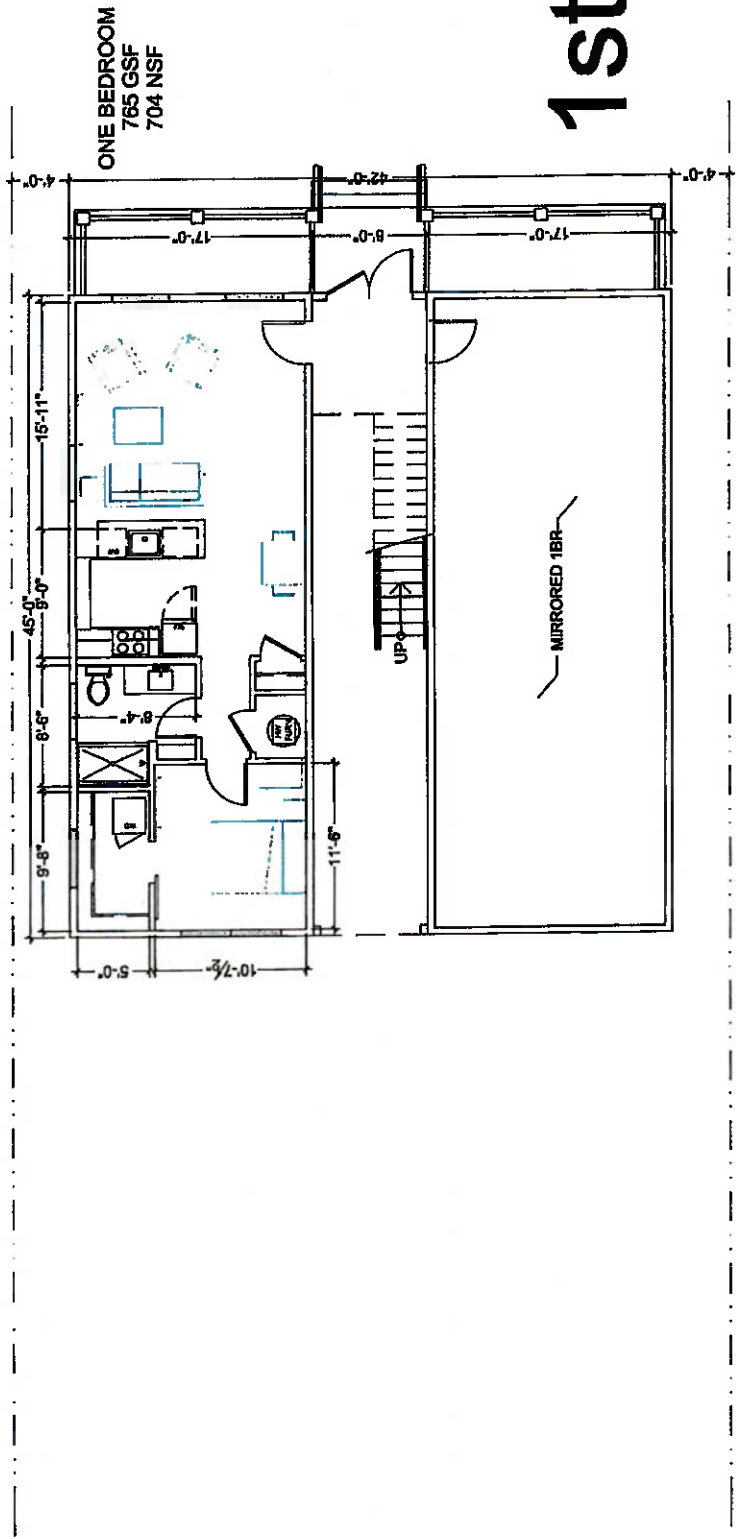
CARRIAGE HOUSE AND BUILDING A (BUILDING B SIMILAR)



3rd



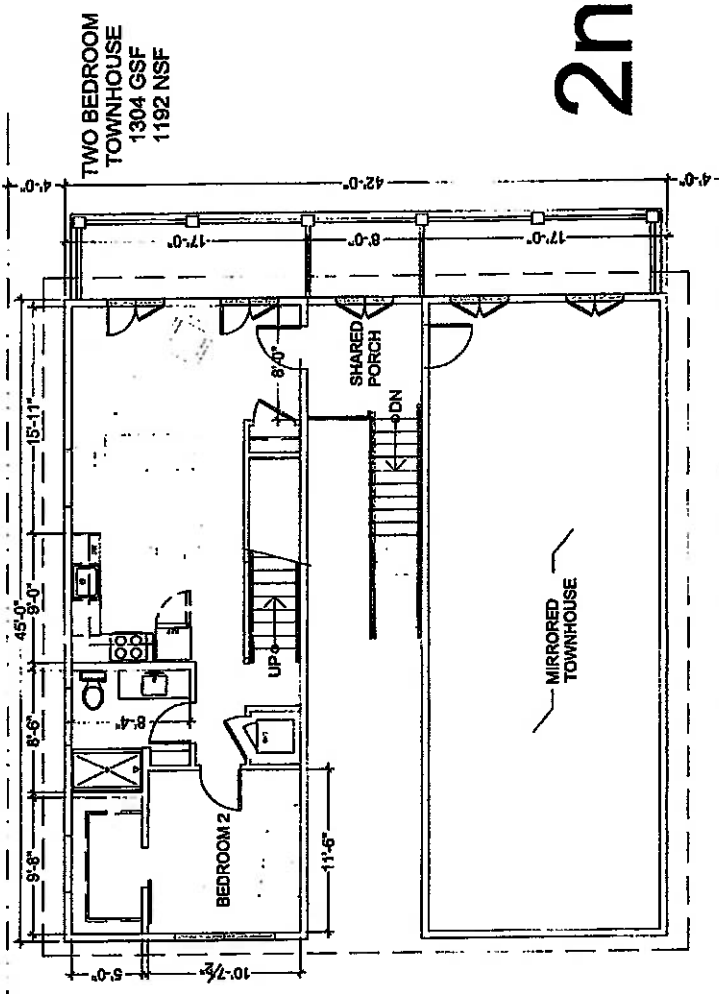
CARRIAGE HOUSE AND BUILDING A (BUILDING B SIMILAR)



1st

BUILDING C

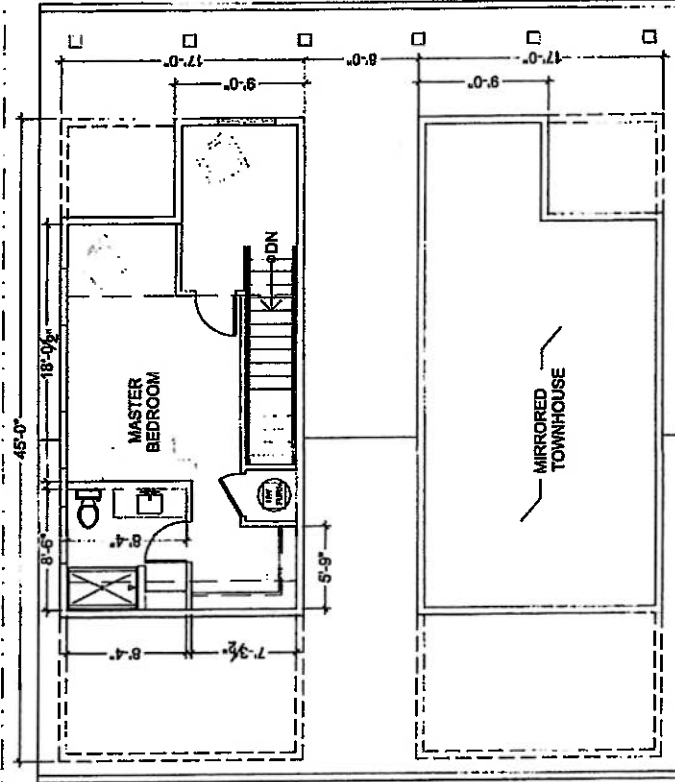




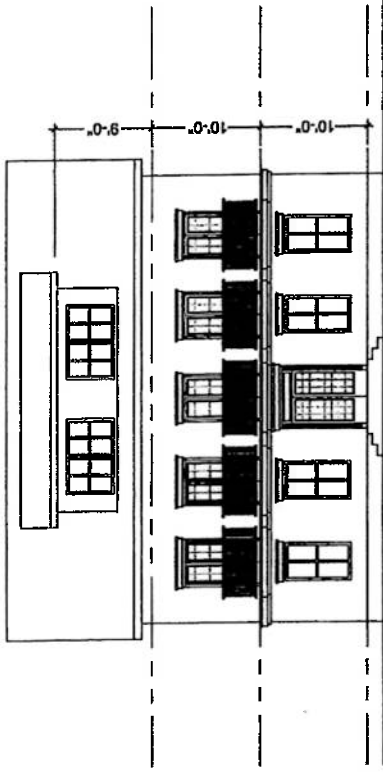
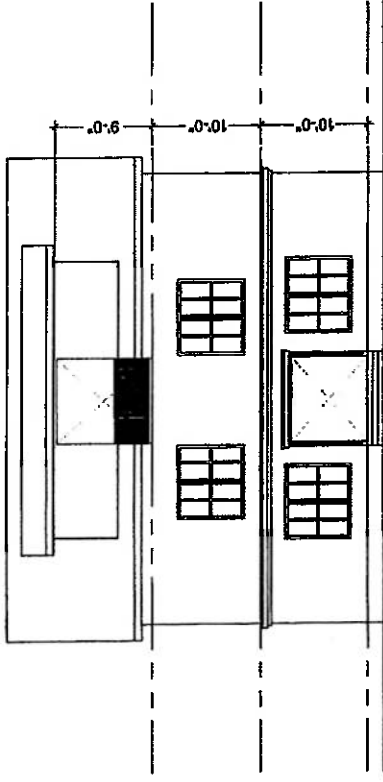
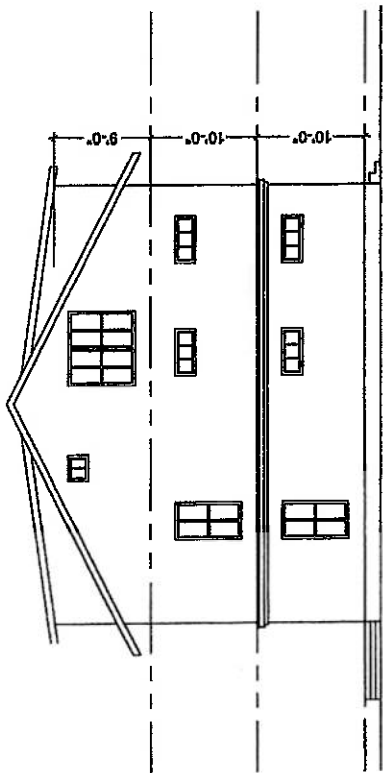
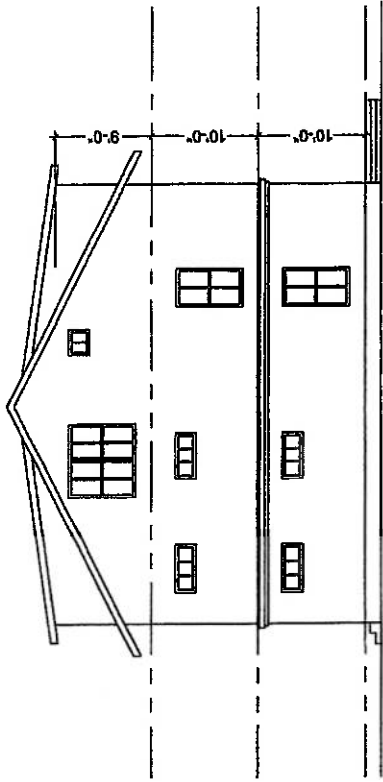
2nd

BUILDING C

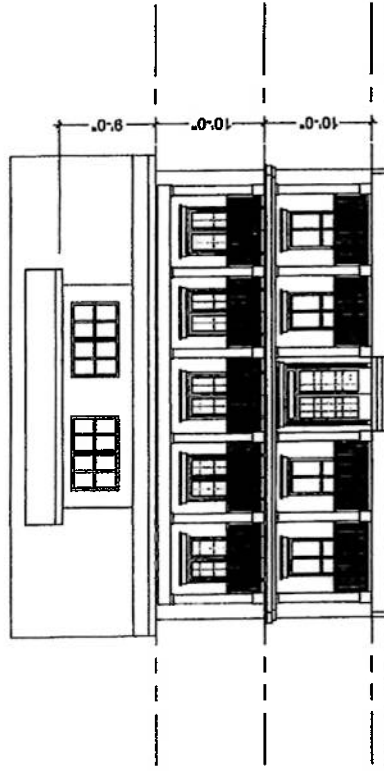
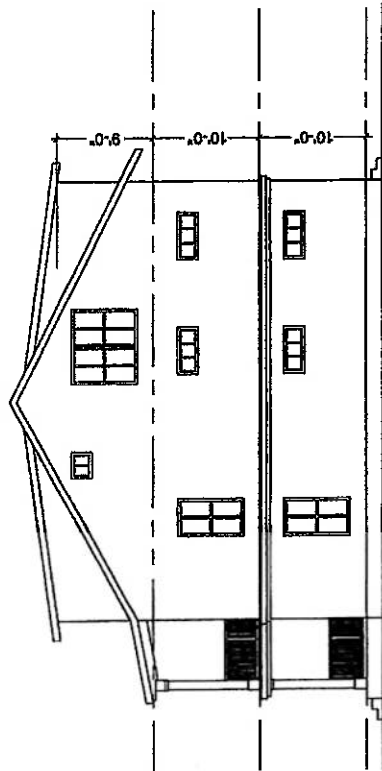
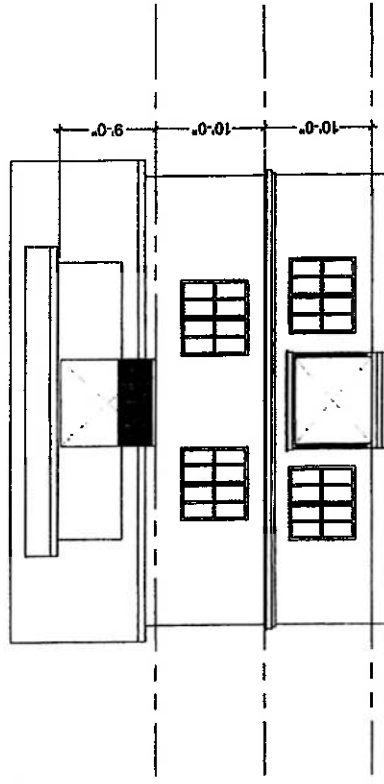
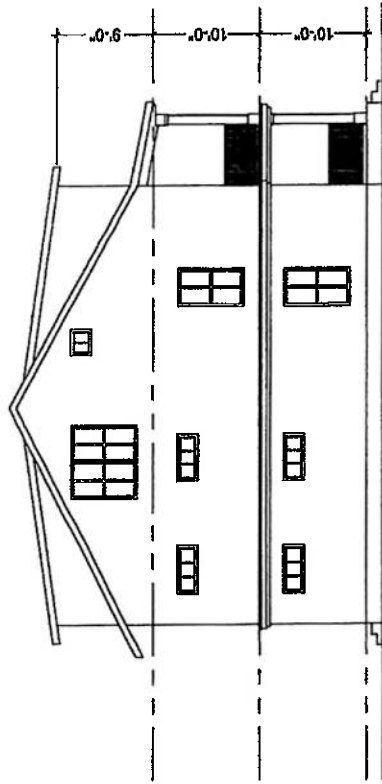
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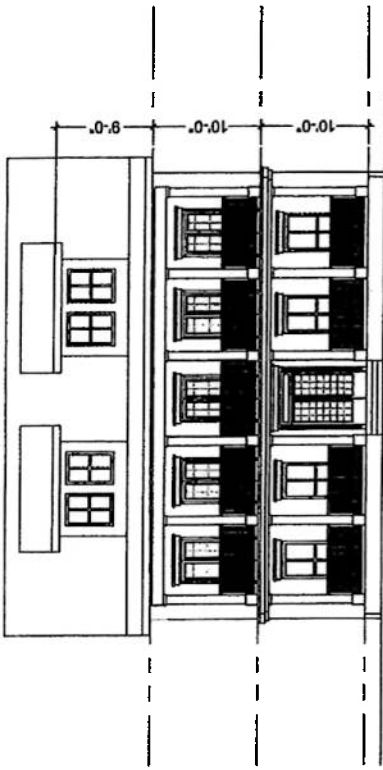
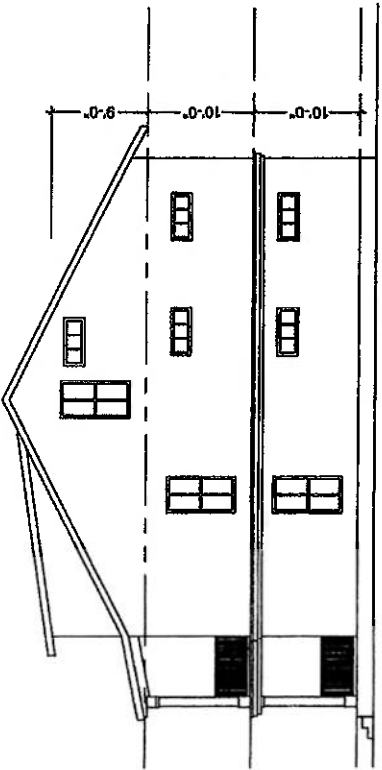
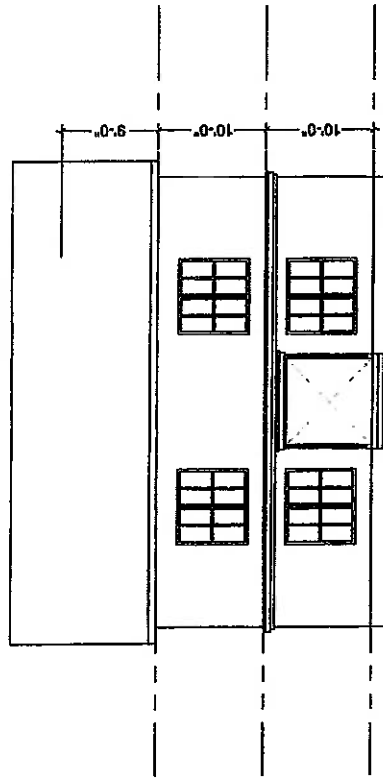
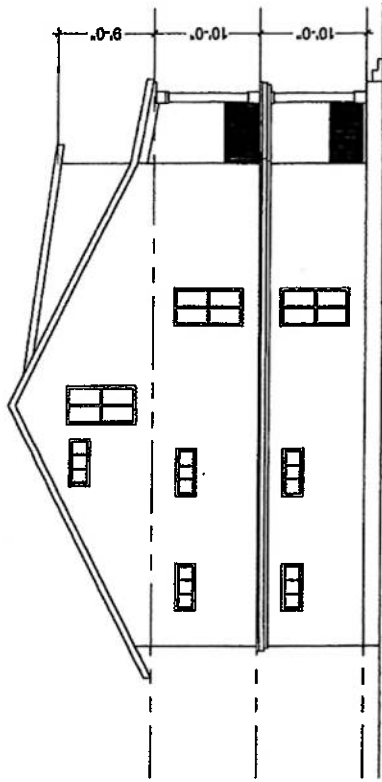
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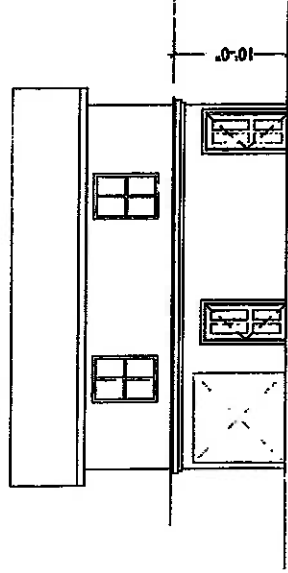
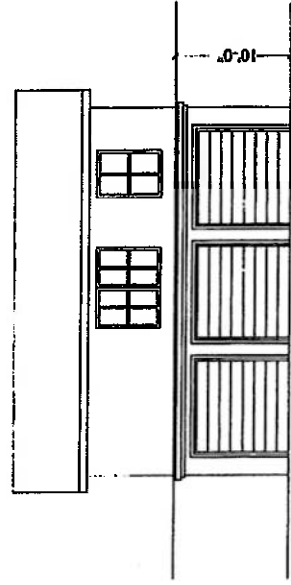
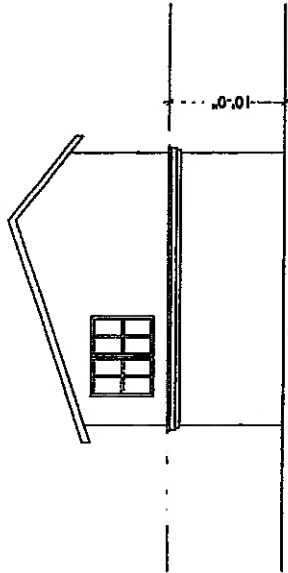
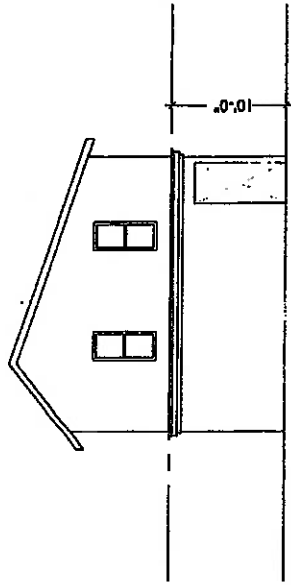
**BUILDING A - ELEVATIONS**



**BUILDING B - ELEVATIONS**



**BUILDING C - ELEVATIONS**



**CARRIAGE HOUSE - ELEVATIONS**

**COMMISSION MEMBERS PRESENT:**

Mr. Randy Bessler  
Mrs. Kathy Clark  
Mrs. Pamela Goetting  
Ms. Corrin Gulick, Vice Chairwoman  
Mr. Steve Harper, Temporary Presiding Officer  
Mr. Rick Lunnemann  
Mr. Charlie Rolfsen, Chairman  
Mr. Bob Schwenke  
Mr. Kenny Vaught

**COMMISSION MEMBERS NOT PRESENT:**

Mr. David Hincks  
Mrs. Janet Kegley  
Mr. Eric Richardson  
Mrs. Jackie Steele, Secretary/Treasurer  
Mr. Tom Szurlinski  
Mr. Steve Turner

**LEGAL COUNSEL PRESENT:**

Mr. Dale Wilson

**STAFF MEMBERS PRESENT:**

Mr. Kevin P. Costello, AICP, Executive Director  
Mr. Michael D. Schwartz, Director, Zoning Services

Chairman Rolfsen introduced the first item on the Agenda at 7:36 p.m.

**ZONING MAP AMENDMENT, Michael Schwartz, Staff**

1. Request of **Charter Commercial, LLC (applicant)** for **Delaware Crossing, LLC (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

Staff member, Michael Schwartz, referred to his PowerPoint presentation (see Staff Report). The request is to rezone the 44 acre site from SR-1 to SR-1/PD. The site has 2 distinct portions. The northern portion contains 19.97 acres. The southern portion contains 24.4 acres. Access to the site is from Holbrook Lane, Watson Lane and Lariat Way. Valley Creek Farms dates back to April, 2018 as the Planning Commission approved a Preliminary Plat. It had 97 single-family detached lots. The northern portion of the site is currently occupied by a single-story single-family residential dwelling with 3 accessory barn structures. The southern portion of the site is currently vacant and is heavily wooded. Gunpowder Crèek is located along the eastern edge of the site. The property surrounding the site is zoned SR-1. Oakbrook Subdivision is zoned SR-1/PD. Pages 1-5 of the Staff Report identify the applicable regulations to the request. Topographically, a valley bisects the site with slopes having a grade change of 70 feet. The eastern edge of the site is located within the 100 year floodplain of Gunpowder Creek. The 2040 Future Land Use Map designates the site for Rural Density Residential (RD) and Suburban Density Residential (SD). Due to the hillsides, a portion of the site is designated as Developmentally Sensitive (DS). Holbrook Lane is a County maintained local street with no sidewalks. Watson Lane is a County maintained local street with sidewalks on both sides of the street. Lariat Way is also a County maintained local street with sidewalks being installed with lot development. Pages 6-11 include the portions of the Comprehensive Plan that are relevant. The western portion of the street is located within a 600 foot wide buffer of an underground pipeline. Mr. Schwartz showed photographs of the site and adjoining properties.

The submitted Concept Development Plan is divided into 2 parts. The northern part is planned to have 139 attached single-family residential dwellings in 24 single-story buildings. The southern portion will have 157 multi-family residential dwellings in 24, 3 story buildings and 13, 2 story garages. There will be a dwelling unit on top of each garage. Public streets will be built in the northern portion of the development. Street connections will be made via Watson Lane, Holbrook Lane and Lariat Way. A private drive will serve the multi-family area in the south portion of the site. There will be stormwater retention areas, amenity space and sidewalks. There will be a walking trail from Lariat Way to Gunpowder Creek. Public utilities will be provided. The developer has provided some building elevations and floor plans. Mr. Schwartz noted that he offered a summary of building intensity of the project as well as neighboring developments on Page 11 of the Staff Report. The applicant has provided a project narrative, a traffic assessment (peak hour traffic generation and trip distribution but no intersection analyses or capacity analysis). Staff has reviewed the submitted plans against the zoning and subdivision regulations. Those findings can be found on Pages 12-14 of the Staff Report. Most notably, the maximum intensity of an SR-1/PD district is 6 units per acre and the applicant is proposing 6.67 units per acre. Comments from outside agencies can be found on Pages 14-15 of the Staff Report. Staff concerns about the project are listed on Pages 15-16 of the Staff Report. It includes a list of requirements that are being requested to be waived or modified using the PD regulations. Staff has found that there is a discrepancy between the number of units listed in the project narrative and the number of units shown on the submitted plans. Staff feels that there needs to be more amenities given the size of the development. Staff also feels that there should be a discussion of the extension of Holbrook Lane to Camp Ernst Road. The Plan only shows the applicant extending it just past the

applicant's entrance. The applicant has also requested some off-site signage for the proposed development. Staff has offered some suggestions.

Chairman Rolfsen asked if the applicant was present and wanted to proceed with their presentation?

Mr. Pat Manger, Loveland, Ohio, represents the applicant and stated that the northern part of the site will be single-story patio homes (2 bedrooms). The southern parcel will include 1 bedroom multi-story townhome buildings (2 and 3 stories high). He offered to comply with Concerns #2, #3, #4, #5 and #6. He stated that he won't exceed the 12% grade for streets. In terms of amenities, Mr. Manger noted that he is working with the County to connect their trail to Oakbrook Park. They are attempting to preserve as much green space as possible. He is working with the County to connect Oakbrook Road to KY 237 via Holbrook Lane. They would be responsible for their portion of Holbrook Lane as it affects their property. He is talking with the Valley Creek Farms HOA about the proposed sign and is in the process of working it out with the HOA. They are agreeable to extend Holbrook Lane to their western property line. They are in agreement with sidewalks. They are willing to comply with Item #5 and connect the southern area to the walking trail. In conclusion, they want to build one and two bedroom units in order to meet the demand as stated in the NKADD Study. There is a housing shortage. There is a need for 6,000 homes (2,000 units for rental) in the next 5 years.

Chairman Rolfsen asked if anyone in the audience would like to speak in favor or against the request or had any questions?

Mr. Michael Koseruba, 1959 Cliffview Lane, stated that no one from his neighborhood was notified. The proposal doesn't add up. There is a bonus in which 266 units becomes 296 units because of the PD. Don't be fooled. It is an apartment unit bomb in the middle of single-family residences and semi-rural residences. The roads, parks and schools can't take it. We are looking at a piece of property that doesn't have a road to it. There is no traffic coming out of a vacant piece of land. What about the traffic elsewhere in the area? Traffic will come from the west and the south. We are opposed to the project as we prefer single-family housing. Don't let the developer touch Gunpowder Creek. Oakbrook Park is not large enough for the project.

Mr. Charie Schneckenberger, 6516 Cannondale Lane, read his comments for the record (see Exhibit A). He stated that the zone change request should be denied. The applicant is requesting a 67% increase above the SR-1 zone (4 units per acre). The applicant didn't even follow the requirements under the PD. Over 60% of the land in the subdivision is being rezoned including the open space. If approved, the current subdivision will be reduced to 18 acres. The 89 residential lots will have a density of 5 units per acre. The zone change will adversely affect his property density and will violate his SR-1 zoning.

Mr. Schwartz showed the current and proposed density of the subdivision and project using a map for illustrative purposes.

Mr. Mike Parker, realtor, stated that the NKADD study was wrong. There is not a sufficient amount of housing inventory in Northern Kentucky. They are 60% short. There is no need for one bedroom apartments. The project will impact the schools and roads. It will change the character of the area. It will affect the quality of life. There is a lack of community support. The Board's ultimate decision on the project will tell Boone County residents whether you believe in single-family and home ownership or whether you believe in putting as many units as possible. If this change occurs, there will be other developers that will come forward to change single-family to multi-family. In this week of 2018, there were 322 active homes for sale for an average of \$247,000. In a normal market, it is 700 homes in Boone County. In 2021, there were 82 homes active with an average sales price of \$323,000. This week there are 181 active homes with an average sales price of \$393,000. Don't change the SR-1.

Chairman Rolfsen asked the audience how many people think traffic is the issue? Over 100 residents raised their hand.

Ms. Stacy Blevins, 2968 Lariat Way, stated that her house is in front of the zone change sign. She just built a half million dollar home. She thought it would be a quiet neighborhood and not full of apartments. She doesn't feel good about having her kids play in the area. Her street is a hill. It will be the main access for the new residents. It won't be safe to cross the street with 200 more residents. If the project goes, they will sell their home.

Mr. Jeff Schaefer, 6501 Cannondale Drive, stated that he was the first person to build in Valley Creek Farms Subdivision. He signed a contract with Brookstone Homes. The builder has neglected the neighborhood for the last 4 years. The lots are in terrible shape. The reason the subdivision has not sold out in the last 7.5 years is due to Brookstone Homes. The apartments will trash their home values. There is only one way in and out. It is through Lariat Way. There are no stop signs in the subdivision. They don't have complete sidewalks after 7.5 years. Drees and Fischer Home Subdivisions have them. If the developer would have sold the lots, they would not need the apartments. There was no model home or salesperson. Brookstone has gotten out of the single-family development. Maronda Homes has bought the remaining lots. The applicant's traffic study showed 2,000 trips per day in and out of the subdivision and no stop signs. The number of units proposed will put stress on the schools. They are overwhelmed both in the school and on the buses. The original subdivision only had 250 people. With the proposed change, it will increase to 734 people. The infrastructure in place now only accommodates 250 people. There is only one way in and out of the subdivision for emergency response. Are there plenty of apartment complexes planned for the area? Chairman Rolfsen replied yes. There was an approved project north of the site on KY 237 for 324 apartments. Why put apartments in the middle of single-family? Just build the 324 units already approved. Chairman Rolfsen stated that project may not be built if the traffic signal is not built. Mr. Schaefer stated that he bought his house because he thought the subdivision was only for single-family homes.

Mr. Bob Lusmyer, 6520 Cannondale Drive, stated it is tough to turn left from Cannondale Drive between 7:00 and 9:00 a.m. and 4:00 and 6:00 p.m. There is too much traffic to make a left turn without a signal. It is dangerous. The grade for Lariat Way is very steep. The traffic study is extremely misplaced. There are a lot of people coming up Camp Ernst and Pleasant Valley. Their subdivision will be a great short cut. In addition, there is a feeder creek that runs to Gunpowder Creek. Will it be impacted? It is supposed to be a green area.

Mr. Dennis Stock, 6505 Cannondale Drive, stated that the roads in the subdivision don't have the final pavement. They still have the construction grade. They have been that way for 8 years. Mr. Costello explained the bond process for street completion. It is up to the County to cash in the bond to complete the paving work if the developer can't. If a traffic signal is installed on KY 237 and Cannondale Drive, it may spoil the flow of traffic at the roundabout. The current zoning of SR-1 is still appropriate. The apartments are inappropriate. The single-family homes are okay for the area. They just have to be marketed and built. The economic value is still valid. Maronda Homes will do a better job marketing.

Ms. Tangerine Pike, stated that she owns the property on the other side of Holbrook Lane next to the subject site. She explained that her property rights will be significantly harmed because of the apartments (rental property). It is a revolving door of residents. They don't care.

Ms. Danielly Bruseth, 6589 Watson Lane, stated that her concern is privacy. There is also a blind curve.

Chairman Rolfsen inquired about connectivity. Mr. Schwartz replied that there was a stub at Watson Lane and Trevino Lane.

Mr. Shawn Gross, 6505 Watson Lane, stated that he moved from Price Hill. He moved because of density and transients. All the streets in Shaker Run Subdivision that connect to Holbrook Lane are at a decline. He is concerned about speed from additional traffic using Watson Lane to get to Holbrook Lane. There are a lot of kids in the neighborhood. Parking is on one side of the street.

Ms. Susan Schwendemann, 1911 Cliffview Lane, stated that the building design looks cheap and like tenement housing. Who needs one bedroom housing?

Mr. Russell Alexander, 1629 Hogan Lane, stated that he has been in the pest control business for 25 years. One bedroom means 5 people and drug dealers. It is hard to stop on Holbrook Lane due to its slope. He is concerned about safety and property values.

Mr. Jeff Rice, 1805 Promontory Drive, asked how much will the apartments be rented for each month? We need rent control based on income. That is why people are bundling up in units. It should be affordable. What about SD1? Can sanitary sewer service be provided? Chairman Rolfsen asked if the Staff received a letter from SD1? Mr. Schwartz replied no. He asked if the County had a plan for future development?

Mr. Costello replied that there is a plan in place currently. The plan is being updated. He explained the process and the areas for growth and preservation. He advised Mr. Rice and others to participate in the process and visit the Planning Commission's website for more information. Mr. Rice inquired about the new stormwater from the project and its affect on Gunpowder Creek. Chairman Rolfsen explained the new requirements of stormwater management and requirements for development. Water has to be retained on-site for a period of time and then released slowly and naturally from the site.

Mr. Brendon Aragon, 6500 Cannondale Drive, stated that he spoke to SD1 officials and they offered some of the site had erosion control measures in place and some have not been put in place because it hasn't been reviewed yet. Chairman Rolfsen explained that it would be the next step unless the project is not approved.

Mr. Koseruba asked why can't the Board or Staff not accept the application if it is a bad project or plan? Mr. Dale Wilson explained that KRS Chapter 100 allows any property owner to apply for a zone change for their own property. That individual has a right to apply. The denial of that right is a denial of due process.

Ms. Julie Haley, Cliffs section of Oakbrook stated that Oakbrook Road needs repaving. The County should take care of existing roads first before building new ones. The patio homes look the same. It will deeply lower the value of their homes.

Ms. Veronica Huffman, 6647 Sarazen Court, stated that she bought her home 19 years ago. The school buses are overcrowded and late. People run the stop signs in her subdivision. Would you want this type of development in your backyard? Stephens Elementary School is overpopulated. There is no lighting in Oakbrook Subdivision.

Ms. Judy Forbes, 6604 Trapside, asked if the Board thought about the American dream or owning a home. Rental is not the American dream. We need more homes in the County.

Ms. Cassie Ames, 6509 Cannondale Drive, explained the double bus run procedure. She asked who makes sure the stormwater is directed to the pond because the developer doesn't have a good track record with water. Lighting is an issue on Lariat Way. It should be added. Chairman Rolfsen explained that the Planning Commission reviews the stormwater plans to make sure it is designed properly and built.

Mr. T.J. Roberts, 8062 East Bend Road, stated that Boone County is ripe for home ownership. America is losing homeowners. The Board must keep the single-family home alive as well as home ownership.

Mr. Costello noted that in the early 2000s, Boone County was generating about 1,500 single-family homes per year. In the last five years that number has been reduced to 500 homes per year. Part of the problem is the fact that we only have 3 main home builders. There needs to be

more builders. The small homebuilders left the market in 2008 with the recession. No one is building starter homes. There are over 4,000 lots throughout the County where homes can be built. It makes sense to have a good supply of homes at various income levels. The Planning Commission has a strong history of supporting single-family residential development.

Mr. Steve Rawlings, State Representative, District #66, noted that he lives in Burlington. When he was campaigning in Florence, the biggest issue was in rental homes. They are owned by corporations that are buying up these properties and renting them out. Please maintain the integrity of Boone County and he asked the Board to respectfully deny the rule change and keep the zoning the way it is now.

Mr. Michael Ames, 6509 Cannondale Drive, stated that he informed the neighborhood about tonight's Public Hearing. The builder for the proposed project is the same builder of the subdivision. For 7 long years, he only built out half of it. He mismanaged it. Why would you let him continue? Is Fischer or Drees having problems selling homes? The builder should have a decent product.

Chairman Rolfsen asked if the Commissioners had any questions or comments?

Mr. Schwenke asked the Staff to give the Committee more history about the density of both projects. He expressed a concern about factoring in the open space permanently. He also had a concern about the grade of the streets for fire trucks, etc. Can the grades be reduced?

Ms. Gulick had a concern about the upkeep of the existing development. What is going on with completing the work? She asked Staff to research it.

Mr. Rolfsen asked if it was the same builder or did he sell it? Mr. Schwartz doesn't know for sure.

Mrs. Goetting asked about the proposed 139 attached single-family dwellings. Are those to be purchased or rented?

Mr. Manger responded that all 296 units are rentals.

Mr. Costello stated there were questions about the owner of the original subdivision? Is that person or company still around or is there a new entity? Is the previous builder associated with the new entity?

Mr. Manger replied that he was unfamiliar with it. He is only present to answer the questions about the zone change. He is not prepared to provide that information tonight. He said he would later.

Mr. Costello explained that the Board wants to know how the proposal fits in with the existing development and what was planned and who is doing the work? Is it the same company?

Chairman Rolfsen asked if it is affordable housing in terms of monthly rents? Mr. Manger responded that they don't have that information available at this time. There is a lot that goes into those figures – cost of on-site and off-site improvements. This is the first part of the process. Additional information is needed like the traffic study.

Dr. Clark asked if the applicant had additional information of how the project impacts the schools? Any numbers? Mr. Manger explained that the demographic they are targeting are baby boomers and empty nesters. Dr. Clark stated that those people could have kids or grandkids.

Mr. Lunnemann asked for more information of what the multi-family will look like in terms of architecture and materials? Due to the proposed number of units, what about amenities? A pool, playground, pickleball courts, etc.? Where will it be located?

Ms. Gulick stated that the submitted traffic study does tell her a lot. She asked for ADT information on the existing roadways and whether the developer will have a more detailed traffic study?

Chairman Rolfsen asked if the developer will have this information in time for the September 4<sup>th</sup> Committee Meeting? Mr. Manger replied that he would have to confirm and verify.

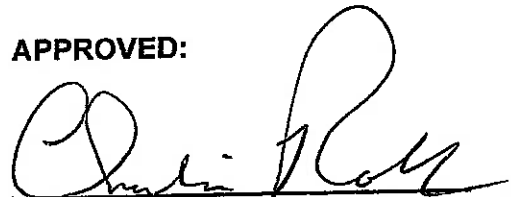
Mr. Vaught asked for more information about the impact of the project on Gunpowder Creek?

Mrs. Goetting asked if someone wanted to build more single-family homes, is the property already zoned for it? Mr. Schwartz replied yes as 97 homes could be built on the remaining lots. Brookstone Builders committed to build the lots from the beginning. Mr. Schwartz will verify that information.

Chairman Rolfsen explained the next steps of the process with the Committee Meeting and action by the full Board.

**Seeing no further questions or comments, Chairman Rolfsen announced that the Committee Meeting for this item will be on September 4, 2024 at 5:00 P.M. This item will be on the Agenda for the Business Meeting on September 18, 2024 at 7:00 p.m. in the Fiscal Courtroom. Chairman Rolfsen closed the Public Hearing at 9:35 p.m.**

**APPROVED:**



**Charlie Rolfsen**  
**Chairman**

**Attest:**



**Kevin P. Costello, AICP**  
**Executive Director**

Exhibit A – Packet from Charlie Schneckenberger  
Exhibit B – Packet from Mike Parker

Charlie Schneckenberger

6516 Cannondale Dr.

Burlington, KY 41005

(Valley Creek Farms)

Why I think the zone request should be denied:

1. Zoning code Article 15 section 1501.A.2 states "intensity of use in a Planned Development District may exceed the maximum intensity permitted in the underlying zoning district by up to 50%. Zone SR-1 allows for 4 units per acre and the request is asking for 6.69 units per acre, which is above the zoning code. Delaware Crossing LLC didn't even bother to follow the zoning code.
2. According to Article 15 section 1503.A "uses proposed for a Planned Development District Shall not adversely affect adjacent property." Looking at the maps that I provided, you will see Valley Creek Farms in an approved cluster subdivision. All of parcel 050.00-00-001.05 was originally part of the Valley Creek Farms cluster subdivision zoned SR-1. This Parcel of land was used to make lot sizes in Valley Creek Farms smaller than .25 acres. Boone county subdivision regulation, Section 401.A open space and residential subdivisions, states: "in Cluster residential subdivisions the remnant land not designated as building lots is required to be undeveloped..." By rezoning parcel 050.00-00-001.05 and putting it in the Planned Development District, 60% of our subdivision will be rezoned including all the free space. If this rezoning request is approved, Valley Creek Farms will be reduced to approximately 18 acres. The 89 residential lots will have a density of 5 units per acre. This zoning change will **adversely affect my property density and will violate the SR-1 zoning code for my property.** Delaware Crossing LLC appears to hope the planning board doesn't notice the change to Valley Creek Farms cluster subdivision density.

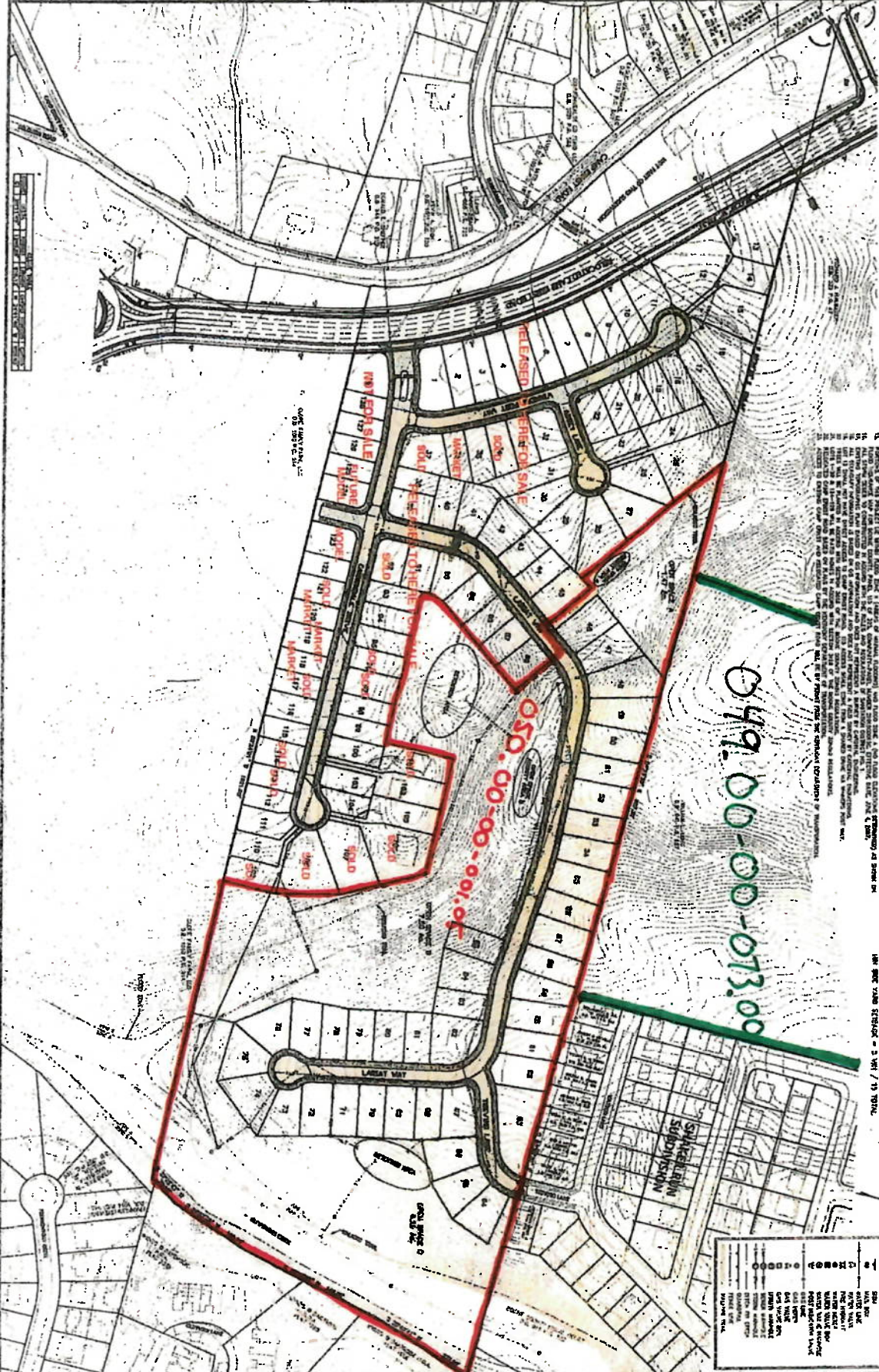
Respectfully,

Charlie Schneckenberger.

# VALLEY CREEK FARMS

CLUSTER SUBDIVISION

TOTAL SITE = 45.37 ACRES  
 R/W TO BE DEDICATED = 5.36 ACRES  
 OPEN SPACE PARCELS = 15.26 ACRES (33.6%)  
 126 RESIDENTIAL LOTS = 24.75 ACRES



**NOTES:**

1. ALL LOTS SHALL BE 1/2 ACRE MINIMUM.
2. ALL LOTS SHALL BE 1/2 ACRE MINIMUM.
3. ALL LOTS SHALL BE 1/2 ACRE MINIMUM.
4. ALL LOTS SHALL BE 1/2 ACRE MINIMUM.
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19. ALL LOTS SHALL BE 1/2 ACRE MINIMUM.
20. ALL LOTS SHALL BE 1/2 ACRE MINIMUM.

049.00-00-073.00

ZONE: RPD-100-CLUSTER SUBDIVISION  
 MAX. DENSITY: 10 UNITS PER ACRE  
 MAX. HEIGHT: 35 FT. MAX.  
 MIN. SETBACK: 10 FT. FRONT, 5 FT. SIDE, 5 FT. REAR  
 MIN. FRONT YARD SETBACK: 25 FT. (SEE PRELIMINARY PLAN)  
 MIN. SIDE YARD SETBACK: 5 FT. / 10 FT. (SEE PRELIMINARY PLAN)  
 MIN. REAR YARD SETBACK: 5 FT. (SEE PRELIMINARY PLAN)  
 MIN. OPEN SPACE: 15.26 ACRES (SEE PRELIMINARY PLAN)

SYMBOL	DESCRIPTION
○	EXISTING LOT CORNER
●	PROPOSED LOT CORNER
○	EXISTING LOT CENTER
●	PROPOSED LOT CENTER
○	EXISTING LOT CORNER
●	PROPOSED LOT CORNER
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○	EXISTING LOT CORNER
●	PROPOSED LOT CORNER
○	EXISTING LOT CENTER
●	PROPOSED LOT CENTER

<p>PROJECT: PRELIMINARY SUBDIVISION PLAN                  VALLEY CREEK FARMS                  BOONE COUNTY, KENTUCKY</p> <p>CLIENT: MADEIRA PLACE, LLC                  7241 WOOSTER PIKE                  CINCINNATI, OHIO 45227</p>	<p>DATE: 10-26-15</p> <p>SHEET: C-1.0</p>	<p>SCALE: 1" = 100'</p>	<p>PROJECT NO: 15-124</p>	<p>DATE: 10-26-15</p>	<p>REVISIONS</p>	



**Boone County**  
**Planning Commission**  
**8/21/24**

**Mike Parker - 38 Year Vet of Boone County**  
**Real Estate**

Request of Charter Commercial, LLC (applicant) for Delaware Crossing, LLC (owner) for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

**Question - 139 attached Single Family residential dwellings - what are they?**

**157 Multi - are what**

**Cons - Has been zoned SR1**

**Emergency Traffic Police, Fire, Life Squad**

**Apartments brings more Police Calls just  
because of the density**

**Schools are crowded already, the density  
has the possibility of more students than  
if it was Single Family!**

**It will affect Property Values, period!**

**LOW INVENTORY Single Family**

**in Boone County**

- **Impact on Infrastructure:** Emphasize the potential strain on existing infrastructure, including roads, water, sewer systems, and public services. Discuss how the increase in density from 139 attached single-family homes and 157 multi-family units might overwhelm these systems, leading to traffic congestion, overburdened schools, and inadequate emergency services.
- **Character of the Neighborhood:** Argue that the proposed zoning change and the subsequent development are inconsistent with the existing character of the neighborhood. The shift from Suburban Residential One (SR-1) to a Planned Development (PD) might lead to higher density housing, which could alter the suburban feel and reduce property values for current residents.
- **Environmental Concerns:** Raise concerns about the environmental impact of the proposed development. This could include potential damage to local wildlife habitats, increased stormwater runoff

leading to flooding, or the loss of green space that contributes to the community's quality of life.

- **Traffic and Safety Issues:** Point out the likely increase in traffic, which could lead to safety concerns, especially on roads like Holbrook Lane and Lariat Way. Discuss how the added volume of cars could contribute to accidents and make it difficult for residents to move safely within their neighborhood.
- **Lack of Compatibility with the Comprehensive Plan:** If applicable, argue that the proposed development is not in alignment with the county's comprehensive plan, which might prioritize low-density residential development in this area. Highlight any goals or policies from the plan that the proposal contradicts.
- **Quality of Life:** Stress how the proposed development could negatively impact the quality of life for current residents. Discuss concerns about noise, pollution, overcrowding, and the potential loss of the community's suburban lifestyle.

- **Lack of Community Support:** If you have evidence or anecdotes showing that a significant portion of the community opposes this development, bring that to the commission's attention. Public opinion can be a powerful factor in planning decisions.

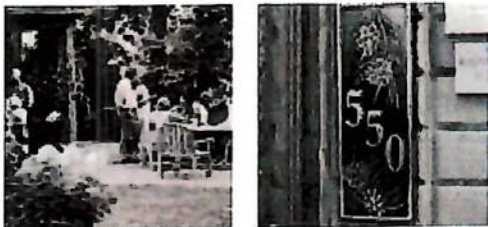
- **Potential for Overdevelopment:** Argue that the proposed development could lead to overdevelopment of the area, which might not be sustainable in the long term. Discuss the risks of creating a densely populated area without the necessary infrastructure or services to support it.

Here is the main reason you turn this down, what your decision tonight or when you vote on it will tell Boone County Residents whether you believe in Single Family Homeownership, the American Dream or you believe in as many units as possible on ever square inch of land left in Boone County!

Because if you approve this, trust me many more developers will start looking at other zoning parcels like this and want to build more multi family like this!

**Please PROTECT BOONE COUNTY SINGLE  
FAMILIY HOMES!**

# BOONE COUNTY SINGLE FAMILY



Su	M	Tu	W	Th	F	Sa
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Sunday, August 18

# 2024

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### Parker Real Estate Group

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 Mike@MikeParker.com  
 859-647-0700 Office

*ACTIVE  
 181  
 \$395,192*

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# BOONE COUNTY SINGLE FAMILY

Sunday, August 18, 2024

Parker Real Estate Group

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## Let's take a look

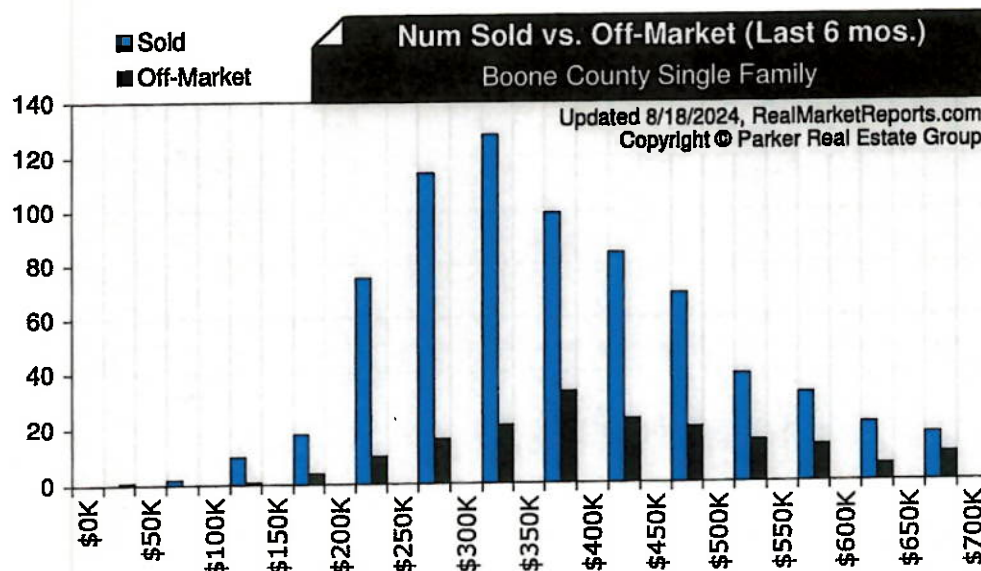
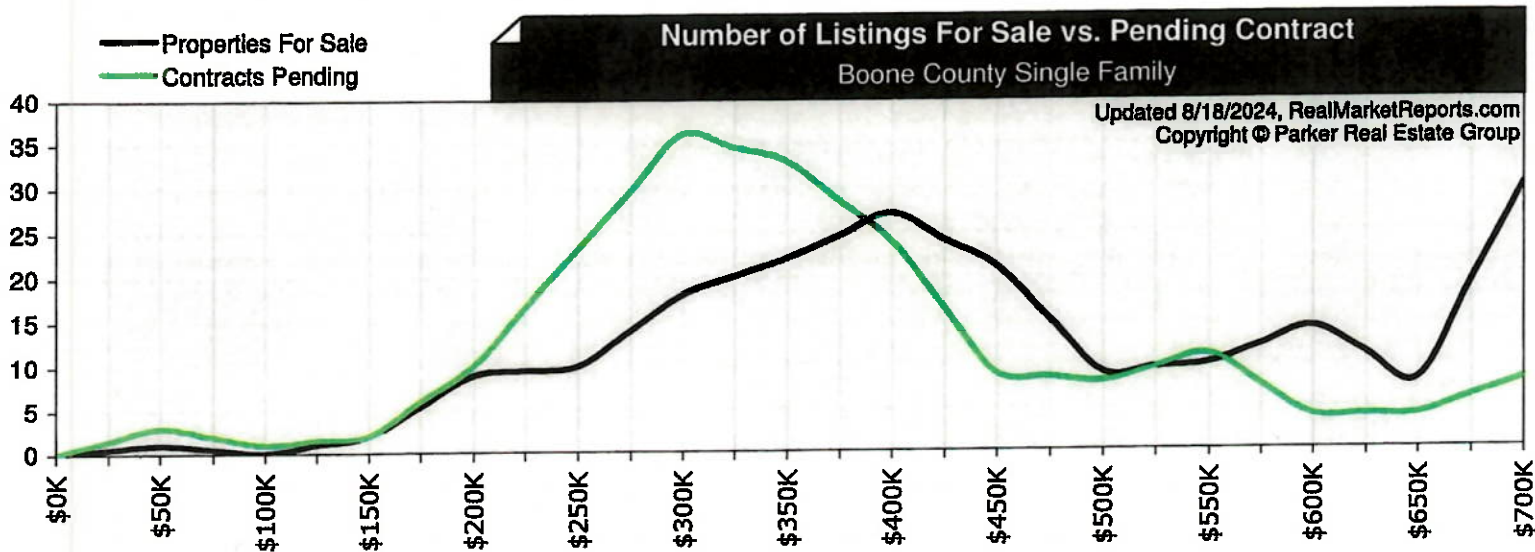
at the real estate market. Currently there are 176 sales pending in the market overall, leaving 181 listings still for sale. The resulting pending ratio is 49.3% (176 divided by 357). So you might be asking yourself, that's great... but what exactly does it mean? I'm glad you asked!

The pending ratio indicates the supply & demand of the market. Specifically, a high ratio means that listings are in demand and quickly going to contract. Alternatively, a low ratio means there are not enough qualified buyers for the existing supply.

*"Current inventory is described as active."*

Taking a closer look, we notice that the \$300K - \$350K price range has a relatively large number of contracts pending sale.

We also notice that the \$700K + price range has a relatively large inventory of properties for sale at 30 listings. The median list price (or asking price) for all properties in this market is \$455,000.



A total of 743 contracts have closed in the last 6 months with a median sold price of \$360,000. Breaking it down, we notice that the \$300K - \$350K price range contains the highest number of sold listings.

Alternatively, a total of 196 listings have failed to sell in that same period of time. Listings may fail to sell for many reasons such as being priced too high, having been inadequately marketed, the property was in poor condition, or perhaps the owner had second thoughts about selling at this particular time. The \$350K - \$400K price range has the highest number of off-market listings at 33 properties.

# BOONE COUNTY SINGLE FAMILY

Sunday, August 18, 2024

Parker Real Estate Group

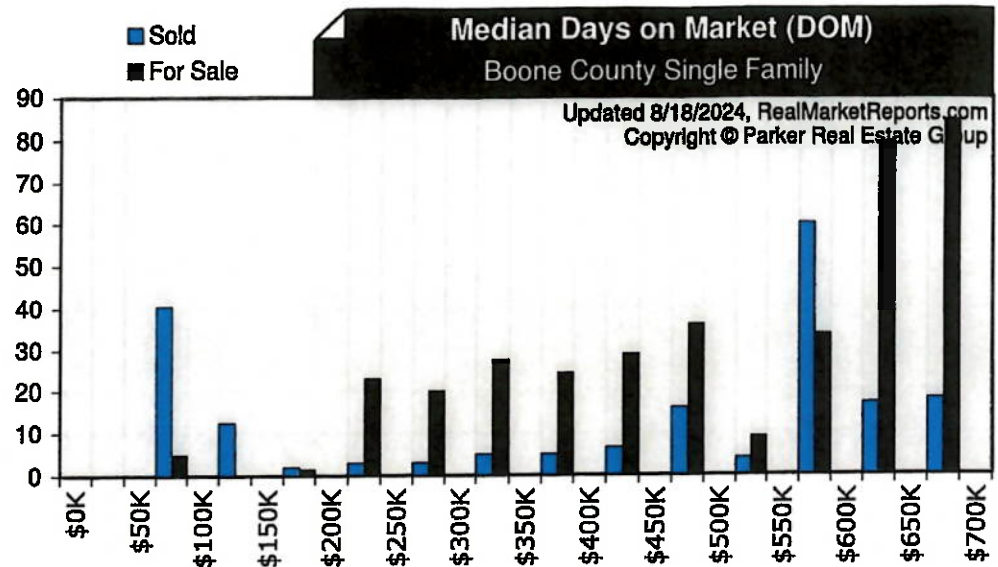
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Looking at the chart to the right, you might be wondering why average days on market (DOM) is important. This is a useful measurement because it can help us to determine whether we are in a buyer's market (indicated by high DOM), or a seller's market (indicated by low DOM). Active listings (properties for sale) have been on the market for a median time of 31 days.

Analysis of sold properties for the last six months reveals a median sold price of \$360,000 and 5 days on market. Notice that properties in the \$150K - \$200K price range have sold quickest over the last six months.

The recent history of sales can be seen in the two charts below. The median sold price for the last 30 days was \$391,250 with a DOM of 5 days.

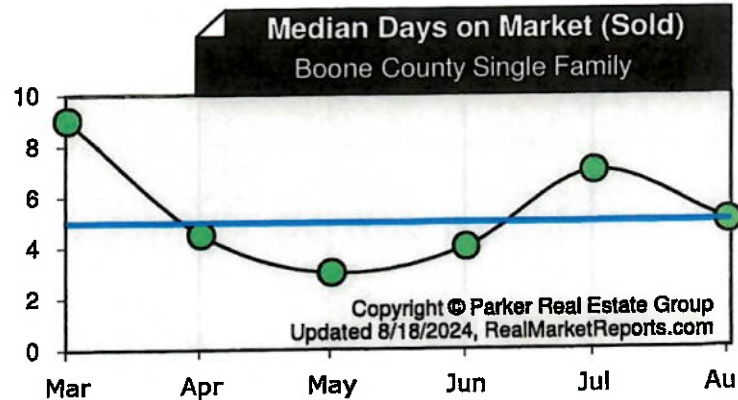
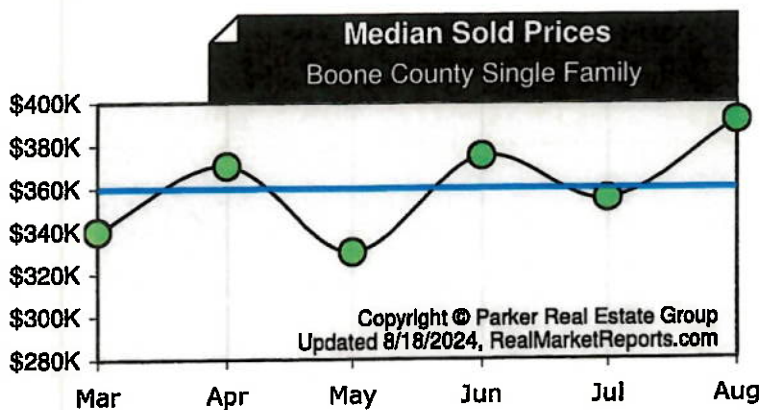
Since the recent DOM is greater than the median DOM for the last 6 months, it is a negative indicator for demand. It is always important to realize that real estate markets can fluctuate due to many factors, including shifting interest rates, the economy, or seasonal changes.



**"The median list-to-sales ratio for this area is 100.0%."**

Ratios are simple ways to express the difference between two values such as list price and sold price. In our case, we typically use the list-to-sale ratio to determine the percentage of the final list price that the buyer ultimately paid. It is a very common method to help buyers decide how much to offer on a property.

Analysis of the absorption rate indicates an inventory of 1.5 months based on the last 6 months of sales. This estimate is often used to determine how long it would take to sell off the current inventory of properties if all conditions remained the same. It is significant to mention that this estimate does not take into consideration any additional properties that will come onto the market in the future.



# BOONE COUNTY SINGLE FAMILY

Sunday, August 18, 2024

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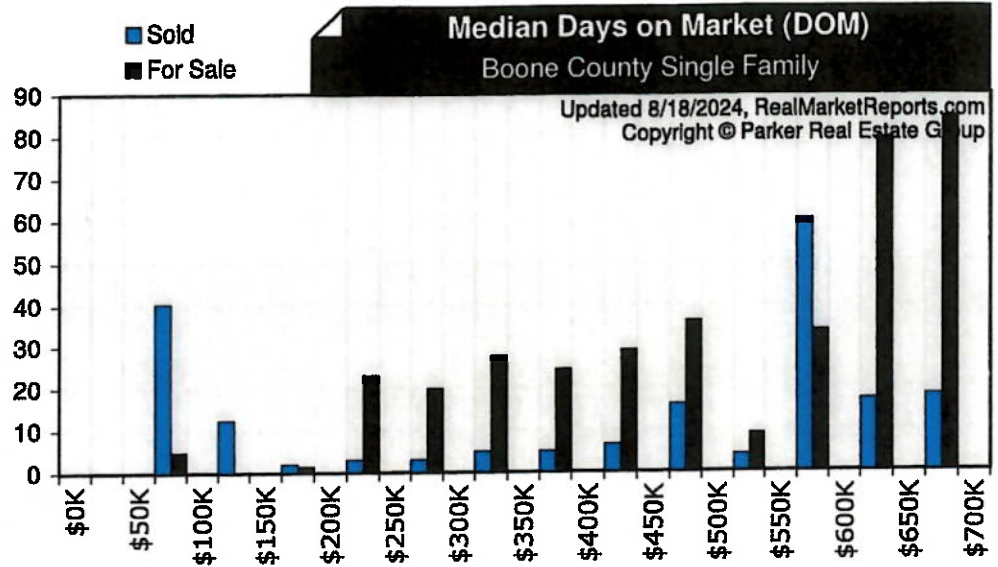
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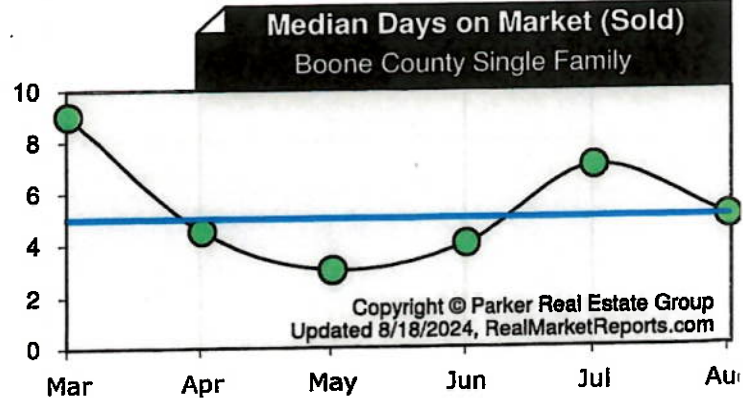
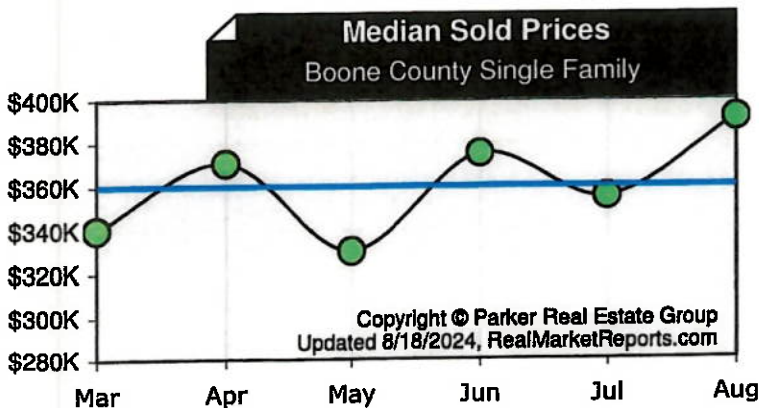
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*"The median list-to-sales ratio for this area is 100.0%."*

Ratios are simple ways to express the difference between two values such as list price and sold price. In our case, we typically use the list-to-sale ratio to determine the percentage of the final list price that the buyer ultimately paid. It is a very common method to help buyers decide how much to offer on a property.

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# BOONE COUNTY SINGLE FAMILY

Sunday, August 18, 2024

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MARKET SUMMARY TABLE		176 PENDING <sup>[2]</sup>					196 OFF-MARKET (last 6 mos) <sup>[3]</sup>					ABSORPTION RATE (months of inventory)		
A = Average Value M = Median Value N/A = Not Available		181 ACTIVE <sup>[1]</sup>			743 SOLD/CLOSED (last 6 months) <sup>[4]</sup>									
Price Range		Num #	Days on Market	Current List Price	Num #	Pend Ratio	Num #	Num #	Days on Market	Original List Price	Final List Price	Sold/Closed Price	List-Sale Ratio	Est. Mos.
Low	High													
\$0	\$49,999	0			0		1	0						
\$50,000	\$99,999	1	A 5	\$ 79,900	3	75%	0	2	A 41	\$ 101,750	\$ 101,750	\$ 82,500	81%	3
			M 5	\$ 79,900					M 41	\$ 101,750	\$ 101,750	\$ 82,500	81%	
\$100,000	\$149,999	0			1	N/A	1	10	A 23	\$ 107,990	\$ 118,495	\$ 115,995	98%	N/A
			M 13	\$ 102,500					\$ 105,000	\$ 109,950	105%			
\$150,000	\$199,999	2	A 2	\$ 179,950	2	50%	4	18	A 7	\$ 175,972	\$ 173,194	\$ 177,589	103%	0.7
			M 2	\$ 179,950					M 2	\$ 179,450	\$ 179,400	\$ 175,000	98%	
\$200,000	\$249,999	9	A 29	\$ 223,089	10	53%	10	75	A 14	\$ 230,100	\$ 227,553	\$ 229,206	101%	0.7
			M 23	\$ 219,000					M 3	\$ 230,000	\$ 229,900	\$ 231,000	100%	
\$250,000	\$299,999	10	A 38	\$ 281,880	23	70%	16	114	A 10	\$ 272,974	\$ 270,846	\$ 272,759	101%	0.5
			M 20	\$ 287,000					M 3	\$ 275,000	\$ 274,900	\$ 274,450	100%	
\$300,000	\$349,999	18	A 41	\$ 331,228	36	67%	21	128	A 18	\$ 324,538	\$ 323,075	\$ 322,366	100%	0.8
			M 28	\$ 330,000					M 5	\$ 325,000	\$ 324,900	\$ 324,900	100%	
\$350,000	\$399,999	22	A 56	\$ 383,586	33	60%	33	99	A 17	\$ 380,110	\$ 376,580	\$ 373,436	99%	1.3
			M 25	\$ 391,088					M 5	\$ 376,015	\$ 375,000	\$ 374,900	100%	
\$400,000	\$449,999	27	A 50	\$ 420,201	24	47%	23	84	A 33	\$ 428,717	\$ 424,131	\$ 422,483	100%	1.9
			M 29	\$ 419,900					M 7	\$ 426,000	\$ 424,900	\$ 421,498	99%	
\$450,000	\$499,999	21	A 44	\$ 478,968	9	30%	20	69	A 47	\$ 488,277	\$ 478,408	\$ 474,099	99%	1.8
			M 36	\$ 480,738					M 16	\$ 479,900	\$ 475,000	\$ 474,258	100%	
\$500,000	\$549,999	9	A 14	\$ 520,989	8	47%	15	39	A 39	\$ 538,877	\$ 532,566	\$ 526,084	99%	1.4
			M 9	\$ 515,000					M 4	\$ 540,000	\$ 530,000	\$ 525,000	99%	
\$550,000	\$599,999	10	A 67	\$ 578,823	11	52%	13	32	A 65	\$ 591,315	\$ 581,099	\$ 576,946	99%	1.9
			M 34	\$ 579,950					M 60	\$ 599,617	\$ 581,897	\$ 575,000	99%	
\$600,000	\$649,999	14	A 73	\$ 631,885	4	22%	6	21	A 51	\$ 641,044	\$ 629,254	\$ 624,971	99%	4
			M 80	\$ 628,950					M 17	\$ 639,000	\$ 629,900	\$ 621,500	99%	
\$650,000	\$699,999	8	A 88	\$ 672,113	4	33%	10	17	A 42	\$ 708,412	\$ 690,224	\$ 677,833	98%	2.8
			M 85	\$ 664,900					M 18	\$ 699,900	\$ 699,000	\$ 680,000	97%	
\$700,000	+	30	A 75	\$ 894,026	8	21%	23	35	A 57	\$ 908,618	\$ 867,039	\$ 878,861	99%	5.1
			M 52	\$ 809,949					M 18	\$ 811,346	\$ 799,900	\$ 799,900	100%	
Market Totals		181			176	49%	196	743						1.5
Market Averages			53	\$ 511,560					27	\$ 402,352	\$ 397,148	\$ 395,192	100%	
Market Medians			31	\$ 455,000					5	\$ 365,000	\$ 360,000	\$ 360,000	100%	

Date Range (Off-Market & Sold) = 02/18/2024 to 08/18/2024

Data believed to be accurate but not guaranteed.

Status = [1] A; [2] P; [3] E, L, W; [4] C

Area = K09, K10, K11

Property Sub Type = SF

Favors Sellers Favors Buyers

# BOONE COUNTY SINGLE FAMILY

Sunday, August 18, 2024

Parker Real Estate Group

HUFF Realty

Mike@MikeParker.com

859-647-0700 Office

## 1. PROPERTIES FOR SALE (ACTIVE)

- Number Active:** The number of listings for sale which are currently being marketed but do not yet have a purchase agreement.
- Days on Market (DOM):** The marketing period of currently active listings. This does not account for some listings which have had a previous listing period, but were re-entered as a new listing.
- Current List Price:** The price that a property seller is currently asking.

## 2. CONTRACTS PENDING

- Number Pending:** The number of current listings for which a contract has been signed but has not yet closed.
- Pending Ratio:** Sometimes called a "list-to-pending ratio". This is a measure of how fast properties are going under contract vs. how fast they are being listed.

$$\text{Pending Ratio} = \frac{P \text{ (Number of Pending Listings)}}{A+P \text{ (Number of Active + Pending)}}$$

(P) represents properties that buyers have chosen  
(A+P) represents the entire pool from which they have chosen

## 3. OFF-MARKET

- Number Off-Market:** The number of listing agreements that have failed to close in the last 6 months. Some owners may choose to re-offer their property for sale.

## 4. PROPERTIES SOLD (CLOSED CONTRACT)

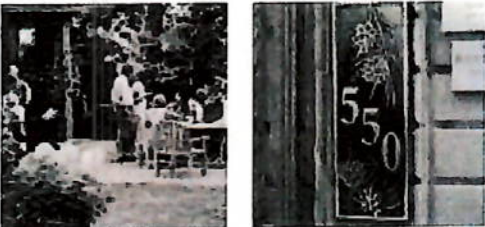
- Number Sold:** The number of properties that have gone to a closing in the last 6 months.
- Days on Market (DOM):** The marketing time it has taken properties to sell in the last 6 months.
- Original List Price:** The price at which a sold property was originally marketed.
- Final List Price:** The price at which a sold property was marketed just prior to selling.
- Sold/Closed Price:** The price for which a property sold.
- List to Sales Ratio:** The percentage of the list price that the buyer ultimately paid for the property.

$$\text{List to Sales Ratio} = \frac{\text{Sold Price}}{\text{Final List Price}}$$

## 5. ABSORPTION RATE / MONTHS OF INVENTORY

- Absorption Rate / Months of Inventory:** An estimate of how fast listings are currently selling measured in months. For example, if 100 properties sell per month and there are 800 properties for sale - there is an 8 month supply of inventory before counting the additional properties that will come on the market.

# BOONE COUNTY SINGLE FAMILY



Su	M	Tu	W	Th	F	Sa
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Sunday, August 22

# 2021

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### Mike Parker Team

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[mike@mikeparker.com](mailto:mike@mikeparker.com)  
 859-647-0700

*82 ACTIVE  
 ASP \$323,000*

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# BOONE COUNTY SINGLE FAMILY

Sunday, August 22, 2021

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 HUFF Realty  
 mike@mikeparker.com  
 859-647-0700

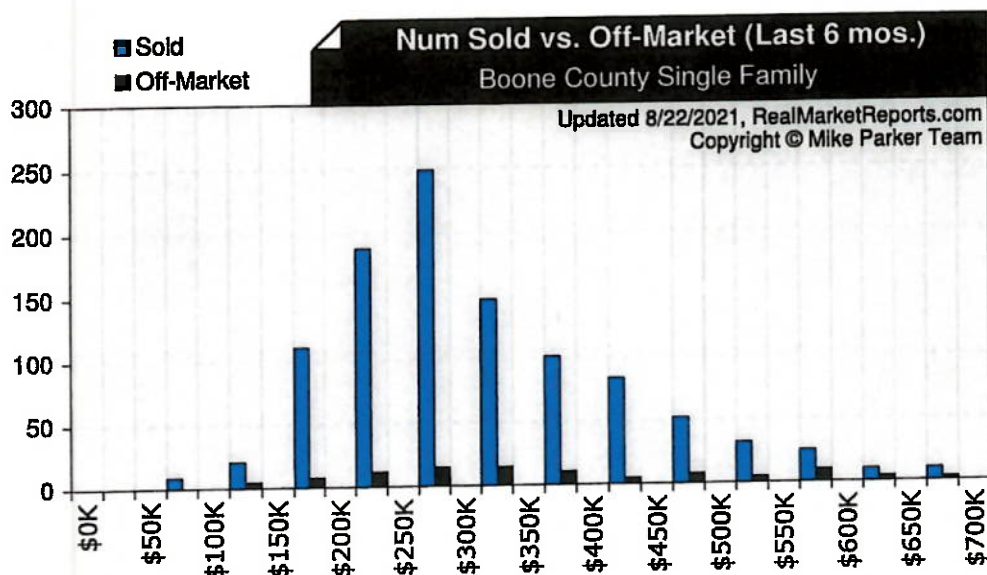
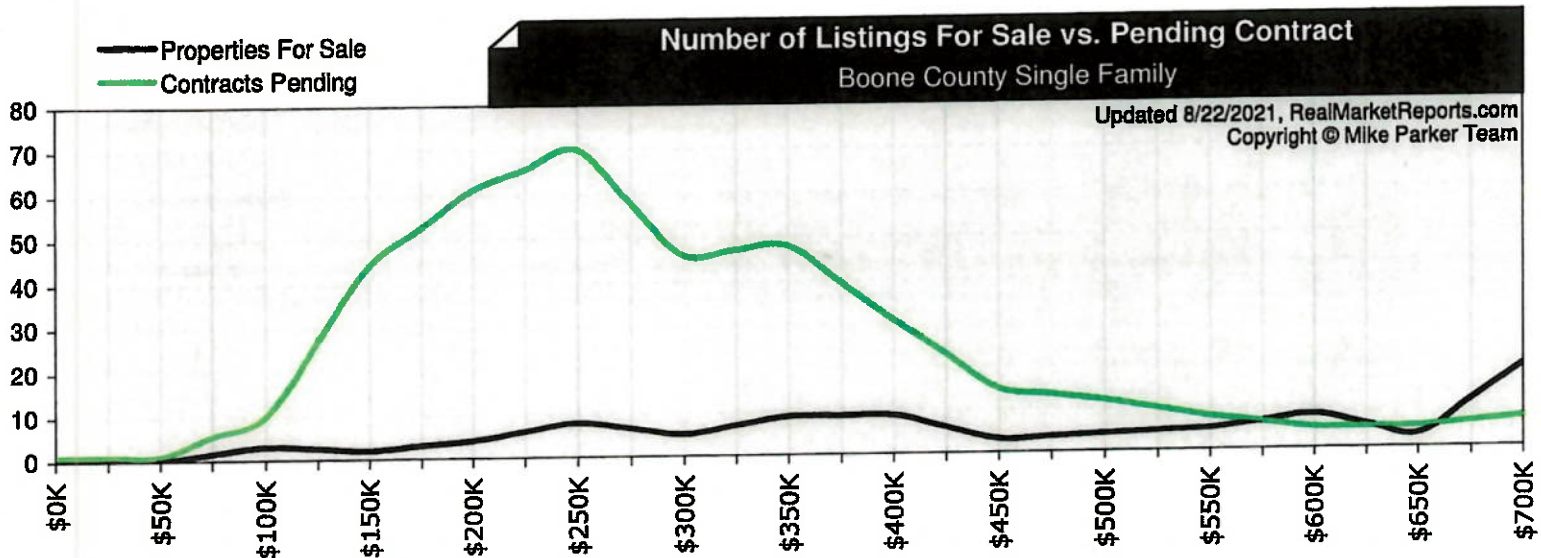
*Let's take a look* at the real estate market. Currently there are 364 sales pending in the market overall, leaving 82 listings still for sale. The resulting pending ratio is 81.6% (364 divided by 446). So you might be asking yourself, that's great... but what exactly does it mean? I'm glad you asked!

The pending ratio indicates the supply & demand of the market. Specifically, a high ratio means that listings are in demand and quickly going to contract. Alternatively, a low ratio means there are not enough qualified buyers for the existing supply.

*"Current inventory is described as very active."*

Taking a closer look, we notice that the \$250K - \$300K price range has a relatively large number of contracts pending sale.

We also notice that the \$700K + price range has a relatively large inventory of properties for sale at 19 listings. The median list price (or asking price) for all properties in this market is \$497,450.



A total of 1065 contracts have closed in the last 6 months with a median sold price of \$287,000. Breaking it down, we notice that the \$250K - \$300K price range contains the highest number of sold listings.

Alternatively, a total of 119 listings have failed to sell in that same period of time. Listings may fail to sell for many reasons such as being priced too high, having been inadequately marketed, the property was in poor condition, or perhaps the owner had second thoughts about selling at this particular time. The \$700K + price range has the highest number of off-market listings at 18 properties.

# BOONE COUNTY SINGLE FAMILY

Sunday, August 22, 2021

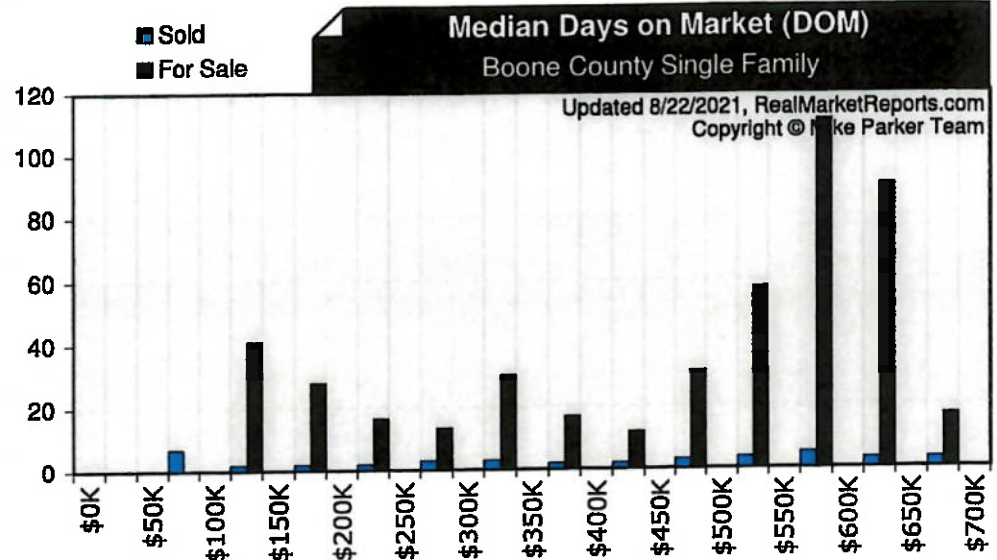
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 mike@mikeparker.com  
 859-647-0700

Looking at the chart to the right, you might be wondering why average days on market (DOM) is important. This is a useful measurement because it can help us to determine whether we are in a buyer's market (indicated by high DOM), or a seller's market (indicated by low DOM). Active listings (properties for sale) have been on the market for a median time of 45 days.

Analysis of sold properties for the last six months reveals a median sold price of \$287,000 and 2 days on market. Notice that properties in the \$200K - \$250K price range have sold quickest over the last six months.

The recent history of sales can be seen in the two charts below. The median sold price for the last 30 days was \$285,000 with a DOM of 2 days.

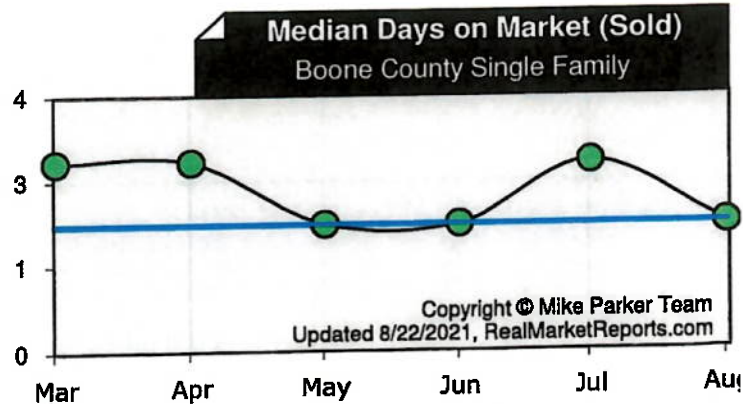
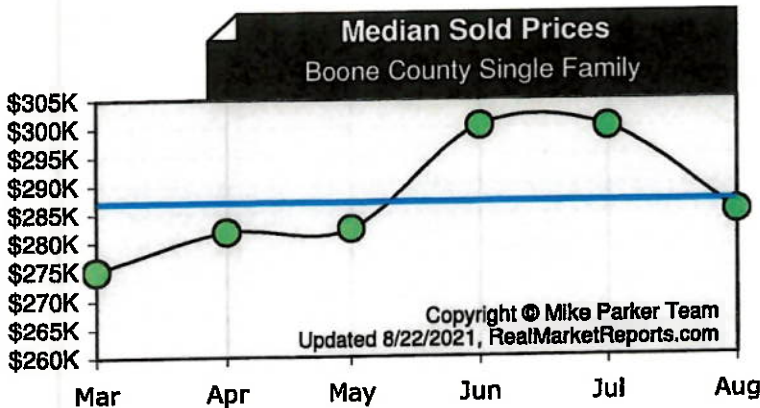
Since the recent DOM is greater than the median DOM for the last 6 months, it is a negative indicator for demand. It is always important to realize that real estate markets can fluctuate due to many factors, including shifting interest rates, the economy, or seasonal changes.



*"The median list-to-sales ratio for this area is 100.7%."*

Ratios are simple ways to express the difference between two values such as list price and sold price. In our case, we typically use the list-to-sale ratio to determine the percentage of the final list price that the buyer ultimately paid. It is a very common method to help buyers decide how much to offer on a property.

Analysis of the absorption rate indicates an inventory of 0.5 months based on the last 6 months of sales. This estimate is often used to determine how long it would take to sell off the current inventory of properties if all conditions remained the same. It is significant to mention that this estimate does not take into consideration any additional properties that will come onto the market in the future.



# BOONE COUNTY SINGLE FAMILY

Sunday, August 22, 2021

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MARKET SUMMARY TABLE		364 PENDING <sup>[2]</sup>				119 OFF-MARKET (last 6 mos) <sup>[3]</sup>				ABSORPTION RATE (months of inventory)				
Price Range		82 ACTIVE <sup>[1]</sup>					1065 SOLD/CLOSED (last 6 months) <sup>[4]</sup>							
Low	High	Num #	Days on Market	Current List Price	Num #	Pend Ratio	Num #	Num #	Days on Market	Original List Price	Final List Price	Sold/Closed Price	List-Sale Ratio	Est. Mos.
\$0	\$49,999	0			1	N/A	0	0						
\$50,000	\$99,999	0			1	N/A	0	9	A 18 M 7	\$ 91,856 \$ 90,000	\$ 87,300 \$ 85,000	\$ 76,222 \$ 72,000	87% 85%	N/A
\$100,000	\$149,999	3	A 45 M 41	\$ 129,633 \$ 139,000	10	77%	5	21	A 11 M 2	\$ 124,519 \$ 125,000	\$ 130,895 \$ 129,900	\$ 131,586 \$ 130,000	101% 100%	0.9
\$150,000	\$199,999	2	A 28 M 28	\$ 182,450 \$ 182,450	44	96%	8	111	A 8 M 2	\$ 175,192 \$ 174,900	\$ 175,232 \$ 174,900	\$ 176,222 \$ 175,000	101% 100%	0.1
\$200,000	\$249,999	4	A 25 M 17	\$ 220,200 \$ 217,450	61	94%	12	188	A 8 M 2	\$ 218,473 \$ 218,750	\$ 218,476 \$ 219,450	\$ 224,596 \$ 225,000	103% 103%	0.1
\$250,000	\$299,999	8	A 88 M 14	\$ 277,188 \$ 274,000	70	90%	15	249	A 12 M 3	\$ 268,094 \$ 265,000	\$ 268,132 \$ 265,000	\$ 272,540 \$ 270,614	102% 102%	0.2
\$300,000	\$349,999	5	A 76 M 30	\$ 328,180 \$ 325,000	46	90%	15	148	A 16 M 3	\$ 319,289 \$ 319,000	\$ 318,955 \$ 319,000	\$ 321,987 \$ 321,375	101% 101%	0.2
\$350,000	\$399,999	9	A 61 M 17	\$ 381,252 \$ 391,388	48	84%	11	102	A 9 M 2	\$ 371,184 \$ 370,000	\$ 369,060 \$ 369,900	\$ 371,362 \$ 370,000	101% 100%	0.5
\$400,000	\$449,999	9	A 19 M 12	\$ 420,355 \$ 420,000	31	78%	5	84	A 15 M 2	\$ 424,932 \$ 425,000	\$ 424,583 \$ 425,000	\$ 425,597 \$ 427,545	100% 101%	0.6
\$450,000	\$499,999	3	A 33 M 31	\$ 498,267 \$ 499,900	15	83%	8	52	A 29 M 3	\$ 476,107 \$ 478,784	\$ 475,097 \$ 475,000	\$ 471,224 \$ 474,900	99% 100%	0.3
\$500,000	\$549,999	4	A 63 M 58	\$ 531,840 \$ 532,580	12	75%	5	32	A 14 M 4	\$ 521,689 \$ 531,950	\$ 514,863 \$ 525,000	\$ 523,091 \$ 525,000	102% 100%	0.8
\$550,000	\$599,999	5	A 124 M 111	\$ 581,016 \$ 580,243	8	62%	10	25	A 17 M 5	\$ 577,298 \$ 575,000	\$ 574,102 \$ 575,000	\$ 569,057 \$ 569,000	99% 99%	1.2
\$600,000	\$649,999	8	A 92 M 90	\$ 621,616 \$ 624,535	5	38%	4	10	A 12 M 3	\$ 615,401 \$ 617,983	\$ 615,664 \$ 619,000	\$ 619,023 \$ 615,194	101% 99%	4.8
\$650,000	\$699,999	3	A 43 M 17	\$ 674,687 \$ 675,000	5	63%	3	10	A 14 M 3	\$ 683,860 \$ 684,450	\$ 681,370 \$ 677,000	\$ 669,650 \$ 672,500	98% 99%	1.8
\$700,000	+	19	A 89 M 89	\$ 1,055,282 \$ 875,000	7	27%	18	24	A 81 M 6	\$ 956,379 \$ 897,000	\$ 926,250 \$ 887,000	\$ 886,877 \$ 823,500	96% 93%	4.8
Market Totals		82			364	82%	119	1065						0.5
Market Averages		68		\$ 564,415					14	\$ 322,399	\$ 321,192	\$ 323,024	101%	
Market Medians		45		\$ 497,450					2	\$ 285,000	\$ 285,000	\$ 287,000	101%	

Date Range (Off-Market & Sold) = 02/21/2021 to 08/22/2021  
 Data believed to be accurate but not guaranteed.

Status = [1] Active; [2] Pending; [3] Cancelled, Expired; [4] Sold  
 Area = K09, K10, K11  
 Property Sub Type = Single Family

Favors Sellers Favors Buyers

# BOONE COUNTY SINGLE FAMILY

Sunday, August 22, 2021

Mike Parker Team

HUFF Realty

mike@mikeparker.com

859-647-0700

## 1. PROPERTIES FOR SALE (ACTIVE)

- a. **Number Active:** The number of listings for sale which are currently being marketed but do not yet have a purchase agreement.
- b. **Days on Market (DOM):** The marketing period of currently active listings. This does not account for some listings which have had a previous listing period, but were re-entered as a new listing.
- c. **Current List Price:** The price that a property seller is currently asking.

## 2. CONTRACTS PENDING

- a. **Number Pending:** The number of current listings for which a contract has been signed but has not yet closed.
- b. **Pending Ratio:** Sometimes called a "list-to-pending ratio". This is a measure of how fast properties are going under contract vs. how fast they are being listed.

$$\text{Pending Ratio} = \frac{P \text{ (Number of Pending Listings)}}{A+P \text{ (Number of Active + Pending)}}$$

(P) represents properties that buyers have chosen

(A+P) represents the entire pool from which they have chosen

## 3. OFF-MARKET

- a. **Number Off-Market:** The number of listing agreements that have failed to close in the last 6 months. Some owners may choose to re-offer their property for sale.

## 4. PROPERTIES SOLD (CLOSED CONTRACT)

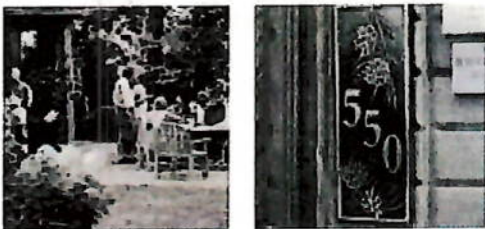
- a. **Number Sold:** The number of properties that have gone to a closing in the last 6 months.
- b. **Days on Market (DOM):** The marketing time it has taken properties to sell in the last 6 months.
- c. **Original List Price:** The price at which a sold property was originally marketed.
- d. **Final List Price:** The price at which a sold property was marketed just prior to selling.
- e. **Sold/Closed Price:** The price for which a property sold.
- f. **List to Sales Ratio:** The percentage of the list price that the buyer ultimately paid for the property.

$$\text{List to Sales Ratio} = \frac{\text{Sold Price}}{\text{Final List Price}}$$

## 5. ABSORPTION RATE / MONTHS OF INVENTORY

- a. **Absorption Rate / Months of Inventory:** An estimate of how fast listings are currently selling measured in months. For example, if 100 properties sell per month and there are 800 properties for sale - there is an 8 month supply of inventory before counting the additional properties that will come on the market.

# BOONE COUNTY SINGLE FAMILY



Su	M	Tu	W	Th	F	Sa
			1	2	3	4
5	6	7	8	9	10	11
12	13	14	15	16	17	18
19	20	21	22	23	24	25
26	27	28	29	30	31	

Sunday, August 19

# 2018

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# BOONE COUNTY SINGLE FAMILY

Sunday, August 19, 2018

**Mike Parker Team**  
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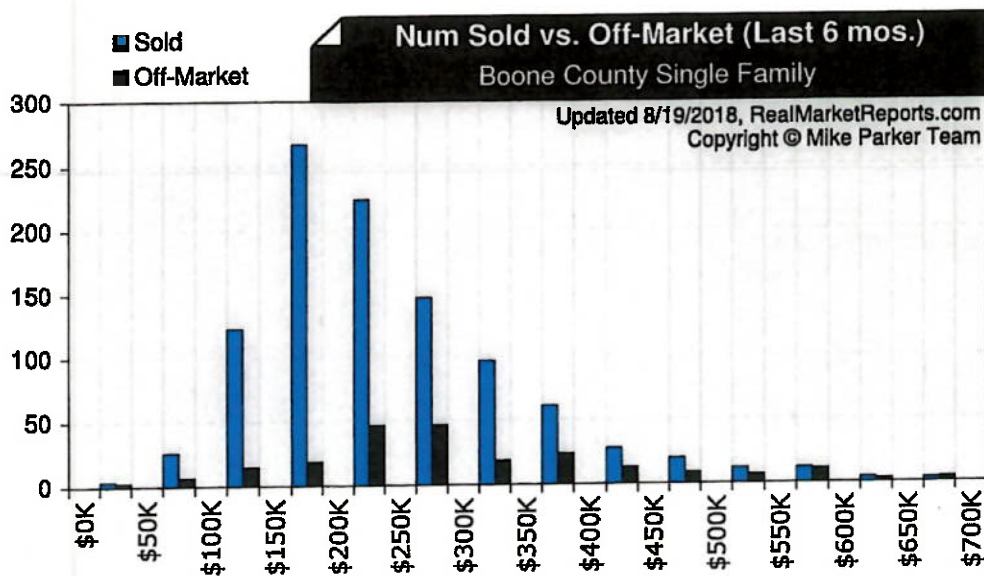
*Let's take a look* at the real estate market. Currently there are 299 sales pending in the market overall, leaving 322 listings still for sale. The resulting pending ratio is 48.1% (299 divided by 621). So you might be asking yourself, that's great... but what exactly does it mean? I'm glad you asked!

The pending ratio indicates the supply & demand of the market. Specifically, a high ratio means that listings are in demand and quickly going to contract. Alternatively, a low ratio means there are not enough qualified buyers for the existing supply.

*"Current inventory is described as active."*

Taking a closer look, we notice that the \$150K - \$200K price range has a relatively large number of contracts pending sale.

We also notice that the \$200K - \$250K price range has a relatively large inventory of properties for sale at 56 listings. The median list price (or asking price) for all properties in this market is \$308,250.



A total of 1038 contracts have closed in the last 6 months with a median sold price of \$222,250. Breaking it down, we notice that the \$150K - \$200K price range contains the highest number of sold listings.

Alternatively, a total of 241 listings have failed to sell in that same period of time. Listings may fail to sell for many reasons such as being priced too high, having been inadequately marketed, the property was in poor condition, or perhaps the owner had second thoughts about selling at this particular time. The \$200K - \$250K price range has a high number of off-market listings at 47 properties.

# BOONE COUNTY SINGLE FAMILY

Sunday, August 19, 2018

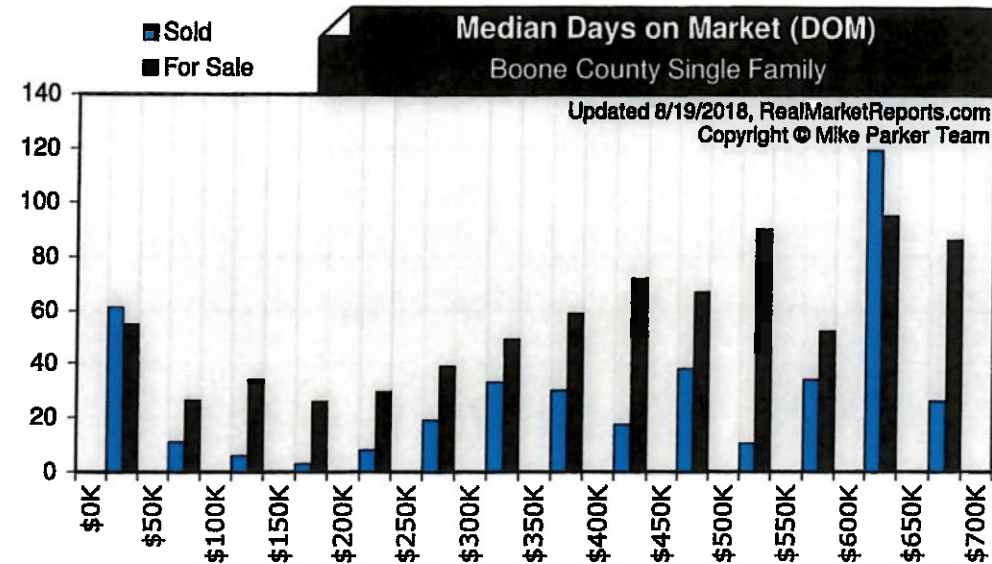
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Looking at the chart to the right, you might be wondering why average days on market (DOM) is important. This is a useful measurement because it can help us to determine whether we are in a buyer's market (indicated by high DOM), or a seller's market (indicated by low DOM). Active listings (properties for sale) have been on the market for a median time of 48 days.

Analysis of sold properties for the last six months reveals a median sold price of \$222,250 and 8 days on market. Notice that properties in the \$150K - \$200K price range have sold quickest over the last six months.

The recent history of sales can be seen in the two charts below. The median sold price for the last 30 days was \$220,000 with a DOM of 5 days.

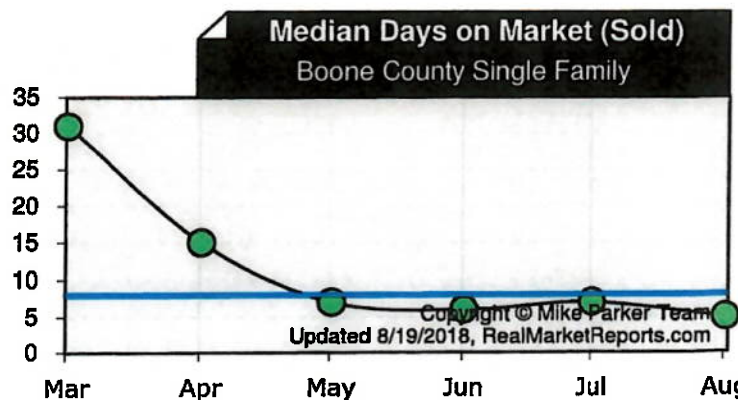
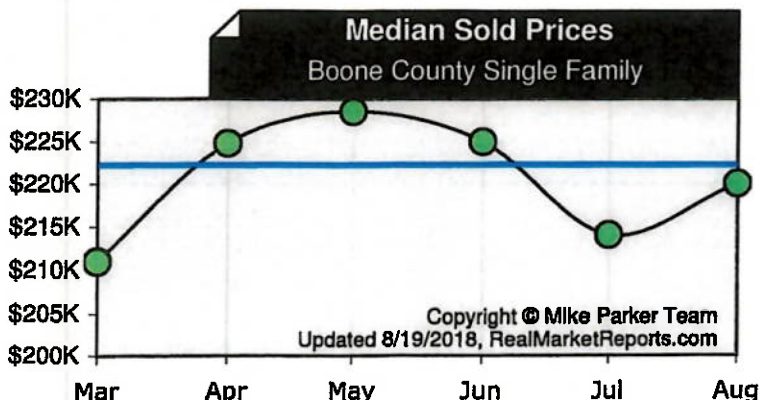
Since the recent DOM is less than the median DOM for the last 6 months, it is a positive indicator for demand. It is always important to realize that real estate markets can fluctuate due to many factors, including shifting interest rates, the economy, or seasonal changes.



*"The median list-to-sales ratio for this area is 98.8%."*

Ratios are simple ways to express the difference between two values such as list price and sold price. In our case, we typically use the list-to-sale ratio to determine the percentage of the final list price that the buyer ultimately paid. It is a very common method to help buyers decide how much to offer on a property.

Analysis of the absorption rate indicates an inventory of 1.9 months based on the last 6 months of sales. This estimate is often used to determine how long it would take to sell off the current inventory of properties if all conditions remained the same. It is significant to mention that this estimate does not take into consideration any additional properties that will come onto the market in the future.



# BOONE COUNTY SINGLE FAMILY

Sunday, August 19, 2018

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MARKET SUMMARY TABLE		299 PENDING <sup>[2]</sup>				241 OFF-MARKET (last 6 mos) <sup>[3]</sup>				ABSORPTION RATE (months of inventory)				
A = Average Value M = Median Value N/A = Not Available		322 ACTIVE <sup>[1]</sup>				1038 SOLD/CLOSED (last 6 months) <sup>[4]</sup>								
Price Range		Num #	Days on Market	Current List Price	Num #	Pend Ratio	Num #	Num #	Days on Market	Original List Price	Final List Price	Sold/Closed Price	List-Sale Ratio	Est. Mos.
Low	High													
\$0	\$49,999	3	A 66 M 55	\$ 29,900 \$ 29,900	2	40%	3	4	A 89 M 62	\$ 52,425 \$ 54,900	\$ 46,175 \$ 52,400	\$ 42,125 \$ 42,000	91% 80%	4.5
\$50,000	\$99,999	6	A 38 M 27	\$ 86,450 \$ 85,000	12	67%	7	26	A 25 M 11	\$ 86,928 \$ 85,000	\$ 84,286 \$ 84,950	\$ 81,085 \$ 84,500	96% 99%	1.4
\$100,000	\$149,999	15	A 55 M 34	\$ 133,087 \$ 139,500	40	73%	15	123	A 23 M 6	\$ 133,662 \$ 136,500	\$ 132,632 \$ 135,000	\$ 131,183 \$ 135,000	99% 100%	0.7
\$150,000	\$199,999	28	A 45 M 28	\$ 178,146 \$ 184,950	86	75%	19	267	A 17 M 3	\$ 176,761 \$ 175,000	\$ 175,716 \$ 175,000	\$ 175,191 \$ 175,000	100% 100%	0.6
\$200,000	\$249,999	56	A 48 M 30	\$ 226,479 \$ 228,950	56	50%	47	224	A 23 M 8	\$ 230,045 \$ 229,900	\$ 227,470 \$ 227,500	\$ 224,685 \$ 224,950	99% 99%	1.5
\$250,000	\$299,999	49	A 78 M 39	\$ 274,092 \$ 272,000	35	42%	47	147	A 43 M 19	\$ 277,739 \$ 275,000	\$ 273,318 \$ 269,900	\$ 268,190 \$ 265,000	98% 98%	2
\$300,000	\$349,999	40	A 92 M 49	\$ 327,398 \$ 333,915	27	40%	19	97	A 61 M 33	\$ 337,530 \$ 336,500	\$ 331,458 \$ 332,900	\$ 324,039 \$ 325,000	98% 98%	2.5
\$350,000	\$399,999	27	A 85 M 59	\$ 379,169 \$ 379,900	14	34%	24	61	A 58 M 30	\$ 386,846 \$ 387,950	\$ 379,171 \$ 379,900	\$ 371,948 \$ 370,000	98% 97%	2.7
\$400,000	\$449,999	22	A 96 M 72	\$ 422,048 \$ 422,358	8	27%	13	28	A 55 M 18	\$ 438,880 \$ 439,900	\$ 432,830 \$ 434,445	\$ 422,971 \$ 422,500	98% 97%	4.7
\$450,000	\$499,999	19	A 71 M 67	\$ 476,343 \$ 475,000	9	32%	9	20	A 77 M 38	\$ 502,859 \$ 504,376	\$ 487,364 \$ 482,500	\$ 472,052 \$ 474,000	97% 98%	5.7
\$500,000	\$549,999	13	A 85 M 90	\$ 532,365 \$ 534,900	5	26%	7	12	A 54 M 11	\$ 555,132 \$ 542,450	\$ 543,007 \$ 533,950	\$ 524,707 \$ 526,250	97% 99%	6.5
\$550,000	\$599,999	7	A 73 M 52	\$ 580,100 \$ 575,000	1	13%	11	12	A 75 M 34	\$ 603,693 \$ 587,500	\$ 593,610 \$ 587,500	\$ 571,792 \$ 572,500	96% 97%	3.5
\$600,000	\$649,999	7	A 72 M 95	\$ 621,329 \$ 617,500	0		3	4	A 111 M 120	\$ 691,475 \$ 685,950	\$ 656,200 \$ 654,950	\$ 619,500 \$ 617,500	94% 94%	10.5
\$650,000	\$699,999	5	A 130 M 86	\$ 678,390 \$ 675,000	1	17%	4	3	A 32 M 26	\$ 724,100 \$ 697,500	\$ 718,233 \$ 715,000	\$ 682,633 \$ 679,500	95% 95%	10
\$700,000	+	25	A 163 M 117	\$ 974,505 \$ 849,900	3	11%	13	10	A 197 M 197	\$ 929,770 \$ 822,450	\$ 910,769 \$ 796,950	\$ 889,305 \$ 817,500	98% 103%	15
Market Totals		322			299	48%	241	1038						1.9
Market Averages		79		\$ 367,890					35	\$ 255,579	\$ 251,841	\$ 247,518	98%	
Market Medians		48		\$ 308,250					8	\$ 227,250	\$ 224,950	\$ 222,250	99%	

Date Range (Off-Market & Sold) = 02/18/2018 to 08/19/2018

Data believed to be accurate but not guaranteed.

Status = [1] Active; [2] Pending; [3] Cancelled, Expired; [4] Sold

Area = K09, K10, K11

Property Sub Type = Single Family

Favors Sellers Favors Buyers

# BOONE COUNTY SINGLE FAMILY

Sunday, August 19, 2018

Mike Parker Team  
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 859-647-0700

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- Absorption Rate / Months of Inventory:** An estimate of how fast listings are currently selling measured in months. For example, if 100 properties sell per month and there are 800 properties for sale - there is an 8 month supply of inventory before counting the additional properties that will come on the market.

**BOONE COUNTY PLANNING COMMISSION  
BOONE COUNTY FISCAL COURTROOM  
BURLINGTON, KENTUCKY  
BUSINESS MEETING  
OCTOBER 16, 2024  
7:00 P.M.**

---

Chairman Rolfsen opened the Business Meeting at 7:02 p.m. and welcomed the audience to the Planning Commission's October 16, 2024 Business Meeting. Chairman Rolfsen then invited everyone to join him in reciting the Pledge of Allegiance.

**COMMISSION MEMBERS PRESENT:**

Mr. Randy Bessler  
Mrs. Pamela Goetting  
Ms. Corrin Gulick, Vice Chairwoman  
Mr. Steve Harper, Temporary Presiding Officer  
Mr. David Hincks  
Mrs. Janet Kegley  
Mr. Rick Lunnemann  
Mr. Eric Richardson  
Mr. Charlie Rolfsen, Chairman  
Mr. Bob Schwenke  
Mrs. Jackie Steele, Secretary/Treasurer  
Mr. Tom Szurlinski  
Mr. Steve Turner

**COMMISSION MEMBERS NOT PRESENT:**

Mrs. Kathy Clark  
Mr. Kenny Vaught

**LEGAL COUNSEL PRESENT:**

Mr. Dale Wilson

**STAFF MEMBERS PRESENT:**

Mr. Kevin P. Costello, AICP, Executive Director  
Mr. Michael Schwartz, Director, Zoning Services  
Mr. Steve Lilly, GISP, Planner  
Mr. Steve Gay, GISP, Director, GIS Services

**APPROVAL OF THE MINUTES:**

Chairman Rolfsen stated that the Commissioners received copies of the Minutes from the September 18, 2024 Business Meeting and the October 2, 2024 Public Hearings. He asked if there were any comments or corrections?

**Mr. Szurlinski moved to approve the Minutes as presented. Mr. Hincks seconded the motion and it carried unanimously.**

**BILLS:**

Mr. Kevin P. Costello, AICP, Executive Director, advised the Commissioners of the **Bills to be Paid**, which included a list of **Expenses** and **Salaries and Benefits** for a time period between September 5, 2024 and October 2, 2024.

**EXPENSES:**

Accounting Fees	942.00
Attorney Fees	4,800.00
Consultant/Professional Svcs Fees	11,460.00
Filing Fees	900.00
Legal Ads/Recruitment	469.83
Miscellaneous Expense	155.00
Office & Board Meeting Supplies	1,724.75
Office Equipment / Expense	331.39
Postage Expense	502.25
Printing/Pub/Dues/Subscriptions	100.00
Professional Development	<u>397.32</u>

**TOTAL: \$ 21,782.54**

**SALARIES AND BENEFITS:**

FICA-BCPC Portion	\$ 7,023.34
Health/Dental/Life/LTD	17,872.93
Retirement – BCPC Portion	18,726.86
Salaries – Staff Expenses	96,479.09
Salaries – BCPC & BOA	<u>1,540.00</u>

**TOTAL: \$ 141,642.22**

**GRAND TOTAL: \$ 163,424.76**

**Ms. Gulick moved that the bills (expenses and salaries and benefits) be approved and paid as presented and indicated approval of those items, which have been previously paid. Mrs. Goetting seconded the motion and it carried unanimously.**

**ACTION ON PLAN REVIEWS:**

**ZONING MAP AMENDMENT - Rick Lunnemann, Chairman, Michael Schwartz, Staff**

1. Request of **LDG Multifamily LLC (applicant) for Joyce Robinson Foundation, Inc. (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Urban Residential Two (UR-2) for an approximate 19 acre area located at 10637 Dixie Highway, Boone County, Kentucky. The request is for a zone change to allow for the development of a 312 unit multi-family residential development with on-site amenities and a Variance increasing the height of a fence along the rear property line from six (6) feet to eight (8) feet.

Staff Member, Michael Schwartz, read the Committee Report, which recommended approval based upon Findings of Facts and Conditions for the Zoning Map Amendment and Variance. The applicant/owner has signed the Condition Letter agreeing to the six conditions. The Committee vote was unanimous by the five Committee Members.

Chairman Rolfsen asked if there was anyone in the audience who wanted to speak in favor or against the request?

Mr. David Johnson, attorney for the applicant, stated that he agreed with the Committee's recommendation. He is available to answer any questions.

Seeing no further comment, **Mr. Lunnemann moved, by Resolution to the Boone County Fiscal Court, to recommend approval of the Zoning Map Amendment and Variance based upon the Committee Report and Findings of Fact and Conditions. Mr. Schwenke seconded the motion.**

Chairman Rolfsen asked the applicant how he was going to tie into the public sanitary sewer? Ms. Michelle Bollman, Viox & Viox, Inc. replied that she has been working with the Kentucky Transportation Cabinet on an easement to connect to the existing public sewer owned by SD1. That is the preferred route. Also, they are working with the adjoiner to the north. If that doesn't work out, they have spoken to SD1 to install a public pump station on the south side.

Mr. Bob Schwenke expressed a concern about lower income housing at that location with children that don't have adequate transportation. There is no public transportation by the site. What about TANK providing service? There are no sidewalks on U.S. 25. There is no lighting along Dixie Highway. He has trouble voting for the request. There are a lot of warehouses and truck traffic along Dixie Highway.

Chairman Rolfsen inquired about amenities like a playground for kids? Mr. Brett Budd from LDG Communities, stated they will have amenities such as a playground and club house/fitness center along with a resident service room. Mr. Budd replied that it is affordable housing and limited to 60% income. Most of their residents have transportation because it is considered a rural area.

Chairman Rolfsen stated that it was shocking that the School Board did not respond or comment on the proposal considering it had 3 bedroom units. Those types of units usually have more than 2.1 kids per unit in lower income housing. There was no opposition to the project and it backs up to a rest area. Mr. Johnson noted that a bus Park & Ride is located at Mt. Zion Road. Mr. Schwenke thought the project needs a sidewalk along Dixie Highway.

Chairman Rolfsen stated that he has been hearing about the widening of Dixie Highway for decades. People will walk towards Mt. Zion Road and Richwood Road and there is no sidewalk.

**Seeing no further discussion, Chairman Rolfsen asked for a vote on the original motion made by Mr. Lunnemann and seconded by Mr. Schwenke. The vote found all by Mr. Schwenke voting in favor of the request. The motion passed by a vote of 12 in favor and 1 against (Mr. Schwenke).**

**ZONING MAP AMENDMENT - Steve Harper, Chairman, Steve Lilly, Staff**

2. Request of **Sinkula Investments (applicant)** for **Mt. Zion MHC LP (owner)** for a Zoning Map Amendment from Mobile Home Park (MHP) to Commercial Four (C-4), including the following variances: (1) reducing the Buffer Yard A width from ten (10) feet to two (2) feet along Lakeside Drive; and (2) reducing the Buffer Yard A width from ten (10) feet to two (2) feet along the west property line, for an approximate 1.2 acre area located at the southwest corner of the intersection of Mt Zion Road and Lakeside Drive, including the properties at 10013, 10017, 10021, 10025, 10029, and 10033 Lakeside Drive, Boone County, Kentucky. The request is for a Zoning Map Amendment to allow the development of an eating and drinking establishment with drive-through service.

**Mrs. Kegley moved to defer the request until the December 4, 2024 Business Meeting. Mr. Lunnemann seconded the motion and it passed unanimously.**

**CHANGE OF CONCEPT DEVELOPMENT PLAN - David Hincks, Chairman, Steve Lilly, Staff**

3. Request of **Steve Berling (applicant)** for **BS NKY Properties (owner)** for a Change of Concept Development Plan in a Commercial Services (C-3) district, for an approximate 2.06 acre area located at 3005 Verona-Mudlick Road, Boone County, Kentucky. The request is for: (1) a Change of Concept Development Plan to allow for an expanded outdoor storage area for the existing farm equipment business; and (2) Variances reducing the buffer yard width along the rear property line from sixty (60) feet, with a thirty (30) foot option, to ten (10) feet, reducing the buffer yard width along the side property line from sixty (60) feet, with a thirty (30) foot option, to twenty (20) feet, and reducing the buffer yard width along the front property line from ten (10) feet to three (3) feet.

Staff Member, Steve Lilly, read the Committee Report, which recommended approval based upon Findings of Facts and Conditions for the Change in Concept Development Plan and Variances. The owner has signed the Condition Letter. Mr. Lilly reminded the Board that the Planning Commission takes final action on the Variances.

Chairman Rolfsen asked if there was anyone in the audience who wanted to speak in favor or against the request?

Mr. Steve Berling, applicant, stated that he was present to answer any questions.

Seeing no further comment, **Mr. Hincks moved, by Resolution to the Boone County Fiscal Court, to recommend approval of the Change in Concept Development Plan and Variances based upon the Committee Report and Findings of Fact and Conditions. He noted that the Committee addressed the neighbor's concern about an adequate buffer. Mrs. Kegley seconded the motion and it passed unanimously.**

**ZONING MAP AMENDMENT - Corrin Gulick, Chairwoman, Michael Schwartz, Staff**

4. Request of **Charter Commercial, LLC (applicant) for Delaware Crossing, LLC (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

Staff Member, Michael Schwartz, read the Committee Report, which recommended denial based upon Findings of Facts of the Zoning Map Amendment.

Chairman Rolfsen asked if there was anyone in the audience who wanted to speak in favor or against the request?

Mr. Pat Manger, applicant, referred to his PowerPoint presentation He showed color renderings of two types of products they planned to build on the site – patio homes (two bedroom ranch type) and a multi-family development. The overall plan is to connect sidewalks to local parks. The Map Amendment complies with the overall Goals and Objectives (Items #1,3,4,5,6 and 7) as well as Goal A of the Demographic Element. It is also consistent with the Environmental Section, Natural and Cultural Resources Element, Economy, Public Facilities and Transportation. The major changes in the area are the Airhub, DHL and Mazak. In regard to the social side, there is the recent NKADD Housing Study and discussion about a housing shortage. The traffic study has been completed. The conclusion was that there could be a sight impact on the existing road network. No improvements are required at the intersection of Cannondale and Camp Ernst Road. The connection to Lariat Way, etc. were previously approved as well as with Holbrook and Trevino. It is nothing new. It can happen today.

Mr. Jeff Schafer, 6501 Cannondale Drive, stated that concerns over this development are numerous. It doesn't meet the criteria in evaluating the Zoning Change request to putting 300 rental units with an established single family community. They applicant has completely disregarded the residents. There will be 2,100 trips daily plus delivery trucks, etc. and will cause major safety concerns. Instead of 25 single family homes, the applicant wanted to put in 157 one bedroom apartments. There is no traffic light at Camp Ernst Road. There can't be one because it will result in a useless roundabout. What about the additional strain on schools? Baby Boomers are not looking at one bedroom apartments. The project would result in 700 people in an area that is approved only for SR-1. The residential streets were built for only 200 people. What is the environmental impact of the increased density? The proposed project will impact their property values. In March, 2017, he paid a lot premium to Brookstone. If he would have known about future apartments, he would not have purchased his lot. He would not be here tonight if it wasn't for the neglect of the community by the developer. The community should have been finished. For the past 4 years, they has been no salesman or model home even though other homes were

being built in the area. There has not been a finished street or sidewalks in the past 7 years. There has not been an HOA meeting in the last 3 years. There is a pattern of mismanagement - Brookstone Homes, Madeira Place, LLC, Delaware Crossing, LLC and Charter Companies. In 2022, there was a conveyance plat done and it is stamped that the southern 24 acres of this lot in and of itself is non-buildable. Is that true? Mr. Schwartz replied yes, unless it is attached to another parcel that has road frontage or a public street is built on the property.

Seeing no further comment, **Ms. Gulick moved, by Resolution to the Boone County Fiscal Court, to recommend denial of the Zoning Map Amendment based upon the Committee Report and Findings of Fact. Mrs. Kegley seconded the motion.**

Ms. Gulick stated that she voted no on the request because she has a serious concern about the connection to KY 237. If this was approved previously and she voted for it, she was wrong because the proposal takes streets that were local and makes them operate as a collector. That can create driver expectations and safety concerns. The Traffic Impact Study does take into consideration traffic that uses the route as a cut through. Additional traffic from KY 237 and Oakbrook will use these streets in the subdivision. Only 3 years out, there is a significant decrease in the level of service turning left. It is already a "D" level service. A driver will accept a small gap to turn into traffic. It creates a safety concern. A traffic signal will cause more problems for the roundabout traffic. Because of this, she will vote no.

Chairman Rolfsen stated that the only thing that has changed in Boone County is the phase "affordable housing." What does it mean? Developers continue to use it to save Boone County. Why aren't Campbell and Kenton counties doing anything about it?

**Chairman Rolfsen asked for a vote on the original motion made by Ms. Gulick to deny the request and seconded by Mrs. Kegley. The motion to deny the request passed unanimously.**

**ZONING TEXT AMENDMENTS - Rick Lunnemann, Chairman, Michael Schwartz, Staff**

5. Request of the **Boone County Planning Commission Technical/Design Review Committee** to consider a Zoning Text Amendment modifying the definitions of "Electric Vehicle Charging Station, Public" and "Gasoline Filling Station".

Staff Member, Michael Schwartz, read the Committee Report, which recommended approval based upon Findings of Facts for the Zoning Text Amendment.

Seeing no further comment, **Mrs. Goetting moved, by Resolution to the Boone County Fiscal Court, City of Florence, City of Union and City of Walton to recommend approval of the Zoning Text Amendment based upon the Committee Report and Findings of Fact. Mr. Turner seconded the motion and it passed unanimously.**

**TECHNICAL/DESIGN REVIEW – Rick Lunnemann, Chairman, Steve Lilly, Staff**

6. Microtel Inn & Suites – 7490 Woodspoint Drive

Mr. Steve Lilly presented the request. The applicant wants to repaint the building. He showed what the building looks like now and with the paint work completed. The Committee met prior to

the Business Meeting and recommended approval with one condition – the yellow stripe will be removed. It is LED lighting that is related to signage.

**Mr. Lunnemann moved to approve the request with one condition. Mr. Hincks seconded the motion and it passed unanimously.**

**G. EXECUTIVE DIRECTOR'S REPORT:**

Mr. Kevin P. Costello, AICP, explained that it was time to renew the subscription for aerial photography. He referred to his September 30, 2024 memo to the Board. The total cost for 2 flights, etc., is \$32,591 for FY 2024-2025. This amount is in the current Planning Commission budget and a map shows the flight area. Mr. Steve Gay explained the coverage area. **Mrs. Steele moved to authorize Mr. Costello to sign a contract with NearMap not to exceed \$32,591 according to the terms outlined in the memo. Mrs. Goetting seconded the motion and it passed unanimously.**

**H. COMMITTEE REPORTS:**

- Airport (Randy Bessler)  
No Report
- Administrative/Personnel (Tom Szurlinski)  
No Report
- Enforcement (Tom Szurlinski)  
No Report
- Long-Range Planning/Comprehensive Plan (Bob Schwenke)  
No Report
- Technical/Design Review (Rick Lunnemann)  
No Report
- Executive (Charlie Rolfsen)  
No Report

**I. CHAIRMAN'S REPORT: (Charlie Rolfsen)**

Chairman Rolfsen announced the birth of this third grandchild.

**J. OKI REPORT: (Randy Bessler)  
No Report**

**K. ADJOURNMENT:**

There being no further business to come before the Planning Commission, **Mr. Bessler moved to adjourn the meeting. Mr. Harper seconded the motion and it passed unanimously. The meeting was adjourned at 8:00 P.M.**

**APPROVED**



**Charlie Rolfsen**  
**Chairman**

**Attest:**



**Kevin P. Costello, AICP**  
**Executive Director**

**EXHIBIT**

**“B”**

**ZONE CHANGE/CONCEPT PLAN COMMITTEE REPORT**

**TO:** Boone County Planning Commission

**FROM:** Corrin Gulick, Chair

**DATE:** October 16, 2024

**RE:** Request of **Charter Commercial, LLC (applicant)** for **Delaware Crossing, LLC (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

**REMARKS:**

1. We, the Committee Members were present at the Committee Meeting and voted on the above request or else were absent from voting. Further, based upon the vote, the Committee directs the Staff to draft the finding of fact to complete the Committee Report.
2. We, the Committee, recommend disapproval of the above referenced Zoning Map Amendment based on the following findings of fact:

**FINDINGS OF FACT:**

1. The proposed Zoning Map Amendment is not consistent with the 2040 Future Land Use Map of the comprehensive plan, Our Boone County Plan 2040.

The northern portion of the site is identified for Rural Density Residential, which is described as low density residential uses of up to one dwelling unit per acre. The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, would allow residential uses at a density of approximately 6 units per acre.

The southern portion of the site is identified for Suburban Density Residential, which is described as single-family housing of up to four units per acre. The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, would allow multi-family residential uses at a density of approximately 6 units per acre.

2. The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, is not consistent with the following Goals and Objectives contained in the comprehensive plan, Our Boone County Plan 2040:

- a. Land uses and zoning decisions shall strive to balance the rights of landowners with the rights of neighbors and the community (Overall Goal A, Objective 4).

Areas surrounding the site in question have been developed with detached single-family residential dwellings. The proposed development would introduce

attached single-family and multi-family residential dwelling units into the area, contrary to the established development pattern.

- b. Residential developments shall be evaluated for impact on infrastructure, environment, health and safety considerations, buffering, and design with secondary consideration given to dwelling unit type and density (Demographics Goal B, Objective 4).

Insufficient information has been submitted to determine what impact the proposed development will have on the existing street system and the 100-year floodplain of Gunpowder Creek.

- c. Mixed use, higher density neighborhoods shall be encouraged near existing or proposed transportation nodes and shall be designed to establish neighborhoods with a mix of retail, public, and recreational uses (Demographics Goal B, Objective 6).

The proposed development is not located within a reasonable walking distance of nearby commercial activity, which is located along Burlington Pike and US 42.

- d. Residential developments shall be encouraged to plan and build in a manner compatible with the general housing density and design planned for the area, with the existing conditions of the site, and significant site features (Demographics Goal B, Objective 6).

The proposed development will provide for a housing type and density which is not found within the vicinity of the site in question.

- e. Priority shall be given towards maintaining, protecting, and improving the capacity and safety of the existing road system across jurisdictions (Transportation Goal A, Objective 1).

Holbrook Lane, Watson Lane, and Lariat Way are local streets. The proposed development will generate additional traffic that the existing street system was not designed and built to accommodate.

- f. New land development shall be closely coordinated with the local transportation system to assure efficiency and continuity within Boone County and with the regional multi-modal transportation system (Transportation Goal B, Objective 9).

Insufficient information has been submitted to determine if the proposed development will have a negative impact on the capacity of the existing street system.

3. The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, is not consistent with the following Planned Development criteria, as outlined in Section 1506 of the zoning regulations:

- a. In general, Planned Developments shall have a pedestrian orientation, where it is possible to live, work, shop, and play in the same immediate vicinity without a required dependence on the automobile. This may be accomplished through the use of comprehensive pedestrian circulation networks including multipurpose paths and walks along main routes and open spaces such as stream corridors, between major destinations within the development and adjoining areas, secondary walk connections to the multi-purpose paths, the creation of a designed pedestrian environment including street trees in addition to other required landscaping, decorative street lights and other street furniture, and seating areas, and the use of integral curb walks where appropriate along streets. Disruptions in major paths due to street and drive intersections shall be minimal. Additionally in commercial areas, pedestrian orientation can be accomplished by placing buildings in close proximity to the street with parking areas to the side and rear of buildings, mixing uses within the same multi-story buildings, building entrances directly facing streets with reduced setbacks, architectural design which employs display windows, projecting signs, and awnings at street level, and designed outdoor seating and gathering spaces at the street level. Each development proposal must demonstrate in detail how the project will be made walkable throughout.

Except for sidewalks along the internal street system and a trail from Lariat Way to Gunpowder Creek, the proposed development does not provide for any innovative pedestrian movement systems.

- b. **Compatibility of Uses:** Measures shall be taken to assure compatibility of land uses within a Planned Development itself and adjacent sites. Such measures may include the provisions of buffer zones, common open space areas and landscape features, transitional land uses, or a mixed-use development in which no specific type of land use is dominate. When applicable, the design methods recommended in the "Development Layout, Lot Sizes, and Setbacks" section of the Comprehensive Plan's Land Use Element shall be employed. Compatibility measures/mitigation measures shall exceed the usual minimum standards of these regulations when needed to address impacts of the proposed development.

Shaker Run Subdivision has been developed at an intensity of 4.55 units per acre. The currently platted area of Valley Creek Farms Subdivision has been developed at an intensity of 3.85 units per acre. The proposed development, at a maximum intensity of 6.00 units per acre will be incompatible with the development of the adjacent neighborhoods.

- c. **Landscaping:** Substantial landscaping shall be provided in a Planned Development with emphasis given to street scape areas, buffer zones, and the provision of significant landscaping (in terms of size of landscape areas, and quantity and quality of landscape materials) within the developed portions of the site. The use of landscape design guidelines is required for multi-phased projects. The retention of existing healthy, substantial trees should occur wherever possible. Properly designed street tree plantings may be permitted to fulfill some landscaping requirements as part of an overall amenity package.

Insufficient information has been submitted to determine conformity with this criteria item.

- d. **Transportation Connections and Entry Points:** The provision of transportation connections (street connections, pedestrian paths, multi-purpose trails, sidewalks, and bicycle facilities) shall be provided in all planned development unless physically unfeasible or unsafe. This shall include connections to adjoining properties and developments, and inter-connectivity within the development itself, and contain minimal use of cul-de-sacs or other dead-end types of streets only when necessary. Transportation connections shall account for the County's adopted Transportation Plan and any adopted bikeway and/or pedestrian plans. In addition, the various entry points (streets, paths, etc.) into a Planned Development shall be marked or otherwise defined through the use of landscaping, low-key signage on retaining walls, architectural or sculptural elements, archways, markers, etc. Any structures used to demarcate entry points shall visually correlate to the planned architectural theme by the use of consistent design details, materials, and colors.

Holbrook Lane, if constructed between Oakbrook Road and Camp Ernst Road, would act as a collector street. However, Holbrook Lane does not extend to Camp Ernst Road. Additionally, there is no timeline to construct the extension. Therefore, as proposed, the development will have to use the existing local street system for access. Until Holbrook Lane is extended to Camp Ernst Road, development of the site in question for a higher density than what is recommended by the comprehensive plan is premature.

4. The existing SR-1 zoning of the site in question is appropriate and the proposed SR-1/PD zoning for the site is inappropriate.

The existing SR-1 district, which permits detached single-family residential dwellings at a maximum intensity of four (4) units per acre, is consistent with the 2040 Future Land Use Map of the comprehensive plan, Our Boone County Plan 2040.

The existing SR-1 district provides for a land use and intensity which is compatible with the developed areas adjacent to the site in question.

The proposed SR-1/PD district, along with the submitted Concept Development Plan, would allow the development of attached single-family and multi-family residential dwellings at a maximum intensity of six (6) units per acre, representing an increase of fifty (50) percent over the recommended intensity as outlined in the comprehensive plan.

Lariat Way and Cannondale Drive have been designed and built as local streets, which are defined as a type of street to serve less than one hundred (100) residential lots or units. The proposed SR-1/PD district, along with the submitted Concept Development Plan, would create a situation where these streets would serve more than one hundred (100) residential units.

5. On April 27, 2018, a Preliminary Plat was approved for the development of the Valley Creek Farms subdivision showing the development of 181 detached single-family residential lots. To date, seventy-eight (78) of those lots have been platted. The remaining one hundred three (103) lots are encompassed within the site in question.

It seems reasonable to assume that the owners of the existing lots, and those that are considering purchase of the existing lots, would have done their due diligence and found that the area to the north and west of the existing development was going to be an extension of the subdivision.

The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, significantly changes the character of what those existing/potential homeowners expected.

6. The Committee concluded that there have been no major changes of an economic, physical, or social nature within the area involved which were not anticipated in the adopted comprehensive plan and which have substantially altered the basic character of such area.

A copy of the Public Hearing minutes accompanies the findings and recommendation serving as a summary of the evidence and testimony presented by the proponents and opponents of this request. Attached is the signature page for the Zone Change/Concept Development Plan Committee Vote.

**ZONE CHANGE/CONCEPT PLAN COMMITTEE VOTE**

TO: Boone County Planning Commission

FROM: Corrin Gulick, Chairwoman

DATE: September 18, 2024

**REMARKS:**

We, the Committee Members were present at the Committee Meeting and voted on the above request or else were absent from voting. Further, based upon the vote, the Committee directs the Staff to draft the findings of fact and conditions if deemed necessary in order to complete the Committee Report.

**ZONING MAP AMENDMENT, Corrin Gulick, Chairwoman, Michael Schwartz, Staff**

4. Request of **Charter Commercial, LLC (applicant)** for **Delaware Crossing, LLC (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

~~LDC MULTIFAMILY/ROBINSON FOUNDATION~~  
SEPTEMBER 18, 2024 ~~CHAPTER / DELAWARE~~  
CROSSING

*Janet Kegley*  
**Janet Kegley**  
 For Project \_\_\_ Absent \_\_\_  
 Against Project  \_\_\_  
 Abstain \_\_\_ Deferred \_\_\_

*Corrin Gulick*  
**Corrin Gulick**  
 For Project \_\_\_ Absent \_\_\_  
 Against Project  \_\_\_  
 Abstain \_\_\_ Deferred \_\_\_

*Rick Lunnemann*  
**Rick Lunnemann (Chairman)**  
 For Project \_\_\_ Absent \_\_\_  
 Against Project  \_\_\_  
 Abstain \_\_\_ Deferred \_\_\_

\_\_\_\_\_  
**Kathy Clark (Alternate)**  
 For Project \_\_\_ Absent \_\_\_  
 Against Project \_\_\_  
 Abstain \_\_\_ Deferred \_\_\_

*Steve Harper*  
**Steve Harper**  
 For Project \_\_\_ Absent \_\_\_  
 Against Project  \_\_\_  
 Abstain \_\_\_ Deferred \_\_\_

\_\_\_\_\_  
**Steve Turner (Alternate)**  
 For Project \_\_\_ Absent \_\_\_  
 Against Project \_\_\_  
 Abstain \_\_\_ Deferred \_\_\_

*David Hincks*  
**David Hincks**  
 For Project \_\_\_ Absent \_\_\_  
 Against Project  \_\_\_  
 Abstain \_\_\_ Deferred \_\_\_

\_\_\_\_\_  
**Jackie Steele (Alternate)**  
 For Project \_\_\_ Absent \_\_\_  
 Against Project \_\_\_  
 Abstain \_\_\_

**TOTAL:**    \_\_\_ - DEFERRED    \_\_\_ - FOR PROJECT    \_\_\_ - ABSENT  
                   5 AGAINST PROJECT    \_\_\_ - ABSTAIN

**COMMISSION MEMBERS PRESENT:**

Mr. Randy Bessler  
Mrs. Kathy Clark  
Mrs. Pamela Goetting  
Ms. Corrin Gulick, Vice Chairwoman  
Mr. Steve Harper, Temporary Presiding Officer  
Mr. Rick Lunnemann  
Mr. Charlie Rolfsen, Chairman  
Mr. Bob Schwenke  
Mr. Kenny Vaught

**COMMISSION MEMBERS NOT PRESENT:**

Mr. David Hincks  
Mrs. Janet Kegley  
Mr. Eric Richardson  
Mrs. Jackie Steele, Secretary/Treasurer  
Mr. Tom Szurlinski  
Mr. Steve Turner

**LEGAL COUNSEL PRESENT:**

Mr. Dale Wilson

**STAFF MEMBERS PRESENT:**

Mr. Kevin P. Costello, AICP, Executive Director  
Mr. Michael D. Schwartz, Director, Zoning Services

Chairman Rolfsen introduced the first item on the Agenda at 7:36 p.m.

**ZONING MAP AMENDMENT, Michael Schwartz, Staff**

1. Request of **Charter Commercial, LLC (applicant)** for **Delaware Crossing, LLC (owner)** for a Zoning Map Amendment from Suburban Residential One (SR-1) to Suburban Residential One/Planned Development (SR-1/PD) for an approximate 44 acre area located at 1841 Holbrook Lane and at the terminus of Lariat Way, including the property having a Parcel Identification Number (PIDN) of 050.00-00-001.05, Boone County, Kentucky. The request is for a zone change to allow for the development of 139 attached single-family residential dwellings and 157 multi-family residential dwellings.

Staff member, Michael Schwartz, referred to his PowerPoint presentation (see Staff Report). The request is to rezone the 44 acre site from SR-1 to SR-1/PD. The site has 2 distinct portions. The northern portion contains 19.97 acres. The southern portion contains 24.4 acres. Access to the site is from Holbrook Lane, Watson Lane and Lariat Way. Valley Creek Farms dates back to April, 2018 as the Planning Commission approved a Preliminary Plat. It had 97 single-family detached lots. The northern portion of the site is currently occupied by a single-story single-family residential dwelling with 3 accessory barn structures. The southern portion of the site is currently vacant and is heavily wooded. Gunpowder Creek is located along the eastern edge of the site. The property surrounding the site is zoned SR-1. Oakbrook Subdivision is zoned SR-1/PD. Pages 1-5 of the Staff Report identify the applicable regulations to the request. Topographically, a valley bisects the site with slopes having a grade change of 70 feet. The eastern edge of the site is located within the 100 year floodplain of Gunpowder Creek. The 2040 Future Land Use Map designates the site for Rural Density Residential (RD) and Suburban Density Residential (SD). Due to the hillsides, a portion of the site is designated as Developmentally Sensitive (DS). Holbrook Lane is a County maintained local street with no sidewalks. Watson Lane is a County maintained local street with sidewalks on both sides of the street. Lariat Way is also a County maintained local street with sidewalks being installed with lot development. Pages 6-11 include the portions of the Comprehensive Plan that are relevant. The western portion of the street is located within a 600 foot wide buffer of an underground pipeline. Mr. Schwartz showed photographs of the site and adjoining properties.

The submitted Concept Development Plan is divided into 2 parts. The northern part is planned to have 139 attached single-family residential dwellings in 24 single-story buildings. The southern portion will have 157 multi-family residential dwellings in 24, 3 story buildings and 13, 2 story garages. There will be a dwelling unit on top of each garage. Public streets will be built in the northern portion of the development. Street connections will be made via Watson Lane, Holbrook Lane and Lariat Way. A private drive will serve the multi-family area in the south portion of the site. There will be stormwater retention areas, amenity space and sidewalks. There will be a walking trail from Lariat Way to Gunpowder Creek. Public utilities will be provided. The developer has provided some building elevations and floor plans. Mr. Schwartz noted that he offered a summary of building intensity of the project as well as neighboring developments on Page 11 of the Staff Report. The applicant has provided a project narrative, a traffic assessment (peak hour traffic generation and trip distribution but no intersection analyses or capacity analysis). Staff has reviewed the submitted plans against the zoning and subdivision regulations. Those findings can be found on Pages 12-14 of the Staff Report. Most notably, the maximum intensity of an SR-1/PD district is 6 units per acre and the applicant is proposing 6.67 units per acre. Comments from outside agencies can be found on Pages 14-15 of the Staff Report. Staff concerns about the project are listed on Pages 15-16 of the Staff Report. It includes a list of requirements that are being requested to be waived or modified using the PD regulations. Staff has found that there is a discrepancy between the number of units listed in the project narrative and the number of units shown on the submitted plans. Staff feels that there needs to be more amenities given the size of the development. Staff also feels that there should be a discussion of the extension of Holbrook Lane to Camp Ernst Road. The Plan only shows the applicant extending it just past the

applicant's entrance. The applicant has also requested some off-site signage for the proposed development. Staff has offered some suggestions.

Chairman Rolfsen asked if the applicant was present and wanted to proceed with their presentation?

Mr. Pat Manger, Loveland, Ohio, represents the applicant and stated that the northern part of the site will be single-story patio homes (2 bedrooms). The southern parcel will include 1 bedroom multi-story townhome buildings (2 and 3 stories high). He offered to comply with Concerns #2, #3, #4, #5 and #6. He stated that he won't exceed the 12% grade for streets. In terms of amenities, Mr. Manger noted that he is working with the County to connect their trail to Oakbrook Park. They are attempting to preserve as much green space as possible. He is working with the County to connect Oakbrook Road to KY 237 via Holbrook Lane. They would be responsible for their portion of Holbrook Lane as it affects their property. He is talking with the Valley Creek Farms HOA about the proposed sign and is in the process of working it out with the HOA. They are agreeable to extend Holbrook Lane to their western property line. They are in agreement with sidewalks. They are willing to comply with Item #5 and connect the southern area to the walking trail. In conclusion, they want to build one and two bedroom units in order to meet the demand as stated in the NKADD Study. There is a housing shortage. There is a need for 6,000 homes (2,000 units for rental) in the next 5 years.

Chairman Rolfsen asked if anyone in the audience would like to speak in favor or against the request or had any questions?

Mr. Michael Koseruba, 1959 Cliffview Lane, stated that no one from his neighborhood was notified. The proposal doesn't add up. There is a bonus in which 266 units becomes 296 units because of the PD. Don't be fooled. It is an apartment unit bomb in the middle of single-family residences and semi-rural residences. The roads, parks and schools can't take it. We are looking at a piece of property that doesn't have a road to it. There is no traffic coming out of a vacant piece of land. What about the traffic elsewhere in the area? Traffic will come from the west and the south. We are opposed to the project as we prefer single-family housing. Don't let the developer touch Gunpowder Creek. Oakbrook Park is not large enough for the project.

Mr. Charlie Schneckenberger, 6516 Cannondale Lane, read his comments for the record (see Exhibit A). He stated that the zone change request should be denied. The applicant is requesting a 67% increase above the SR-1 zone (4 units per acre). The applicant didn't even follow the requirements under the PD. Over 60% of the land in the subdivision is being rezoned including the open space. If approved, the current subdivision will be reduced to 18 acres. The 89 residential lots will have a density of 5 units per acre. The zone change will adversely affect his property density and will violate his SR-1 zoning.

Mr. Schwartz showed the current and proposed density of the subdivision and project using a map for illustrative purposes.

Mr. Mike Parker, realtor, stated that the NKADD study was wrong. There is not a sufficient amount of housing inventory in Northern Kentucky. They are 60% short. There is no need for one bedroom apartments. The project will impact the schools and roads. It will change the character of the area. It will affect the quality of life. There is a lack of community support. The Board's ultimate decision on the project will tell Boone County residents whether you believe in single-family and home ownership or whether you believe in putting as many units as possible. If this change occurs, there will be other developers that will come forward to change single-family to multi-family. In this week of 2018, there were 322 active homes for sale for an average of \$247,000. In a normal market, it is 700 homes in Boone County. In 2021, there were 82 homes active with an average sales price of \$323,000. This week there are 181 active homes with an average sales price of \$393,000. Don't change the SR-1.

Chairman Rolfsen asked the audience how many people think traffic is the issue? Over 100 residents raised their hand.

Ms. Stacy Blevins, 2968 Lariat Way, stated that her house is in front of the zone change sign. She just built a half million dollar home. She thought it would be a quiet neighborhood and not full of apartments. She doesn't feel good about having her kids play in the area. Her street is a hill. It will be the main access for the new residents. It won't be safe to cross the street with 200 more residents. If the project goes, they will sell their home.

Mr. Jeff Schaefer, 6501 Cannondale Drive, stated that he was the first person to build in Valley Creek Farms Subdivision. He signed a contract with Brookstone Homes. The builder has neglected the neighborhood for the last 4 years. The lots are in terrible shape. The reason the subdivision has not sold out in the last 7.5 years is due to Brookstone Homes. The apartments will trash their home values. There is only one way in and out. It is through Lariat Way. There are no stop signs in the subdivision. They don't have complete sidewalks after 7.5 years. Drees and Fischer Home Subdivisions have them. If the developer would have sold the lots, they would not need the apartments. There was no model home or salesperson. Brookstone has gotten out of the single-family development. Maronda Homes has bought the remaining lots. The applicant's traffic study showed 2,000 trips per day in and out of the subdivision and no stop signs. The number of units proposed will put stress on the schools. They are overwhelmed both in the school and on the buses. The original subdivision only had 250 people. With the proposed change, it will increase to 734 people. The infrastructure in place now only accommodates 250 people. There is only one way in and out of the subdivision for emergency response. Are there plenty of apartment complexes planned for the area? Chairman Rolfsen replied yes. There was an approved project north of the site on KY 237 for 324 apartments. Why put apartments in the middle of single-family? Just build the 324 units already approved. Chairman Rolfsen stated that project may not be built if the traffic signal is not built. Mr. Schaefer stated that he bought his house because he thought the subdivision was only for single-family homes.

Mr. Bob Lussmyer, 6520 Cannondale Drive, stated it is tough to turn left from Cannondale Drive between 7:00 and 9:00 a.m. and 4:00 and 6:00 p.m. There is too much traffic to make a left turn without a signal. It is dangerous. The grade for Lariat Way is very steep. The traffic study is extremely misplaced. There are a lot of people coming up Camp Ernst and Pleasant Valley. Their subdivision will be a great short cut. In addition, there is a feeder creek that runs to Gunpowder Creek. Will it be impacted? It is supposed to be a green area.

Mr. Dennis Stock, 6505 Cannondale Drive, stated that the roads in the subdivision don't have the final pavement. They still have the construction grade. They have been that way for 8 years. Mr. Costello explained the bond process for street completion. It is up to the County to cash in the bond to complete the paving work if the developer can't. If a traffic signal is installed on KY 237 and Cannondale Drive, it may spoil the flow of traffic at the roundabout. The current zoning of SR-1 is still appropriate. The apartments are inappropriate. The single-family homes are okay for the area. They just have to be marketed and built. The economic value is still valid. Maronda Homes will do a better job marketing.

Ms. Tangerine Pike, stated that she owns the property on the other side of Holbrook Lane next to the subject site. She explained that her property rights will be significantly harmed because of the apartments (rental property). It is a revolving door of residents. They don't care.

Ms. Danielly Bruseth, 6589 Watson Lane, stated that her concern is privacy. There is also a blind curve.

Chairman Rolfsen inquired about connectivity. Mr. Schwartz replied that there was a stub at Watson Lane and Trevino Lane.

Mr. Shawn Gross, 6505 Watson Lane, stated that he moved from Price Hill. He moved because of density and transients. All the streets in Shaker Run Subdivision that connect to Holbrook Lane are at a decline. He is concerned about speed from additional traffic using Watson Lane to get to Holbrook Lane. There are a lot of kids in the neighborhood. Parking is on one side of the street.

Ms. Susan Schwendemann, 1911 Cliffview Lane, stated that the building design looks cheap and like tenement housing. Who needs one bedroom housing?

Mr. Russell Alexander, 1629 Hogan Lane, stated that he has been in the pest control business for 25 years. One bedroom means 5 people and drug dealers. It is hard to stop on Holbrook Lane due to its slope. He is concerned about safety and property values.

Mr. Jeff Rice, 1805 Promontory Drive, asked how much will the apartments be rented for each month? We need rent control based on income. That is why people are bundling up in units. It should be affordable. What about SD1? Can sanitary sewer service be provided? Chairman Rolfsen asked if the Staff received a letter from SD1? Mr. Schwartz replied no. He asked if the County had a plan for future development?

Mr. Costello replied that there is a plan in place currently. The plan is being updated. He explained the process and the areas for growth and preservation. He advised Mr. Rice and others to participate in the process and visit the Planning Commission's website for more information. Mr. Rice inquired about the new stormwater from the project and its affect on Gunpowder Creek. Chairman Rolfsen explained the new requirements of stormwater management and requirements for development. Water has to be retained on-site for a period of time and then released slowly and naturally from the site.

Mr. Brendon Aragon, 6500 Cannondale Drive, stated that he spoke to SD1 officials and they offered some of the site had erosion control measures in place and some have not been put in place because it hasn't been reviewed yet. Chairman Rolfsen explained that it would be the next step unless the project is not approved.

Mr. Koseruba asked why can't the Board or Staff not accept the application if it is a bad project or plan? Mr. Dale Wilson explained that KRS Chapter 100 allows any property owner to apply for a zone change for their own property. That individual has a right to apply. The denial of that right is a denial of due process.

Ms. Julie Haley, Cliffs section of Oakbrook stated that Oakbrook Road needs repaving. The County should take care of existing roads first before building new ones. The patio homes look the same. It will deeply lower the value of their homes.

Ms. Veronica Huffman, 6647 Sarazen Court, stated that she bought her home 19 years ago. The school buses are overcrowded and late. People run the stop signs in her subdivision. Would you want this type of development in your backyard? Stephens Elementary School is overpopulated. There is no lighting in Oakbrook Subdivision.

Ms. Judy Forbes, 6604 Trapside, asked if the Board thought about the American dream or owning a home. Rental is not the American dream. We need more homes in the County.

Ms. Cassie Ames, 6509 Cannondale Drive, explained the double bus run procedure. She asked who makes sure the stormwater is directed to the pond because the developer doesn't have a good track record with water. Lighting is an issue on Lariat Way. It should be added. Chairman Rolfsen explained that the Planning Commission reviews the stormwater plans to make sure it is designed properly and built.

Mr. T.J. Roberts, 8062 East Bend Road, stated that Boone County is ripe for home ownership. America is losing homeowners. The Board must keep the single-family home alive as well as home ownership.

Mr. Costello noted that in the early 2000s, Boone County was generating about 1,500 single-family homes per year. In the last five years that number has been reduced to 500 homes per year. Part of the problem is the fact that we only have 3 main home builders. There needs to be

more builders. The small homebuilders left the market in 2008 with the recession. No one is building starter homes. There are over 4,000 lots throughout the County where homes can be built. It makes sense to have a good supply of homes at various income levels. The Planning Commission has a strong history of supporting single-family residential development.

Mr. Steve Rawlings, State Representative, District #66, noted that he lives in Burlington. When he was campaigning in Florence, the biggest issue was in rental homes. They are owned by corporations that are buying up these properties and renting them out. Please maintain the integrity of Boone County and he asked the Board to respectfully deny the rule change and keep the zoning the way it is now.

Mr. Michael Ames, 6509 Cannondale Drive, stated that he informed the neighborhood about tonight's Public Hearing. The builder for the proposed project is the same builder of the subdivision. For 7 long years, he only built out half of it. He mismanaged it. Why would you let him continue? Is Fischer or Drees having problems selling homes? The builder should have a decent product.

Chairman Rolfsen asked if the Commissioners had any questions or comments?

Mr. Schwenke asked the Staff to give the Committee more history about the density of both projects. He expressed a concern about factoring in the open space permanently. He also had a concern about the grade of the streets for fire trucks, etc. Can the grades be reduced?

Ms. Gulick had a concern about the upkeep of the existing development. What is going on with completing the work? She asked Staff to research it.

Mr. Rolfsen asked if it was the same builder or did he sell it? Mr. Schwartz doesn't know for sure.

Mrs. Goetting asked about the proposed 139 attached single-family dwellings. Are those to be purchased or rented?

Mr. Manger responded that all 296 units are rentals.

Mr. Costello stated there were questions about the owner of the original subdivision? Is that person or company still around or is there a new entity? Is the previous builder associated with the new entity?

Mr. Manger replied that he was unfamiliar with it. He is only present to answer the questions about the zone change. He is not prepared to provide that information tonight. He said he would later.

Mr. Costello explained that the Board wants to know how the proposal fits in with the existing development and what was planned and who is doing the work? Is it the same company?

Chairman Rolfsen asked if it is affordable housing in terms of monthly rents? Mr. Manger responded that they don't have that information available at this time. There is a lot that goes into those figures – cost of on-site and off-site improvements. This is the first part of the process. Additional information is needed like the traffic study.

Dr. Clark asked if the applicant had additional information of how the project impacts the schools? Any numbers? Mr. Manger explained that the demographic they are targeting are baby boomers and empty nesters. Dr. Clark stated that those people could have kids or grandkids.

Mr. Lunnemann asked for more information of what the multi-family will look like in terms of architecture and materials? Due to the proposed number of units, what about amenities? A pool, playground, pickleball courts, etc.? Where will it be located?

Ms. Gulick stated that the submitted traffic study does tell her a lot. She asked for ADT information on the existing roadways and whether the developer will have a more detailed traffic study?

Chairman Rolfsen asked if the developer will have this information in time for the September 4<sup>th</sup> Committee Meeting? Mr. Manger replied that he would have to confirm and verify.

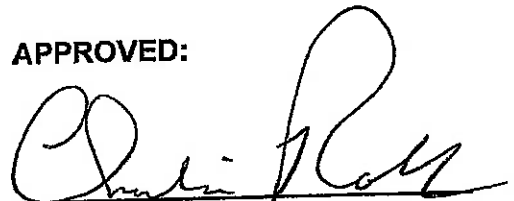
Mr. Vaught asked for more information about the impact of the project on Gunpowder Creek?

Mrs. Goetting asked if someone wanted to build more single-family homes, is the property already zoned for it? Mr. Schwartz replied yes as 97 homes could be built on the remaining lots. Brookstone Builders committed to build the lots from the beginning. Mr. Schwartz will verify that information.

Chairman Rolfsen explained the next steps of the process with the Committee Meeting and action by the full Board.

**Seeing no further questions or comments, Chairman Rolfsen announced that the Committee Meeting for this item will be on September 4, 2024 at 5:00 P.M. This item will be on the Agenda for the Business Meeting on September 18, 2024 at 7:00 p.m. in the Fiscal Courtroom. Chairman Rolfsen closed the Public Hearing at 9:35 p.m.**

**APPROVED:**



**Charlie Rolfsen**  
**Chairman**

**Attest:**



**Kevin P. Costello, AICP**  
**Executive Director**

Exhibit A – Packet from Charlie Schneckenberger  
Exhibit B – Packet from Mike Parker

**EXHIBIT**

**“C”**

**ZONING MAP AMENDMENT  
BOONE COUNTY PLANNING COMMISSION**

**R E C E I V E D**  
90839  
JUL 02 2024  
BOONE COUNTY  
PLANNING COMMISSION

Seven (7) copies of submitted drawings are required

**An application consists of all fees paid in full, submitted drawings, and a completed application form**

**SECTION A: (To be completed by applicant)**

1. Name of Project: Hickory Burlington and Poplar Pointe Apartments
2. Location of Project: Hickory Burlington and Poplar Pointe Apartments
3. Total Acreage of Project: Off Lariat Way
4. Current Zoning of Property: 44.37 acres
5. Proposed Zoning of Property (classification being requested): SR-1 PD
6. Proposed Use(s) (specify each use):  
Patio Rental Homes and Apartments
7. Proposed Building Intensities (specify for each building):  
~~139~~ Patio Rental homes and ~~157~~ Multi-Family Units, totally 296 units.
8. Are you applying for any of the following (check all that apply):  
 Conditional Use Permit     Variance
9. Current Owner: Delaware Crossing, LLC  
 Address: 25 Town Center Boulevard, Suite 104  

<u>Crestview Hills</u>	<u>KY</u>	<u>41017</u>
City	State	Zip Code
Phone Number: <u>pmanger@gallensteincompanies.com</u>	Fax Number: <u>N/A</u>	
Email: <u>pmanger@gallensteincompanies.com</u>		
10. Applicant: Charter Commercial, LLC  
 Address: 25 Town Center Boulevard, Suite 104  

<u>Crestview Hills</u>	<u>KY</u>	<u>41017</u>
City	State	Zip Code
Phone Number: <u>513-403-1335</u>	Fax Number: <u>N/A</u>	
Email: <u>pmanger@gallensteincompanies.com</u>		
11. Are there any existing buildings on the site:  Yes     No  
 If yes, indicate how many: One residence and couple auxiliary structures

1115

572

12. 1196 93 2031  
Deed Book Page Number Group Number

13. Have you had a pre-application meeting with the BCPC staff:  Yes  No

14. Have you submitted a Concept Development Plan:  Yes  No

15. Have you met or discussed your proposed development with any of the following organizations/agencies (check all that apply):

- |  |   |
|--|---|
| <input checked="" type="checkbox"/> Boone County Building Department   | <input type="checkbox"/> Local Fire District                          |
| <input type="checkbox"/> Boone County Public Works Department  | <input type="checkbox"/> Local School District                        |
| <input type="checkbox"/> Boone County Water District   | <input type="checkbox"/> Northern Kentucky Health Department          |
| <input type="checkbox"/> Cincinnati Bell   | <input type="checkbox"/> Owen Cooperative Electric, Inc.              |
| <input checked="" type="checkbox"/> Cincinnati/Northern Kentucky International Airport (Kentucky Airport Zoning Commission for height restrictions near the airport) | <input type="checkbox"/> Sanitation District No. 1                    |
| <input type="checkbox"/> Duke Energy   | <input type="checkbox"/> USDA NRCS/Boone County Conservation District |
| <input type="checkbox"/> Florence Public Services Department   | <input type="checkbox"/> Other: _____                                 |
| <input type="checkbox"/> Kentucky Division of Water  |   |
| <input type="checkbox"/> Kentucky Transportation Cabinet   |   |

16. Concept Development Plan Jurisdiction/Location (check all that apply):  
 Unincorporated Boone  Florence  Walton  Union

17. Waiver of 60 Day Time Requirement by Originator for Final Planning Commission Action:

In accordance with the provisions of KRS 100.211, the applicant(s) and property owner(s) or originator(s) hereby waive the 60 day time limit for the Boone County Planning Commission to take final action on my (our) Zoning Map Amendment/Concept Development Plan application. This time limit waiver is considered effective immediately upon receipt by the Boone County Planning Commission and expires on September 19, 2024

Property Owner's Signature:

Paul May

Applicant's Signature:

Paul May

**SECTION B: (To be completed by Planning Commission staff)**

1. Date Received: 7/2/24 Fee Received: \$3203.31 Receipt #: 90834
2. Number of Copies Received: \_\_\_\_\_
3. Has the following been submitted (check all that apply):
  - Completed Application
  - Concept Development Plan
  - Legal Description
  - Names and Mailing Addresses of Adjacent Property Owners
4. Date the application is Administratively Complete (as defined in KRS 100.211): \_\_\_\_\_
5. Staff Reviewer: M. Schwertz
6. Committee Chairperson: C. Gulick
7. Scheduled Public Hearing Date: 8/21/2024
8. Boone County Planning Commission Action: \_\_\_\_\_ Date of Action: 10/16/2024
  - \_\_\_\_\_ Approved
  - \_\_\_\_\_ Approved with Conditions
  - Denial
  - \_\_\_\_\_ Other
9. Resolution Number: \_\_\_\_\_

Boone County Planning Commission  
Boone County Administration Building  
2950 Washington Street, Room 317  
P.O. Box 958  
Burlington, Kentucky 41005  
Phone: 859-334-2196 Fax: 859-334-2264  
[plancom@boonecountky.org](mailto:plancom@boonecountky.org)  
[www.boonecountky.org](http://www.boonecountky.org)

# **SUPPORTING INFORMATION**

final report

July 1, 2024

## Traffic Assessment

Hickory Burlington & Poplar Pointe Apartments  
Burlington, KY

Prepared for

**Boone County Planning Commission**



**DIANE B. ZIMMERMAN**  
Traffic Engineering, LLC

12803 High Meadows Pike  
Prospect, KY 40059  
502.648.1858  
diane zim@att.net

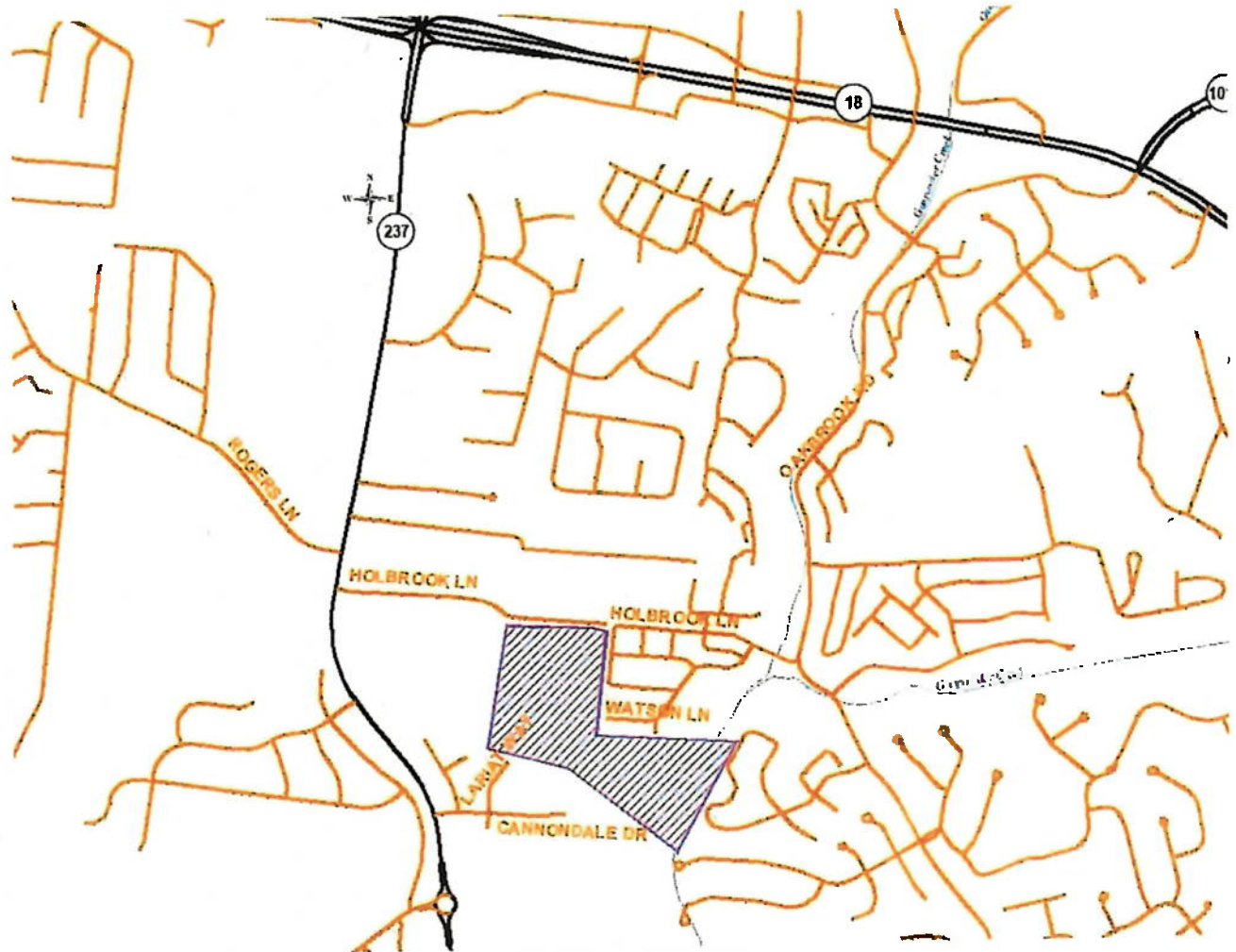


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Table 1. Peak Hour Trips Generated by Site.....	3
Figure 2. Peak Hour Trips Distribution .....	4

## INTRODUCTION

The preliminary site plan for apartments on Holbrook Lane in Burlington, KY shows 157 apartment units and 139 rental townhouses. **Figure 1** displays a map of the site. Access to the development will be from Holbrook Lane, Watson Lane, and Lariat Way. The purpose of this study is to identify the traffic characteristics of this development and the adjacent road.



**Figure 1. Site Map**

## EXISTING CONDITIONS

Holbrook Lane is maintained by Boone County and is part of Shaker Run subdivision. The road has two eleven-foot lanes with curb and gutter, and an eight-foot multi-use path along the south side. The road is classified as a local road. The speed limit is 25 mph. The intersection with Oakwood Road is controlled with a stop sign on Holbrook Lane. There is a right and left turn lane on Holbrook Lane.

Hickory Burlington & Poplar Pointe Apartments  
Traffic Assessment

Lariat Way is maintained by Boone County and is part of Valley Creek Farms subdivision. The road has two eleven-foot lanes with curb and gutter, and sidewalks. The road is classified as a local road. The speed limit is 25 mph. The intersection with Cannondale Drive is controlled with a stop sign on Cannondale Drive. There are no turn lanes.

### TRIP GENERATION

The Institute of Transportation Engineers Trip Generation Manual, 11<sup>th</sup> Edition contains trip generation rates for a wide range of developments. The land uses of "Single-Family Attached (215)" and "Multifamily Housing (Low-Rise) (220)" were reviewed and determined to be the best match. The trip generation results are listed in Table 1. This distribution is based upon the existing traffic counts on KY 237 and Oakwood Road. Figure 2 shows the trip distribution throughout the road network during the peak hours.

**Table 1. Peak Hour Trips Generated by Site**

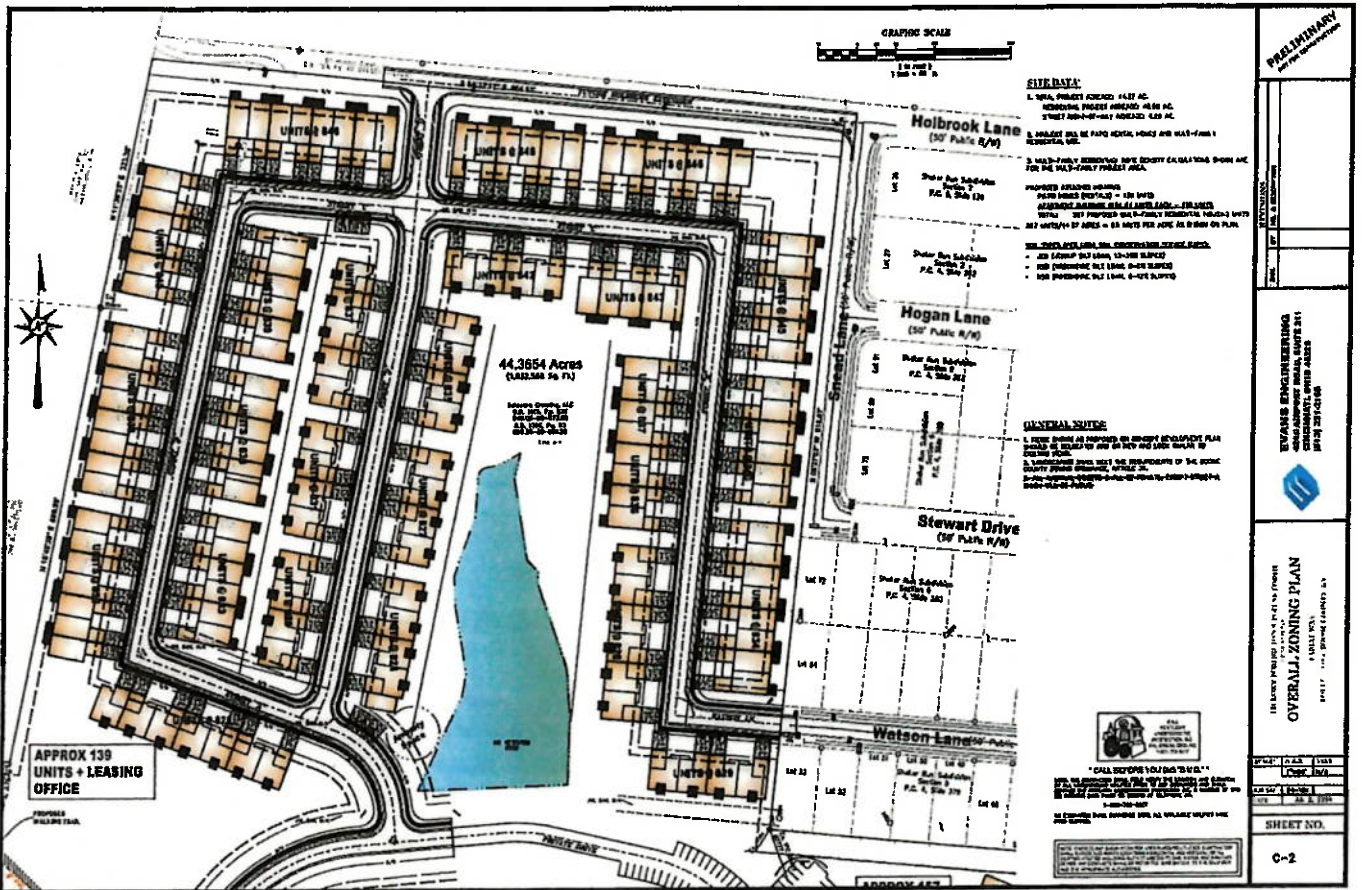
Land Use	A.M. Peak Hour			P.M. Peak Hour			Daily
	Trips	In	Out	Trips	In	Out	Trips
Single Family Attached (139 units)	67	17	50	79	47	32	1,009
Multifamily Low Rise (157 units)	72	17	55	88	55	33	1,082
Total	139	34	105	167	102	65	2,091

Hickory Burlington & Poplar Pointe Apartments  
Traffic Assessment

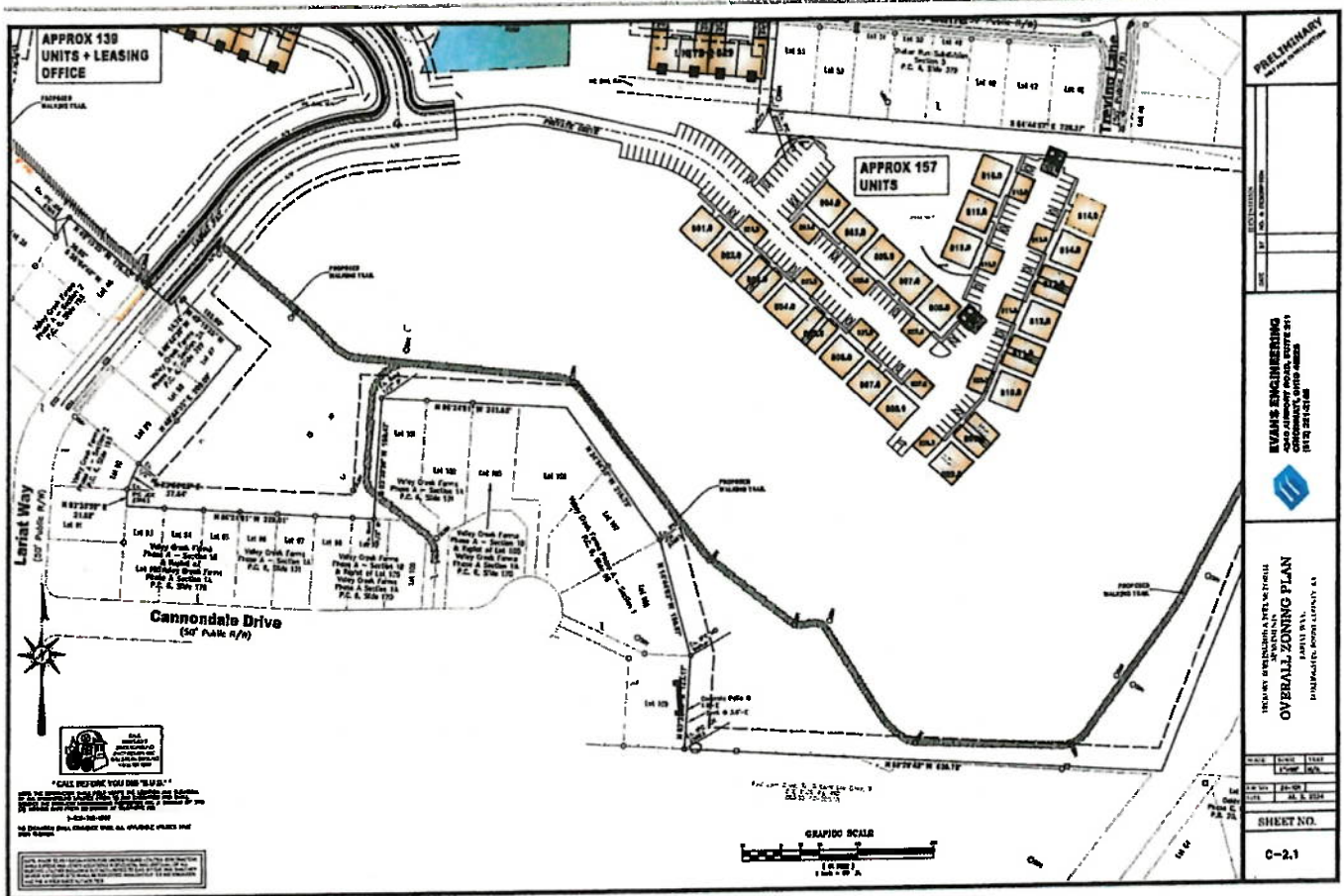


Figure 2. Peak Hour Trips Distribution

# Hickory Burlington & Poplar Pointe Apartments Traffic Assessment



# Hickory Burlington & Poplar Pointe Apartments Traffic Assessment



**PRELIMINARY**

**EVANS ENGINEERING**  
 2000 ALABAMA ROAD, SUITE 201  
 WASHINGTON, MISSISSIPPI 39204  
 (601) 933-1234

**OVERALL ZONING PLAN**  
 PART 111 WAY  
 HICKORY BURLINGTON, POPLAR POINTE, MS

DATE	DESCRIPTION

SHEET NO.  
C-2.1

final report

September 10, 2024

## Traffic Impact Study

Hickory Burlington & Poplar Pointe Apartments  
Burlington, KY

Prepared for

Boone County Planning Commission



DIANE B. ZIMMERMAN  
Traffic Engineering, LLC

12803 High Meadows Place  
Prospect, KY 40059  
502-618-1858  
diane@tze.net



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## INTRODUCTION

The preliminary site plan for apartments on Holbrook Lane in Burlington, KY shows 157 apartment units and 139 rental townhouses. Figure 1 displays a map of the site. Access to the development will be from Holbrook Lane, Watson Lane, and Lariat Way. The purpose of this study is to examine the traffic impacts of the development upon the adjacent highway system. For this study, the impact area was defined to be the intersections of KY 237 at Cannondale Drive, and Holbrook Lane at Watson Lane and Oakbrook Road.

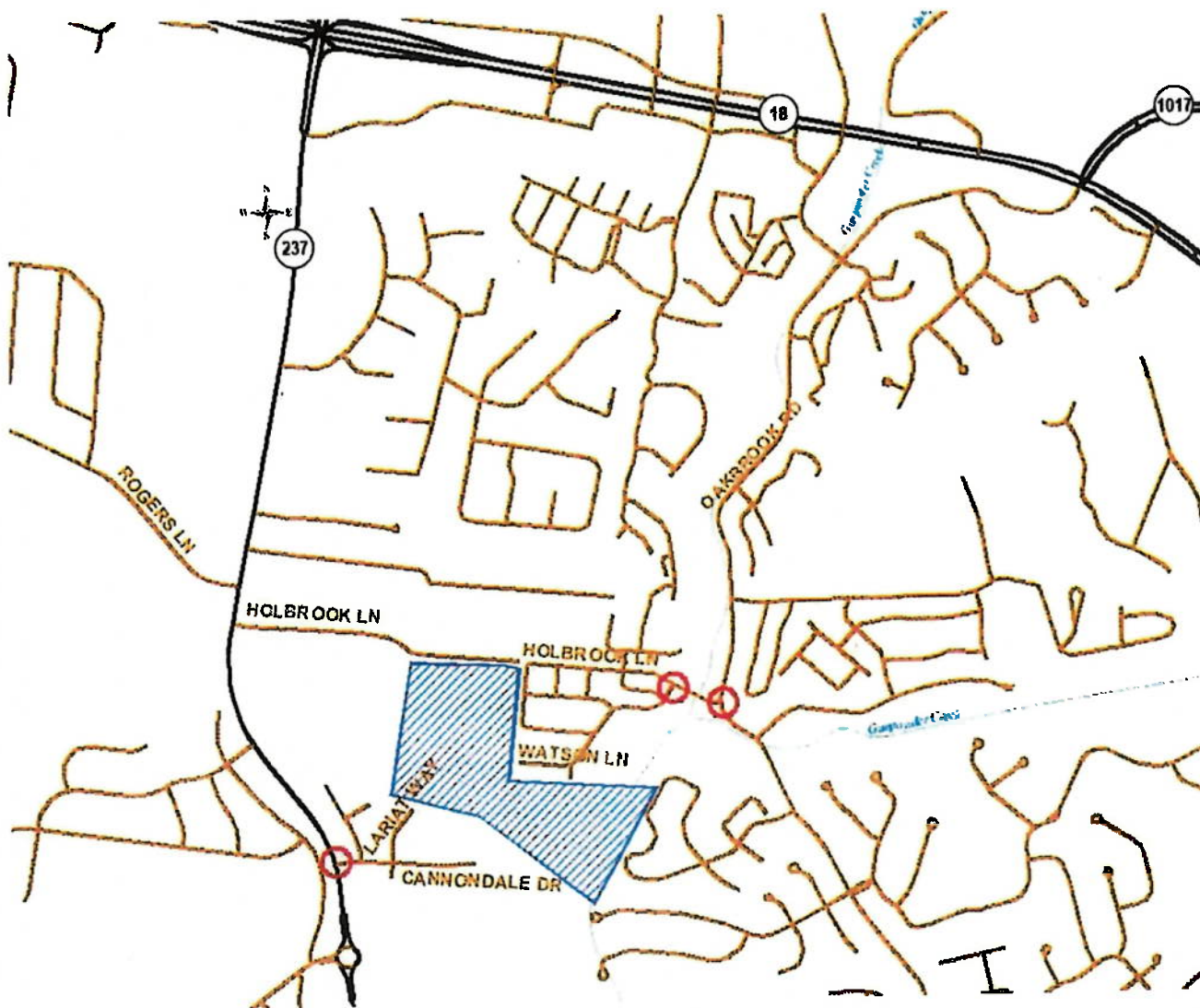


Figure 1. Site Map

## EXISTING CONDITIONS

Holbrook Lane is maintained by Boone County and is part of Shaker Run subdivision. The road has two eleven-foot lanes with curb and gutter, and an eight-foot multi-use path along the south side. The road is classified as a local road.

Hickory Burlington & Poplar Pointe Apartments  
 Traffic Impact Study

The speed limit is 25 mph. The intersection with Oakbrook Road is controlled with a stop sign on Holbrook Lane. There is a right and left turn lane on Holbrook Lane.

Lariat Way is maintained by Boone County and is part of Valley Creek Farms subdivision. The road has two eleven-foot lanes with curb and gutter, and sidewalks. The road is classified as a local road. The speed limit is 25 mph. The intersection with Cannondale Drive is controlled with a stop sign on Cannondale Drive. There are no marked turn lanes.

KY 237, Camp Ernst Road, is a state-maintained road with an estimated 2024 Average Daily Traffic (ADT) of 19,500 vehicles per day between Camp Ernst Road and KY 18, as estimated from the Kentucky Transportation Cabinet (KYTC) 2023 count at station G89. The road has four eleven-foot lanes, curb and gutter, and a 13-foot two-way left-turn lane through the study area. The speed limit is 45 mph. There are sidewalks on both sides.

Oakbrook Road, is a locally maintained road with an estimated 2024 Average Daily Traffic (ADT) of 4,900 vehicles per day between KY 18 and KY 237, as estimated from the Kentucky Transportation Cabinet (KYTC) 2023 count at station M08. The road has two eleven-foot lanes, with a stabilized shoulder, through the study area. The speed limit is 35 mph. There are sidewalks on the west side.

Peak hour traffic counts for the intersections were obtained on Tuesday, August 27, 2024. The a.m. peak hour occurred between 7:00 and 8:00, and the p.m. peak hour varied. Figure 2 illustrates the 2024 a.m. and p.m. peak hour traffic volumes. The Appendix contains the full count data.

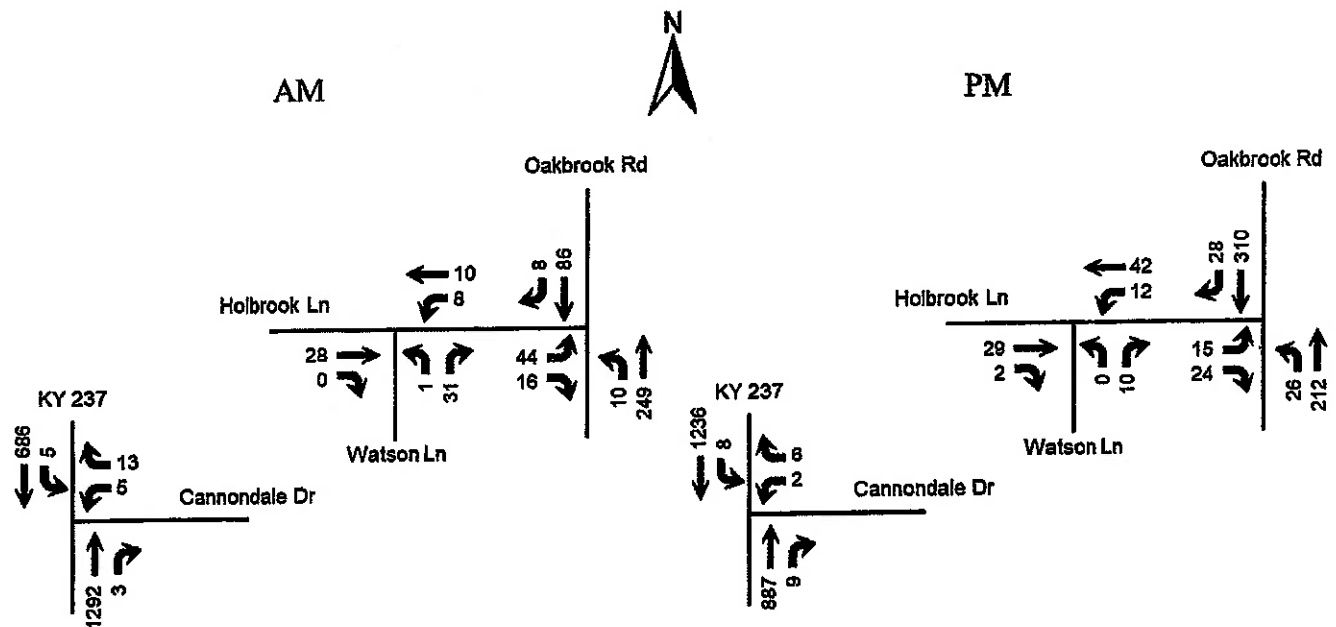
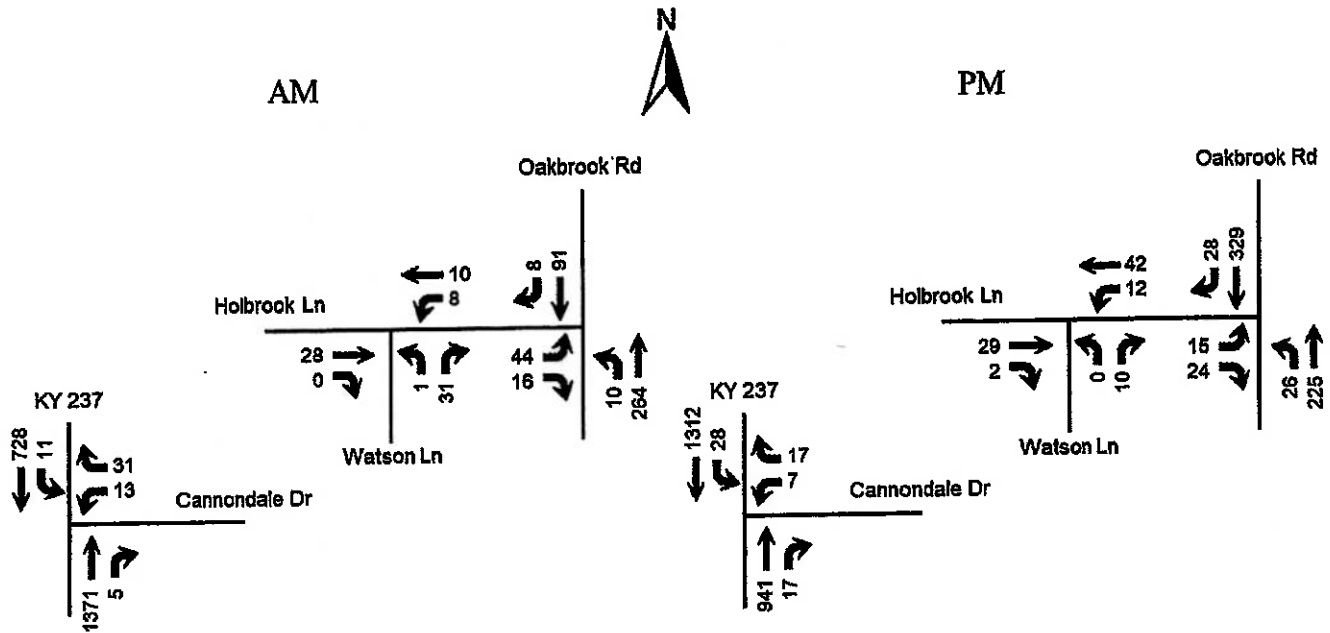


Figure 2. Existing Peak Hour Volumes

## FUTURE CONDITIONS

The project completion date is 2027. An annual growth rate of 2.0 percent was applied to the 2024 thru volumes on KY 237 and Oakbrook Road. This was determined by the historical growth at KYTC stations G89 and B91. Trip generation for the remaining 42 lots with access to Cannondale Drive was included. **Figure 3** displays the 2027 No Build peak hour volumes.



**Figure 3. 2027 No Build Peak Hour Volumes**

## TRIP GENERATION

The Institute of Transportation Engineers Trip Generation Manual, 11<sup>th</sup> Edition contains trip generation rates for a wide range of developments. The land uses of "Single-Family Attached (215)" and "Multifamily Housing (Low-Rise) (220)" were reviewed and determined to be the best match. The trip generation results are listed in **Table 1**. The trips were assigned to the highway network with the percentages shown in **Figure 4**. The study assumes 18 units will utilize the Watson Lane. The distribution is based upon the existing traffic counts on KY 237 and Oakwood Road. **Figure 5** shows the trips generated by this development and distributed throughout the road network during the peak hours. **Figure 6** displays the individual turning movements for the peak hours when the development is completed.

**Table 1. Peak Hour Trips Generated by Site**

Land Use	A.M. Peak Hour			P.M. Peak Hour			Daily Trips
	Trips	In	Out	Trips	In	Out	
Single Family Attached (139 units)	67	17	50	79	47	32	1,009
Multifamily Low Rise (157 units)	72	17	55	88	55	33	1,082
<b>Total</b>	<b>139</b>	<b>34</b>	<b>105</b>	<b>167</b>	<b>102</b>	<b>65</b>	<b>2,091</b>



**Figure 4. Peak Hour Trips Distribution**

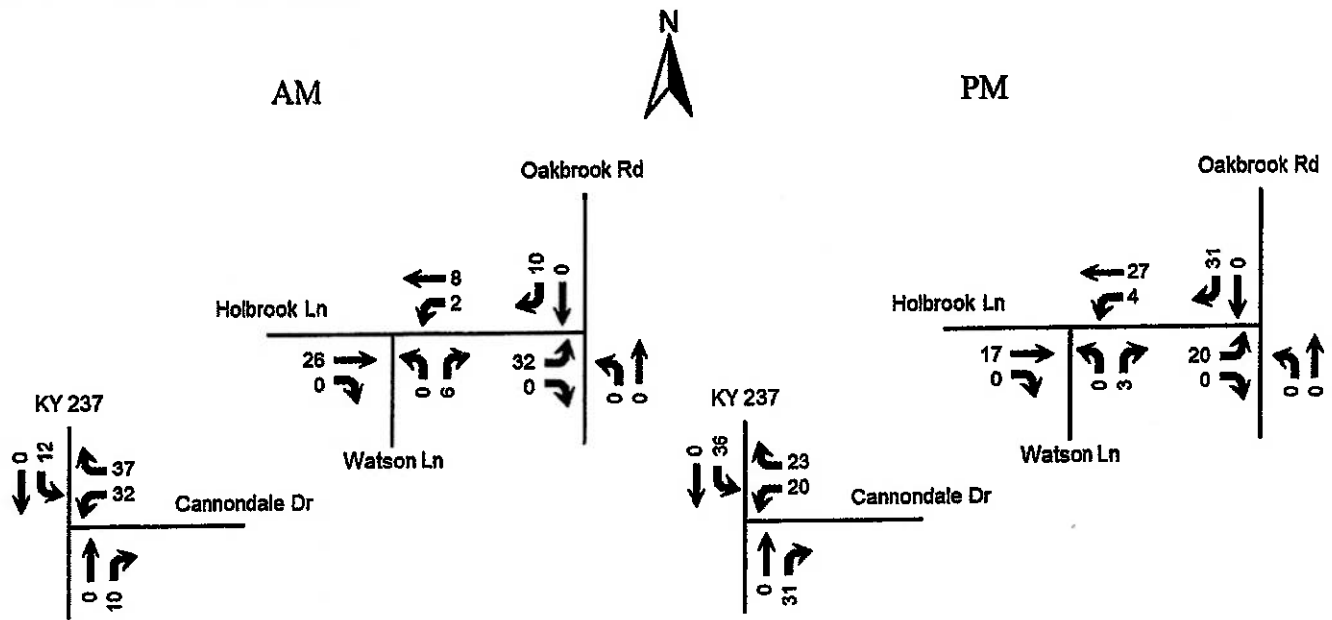


Figure 5. Peak Hour Trips Generated by Site

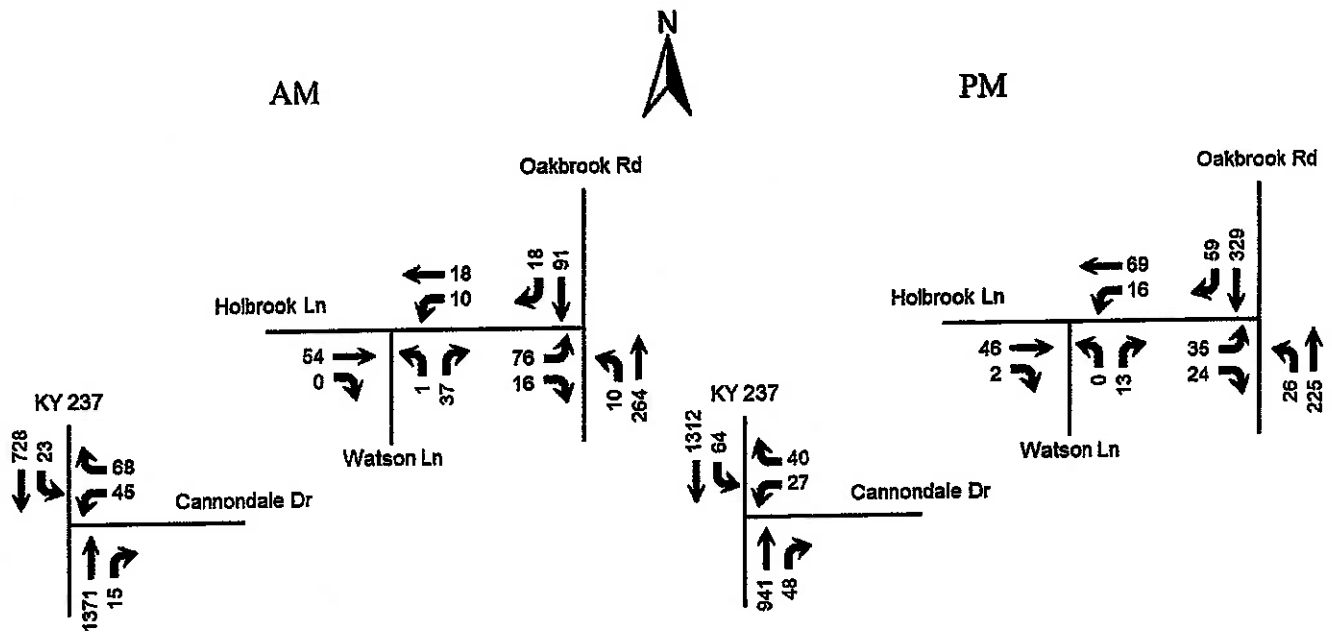


Figure 6. 2027 Build Peak Hour Volumes

## ANALYSIS

The qualitative measure of operation for a roadway facility or intersection is evaluated by assigning a "Level of Service". Level of Service is a ranking scale from A through F, "A" is the best operating condition and "F" is the worst. Level of Service results depend upon the facility that is analyzed. In this case, the Level of Service is based upon the total delay experienced for lanes at stop-controlled intersections.

To evaluate the impact of the proposed development, the vehicle delays at the intersections were determined using procedures detailed in the Highway Capacity Manual, 7<sup>th</sup> edition. Future delays and Level of Service were determined for the intersections using the HCS Two-Way Stop Controlled (version 2024) software. The delays and Level of Service are summarized in Table 2.

**Table 2. Peak Hour Level of Service**

Approach	A.M.			P.M.		
	2024 Existing	2027 No Build	2027 Build	2024 Existing	2027 No Build	2027 Build
<b>KY 237 at Cannondale Drive</b>						
Cannondale Drive Westbound	C 21.0	C 23.4	D 34.9	B 14.6	C 16.4	C 20.9
KY 237 Southbound (left)	B 12.0	B 12.7	B 13.0	A 9.8	B 10.3	B 10.7
<b>Oakbrook Road at Holbrook Lane</b>						
Holbrook Lane Eastbound	B 10.7	B 10.9	B 11.6	B 11.5	B 11.8	B 13.0
Oakbrook Road Northbound (left)	A 7.4	A 7.4	A 7.4	A 8.0	A 8.1	A 8.2
<b>Holbrook Lane at Watson Lane</b>						
Holbrook Lane Westbound (left)	A 7.4	A 7.4	A 7.5	A 7.3	A 7.3	A 7.3
Watson Lane Northbound	A 8.7	A 8.7	A 8.9	A 8.5	A 8.5	A 8.6

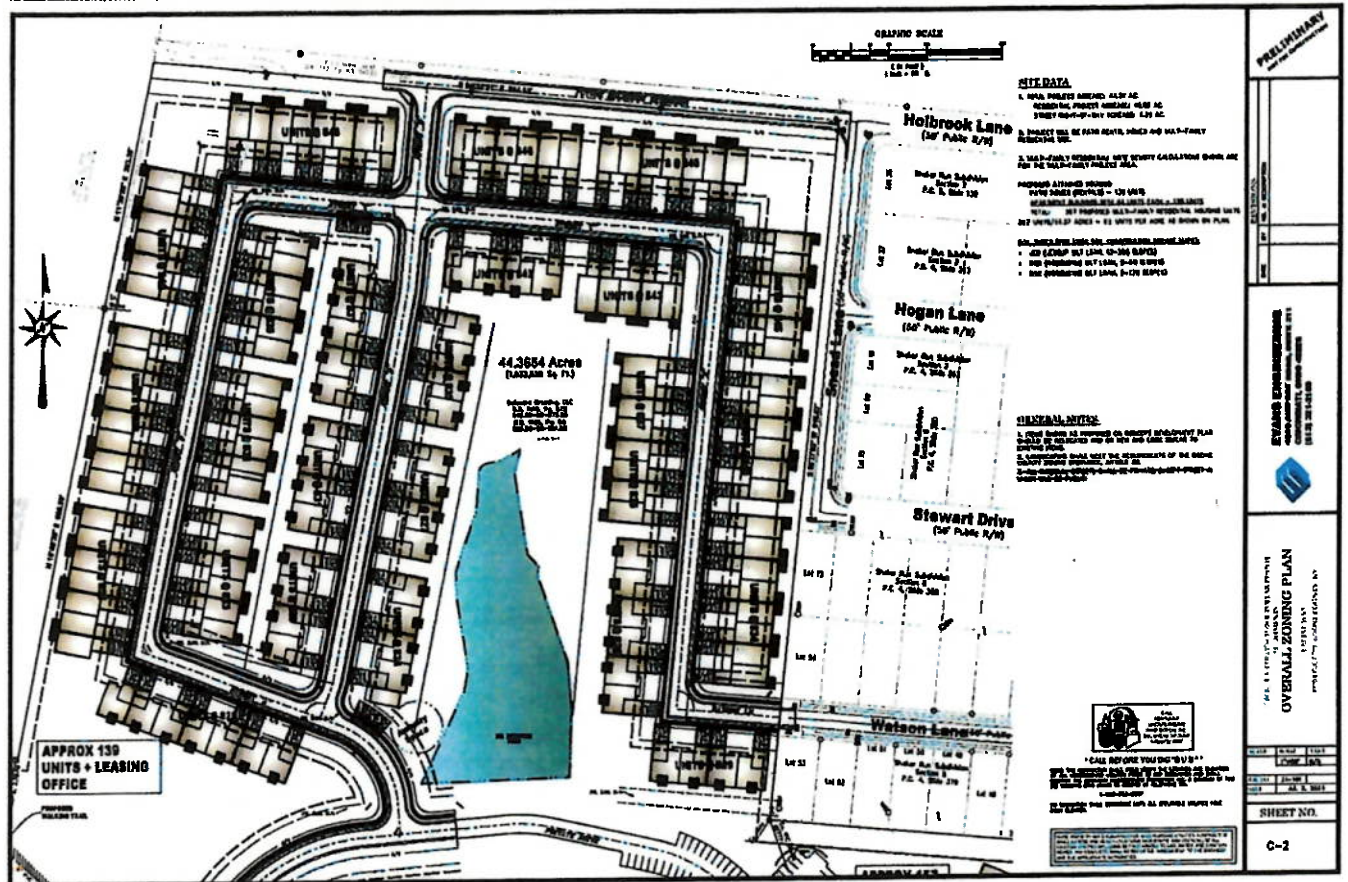
*Key: Level of Service, Delay in seconds per vehicle*

## CONCLUSIONS

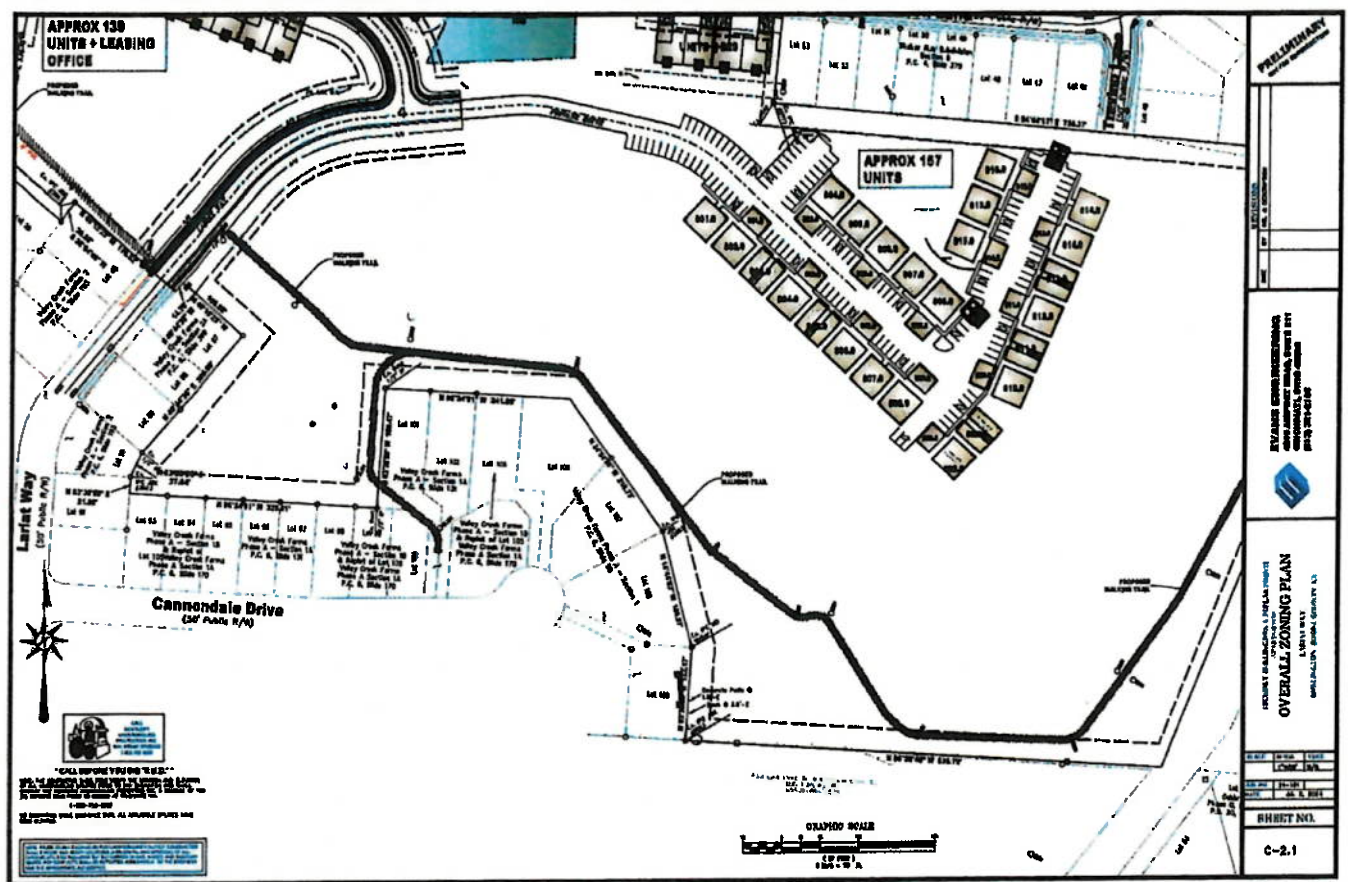
Based upon the volume of traffic generated by the development and the amount of traffic forecasted for the year 2027, there will be a slight impact to the existing highway network. No improvements are required at the intersections to maintain an acceptable level of service.

**APPENDIX**

# Hickory Burlington & Poplar Pointe Apartments Traffic Impact Study



# Hickory Burlington & Poplar Pointe Apartments Traffic Impact Study



Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

TRAFFIC COUNTS

Classified Turn Movement Count | All vehicles



www.marrtraffic.com

Burlington, KY

Site 1  
KY-237 Camp Ernst Rd (South)  
KY-237 Camp Ernst Rd (North)

Date  
Tuesday, August 27, 2024

Weather

Fair  
83°F

Lat/Long  
39.002805°, -84.701596°  
[Click here for Map](#)

[Click here for Detailed Weather:](#)

Cannondale Dr

0700 - 0900 (Weekday 2h Session) (08-27-2024)  
All vehicles

TIME	Northbound				Southbound			
	KY-237 Camp Ernst Rd (South)		KY-237 Camp Ernst Rd (North)		KY-237 Camp Ernst Rd (South)		KY-237 Camp Ernst Rd (North)	
	Thru	Right	U-Turn	App Total	Left	Thru	U-Turn	App Total
0700 - 0715	296	1	0	297	2	214	0	216
0715 - 0730	339	0	0	339	1	174	0	175
0730 - 0745	346	1	0	347	0	166	0	166
0745 - 0800	311	1	0	312	3	132	0	135
Hourly Total	1292	3	0	1295	6	686	0	692
0800 - 0815	246	0	0	246	0	102	0	102
0815 - 0830	255	0	0	255	4	107	0	111
0830 - 0845	214	1	0	215	2	133	0	135
0845 - 0900	193	0	0	193	3	135	0	138
Hourly Total	908	1	0	909	9	477	0	486
Grand Total	2200	4	0	2204	15	1163	0	1178
Approach %	99.82	0.18	0.00	-	1.27	98.73	0.00	-
Intersection %	64.37	0.12	0.00	64.48	0.44	34.03	0.00	34.46
Heavy Vehicle %	5	25	-	5	13	5	-	5
PHF	0.93	0.75	0.00	0.93	0.50	0.80	0.00	0.80

Westbound				
Cannondale Dr				
Left	Right	U-Turn	App Total	Int Total
1.7	1.8	1.9		
3	6	0	9	522
1	2	0	3	517
0	4	0	4	517
1	1	0	2	449
5	13	0	18	2005
0	2	0	2	350
2	6	0	8	374
2	5	0	7	357
0	1	0	1	332
4	14	0	18	1413
9	27	0	36	3418
25.00	75.00	0.00	-	-
0.26	0.79	0.00	1.05	-
11	7	-	8	5
0.42	0.54	0.00	0.50	0.96

1600 - 1800 (Weekday 2h Session) (08-27-2024)  
All vehicles

TIME	Northbound				Southbound			
	KY-237 Camp Ernst Rd (South)		KY-237 Camp Ernst Rd (North)		KY-237 Camp Ernst Rd (South)		KY-237 Camp Ernst Rd (North)	
	Thru	Right	U-Turn	App Total	Left	Thru	U-Turn	App Total
1600 - 1615	260	0	0	260	1	278	0	279
1615 - 1630	232	2	0	234	1	279	0	280
1630 - 1645	200	0	0	200	2	299	0	301
1645 - 1700	202	0	0	202	1	328	0	329
Hourly Total	894	2	0	896	5	1184	0	1189
1700 - 1715	239	2	0	241	4	274	0	278
1715 - 1730	227	5	0	232	1	316	0	317
1730 - 1745	219	2	0	221	2	318	0	320
1745 - 1800	209	0	0	209	3	230	0	233
Hourly Total	894	9	0	903	10	1138	0	1148
Grand Total	1788	11	0	1799	15	2322	0	2337
Approach %	99.39	0.61	0.00	-	0.64	99.36	0.00	-
Intersection %	43.07	0.26	0.00	43.34	0.36	55.94	0.00	56.30
Heavy Vehicle %	4	0	-	4	0	2	-	2
PHF	0.93	0.45	0.00	0.93	0.50	0.94	0.00	0.95

Westbound				
Cannondale Dr				
Left	Right	U-Turn	App Total	Int Total
1.7	1.8	1.9		
0	1	0	1	540
1	1	0	2	516
1	2	0	3	504
0	3	0	3	534
2	7	0	9	2094
2	0	0	2	521
0	1	0	1	550
0	2	0	2	543
0	1	0	1	443
2	4	0	6	2057
4	11	0	15	4151
26.67	73.33	0.00	-	-
0.10	0.26	0.00	0.36	-
0	0	-	0	3
0.25	0.50	0.00	0.67	0.98

# Hickory Burlington & Poplar Pointe Apartments Traffic Impact Study



www.marrtraffic.com

## Classified Turn Movement Count || All vehicles

Burlington, KY

Site 2

Watson Ln

Holbrook Ln (West)  
Holbrook Ln (East)

Date

Tuesday, August 27, 2024

Weather

Fair  
83°F

[Click here for Detailed Weather](#)

Lat/Long

39.006616°, -84.692065°

[Click here for Map](#)

0700 - 0900 (Weekday 2h Session) (08-27-2024)

All vehicles

Northbound Watson Ln				
TIME	Left 2.1	Right 2.2	U-Turn 2.3	App Total
0700 - 0715	0	12	0	12
0715 - 0730	0	10	0	10
0730 - 0745	0	2	0	2
0745 - 0800	1	7	0	8
Hourly Total	1	31	0	32
0800 - 0815	0	0	0	0
0815 - 0830	1	7	0	8
0830 - 0845	0	3	0	3
0845 - 0900	0	2	0	2
Hourly Total	1	12	0	13
Grand Total	2	43	0	45
Approach %	4.44	95.56	0.00	-
Intersection %	1.59	34.13	0.00	35.71
Heavy Vehicle %	0	2	-	2
PHF	0.25	0.65	0.00	0.67

Eastbound Holbrook Ln (West)				Westbound Holbrook Ln (East)				
Thru 2.4	Right 2.5	U-Turn 2.6	App Total	Left 2.7	Thru 2.8	U-Turn 2.9	App Total	Int Total
8	0	0	8	1	5	0	6	26
10	0	0	10	1	3	0	4	24
5	0	0	5	3	1	0	4	11
5	0	0	5	3	1	0	4	17
28	0	0	28	8	10	0	18	78
4	1	0	5	4	4	0	8	13
5	0	0	5	2	3	0	5	18
4	0	0	4	3	0	0	3	10
4	0	0	4	0	1	0	1	7
17	1	0	18	9	8	0	17	48
45	1	0	46	17	18	0	35	126
97.83	2.17	0.00	-	48.57	51.43	0.00	-	-
95.71	0.79	0.00	36.51	13.49	14.29	0.00	27.78	-
0	0	-	0	12	0	-	6	2
0.70	0.00	0.00	0.70	0.67	0.50	0.00	0.75	0.75

1600 - 1800 (Weekday 2h Session) (08-27-2024)

All vehicles

Northbound Watson Ln				
TIME	Left 2.1	Right 2.2	U-Turn 2.3	App Total
1600 - 1615	0	5	0	5
1615 - 1630	0	2	0	2
1630 - 1645	0	3	0	3
1645 - 1700	0	4	0	4
Hourly Total	0	14	0	14
1700 - 1715	0	1	0	1
1715 - 1730	0	2	0	2
1730 - 1745	0	1	0	1
1745 - 1800	0	5	0	5
Hourly Total	0	9	0	9
Grand Total	0	23	0	23
Approach %	0.00	100.00	0.00	-
Intersection %	0.00	12.23	0.00	12.23
Heavy Vehicle %	-	9	-	9
PHF	0.00	0.63	0.00	0.63

Eastbound Holbrook Ln (West)				Westbound Holbrook Ln (East)				
Thru 2.4	Right 2.5	U-Turn 2.6	App Total	Left 2.7	Thru 2.8	U-Turn 2.9	App Total	Int Total
9	0	0	9	5	12	1	18	32
7	0	0	7	2	9	0	11	20
9	0	0	9	2	11	0	13	25
7	2	0	9	4	8	0	12	25
32	2	0	34	13	40	1	54	102
5	0	0	5	4	9	0	13	19
8	0	0	8	2	14	0	16	26
5	1	0	6	8	6	1	15	22
6	1	0	7	3	4	0	7	19
24	2	0	26	17	33	1	51	86
56	4	0	60	30	73	2	105	188
93.33	6.67	0.00	-	28.57	69.52	1.90	-	-
29.79	2.13	0.00	31.91	15.96	38.83	1.06	55.85	-
0	0	-	0	0	0	0	0	1
0.81	0.25	0.00	0.86	0.75	0.75	0.00	0.84	0.91

# Hickory Burlington & Poplar Pointe Apartments Traffic Impact Study

## Classified Turn Movement Count || All vehicles



Burlington, KY

Site 3  
Oakbrook Dr (South)  
Oakbrook Dr (North)  
Holbrook Ln

Date  
Tuesday, August 27, 2024

Weather  
Fair  
83°F  
[Click here for Detailed Weather](#)

Lat/Long  
39.006195°, -84.690733°  
[Click here for Map](#)

0700 - 0900 (Weekday 2h Session) (08-27-2024)  
All vehicles

TIME	Northbound				Southbound				Eastbound				Int Total
	Oakbrook Dr (South)		U-Turn		Oakbrook Dr (North)		U-Turn		Holbrook Ln		U-Turn		
	Left 3.1	Thru 3.2	3.3	App Total	Thru 3.4	Right 3.5	3.6	App Total	Left 3.7	Right 3.8	3.9	App Total	
0700 - 0715	3	61	0	64	17	3	0	20	15	4	0	19	108
0715 - 0730	3	67	0	70	16	1	0	17	15	6	0	21	90
0730 - 0745	3	55	0	58	23	1	0	24	6	2	0	8	112
0745 - 0800	1	66	0	67	30	3	0	33	8	4	0	12	413
Hourly Total	10	249	0	259	86	8	0	94	44	16	0	60	82
0800 - 0815	1	57	0	58	14	6	0	20	2	2	0	4	90
0815 - 0830	1	52	0	53	21	4	0	25	7	5	0	12	87
0830 - 0845	0	49	0	49	28	3	0	31	5	2	0	7	71
0845 - 0900	1	44	0	45	20	0	0	20	3	3	0	6	330
Hourly Total	3	202	0	205	83	13	0	96	17	12	0	29	743
Grand Total	13	451	0	464	169	21	0	190	61	28	0	89	2
Approach %	2.80	97.20	0.00	-	88.95	11.05	0.00	-	68.54	31.46	0.00	-	0.92
Intersection %	1.75	60.70	0.00	62.45	22.75	2.83	0.00	25.57	8.21	3.77	0.00	11.98	
Heavy Vehicle %	0	1	-	1	5	10	-	5	2	0	-	1	
PHF	0.83	0.93	0.00	0.93	0.72	0.67	0.00	0.71	0.73	0.67	0.00	0.71	

1600 - 1800 (Weekday 2h Session) (08-27-2024)  
All vehicles

TIME	Northbound				Southbound				Eastbound				Int Total
	Oakbrook Dr (South)		U-Turn		Oakbrook Dr (North)		U-Turn		Holbrook Ln		U-Turn		
	Left 3.1	Thru 3.2	3.3	App Total	Thru 3.4	Right 3.5	3.6	App Total	Left 3.7	Right 3.8	3.9	App Total	
1600 - 1615	5	42	0	47	60	11	0	71	8	6	0	14	146
1615 - 1630	4	61	0	65	65	7	0	72	2	7	0	9	160
1630 - 1645	8	64	0	72	70	6	0	76	3	9	0	12	146
1645 - 1700	5	41	0	46	84	5	0	89	4	6	1	11	594
Hourly Total	22	208	0	230	279	29	0	308	17	28	1	46	145
1700 - 1715	5	53	0	58	72	9	0	81	3	3	0	6	164
1715 - 1730	8	54	0	62	84	8	0	92	4	6	0	10	149
1730 - 1745	6	52	0	58	75	9	0	84	3	4	0	7	134
1745 - 1800	6	47	0	53	69	1	0	70	6	5	0	11	592
Hourly Total	25	206	0	231	300	27	0	327	16	18	0	34	1176
Grand Total	47	414	0	461	579	56	0	635	33	46	1	80	1
Approach %	10.20	89.80	0.00	-	91.18	8.82	0.00	-	41.25	57.50	1.25	-	0.94
Intersection %	4.00	35.20	0.00	39.20	49.23	4.76	0.00	54.00	2.81	3.91	0.09	6.80	
Heavy Vehicle %	0	0	-	0	1	0	-	0	6	0	0	3	
PHF	0.81	0.83	0.00	0.83	0.92	0.78	0.00	0.92	0.88	0.67	0.25	0.81	

**K** TIS Simplified Traffic Forecast

Count Year	2024	Number of Counts	13
Opening Year	2027	Growth Rate	0.21%
Design Year	2037		
Years Back	15		

**KYTC Traffic Count Station #1**

STA ID	008G89
Paste Count Data Here	
2024	
2023	19128
2022	
2021	
2020	16527
2019	
2018	
2017	18491
2016	
2015	
2014	13135
2013	
2012	
2011	15300
2010	
2009	
2008	13300
2007	
2006	
2005	12700
2004	
2003	
2002	13600
2001	
2000	
1999	10200

**KYTC Traffic Count Station #2**

STA ID	008M08
Paste Count Data Here	
2024	
2023	4854
2022	
2021	
2020	4700
2019	
2018	
2017	5656
2016	
2015	
2014	
2013	
2012	
2011	
2010	
2009	
2008	
2007	
2006	
2005	
2004	
2003	
2002	
2001	
2000	
1999	

**KYTC Traffic Count Station #3**

STA ID	008B91
Paste Count Data Here	
2024	
2023	17804
2022	
2021	
2020	11519
2019	
2018	
2017	12257
2016	
2015	
2014	13831
2013	
2012	
2011	13200
2010	
2009	
2008	15100
2007	
2006	
2005	13600
2004	
2003	
2002	11300
2001	
2000	
1999	8740

Without MO8

**TIS Simplified Traffic Forecast**

Count Year	2024	Number of Counts	10
Opening Year	2027	Growth Rate	1.97%
Design Year	2037		
Years Back	15		

**KYTC Traffic Count Station #1**

STA ID	008G89
Paste Count Data Here	
2024	
2023	19128
2022	
2021	
2020	16527
2019	
2018	
2017	18491
2016	
2015	
2014	13135
2013	
2012	
2011	15300
2010	
2009	
2008	13300
2007	
2006	
2005	12700
2004	
2003	
2002	13600
2001	
2000	
1999	10200
1998	
1997	
1996	9830

**KYTC Traffic Count Station #2**

STA ID	008M08
Paste Count Data Here	
2024	
2023	
2022	
2021	
2020	
2019	
2018	
2017	
2016	
2015	
2014	
2013	
2012	
2011	
2010	
2009	
2008	
2007	
2006	
2005	
2004	
2003	
2002	
2001	
2000	
1999	
1998	
1997	
1996	

**KYTC Traffic Count Station #3**

STA ID	008B91
Paste Count Data Here	
2024	
2023	17804
2022	
2021	
2020	11519
2019	
2018	
2017	12257
2016	
2015	
2014	13831
2013	
2012	
2011	13200
2010	
2009	
2008	15100
2007	
2006	
2005	13600
2004	
2003	
2002	11300
2001	
2000	
1999	8740
1998	
1997	
1996	10700

HCS REPORTS

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	KY 237 at Cannondale Dr							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Cannondale Dr							
Analysis Year	2024							North/South Street	KY 237							
Time Analyzed	AM Peak							Peak Hour Factor	0.96							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
<b>Lanes</b>																
<b>Vehicle Volumes and Adjustments</b>																
Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement									1U	1	2	3	4U	4	5	6
Priority		10	11	12		7	8	9								
Number of Lanes		0	0	0		1	0	1	0	0	2	0	0	1	2	0
Configuration						L		R			T	TR		L	T	TR
Volume (veh/h)						5		13			1292	3	0	5	686	0
Percent Heavy Vehicles (%)						20		0					3	0		
Proportion Time Blocked																
Percent Grade (%)							0									
Right Turn Channelized							No									
Median Type   Storage						Left Only							1			
<b>Critical and Follow-up Headways</b>																
Base Critical Headway (sec)						7.5		6.9							4.1	
Critical Headway (sec)						7.90		6.90							4.10	
Base Follow-Up Headway (sec)						3.5		3.3							2.2	
Follow-Up Headway (sec)						3.70		3.30							2.20	
<b>Delay, Queue Length, and Level of Service</b>																
Flow Rate, v (veh/h)						5		14							5	
Capacity, c (veh/h)						112		402							517	
v/c Ratio						0.05		0.03							0.01	
95% Queue Length, Q <sub>95</sub> (veh)						0.1		0.1							0.0	
95% Queue Length, Q <sub>95</sub> (ft)						2.9		2.5							0.0	
Control Delay (s/veh)						38.6		14.3							12.0	
Level of Service (LOS)						E		B							B	
Approach Delay (s/veh)						21.0									0.1	
Approach LOS						C									A	

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	KY 237 at Cannondale Dr							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Cannondale Dr							
Analysis Year	2027							North/South Street	KY 237							
Time Analyzed	AM Peak No Build							Peak Hour Factor	0.95							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
Lanes																
Vehicle Volumes and Adjustments																
Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	0	0		1	0	1	0	0	2	0	0	1	2	0
Configuration						L		R			T	TR		L	T	TR
Volume (veh/h)						13		31			1371	5	0	11	728	0
Percent Heavy Vehicles (%)						8		0					3	0		
Proportion Time Blocked																
Percent Grade (%)								0								
Right Turn Channelized								No								
Median Type   Storage						Left Only										1
Critical and Follow-up Headways																
Base Critical Headway (sec)								7.5								4.1
Critical Headway (sec)								7.65								4.10
Base Follow-Up Headway (sec)								3.5								2.2
Follow-Up Headway (sec)								3.58								2.20
Delay, Queue Length, and Level of Service																
Flow Rate, v (veh/h)								14								11
Capacity, c (veh/h)								110								480
v/c Ratio								0.12								0.02
95% Queue Length, Q <sub>95</sub> (veh)								0.4								0.1
95% Queue Length, Q <sub>95</sub> (ft)								10.6								2.5
Control Delay (s/veh)								42.4								12.7
Level of Service (LOS)								E								B
Approach Delay (s/veh)								23.4							0.2	
Approach LOS								C							A	

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	KY 237 at Cannondale Dr							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Cannondale Dr							
Analysis Year	2027							North/South Street	KY 237							
Time Analyzed	AM Peak Build							Peak Hour Factor	0.96							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
<b>Lanes</b>																
<p>Major Street North-South</p>																
<b>Vehicle Volumes and Adjustments</b>																
Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	10	1	2	3	4	5	6	
Number of Lanes		0	0	0		1	0	1	0	0	2	0	0	1	2	0
Configuration						L		R			T	TR		L	T	TR
Volume (veh/h)						45		88			1371	15	0	23	728	0
Percent Heavy Vehicles (%)						8		0					3	0		
Proportion Time Blocked																
Percent Grade (%)								0								
Right Turn Channelized								No								
Median Type   Storage						Left Only							1			
<b>Critical and Follow-up Headways</b>																
Base Critical Headway (sec)						7.5		6.9							4.1	
Critical Headway (sec)						7.66		6.90							4.10	
Base Follow-Up Headway (sec)						3.5		3.3							2.2	
Follow-Up Headway (sec)						3.58		3.30							2.20	
<b>Delay, Queue Length, and Level of Service</b>																
Flow Rate, v (veh/h)						47		71							24	
Capacity, c (veh/h)						108		374							476	
v/c Ratio						0.44		0.19							0.05	
95% Queue Length, Q <sub>95</sub> (veh)						1.9		0.7							0.2	
95% Queue Length, Q <sub>95</sub> (ft)						50.5		17.5							5.0	
Control Delay (s/veh)						62.1		16.9							13.0	
Level of Service (LOS)						F		C							B	
Approach Delay (s/veh)						34.9							0.4			
Approach LOS						D							A			

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	KY 237 at Cannondale Dr							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Cannondale Dr							
Analysis Year	2024							North/South Street	KY 237							
Time Analyzed	PM Peak							Peak Hour Factor	0.98							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
<b>Lanes</b>																
<b>Vehicle Volumes and Adjustments</b>																
Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	10	1	2	3	4	5	6	
Number of Lanes		0	0	0		1	0	1	0	0	2	0	0	1	2	0
Configuration						L		R			T	TR		L	T	TR
Volume (veh/h)						2		6			887	9		8	1236	0
Percent Heavy Vehicles (%)						0		0						0	0	
Proportion Time Blocked																
Percent Grade (%)							0									
Right Turn Channelized							No									
Median Type   Storage						Left Only							1			
<b>Critical and Follow-up Headways</b>																
Base Critical Headway (sec)						7.5		6.9							4.1	
Critical Headway (sec)						7.50		6.90							4.10	
Base Follow-Up Headway (sec)						3.5		3.3							2.2	
Follow-Up Headway (sec)						3.90		3.30							2.20	
<b>Delay, Queue Length, and Level of Service</b>																
Flow Rate, v (veh/h)						2		6							8	
Capacity, c (veh/h)						195		556							754	
v/c Ratio						0.01		0.01							0.01	
95% Queue Length, Q <sub>95</sub> (veh)						0.0		0.0							0.0	
95% Queue Length, Q <sub>95</sub> (ft)						0.0		0.0							0.0	
Control Delay (s/veh)						23.7		11.5							9.8	
Level of Service (LOS)						C		B							A	
Approach Delay (s/veh)						14.6							0.1			
Approach LOS						B							A			

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	KY 237 at Cannondale Dr								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Cannondale Dr								
Analysis Year	2027							North/South Street	KY 237								
Time Analyzed	PM Peak No Build							Peak Hour Factor	0.98								
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
Lanes																	
Vehicle Volumes and Adjustments																	
Approach	Eastbound				Westbound				Northbound				Southbound				
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Movement									1U	1	2	3	4U	4	5	6	
Priority		10	11	12		7	8	9	0	0	2	0	0	1	2	0	
Number of Lanes		0	0	0		1	0	1	0	0	2	0	0	1	2	0	
Configuration						L		R			T	TR		L	T	TR	
Volume (veh/h)						7		17			941	17	0	28	1312	0	
Percent Heavy Vehicles (96)						0		0					0	0			
Proportion Time Blocked																	
Percent Grade (96)							0										
Right Turn Channelized							No										
Median Type   Storage							Left Only									1	
Critical and Follow-up Headways																	
Base Critical Headway (sec)						7.5		6.9								4.1	
Critical Headway (sec)						7.50		6.90								4.10	
Base Follow-Up Headway (sec)						3.5		3.3								2.2	
Follow-Up Headway (sec)						3.50		3.30								2.20	
Delay, Queue Length, and Level of Service																	
Flow Rate, v (veh/h)						7		17								29	
Capacity, c (veh/h)						171		530								714	
v/c Ratio						0.04		0.03								0.04	
95% Queue Length, Q <sub>95</sub> (veh)						0.1		0.1								0.1	
95% Queue Length, Q <sub>95</sub> (ft)						2.5		2.5								2.5	
Control Delay (s/veh)						27.0		12.0								10.3	
Level of Service (LOS)						D		B								B	
Approach Delay (s/veh)							16.4									0.2	
Approach LOS							C									A	

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	KY 237 at Cannondale Dr								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Cannondale Dr								
Analysis Year	2027							North/South Street	KY 237								
Time Analyzed	PM Peak Build							Peak Hour Factor	0.98								
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
<b>Lanes</b>																	
<p>Major Street North-South</p>																	
<b>Vehicle Volumes and Adjustments</b>																	
Approach	Eastbound				Westbound				Northbound				Southbound				
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6	
Number of Lanes		0	0	0		1	0	1		0	2	0	0	1	2	0	
Configuration						L		R			T	TR		L	T	TR	
Volume (veh/h)						27		40			941	48	0	64	1912	0	
Percent Heavy Vehicles (%)						0		0					0	0			
Proportion Time Blocked																	
Percent Grade (%)								0									
Right Turn Channelized								No									
Median Type   Storage						Left Only							1				
<b>Critical and Follow-up Headways</b>																	
Base Critical Headway (sec)						7.5		6.9								4.1	
Critical Headway (sec)						7.50		6.90								4.10	
Base Follow-Up Headway (sec)						3.5		3.3								2.2	
Follow-Up Headway (sec)						3.50		3.30								2.20	
<b>Delay, Queue Length, and Level of Service</b>																	
Flow Rate, v (veh/h)						28		41								65	
Capacity, c (veh/h)						155		518								695	
v/c Ratio						0.18		0.08								0.09	
95% Queue Length, Q <sub>95</sub> (veh)						0.6		0.3								0.3	
95% Queue Length, Q <sub>95</sub> (ft)						15.0		7.5								7.5	
Control Delay (s/veh)						33.3		12.5								10.7	
Level of Service (LOS)						D		B								B	
Approach Delay (s/veh)					20.9								0.5				
Approach LOS					C								A				

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Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	Oakbrook Rd at Holbrook Ln								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Holbrook Ln								
Analysis Year	2024							North/South Street	Oakbrook Dr								
Time Analyzed	AM Peak							Peak Hour Factor	0.92								
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
<b>Lanes</b>																	
<b>Vehicle Volumes and Adjustments</b>																	
Approach	Eastbound				Westbound				Northbound				Southbound				
Movement	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6	
Number of Lanes		1	0	1		0	0	0	0	0	1	0	0	0	1	0	
Configuration		L		R						LT						TR	
Volume (veh/h)		44		16						10	249				86	8	
Percent Heavy Vehicles (%)		2		0						0							
Proportion Time Blocked																	
Percent Grade (%)		0															
Right Turn Channelized		No															
Median Type   Storage		Undivided															
<b>Critical and Follow-up Headways</b>																	
Base Critical Headway (sec)		7.1		6.2									4.1				
Critical Headway (sec)		6.42		6.20									4.10				
Base Follow-Up Headway (sec)		3.5		3.3									2.2				
Follow-Up Headway (sec)		3.52		3.30									2.20				
<b>Delay, Queue Length, and Level of Service</b>																	
Flow Rate, v (veh/h)		48		17									11				
Capacity, c (veh/h)		608		964									1503				
v/c Ratio		0.08		0.02									0.01				
95% Queue Length, Q <sub>95</sub> (veh)		0.3		0.1									0.0				
95% Queue Length, Q <sub>95</sub> (ft)		7.6		2.5									0.0				
Control Delay (s/veh)		11.4		8.8									7.4	0.1			
Level of Service (LOS)		B		A									A	A			
Approach Delay (s/veh)		10.7											0.3				
Approach LOS		B											A				

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	Oakbrook Rd at Holbrook Ln								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Holbrook Ln								
Analysis Year	2027							North/South Street	Oakbrook Dr								
Time Analyzed	AM Peak No Build							Peak Hour Factor	0.92								
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
<b>Lanes</b>																	
<p style="text-align: center;">Major Street North-South</p>																	
<b>Vehicle Volumes and Adjustments</b>																	
Approach	Eastbound				Westbound				Northbound				Southbound				
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Movement		10	11	12		7	8	9	10	1	2	3	4	5	6		
Priority											1	0	0	0	1	0	
Number of Lanes		1	0	1		0	0	0	0	0	1	0	0	0	1	0	
Configuration		L		R						LT						TR	
Volume (veh/h)		44		16						10	264				91	8	
Percent Heavy Vehicles (%)		2		0						0							
Proportion Time Blocked																	
Percent Grade (%)		0															
Right Turn Channelized		No															
Median Type   Storage		Undivided															
<b>Critical and Follow-up Headways</b>																	
Base Critical Headway (sec)		7.1		6.2									4.1				
Critical Headway (sec)		6.42		6.20									4.10				
Base Follow-Up Headway (sec)		3.5		3.3									2.2				
Follow-Up Headway (sec)		3.52		3.30									2.20				
<b>Delay, Queue Length, and Level of Service</b>																	
Flow Rate, v (veh/h)		48		17									11				
Capacity, c (veh/h)		591		957									1498				
v/c Ratio		0.08		0.02									0.01				
95% Queue Length, Q <sub>95</sub> (veh)		0.3		0.1									0.0				
95% Queue Length, Q <sub>95</sub> (ft)		7.6		2.5									0.0				
Control Delay (s/veh)		11.6		8.8									7.4	0.1			
Level of Service (LOS)		B		A									A	A			
Approach Delay (s/veh)		10.9											0.3				
Approach LOS		B											A				

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	Oakbrook Rd at Holbrook Ln							
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Holbrook Ln							
Analysis Year	2027							North/South Street	Oakbrook Dr							
Time Analyzed	AM Peak Build							Peak Hour Factor	0.92							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
Lanes																
Vehicle Volumes and Adjustments																
Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		1	0	1		0	0	0	0	0	1	0	0	0	1	0
Configuration		L		R						LT						TR
Volume (veh/h)		78		16						10	264				91	18
Percent Heavy Vehicles (%)		2		0						0						
Proportion Time Blocked																
Percent Grade (%)		0														
Right Turn Channelized		No														
Median Type   Storage		Undivided														
Critical and Follow-up Headways																
Base Critical Headway (sec)		7.1		6.2									4.1			
Critical Headway (sec)		6.42		6.20									4.10			
Base Follow-Up Headway (sec)		3.5		3.3									2.2			
Follow-Up Headway (sec)		3.52		3.30									2.20			
Delay, Queue Length, and Level of Service																
Flow Rate, v (veh/h)		83		17									11			
Capacity, c (veh/h)		587		950									1482			
v/c Ratio		0.14		0.02									0.01			
95% Queue Length, Q <sub>95</sub> (veh)		0.5		0.1									0.0			
95% Queue Length, Q <sub>95</sub> (ft)		12.7		2.5									0.0			
Control Delay (s/veh)		12.1		8.9									7.4	0.1		
Level of Service (LOS)		B		A									A	A		
Approach Delay (s/veh)		11.6											0.3			
Approach LOS		B											A			

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	Oakbrook Rd at Holbrook Ln								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Holbrook Ln								
Analysis Year	2024							North/South Street	Oakbrook Dr								
Time Analyzed	PM Peak							Peak Hour Factor	0.94								
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
<b>Lanes</b>																	
<b>Vehicle Volumes and Adjustments</b>																	
Approach	Eastbound				Westbound				Northbound				Southbound				
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Movement		10	11	12		7	8	9	10	1	2	3	4	5	6		
Priority																	
Number of Lanes		1	0	1		0	0	0		0	1	0		0	1	0	
Configuration		L		R						LT						TR	
Volume (veh/h)		15		24						26	212				310	28	
Percent Heavy Vehicles (%)		0		0						0							
Proportion Time Blocked																	
Percent Grade (%)	0																
Right Turn Channelized	No																
Median Type   Storage	Undivided																
<b>Critical and Follow-up Headways</b>																	
Base Critical Headway (sec)		7.1		6.2									4.1				
Critical Headway (sec)		6.40		6.20									4.10				
Base Follow-Up Headway (sec)		3.5		3.3									2.2				
Follow-Up Headway (sec)		3.50		3.30									2.20				
<b>Delay, Queue Length, and Level of Service</b>																	
Flow Rate, v (veh/h)		16		26									28				
Capacity, c (veh/h)		440		703									1210				
v/c Ratio		0.04		0.04									0.02				
95% Queue Length, Q <sub>95</sub> (veh)		0.1		0.1									0.1				
95% Queue Length, Q <sub>95</sub> (ft)		2.5		2.5									2.5				
Control Delay (s/veh)		13.5		10.3									8.0	0.2			
Level of Service (LOS)		B		B									A	A			
Approach Delay (s/veh)		11.5												1.1			
Approach LOS		B												A			

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																
General Information								Site Information								
Analyst	DBZ							Intersection	Oakbrook Rd at Holbrook Ln							
Agency/Co.	Diana B. Zimmerman Traffic Engineering LLC							Jurisdiction								
Date Performed	9/11/2024							East/West Street	Holbrook Ln							
Analysis Year	2027							North/South Street	Oakbrook Dr							
Time Analyzed	PM Peak No Build							Peak Hour Factor	0.94							
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25							
Project Description	Hickory															
Lanes																
<p>Major Street North-South</p>																
Vehicle Volumes and Adjustments																
Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		1	0	1		0	0	0	0	0	1	0	0	0	1	0
Configuration		L		R						LT						TR
Volume (veh/h)		15		24						26	225				320	28
Percent Heavy Vehicles (%)		0		0						0						
Proportion Time Blocked																
Percent Grade (%)		0														
Right Turn Channelized		No														
Median Type   Storage		Undivided														
Critical and Follow-up Headways																
Base Critical Headway (sec)		7.1		6.2									4.1			
Critical Headway (sec)		6.40		6.20									4.10			
Base Follow-Up Headway (sec)		3.5		3.3									2.2			
Follow-Up Headway (sec)		3.90		3.30									2.20			
Delay, Queue Length, and Level of Service																
Flow Rate, v (veh/h)		16		26									26			
Capacity, c (veh/h)		420		685									1190			
v/c Ratio		0.04		0.04									0.02			
95% Queue Length, Q <sub>95</sub> (veh)		0.1		0.1									0.1			
95% Queue Length, Q <sub>95</sub> (ft)		2.5		2.5									2.5			
Control Delay (s/veh)		13.9		10.5									8.1	0.2		
Level of Service (LOS)		B		B									A	A		
Approach Delay (s/veh)		11.8											1.0			
Approach LOS		B											A			

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	Oakbrook Rd at Holbrook Ln								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Holbrook Ln								
Analysis Year	2027							North/South Street	Oakbrook Dr								
Time Analyzed	PM Peak Build							Peak Hour Factor	0.94								
Intersection Orientation	North-South							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
Lanes																	
Vehicle Volumes and Adjustments																	
Approach	Eastbound				Westbound				Northbound				Southbound				
Movement	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6	
Number of Lanes		1	0	1		0	0	0	0	0	1	0	0	0	1	0	
Configuration		L		R						LT						TR	
Volume (veh/h)		35		24						26	225				329	59	
Percent Heavy Vehicles (%)		0		0						0							
Proportion Time Blocked																	
Percent Grade (%)		0															
Right Turn Channelized		No															
Median Type   Storage		Undivided															
Critical and Follow-up Headways																	
Base Critical Headway (sec)		7.1		6.2									4.1				
Critical Headway (sec)		6.40		6.20									4.10				
Base Follow-Up Headway (sec)		3.5		3.3									2.2				
Follow-Up Headway (sec)		3.50		3.30									2.20				
Delay, Queue Length, and Level of Service																	
Flow Rate, v (veh/h)		37		26									28				
Capacity, c (veh/h)		410		670									1157				
v/c Ratio		0.09		0.04									0.02				
95% Queue Length, Q <sub>95</sub> (veh)		0.3		0.1									0.1				
95% Queue Length, Q <sub>95</sub> (ft)		7.5		2.5									2.5				
Control Delay (s/veh)		14.6		10.6									8.2	0.2			
Level of Service (LOS)		B		B									A	A			
Approach Delay (s/veh)		13.0												1.1			
Approach LOS		B												A			

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	Holbrook Ln at Watson Ln								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Holbrook Ln								
Analysis Year	2027							North/South Street	Watson Ln								
Time Analyzed	AM Peak No Build							Peak Hour Factor	0.75								
Intersection Orientation	East-West							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
<b>Lanes</b>																	
<b>Vehicle Volumes and Adjustments</b>																	
Approach	Eastbound				Westbound				Northbound				Southbound				
Movement	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12	
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0	
Configuration				TR		LT					LR						
Volume (veh/h)			28	0		8	10			1		31					
Percent Heavy Vehicles (%)						12				0		3					
Proportion Time Blocked																	
Percent Grade (%)										0							
Right Turn Channelized																	
Median Type   Storage					Undivided												
<b>Critical and Follow-up Headways</b>																	
Base Critical Headway (sec)						4.1				7.1		6.2					
Critical Headway (sec)						4.22				6.40		6.23					
Base Follow-Up Headway (sec)						2.2				3.5		3.3					
Follow-Up Headway (sec)						2.31				3.50		3.33					
<b>Delay, Queue Length, and Level of Service</b>																	
Flow Rate, v (veh/h)						11				43							
Capacity, c (veh/h)						1511				1028							
v/c Ratio						0.01				0.04							
95% Queue Length, Q <sub>95</sub> (veh)						0.0				0.1							
95% Queue Length, Q <sub>95</sub> (ft)						0.0				2.6							
Control Delay (s/veh)						7.4	0.1			8.7							
Level of Service (LOS)						A	A			A							
Approach Delay (s/veh)						3.3				8.7							
Approach LOS						A				A							

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	Holbrook Ln at Watson Ln								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Holbrook Ln								
Analysis Year	2027							North/South Street	Watson Ln								
Time Analyzed	AM Peak Build							Peak Hour Factor	0.75								
Intersection Orientation	East-West							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
Lanes																	
Vehicle Volumes and Adjustments																	
Approach	Eastbound				Westbound				Northbound				Southbound				
Movement	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12	
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0	
Configuration				TR		LT					LR						
Volume (veh/h)			54	0		10	18			1		37					
Percent Heavy Vehicles (%)						12				0		3					
Proportion Time Blocked																	
Percent Grade (%)										0							
Right Turn Channelized																	
Median Type   Storage					Undivided												
Critical and Follow-up Headways																	
Base Critical Headway (sec)						4.1				7.1		6.2					
Critical Headway (sec)						4.22				6.40		6.23					
Base Follow-Up Headway (sec)						2.2				3.5		3.3					
Follow-Up Headway (sec)						2.31				3.50		3.33					
Delay, Queue Length, and Level of Service																	
Flow Rate, v (veh/h)						13						51					
Capacity, c (veh/h)						1467						984					
v/c Ratio						0.01						0.05					
95% Queue Length, Q <sub>95</sub> (veh)						0.0						0.2					
95% Queue Length, Q <sub>95</sub> (ft)						0.0						5.1					
Control Delay (s/veh)						7.5	0.1					8.9					
Level of Service (LOS)						A	A					A					
Approach Delay (s/veh)						2.7				8.9							
Approach LOS						A				A							

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	Holbrook Ln at Watson Ln								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Holbrook Ln								
Analysis Year	2024							North/South Street	Watson Ln								
Time Analyzed	PM Peak							Peak Hour Factor	0.91								
Intersection Orientation	East-West							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
<b>Lanes</b>																	
<p style="text-align: center;">Major Street East-West</p>																	
<b>Vehicle Volumes and Adjustments</b>																	
Approach	Eastbound				Westbound				Northbound				Southbound				
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12	
Priority																	
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0	
Configuration				TR			LT					LR					
Volume (veh/h)			29	2			12	42			0		10				
Percent Heavy Vehicles (%)							0				0		0				
Proportion Time Blocked																	
Percent Grade (%)											0						
Right Turn Channelized																	
Median Type   Storage	Undivided																
<b>Critical and Follow-up Headways</b>																	
Base Critical Headway (sec)							4.1					7.1		6.2			
Critical Headway (sec)							4.10					6.40		6.20			
Base Follow-Up Headway (sec)							2.2					3.5		3.3			
Follow-Up Headway (sec)							2.20					3.50		3.30			
<b>Delay, Queue Length, and Level of Service</b>																	
Flow Rate, v (veh/h)							13					11					
Capacity, c (veh/h)							1591					1045					
v/c Ratio							0.01					0.01					
95% Queue Length, Q <sub>95</sub> (veh)							0.0					0.0					
95% Queue Length, Q <sub>95</sub> (ft)							0.0					0.0					
Control Delay (s/veh)							7.3	0.1				8.5					
Level of Service (LOS)							A	A				A					
Approach Delay (s/veh)							1.7				8.5						
Approach LOS							A				A						

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	Holbrook Ln at Watson Ln								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Holbrook Ln								
Analysis Year	2027							North/South Street	Watson Ln								
Time Analyzed	PM Peak No Build							Peak Hour Factor	0.91								
Intersection Orientation	East-West							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
<b>Lanes</b>																	
<p style="text-align: center;">Major Street East-West</p>																	
<b>Vehicle Volumes and Adjustments</b>																	
Approach	Eastbound				Westbound				Northbound				Southbound				
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12	
Priority																	
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0	
Configuration				TR		LT					LR						
Volume (veh/h)			29	2		12	42			0		10					
Percent Heavy Vehicles (%)						0				0		0					
Proportion Time Blocked																	
Percent Grade (%)										0							
Right Turn Channelized																	
Median Type   Storage					Undivided												
<b>Critical and Follow-up Headways</b>																	
Base Critical Headway (sec)						4.1				7.1		6.2					
Critical Headway (sec)						4.10				6.40		6.20					
Base Follow-Up Headway (sec)						2.2				3.5		3.3					
Follow-Up Headway (sec)						2.20				3.50		3.30					
<b>Delay, Queue Length, and Level of Service</b>																	
Flow Rate, v (veh/h)						13						11					
Capacity, c (veh/h)						1591						1046					
v/c Ratio						0.01						0.01					
95% Queue Length, Q <sub>95</sub> (veh)						0.0						0.0					
95% Queue Length, Q <sub>95</sub> (ft)						0.0						0.0					
Control Delay (s/veh)						7.3	0.1					8.5					
Level of Service (LOS)						A	A					A					
Approach Delay (s/veh)						1.7				8.5							
Approach LOS						A				A							



Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

HCS Two-Way Stop-Control Report																	
General Information								Site Information									
Analyst	DBZ							Intersection	Holbrook Ln at Watson Ln								
Agency/Co.	Diane B. Zimmerman Traffic Engineering LLC							Jurisdiction									
Date Performed	9/11/2024							East/West Street	Holbrook Ln								
Analysis Year	2027							North/South Street	Watson Ln								
Time Analyzed	PM Peak Build							Peak Hour Factor	0.91								
Intersection Orientation	East-West							Analysis Time Period (hrs)	0.25								
Project Description	Hickory																
<b>Lanes</b>																	
<b>Vehicle Volumes and Adjustments</b>																	
Approach	Eastbound				Westbound				Northbound				Southbound				
Movement	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12	
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0	
Configuration				TR			LT					LR					
Volume (veh/h)			46	2			16	69					0			13	
Percent Heavy Vehicles (%)							0						0			0	
Proportion Time Blocked																	
Percent Grade (%)													0				
Right Turn Channelized																	
Median Type   Storage																Undivided	
<b>Critical and Follow-up Headways</b>																	
Base Critical Headway (sec)							4.1						7.1			6.2	
Critical Headway (sec)							4.10						6.40			6.20	
Base Follow-Up Headway (sec)							2.2						3.5			3.3	
Follow-Up Headway (sec)							2.20						3.50			3.30	
<b>Delay, Queue Length, and Level of Service</b>																	
Flow Rate, v (veh/h)							18						14				
Capacity, c (veh/h)							1566						1022				
v/c Ratio							0.01						0.01				
95% Queue Length, Q <sub>95</sub> (veh)							0.0						0.0				
95% Queue Length, Q <sub>95</sub> (ft)							0.0						0.0				
Control Delay (s/veh)							7.3	0.1					8.6				
Level of Service (LOS)							A	A					A				
Approach Delay (s/veh)							1.4				8.5						
Approach LOS							A				A						

Hickory Burlington & Poplar Pointe Apartments  
Traffic Impact Study

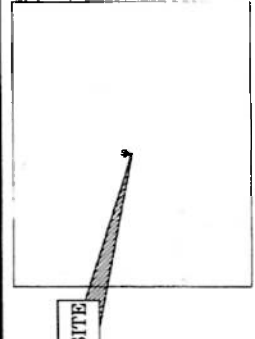
I, Diane Bridwell Zimmerman, certify that this Traffic Impact Study has been prepared under my direct supervision, that I am a Professional Engineer registered in the State of Kentucky and have successfully completed the Traffic Impact Study Requirements training course required by KYTC. Furthermore, I certify that this study has been completed in accordance with the KYTC Traffic Impact Study Requirements and in accordance with engineering standards of practice. The results presented have been determined to be accurate representations of existing and anticipated conditions based on the assumptions and methodologies presented in this report.

Diane Bridwell Zimmerman, Professional Engineer License #16462

 College of Engineering <small>Kentucky Transportation Center</small>	<b>TECHNOLOGY TRANSFER PROGRAM</b>
<b>TRAFFIC IMPACT STUDY COURSE Certificate of Completion (3.5 PDH)</b>	
Diane Zimmerman KY PE License No. 16462	<b>TIM THARPE</b> _____ Tim Tharpe, KYTC Director of Traffic Operations
Completed: 02/18/2022 Expires: 02/18/2026 Company: University of Kentucky	 _____ Adam Kirk, Instructor
<b>The official status of this certificate can be verified with the KYTC Division of Traffic Operations</b>	

# HICKORY BURLINGTON & POPLAR POINTE APARTMENTS

PLANS FOR THE CONSTRUCTION OF A NEW RESIDENTIAL, PLANNED DEVELOPMENT KNOWN AS:  
**LARIAT WAY, BOONE COUNTY, KY**  
 JUNE, 2024



**OWNER:**  
 HICKORY BURLINGTON & POPLAR POINTE APARTMENTS  
 4240 AIRPORT ROAD, SUITE 211  
 CONCORDIA, OHIO 45228

**PRELIMINARY**

**EVANS ENGINEERING**  
 4240 AIRPORT ROAD, SUITE 211  
 CONCORDIA, OHIO 45228  
 (513) 321-2168

**REVISIONS**

NO.	DATE	BY	DESCRIPTION

**COVER SHEET**  
 HICKORY BURLINGTON & POPLAR POINTE APARTMENTS  
 LARIAT WAY,  
 BURLINGTON, BOONE COUNTY, KY

**SHEET NO.**  
 C-1

**SHEET INDEX:**

- C-1 COVER SHEET
- C-2 ZONE INFORMATION
- C-3 IMPERVIOUS SURFACE RATIO
- C-4 CIVIL ENGINEERING
- C-5 SURVEYING
- C-6 LOCATION MAP
- C-7 LEGEND
- C-8 DEPARTMENT UTILITY TABLE
- C-9 CALL BEFORE YOU DIG
- C-10 RECEIPT
- C-11 PHOTOGRAPHS
- C-12 LEGAL DESCRIPTION
- C-13 PLANNING COMMISSION
- C-14 RECEIPT
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- C-99 PLANNING COMMISSION
- C-100 RECEIPT

**IMPERVIOUS SURFACE RATIO:**

TOTAL SITE = 444 ACRES  
 IMPERVIOUS SURFACE AREA = 18.43 ACRES  
 PERCENTAGE IMPERVIOUS SURFACE AREA = 4.15%

**ZONE INFORMATION:**

ZONE: SUBURBAN RESIDENTIAL ONE (SR-1)  
 MAX DENSITY = 4 U.S. UNITS PER ACRE ± 50% WORKSPACE W/P/D = 8 UNITS/ACRE  
 MAX BUILDING HEIGHT = 35' ± 5' ± 10' ± 15' ± 20' ± 25' ± 30' ± 35' ± 40' ± 45' ± 50' ± 55' ± 60' ± 65' ± 70' ± 75' ± 80' ± 85' ± 90' ± 95' ± 100'

**NOTES:**

1. OWNER TO OBTAIN RECORDING PERMITS FROM BOONE COUNTY PLANNING COMMISSION AND THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA).
2. PROPERTY ADDRESS LARIAT WAY, WATSON LN, HENDERSON, KY.
3. FUTURE USE = VACANT.
4. ALL PROPOSED UTILITIES TO BE UNDERGROUND.
5. TELEPHONE = OVERHEAD BELL.
6. ALL PROPOSED UTILITIES TO BE UNDERGROUND.
7. GAS = GAS ENERGY.
8. ELECTRIC = GREY ELECTRIC & DUKES.
9. TELEPHONE = OVERHEAD BELL.
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**LEGEND**

- 1.1 HALLWAY
- 1.2 CANYON BOX
- 1.3 TELEPHONE BOX
- 1.4 ELECTRIC BOX
- 1.5 TRANSFORMER
- 1.6 GAS VALVE
- 1.7 WATER METER
- 1.8 WATER VALVE
- 1.9 FIRE HYDRANT
- 1.10 CATCH BASIN
- 1.11 SINGLE GATE MET
- 1.12 DOUBLE GATE MET
- 1.13 STORM PIPE
- 1.14 STORM MANHOLE
- 1.15 SEWER MANHOLE
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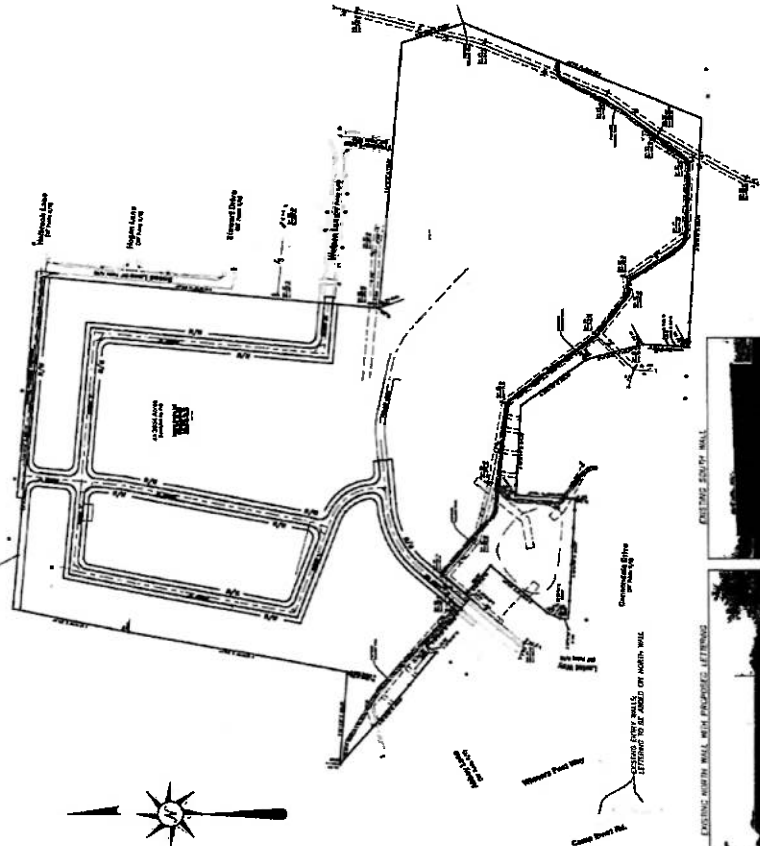
**DEPARTMENT UTILITY TABLE:**

DEPARTMENT	ADDRESS	CONTACT PERSON	PHONE
BOONE COUNTY PLANNING COMMISSION	100 N. 3RD ST., SUITE 201, BOONE, KY 40303	BOONE COUNTY PLANNING COMMISSION	606-338-1100
BOONE COUNTY PUBLIC WORKS DEPARTMENT	100 N. 3RD ST., SUITE 201, BOONE, KY 40303	BOONE COUNTY PUBLIC WORKS DEPARTMENT	606-338-1100
BOONE COUNTY WATER DISTRICT	100 N. 3RD ST., SUITE 201, BOONE, KY 40303	BOONE COUNTY WATER DISTRICT	606-338-1100
BOONE COUNTY SEWER DISTRICT	100 N. 3RD ST., SUITE 201, BOONE, KY 40303	BOONE COUNTY SEWER DISTRICT	606-338-1100
BOONE COUNTY ELECTRIC & GAS	100 N. 3RD ST., SUITE 201, BOONE, KY 40303	BOONE COUNTY ELECTRIC & GAS	606-338-1100
BOONE COUNTY TELEPHONE	100 N. 3RD ST., SUITE 201, BOONE, KY 40303	BOONE COUNTY TELEPHONE	606-338-1100

**GRAPHIC SCALE**  
 1 inch = 100 feet

**CALL BEFORE YOU DIG**

BOONE COUNTY PLANNING COMMISSION  
 100 N. 3RD ST., SUITE 201, BOONE, KY 40303  
 606-338-1100



**DEPARTMENT UTILITY TABLE:**

DEPARTMENT	ADDRESS	CONTACT PERSON	PHONE
BOONE COUNTY PLANNING COMMISSION	100 N. 3RD ST., SUITE 201, BOONE, KY 40303	BOONE COUNTY PLANNING COMMISSION	606-338-1100
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BOONE COUNTY TELEPHONE	100 N. 3RD ST., SUITE 201, BOONE, KY 40303	BOONE COUNTY TELEPHONE	606-338-1100

**RECEIPT**  
 JUN 21 2024  
 BOONE COUNTY PLANNING COMMISSION

**CALL BEFORE YOU DIG**

BOONE COUNTY PLANNING COMMISSION  
 100 N. 3RD ST., SUITE 201, BOONE, KY 40303  
 606-338-1100

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BOONE COUNTY TELEPHONE	100 N. 3RD ST., SUITE 201, BOONE, KY 40303	BOONE COUNTY TELEPHONE	606-338-1100

**PRELIMINARY**  
NOT FOR CONSTRUCTION

DATE	BY	NO. & DESCRIPTION

**EVANS ENGINEERING**  
4540 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45226  
(513) 321-2168

**OVERALL ZONING PLAN**  
RECORDING ON A ZONING PLAN  
APARTMENT  
REXINGTON, BOONE COUNTY, KY  
L1347 WVA  
DATE: 11-20-13  
SCALE: 1" = 50'

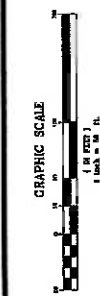
SHEET NO. C-2

**SITE DATA:**

- TOTAL PROJECT ACRES: 44.37 AC.  
REVENUE: PROJECT ACRES: 40.06 AC  
STREET FRONT-OF-WAY ACRES: 4.31 AC
- PROJECT WILL BE DIVIDED INTO 139 UNITS AND 1341 SQUARES OF RESIDENTIAL USE.
- FOR THE MULTI-FAMILY PROJECT, PER THE "MULTI-FAMILY PROJECT" ACT.
- PERMITTED ATTACHED HOUSING: 130 UNITS  
PATIO HOMES (DETACHED) - 130 UNITS  
SINGLE-FAMILY DETACHED HOUSING - 130 UNITS  
CONVERTED SINGLE-FAMILY HOUSING AL. RESIDENTIAL UNITS  
207 UNITS/44,327 SQUARE FEET AS SHOWN ON PLAN.
- 50% OF THE TOTAL UNIT COUNT SHALL BE AFFORDABLE HOUSING UNITS.  
• 43 (9.5%) SQT LOW/ 17-20% (LOWEN)  
• RFR (RESIDENTIAL SQT LOW/ 0-1% SLOPES)  
• RSD (RESIDENTIAL SQT LOW/ 0-1% SLOPES)

**GENERAL NOTES:**

- LOT LINES AS SHOWN ON CONCEPT DEVELOPMENT PLAN SHOULD BE RECHECKED AND ON NEW AND OLD JOBS. SIMILAR TO EXISTING LINES SHALL MEET THE REQUIREMENTS OF THE BOONE COUNTY ZONING ORDINANCE, ARTICLE 33.
- ALL UTILITIES SHALL BE SHOWN AND DEPTH SHALL BE INDICATED.
- ALL UTILITIES SHALL BE SHOWN AND DEPTH SHALL BE INDICATED.
- ALL UTILITIES SHALL BE SHOWN AND DEPTH SHALL BE INDICATED.



**\*CALL BEFORE YOU DIG\***  
BEFORE ANY CONSTRUCTION SHALL BE PERMITTED, THE LOCATION AND DEPTH OF ALL UTILITIES SHALL BE DETERMINED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES.

**APPROX 139 UNITS + LEASING OFFICE**



**PRELIMINARY**  
NOT FOR CONSTRUCTION

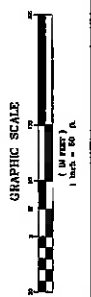
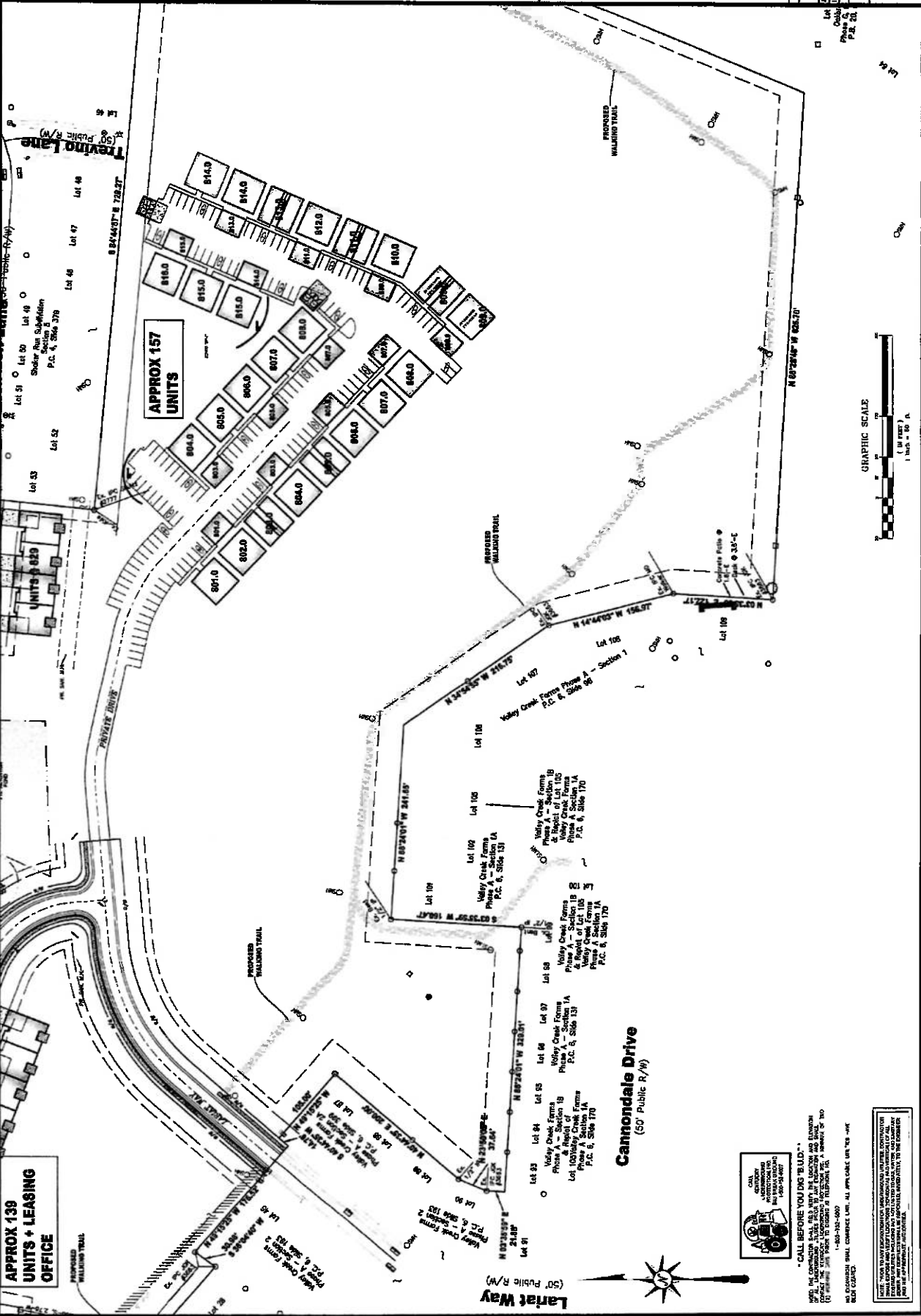
DATE	BY	NO. & DESCRIPTION

**EVANS ENGINEERING**  
4420 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45226  
(513) 231-2108

**OVERALL ZONING PLAN**  
APARTMENTS  
: HICKORY HUNTINGTON & FORKLAND  
LARIAT WAY,  
BURLINGTON, BOONE COUNTY, KY

NAME	DATE	SCALE

SHEET NO. C-2.1



**APPROX 139 UNITS + LEASING OFFICE**



\*CALL BEFORE YOU DIG TO LULU!  
WHO: THE CONTRACTOR SHALL CALL THE LOCAL UTILITY AND DESIGN  
OFFICE, IMMEDIATELY PRIOR TO ANY EXCAVATION OR DRILLING  
OPERATIONS TO IDENTIFY THE LOCATION AND DEPTH OF ALL  
EXISTING UTILITIES AND TO OBTAIN NECESSARY PERMITS TO THE CONTRACTOR.  
\*402-342-0000  
\*402-342-0000  
NOX COUNCIL

THIS PLAN IS THE PROPERTY OF EVANS ENGINEERING AND IS NOT TO BE REPRODUCED OR COPIED IN ANY MANNER WITHOUT THE WRITTEN PERMISSION OF EVANS ENGINEERING. ANY REPRODUCTION OR COPIING OF THIS PLAN WITHOUT THE WRITTEN PERMISSION OF EVANS ENGINEERING IS STRICTLY PROHIBITED.



**PRELIMINARY**  
 DEVELOPMENT PLAN

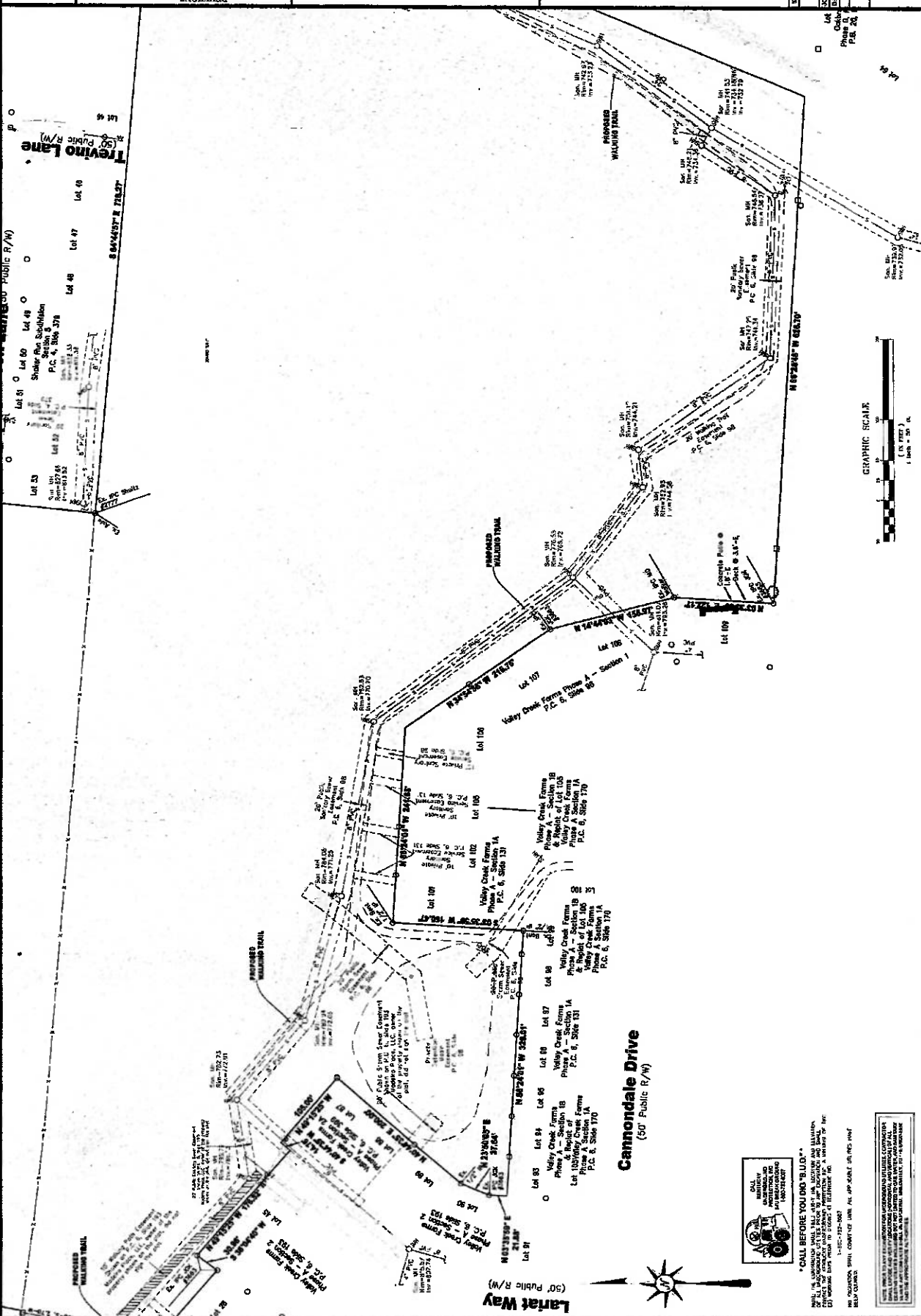
NO.	DATE	NO. & DESCRIPTION

**EVANS ENGINEERING**  
 4240 AIRPORT ROAD, SUITE 211  
 CHEVY CHASE, OHIO 43026  
 (614) 221-6158

**EXISTING CONCEPT DEVELOPMENT PLAN**  
 HICKORY BURLINGTON & TOLLAW POINT APARTMENTS  
 LARAT WAY, BURLINGTON, BOONE COUNTY, KY

**SHEET NO. C-3-1**

DATE: 12-15-2011  
 DRAWN BY: J. W. WILSON  
 CHECKED BY: J. W. WILSON  
 PROJECT NO.: 11-00000000



**\*CALL BEFORE YOU DIG\***  
 1-800-485-5847  
 KY 1-800-485-5847

**NOT TO BE USED FOR CONSTRUCTION**  
 THIS PLAN IS THE PROPERTY OF EVANS ENGINEERING AND SHALL BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. IT IS NOT TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF EVANS ENGINEERING.

**PRELIMINARY**  
NOT FOR CONSTRUCTION

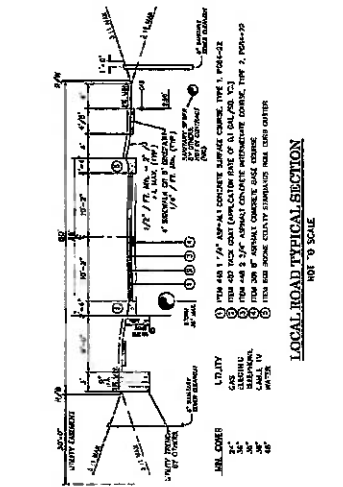
DATE	BY	NO. & REVISION

**EVANS ENGINEERING**  
4340 AIRPORT ROAD, SUITE 214  
CINCINNATI, OHIO 45228  
(513) 321-2188

HICKORY BURLINGTON & FOLAN POINT  
APARTMENTS  
**PROPOSED CONCEPT  
DEVELOPMENT PLAN**  
LARIAT WAY  
MILINGTON, BOONE COUNTY, KY

NO.	DATE	BY	REVISION
1	11-11-10		

**SHEET NO.**  
C-4

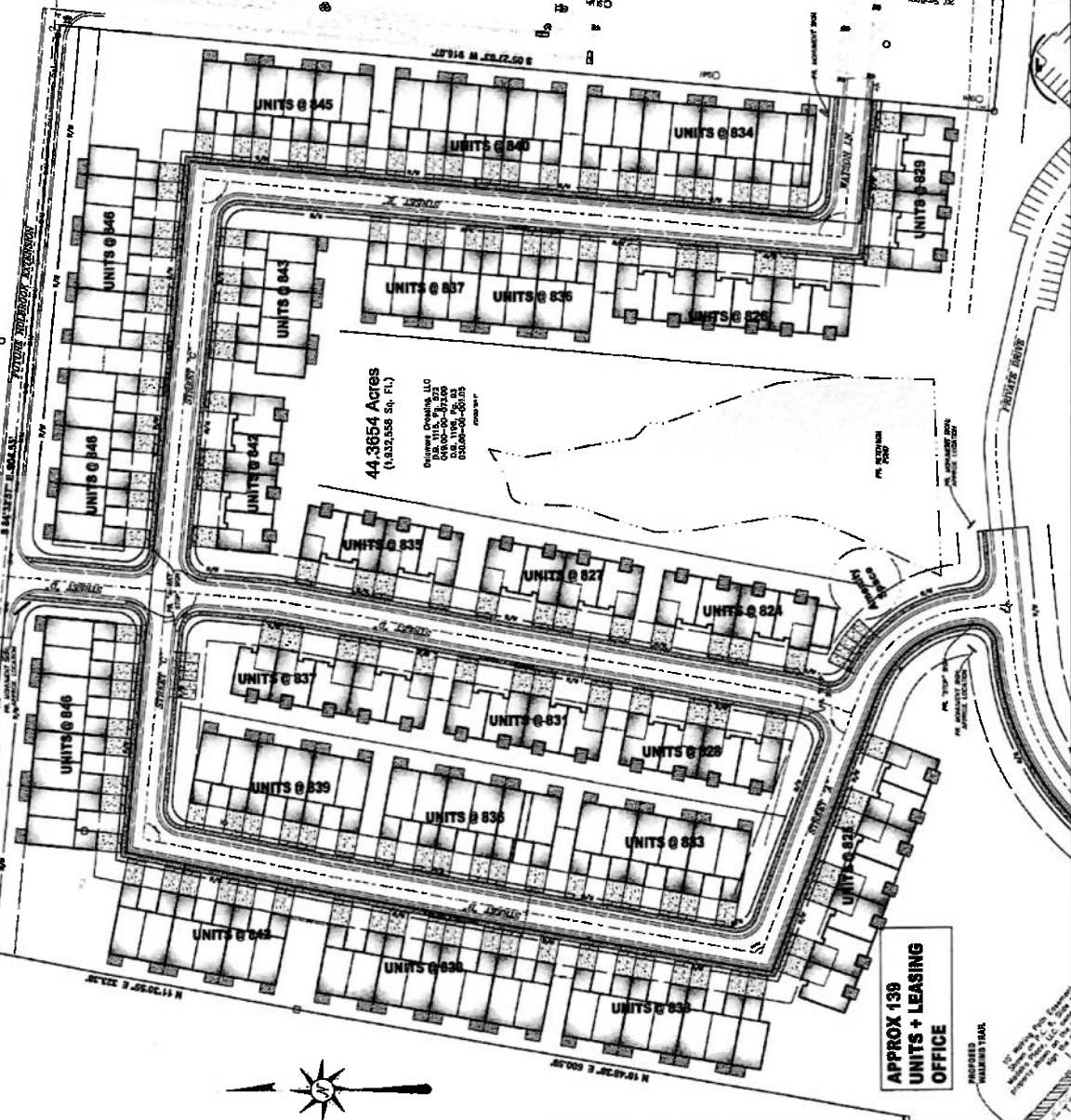


**IMPERVIOUS SURFACE RATIO:**  
TOTAL SITE = 44.4 ACRES  
IMPERVIOUS SURFACE = 16.4 ACRES  
PERVIOUS AREA = 28.0 ACRES  
% P = 63%



**CALL BEFORE YOU DIG BUILDS**  
WE, THE ABOVE-NAMED UTILITY COMPANIES, HAVE BEEN ADVISED THAT YOU ARE CONSIDERING CONSTRUCTION OF A PROJECT THAT MAY CROSS OUR FACILITIES. WE REQUEST THAT YOU CONTACT US AT THE NUMBER LISTED TO THE RIGHT OF THIS MESSAGE TO OBTAIN A "TOLL FREE" IDENTIFICATION NUMBER FOR YOUR PROJECT. THIS NUMBER IS VALID FOR 30 DAYS FROM THE DATE OF THIS MESSAGE. IF YOU DO NOT CONTACT US WITHIN THIS PERIOD, WE WILL BE REQUIRED TO TAKE PRECAUTIONARY MEASURES TO PROTECT OUR FACILITIES. YOUR FAILURE TO CONTACT US MAY BE CAUSED BY THE FOLLOWING:  
1-800-762-0007  
WE REQUEST THAT YOU CONTACT US AT THE NUMBER LISTED TO THE RIGHT OF THIS MESSAGE.

THIS PLAN IS THE PROPERTY OF EVANS ENGINEERING, INC. AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF EVANS ENGINEERING, INC.



**APPROX 139  
UNITS + LEASING  
OFFICE**

PROPOSED  
MARKING THAW

**PRELIMINARY**  
NOT FOR CONSTRUCTION

DATE	NO. & DESCRIPTION

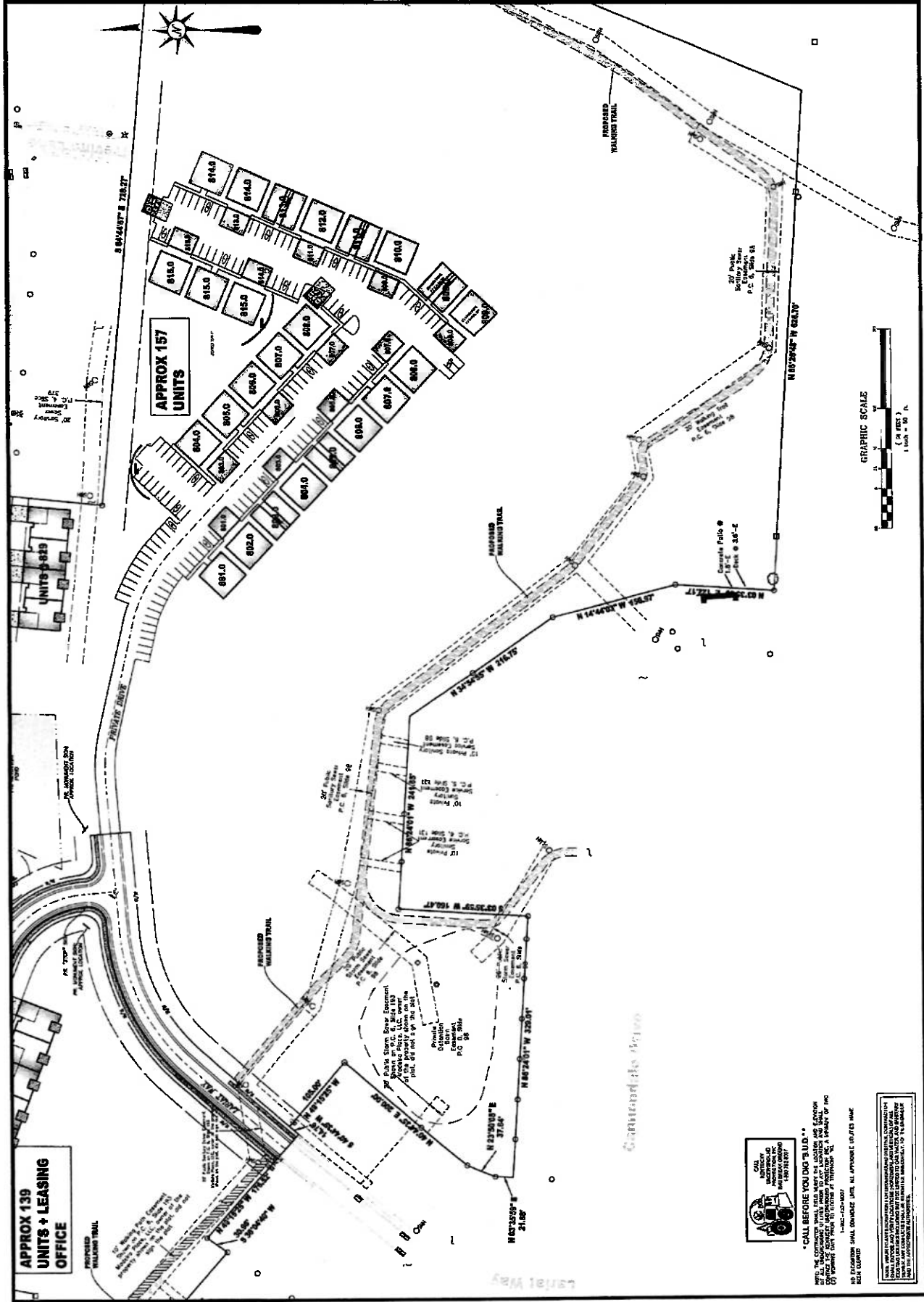
**EVANS ENGINEERING**  
4240 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45226  
(513) 271-2188

**PROPOSED CONCEPT DEVELOPMENT PLAN**  
APARTMENTS  
HICKORY BURLINGTON & KORMAN POINT  
LARKIN WAY  
BURLINGTON, BOONE COUNTY, KY

SHEET NO. **C-4.1**

DATE: 08.15.2024  
SCALE: 1"=50' N/A

APR. 2024 24-101  
JUL. 7, 2024



**APPROX 139 UNITS + LEASING OFFICE**



**CALL BEFORE YOU DIG!**  
CALL 800-4-A-ROOT  
OR 1-800-4-A-ROOT  
FOR A FREE SERVICE

**\* CALL BEFORE YOU DIG! \***  
THE INFORMATION CONTAINED HEREIN IS FOR INFORMATIONAL PURPOSES ONLY AND DOES NOT CONSTITUTE AN OFFER OF ANY FINANCIAL PRODUCT OR SERVICE. THE INFORMATION IS NOT INTENDED TO BE USED AS A BASIS FOR ANY INVESTMENT DECISION. THE INFORMATION IS NOT INTENDED TO BE USED AS A BASIS FOR ANY INVESTMENT DECISION. THE INFORMATION IS NOT INTENDED TO BE USED AS A BASIS FOR ANY INVESTMENT DECISION.



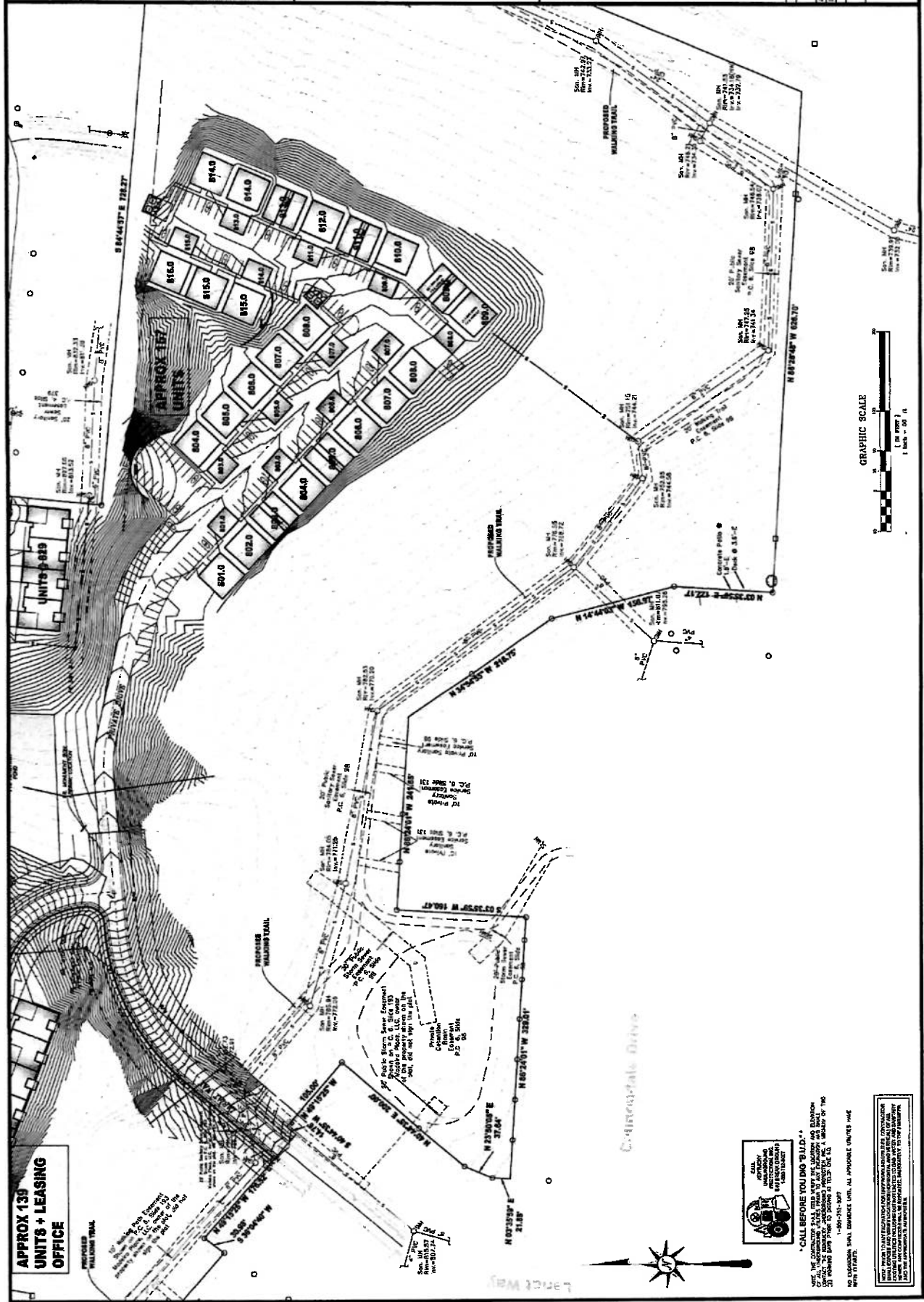
**PRELIMINARY**  
NOT FOR CONSTRUCTION

DATE	BY	NO. & DESCRIPTION

**EVANS ENGINEERING**  
4540 AIRPORT ROAD, SUITE 214  
CINCINNATI, OHIO 45226  
(513) 231-2168

**UTILITY & GRADING PLAN**  
APPROXIMATE  
BENTLEY BUILDING & POWER POINT  
LARKWAY  
BURLINGTON, BOON, COUNTY, KY

SCALE: 1" = 40' H.A.  
DATE: JUL 2, 2024  
SHEET NO. C-5.1



**APPROX 139 UNITS + LEASING OFFICE**



**\*CALL BEFORE YOU DIG 'B.U.D.'\***  
NOTE: THE CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO ANY EXCAVATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO UTILITIES.

NO EXCAVATION SHALL BE MADE UNTIL ALL APPROPRIATE UTILITIES HAVE BEEN LOCATED.  
THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO UTILITIES.

**PROPOSED 1-STORY RESIDENCES FOR: DELAWARE CROSSING, LLC**  
**HOLBROOK LN • LOT 00**

1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042 • BOONE CO. PARCEL ID: 049.00-00-073.00  
 "THE BTR COLLECTION" • CONSTRUCTION DOCUMENTS-1

**BROOKSTONE HOMES, LLC.**

7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200



H O M E S

*Virtual f/x*

10351 KENWOOD RD.  
 BLUE ASH, OHIO 45242  
 (513) 393-0857



Virtual f/x View • Elevation-A

**SHEET INDEX**

TT	THIS SHEET	DATE	BY
TT	THIS SHEET		
DDA.1	E1 20' UNIT * Front Elevation-A & B		
DDA.2	E2 20' UNIT * Other Elevations		
DDA.3	E2 20' Unit * Virtual Tour-A		
DDA.4	1/4" Holbrook 20' * Virtual Tour-A		
DDA.5	1/4" Holbrook 20' * Virtual Tour-A		
DDA.6	1/4" Holbrook 20' * Virtual Tour-A		
DDA.7	1/4" Holbrook 20' * Virtual Tour-A		
DDA.8	1/4" Holbrook 20' * Virtual Tour-A		
DDA.9	1/4" Holbrook 20' * Virtual Tour-A		
DDA.10	1/4" Holbrook 20' * Virtual Tour-A		
DDA.11	1/4" Holbrook 20' * Virtual Tour-A		
DDA.12	1/4" Holbrook 20' * Virtual Tour-A		
DDA.13	1/4" Holbrook 20' * Virtual Tour-A		
DDA.14	1/4" Holbrook 20' * Virtual Tour-A		
DDA.15	1/4" Holbrook 20' * Virtual Tour-A		
DDA.16	1/4" Holbrook 20' * Virtual Tour-A		
DDA.17	1/4" Holbrook 20' * Virtual Tour-A		
DDA.18	1/4" Holbrook 20' * Virtual Tour-A		
DDA.19	1/4" Holbrook 20' * Virtual Tour-A		
DDA.20	1/4" Holbrook 20' * Virtual Tour-A		
DDA.21	1/4" Holbrook 20' * Virtual Tour-A		
DDA.22	1/4" Holbrook 20' * Virtual Tour-A		
DDA.23	1/4" Holbrook 20' * Virtual Tour-A		
DDA.24	1/4" Holbrook 20' * Virtual Tour-A		
DDA.25	1/4" Holbrook 20' * Virtual Tour-A		
DDA.26	1/4" Holbrook 20' * Virtual Tour-A		
DDA.27	1/4" Holbrook 20' * Virtual Tour-A		
DDA.28	1/4" Holbrook 20' * Virtual Tour-A		
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DDA.30	1/4" Holbrook 20' * Virtual Tour-A		
DDA.31	1/4" Holbrook 20' * Virtual Tour-A		
DDA.32	1/4" Holbrook 20' * Virtual Tour-A		
DDA.33	1/4" Holbrook 20' * Virtual Tour-A		
DDA.34	1/4" Holbrook 20' * Virtual Tour-A		
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DDA.38	1/4" Holbrook 20' * Virtual Tour-A		
DDA.39	1/4" Holbrook 20' * Virtual Tour-A		
DDA.40	1/4" Holbrook 20' * Virtual Tour-A		
DDA.41	1/4" Holbrook 20' * Virtual Tour-A		
DDA.42	1/4" Holbrook 20' * Virtual Tour-A		
DDA.43	1/4" Holbrook 20' * Virtual Tour-A		
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DDA.45	1/4" Holbrook 20' * Virtual Tour-A		
DDA.46	1/4" Holbrook 20' * Virtual Tour-A		
DDA.47	1/4" Holbrook 20' * Virtual Tour-A		
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DDA.67	1/4" Holbrook 20' * Virtual Tour-A		
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DDA.70	1/4" Holbrook 20' * Virtual Tour-A		
DDA.71	1/4" Holbrook 20' * Virtual Tour-A		
DDA.72	1/4" Holbrook 20' * Virtual Tour-A		
DDA.73	1/4" Holbrook 20' * Virtual Tour-A		
DDA.74	1/4" Holbrook 20' * Virtual Tour-A		
DDA.75	1/4" Holbrook 20' * Virtual Tour-A		
DDA.76	1/4" Holbrook 20' * Virtual Tour-A		
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DDA.78	1/4" Holbrook 20' * Virtual Tour-A		
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DDA.80	1/4" Holbrook 20' * Virtual Tour-A		
DDA.81	1/4" Holbrook 20' * Virtual Tour-A		
DDA.82	1/4" Holbrook 20' * Virtual Tour-A		
DDA.83	1/4" Holbrook 20' * Virtual Tour-A		
DDA.84	1/4" Holbrook 20' * Virtual Tour-A		
DDA.85	1/4" Holbrook 20' * Virtual Tour-A		
DDA.86	1/4" Holbrook 20' * Virtual Tour-A		
DDA.87	1/4" Holbrook 20' * Virtual Tour-A		
DDA.88	1/4" Holbrook 20' * Virtual Tour-A		
DDA.89	1/4" Holbrook 20' * Virtual Tour-A		
DDA.90	1/4" Holbrook 20' * Virtual Tour-A		
DDA.91	1/4" Holbrook 20' * Virtual Tour-A		
DDA.92	1/4" Holbrook 20' * Virtual Tour-A		
DDA.93	1/4" Holbrook 20' * Virtual Tour-A		
DDA.94	1/4" Holbrook 20' * Virtual Tour-A		
DDA.95	1/4" Holbrook 20' * Virtual Tour-A		
DDA.96	1/4" Holbrook 20' * Virtual Tour-A		
DDA.97	1/4" Holbrook 20' * Virtual Tour-A		
DDA.98	1/4" Holbrook 20' * Virtual Tour-A		
DDA.99	1/4" Holbrook 20' * Virtual Tour-A		
DDA.100	1/4" Holbrook 20' * Virtual Tour-A		

Square Footages:	28' Units	38' Units
• First Floor-A	= 1,324 sqft	= 1,487 sqft
• First Floor-A (End Unit)	= 1,384 sqft	= N/A sqft
• 1st Floor Slab	= 1,301 sqft	= 1,430 sqft
• 2nd Floor Slab (End Unit)	= 1,329 sqft	= N/A sqft
• Garage Slab	= 372 sqft	= 372 sqft
• Front Porch Slab	= 40 sqft	= 40 sqft
• Rear 12 x 8 Pads	= 96 sqft	= 96 sqft

**CODE INFORMATION**  
 \* REFERENCED CODE: INTERNATIONAL CODE OF BUILDING \* IRC-2003  
 \* BUILDING CLASSIFICATION: RESIDENTIAL  
 \* FOUNDATION TYPE: \* PERMITS  
 \* ROOF LIVE LOAD: 20 PSF  
 \* FLOOR LIVE LOAD: 40 PSF  
 \* WIND LOAD: # 30 PSF  
 \* FLOOR JOIST: # 40 PSF

**OWNER**  
 DELAWARE CROSSING, LLC  
**RESIDENTIAL SERVICES**  
 VIRTUAL F/X  
 MICHAEL F. FISHER  
**CONTRACTOR**  
 BROOKSTONE HOMES, LLC.

**DATE:** 06/24/24 • DBI  
**REVISED:**  
**RELEASE:**  
**PERMIT RELEASE:**

**PROPERTY INFORMATION**  
 20 AC ON HOLBROOK LN (49-73)

SHEET NO. 1 OF 29

**T1**  
Title Sheet

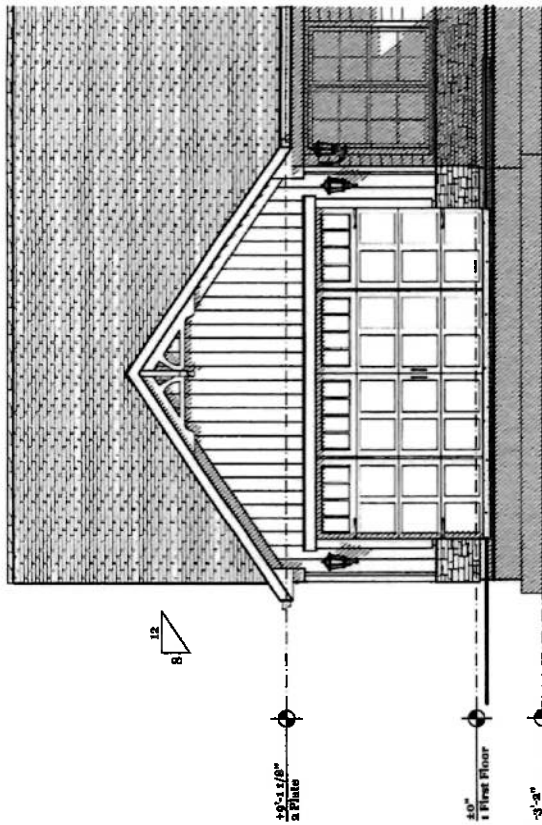
**HBOO**  
The Multi-Fam's  
EIA • C. Yates  
For Delaware Crossing, LLC  
1841 Holbrook Lane • Florence, Kentucky 41042  
BROOKSTONE HOMES, LLC  
7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200

**BROOKSTONE**

VIRTUAL F/X

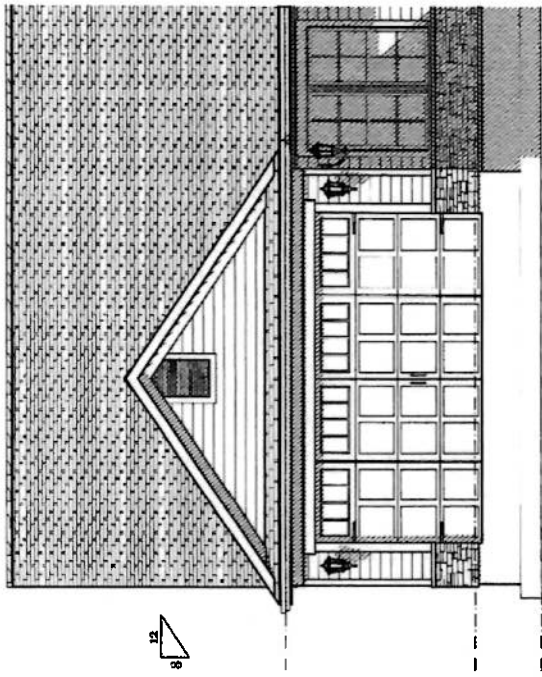
10351 Kenwood Rd. • Blue Ash, OH 45242  
Designed By: P. Michael Fisher • (513) 393-0857  
Email: virtual.f@cloud.com

**Virtual f/x**



**Holbrook Ln. • 28' Unit • Front Elevation-A**  
 $3/16" = 1'-0"$

E1



**Holbrook Ln. • 28' Unit • Front Elevation-B**  
 $3/16" = 1'-0"$

E1

ALL SMOKE ALARMS SHALL BE LISTED IN ACCORDANCE WITH UL-267, ON EACH LEVEL WITHIN EACH DWELLING UNIT SMOKE ALARMS UTILIZING PHOTOVOLTAIC AND IONIZATION TECHNOLOGIES SHALL BE INSTALLED IN EACH SLEEPING AREA. SMOKE ALARMS SHALL BE PHOTOVOLTAIC TYPE. OUTSIDE BEDROOMS TO BE PHOTOVOLTAIC TYPE. SEPARATE OR DUAL-BEeping SMOKE ALARMS MAY BE USED.

**EXHAUST FAN & SMOKE DETECTOR NOTE:**  
 ● DENOTES EXHAUST FAN LOCATIONS (SEE NOTE B & D FOR CODE INFO ON SHEET A-6)  
 ○ DENOTES SMOKE DETECTOR LOCATIONS (SEE NOTE E ON SHEET A-5 & THE NOTE TO THE LEFT FOR ALL SMOKE DETECTOR CODE INFO)  
 ○ DENOTES SMOKE/CO COMBO DETECTOR LOCATIONS (PROGRAMMABLE THERMOSTAT RESET FOR ALL FORCED AIR FURNACES)

**WALL BRACING METHOD**  
 PER ICC 602.10.4  
 OS-WF, CONTINUOUSLY SHEATHED WOOD STRUCTURAL PANEL, 1/2" MIN. FASTENERS COMPLY WITH ICC 602.20.2 INTERIOR SHEATHING ON PLACED WALLS SHALL BE 1/2" MIN. GYPSUM BUT FASTENED IN ACCORDANCE WITH ICC 602.20.2 TABLE PREVIOUS

**Note**  
 All Downspouts connected to approved locations

**CODE INFORMATION CONT...**  
 STRUCTURAL MEMBER ALLOWABLE DEFLECTION  
 - Deflection having slope greater than 3:12 with no finished ceiling attached is values L/180  
 - Deflection with non-partition L/100  
 - All other structural members L/240  
 - Exterior walls with plaster or masonry shall be L/600  
 - Exterior walls with brick shall be L/240  
 - Exterior walls with block shall be L/240  
 - L-Rails, according to manufacturer's instructions L/600

**Virtual f/x**

Designed By: P. Michael Fisher • Suite 2001 • One Ash, Ohio 45412  
 Email: [Virtual.Fisher@red.com](mailto:Virtual.Fisher@red.com)

**BROOKSTONE**

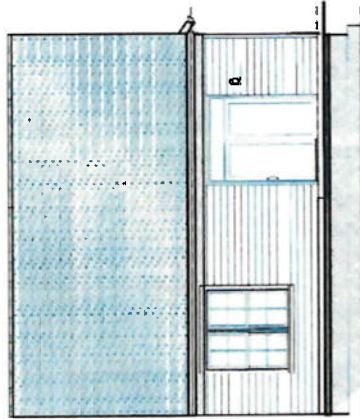
4800/744  
 12345 Main Street  
 Columbus, Ohio 43215

**BROOKSTONE Homes, LLC.**  
 1811 Broomfield Lane • Florence, Kentucky 41042  
 7703 WOOTEN Pkwy • Cincinnati, OH 45227 • 513-340-7200

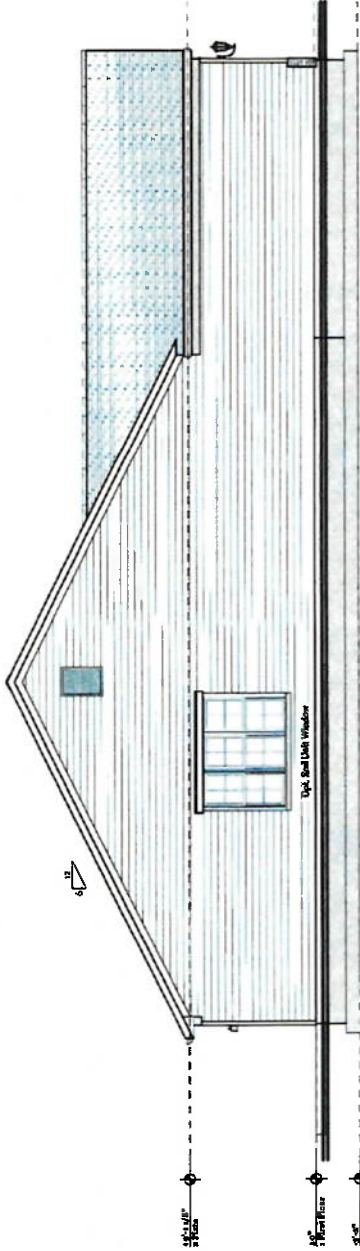
**HBOO The Multi-Farm's**  
 E1-A • C. Yates  
 Fort Delaware Crossing, LLC  
 7703 WOOTEN Pkwy • Cincinnati, OH 45227 • 513-340-7200

**CD**  
 E1-A • Front Elevation-A, B, C

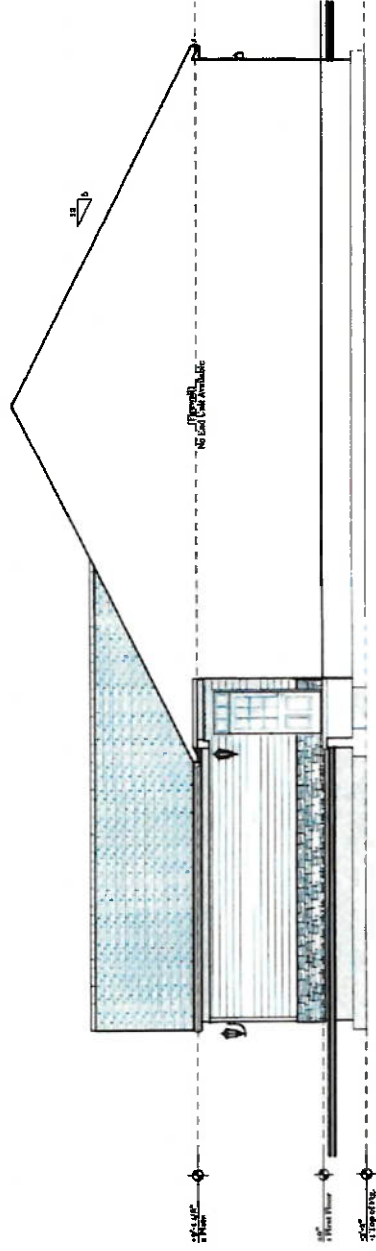
REVISED: 12/20/20



**E2** 28' Unit • Rear Elevation-A  
1/8" = 1'-0"



**E3** Holbrook Ln • 28' Unit • Left Elevation-A  
1/8" = 1'-0"



**E4** Holbrook Ln • 28' Unit • Right Elevation-A  
1/8" = 1'-0"

**Optimal f/x**

Designed By: P. Michael Fisher • (513) 393-0877  
10011 Kenwood Rd. • Suite 2001 • Blue Ash, Ohio 45215  
Email: [pmfisher@ohio.com](mailto:pmfisher@ohio.com)

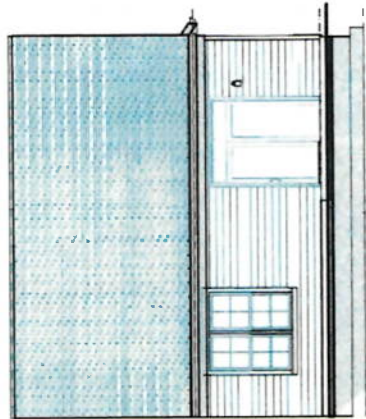
**Optimal f/x**

7210 WORTON PIKE • CINCINNATI, OH 45227 • 513-940-7200  
**BROOKSTONE Homes, LLC.**  
1641 ROSSMORE LANE • FORT WASHINGTON, OH 43082  
For Delaware Crossing, LLC  
E.A. • C. Varles  
**The Multi-Fam's**

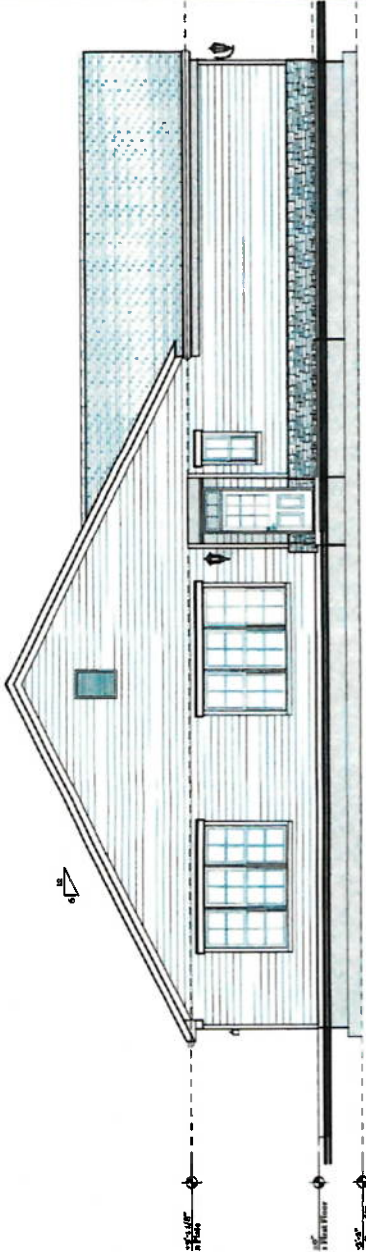
**BROOKSTONE**

**CD**  
For 28' Unit • Other  
Revisions  
**A.2**

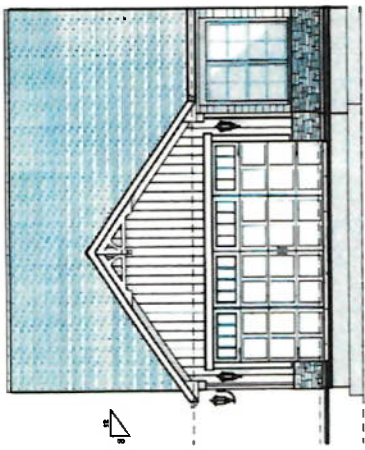
SHEET NO. 12 OF 12



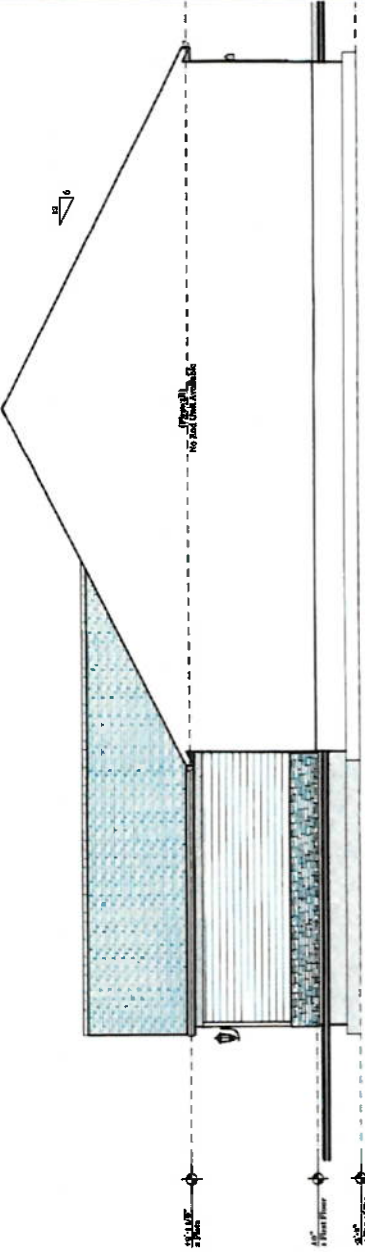
**E2** 28' Unit • Rear Elevation-A  
1/8" = 1'-0"



**E3** Holbrook Ln • 28' End Unit • Left Elevation-A  
1/8" = 1'-0"



**E4** 28' End Unit • Front Elevation-A  
1/8" = 1'-0"



**E4** Holbrook Ln • 28' End Unit • Right Elevation-A  
1/8" = 1'-0"

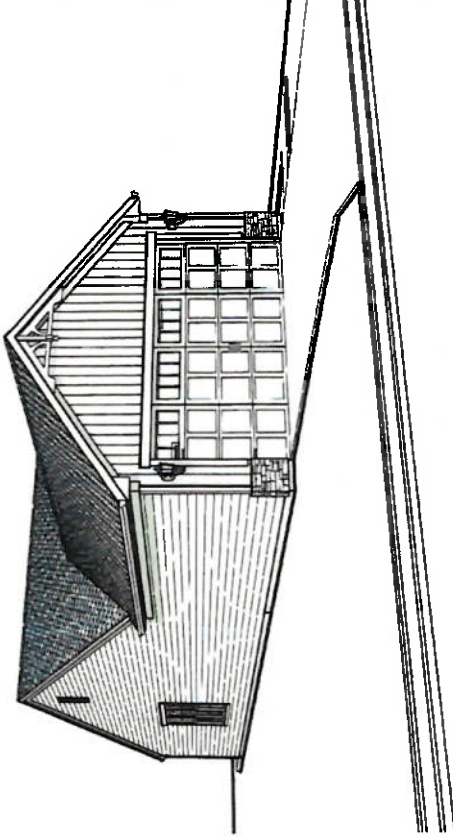
**Virtual f/x**  
 Designed By: P. Michael Fisher • (513) 393-0877  
 1025 Kennel Rd. • Suite 2001 • New Mt., Ohio 45042  
 Email: virtualfx@icloud.com

**Virtual f/x**

**BROOKSTONE**  
 ELEVATIONS  
 2018-10-01

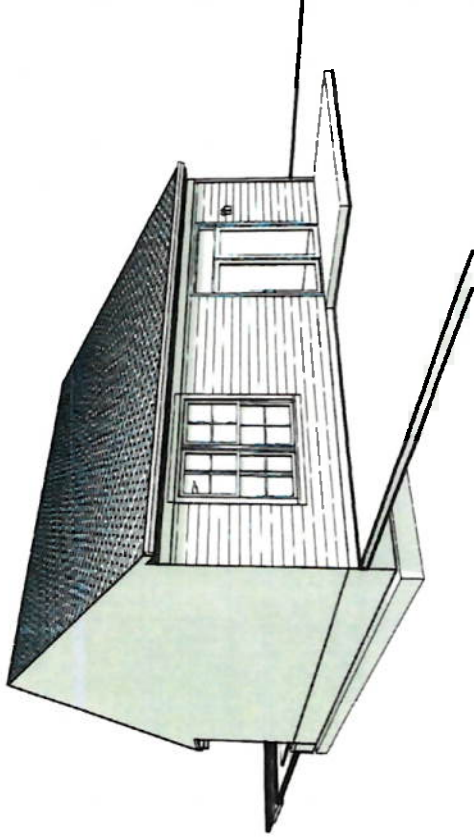
**The Multi-Farm's**  
 E.A. • G. Varles  
 For: Delaware Crossing, LLC  
 1841 Holbrook Lane • Florence, Kentucky 41042  
**BROOKSTONE Homes, LLC.**  
 7203 WOOTEN PKWY • CINCINNATI, OH 45227 • 513-340-7200

**HB00**  
 For 28' End Unit Elevations  
**CD**  
**A.3**  
 SHEET NO. 1 OF 2



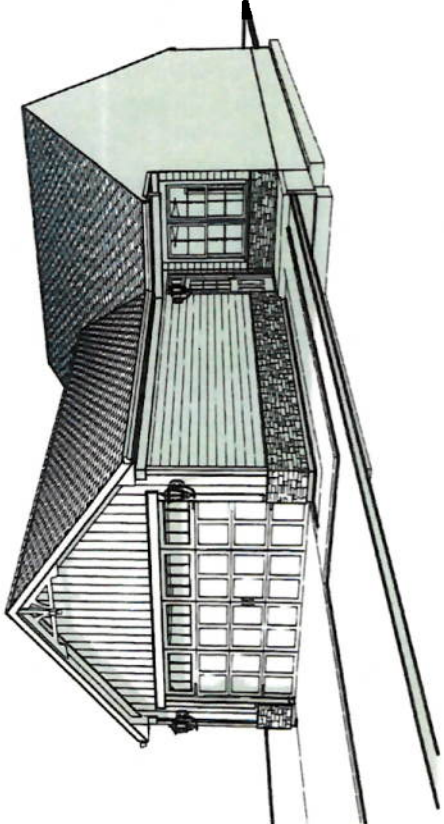
Vfx2

Holbrook 28' Ext • Vfx 3D Tour



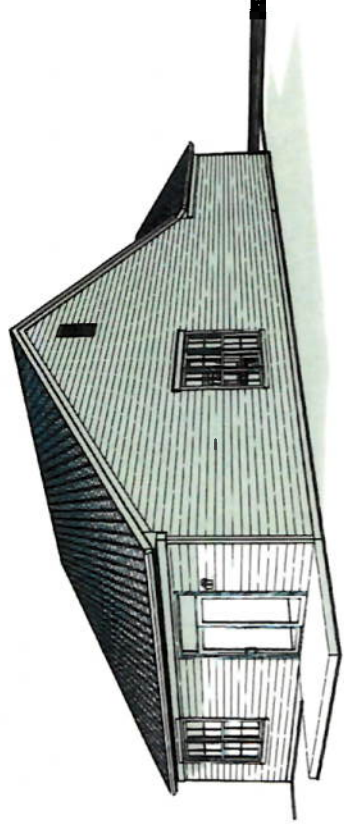
Vfx4

Holbrook 28' Ext • Vfx 3D Tour



Vfx1

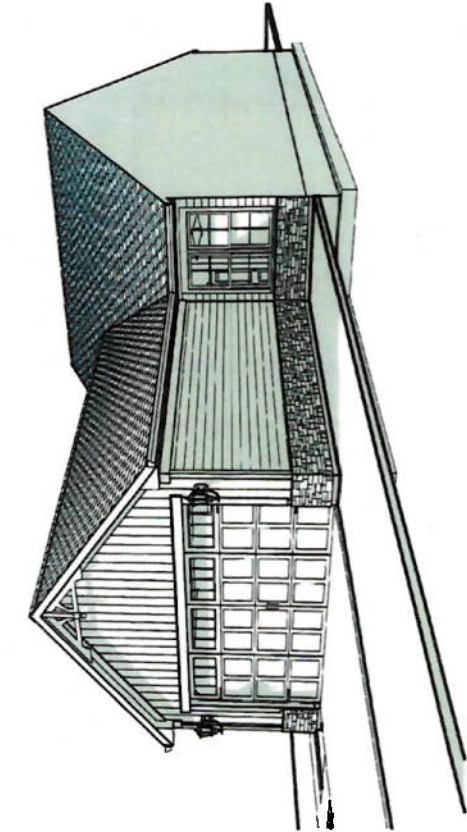
Holbrook 28' Ext • Vfx 3D Tour



Vfx3

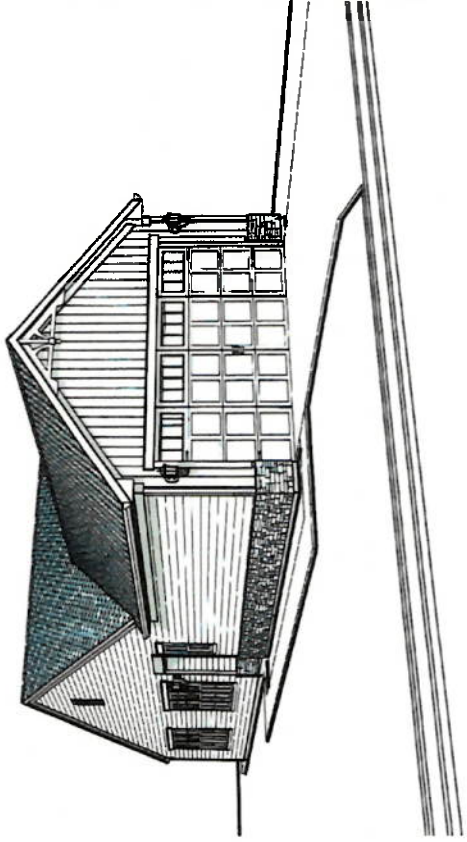
Holbrook 28' Ext • Vfx 3D Tour

<p><b>Vfx/x</b></p>	<p><i>Virtual f/x</i></p> <p>Designed By: Michael Fisher • (203) 993-0887</p> <p>Email: <a href="mailto:virtual_fx@icloud.com">virtual_fx@icloud.com</a></p>	<p>©2013 Virtual f/x</p> <p>1234567890</p>	<p><b>BROOKSTONE</b></p>	<p><b>The Multi-Farm's</b> El-A • G. Varies</p> <p><b>For: Delaware Crossing, LLC</b> 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042</p> <p><b>BROOKSTONE HOMES, LLC</b> 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200</p> <p><b>CD</b> Virtual Tour A</p> <p><b>A.4</b> SHEET NO. 1 OF 2</p>
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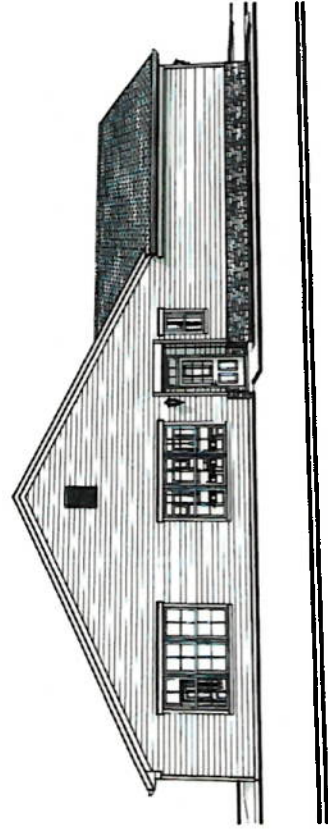
Vfx1

Holbrook 28' End Ext • Vfx 3D Tour



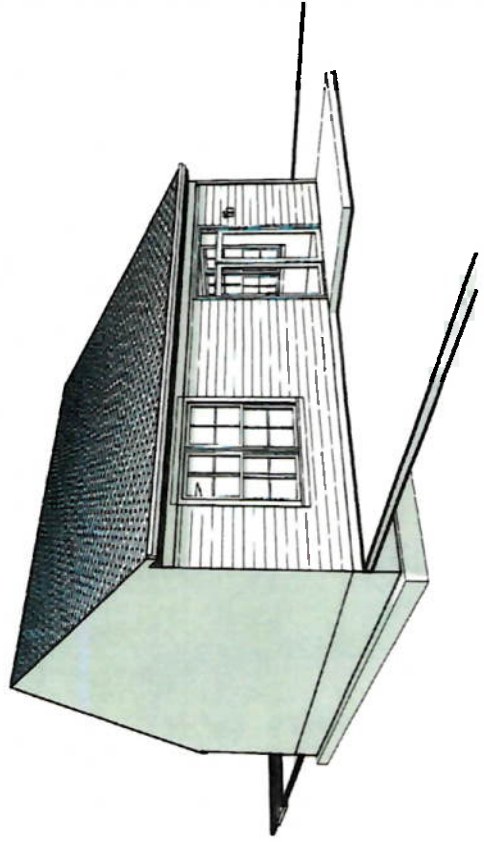
Vfx2

Holbrook 28' End Ext • Vfx 3D Tour



Vfx3

Holbrook 28' End Ext • Vfx 3D Tour



Vfx4

Holbrook 28' End Ext • Vfx 3D Tour



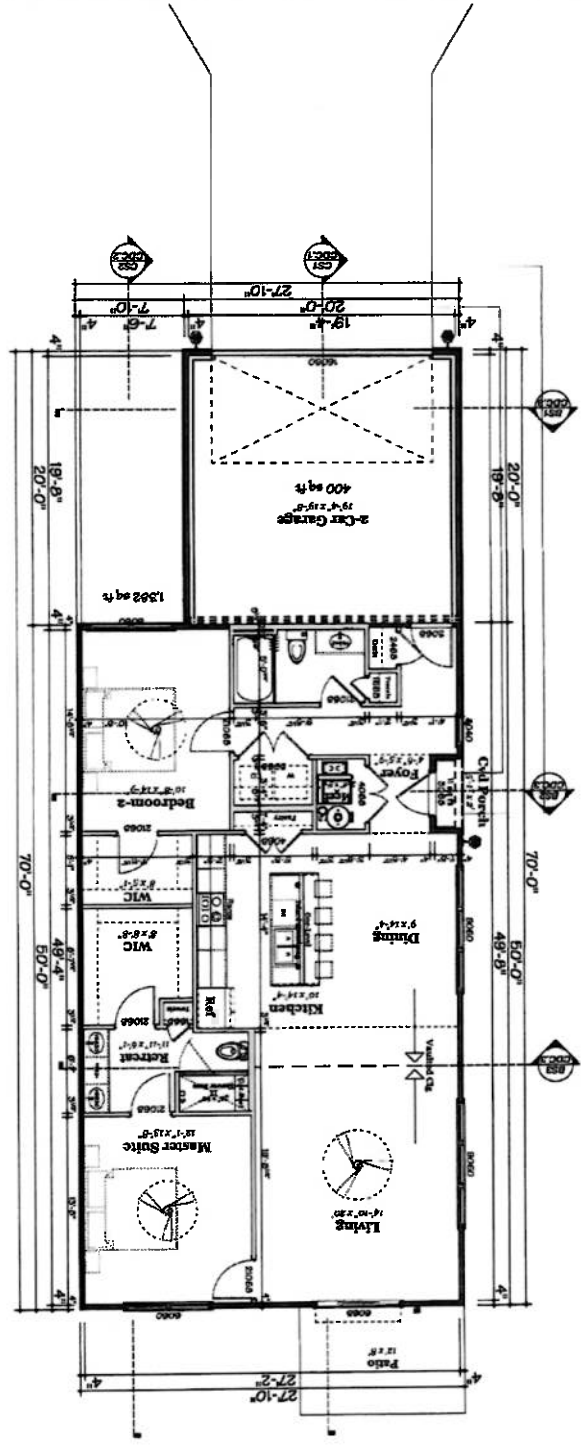

10513 Kenwood Rd. • Suite 2001 • Rose Mt. Ohio 45424  
 Designed By: P. Michael Fisher • (513) 393-0827  
 Email: virtualfx@icloud.com



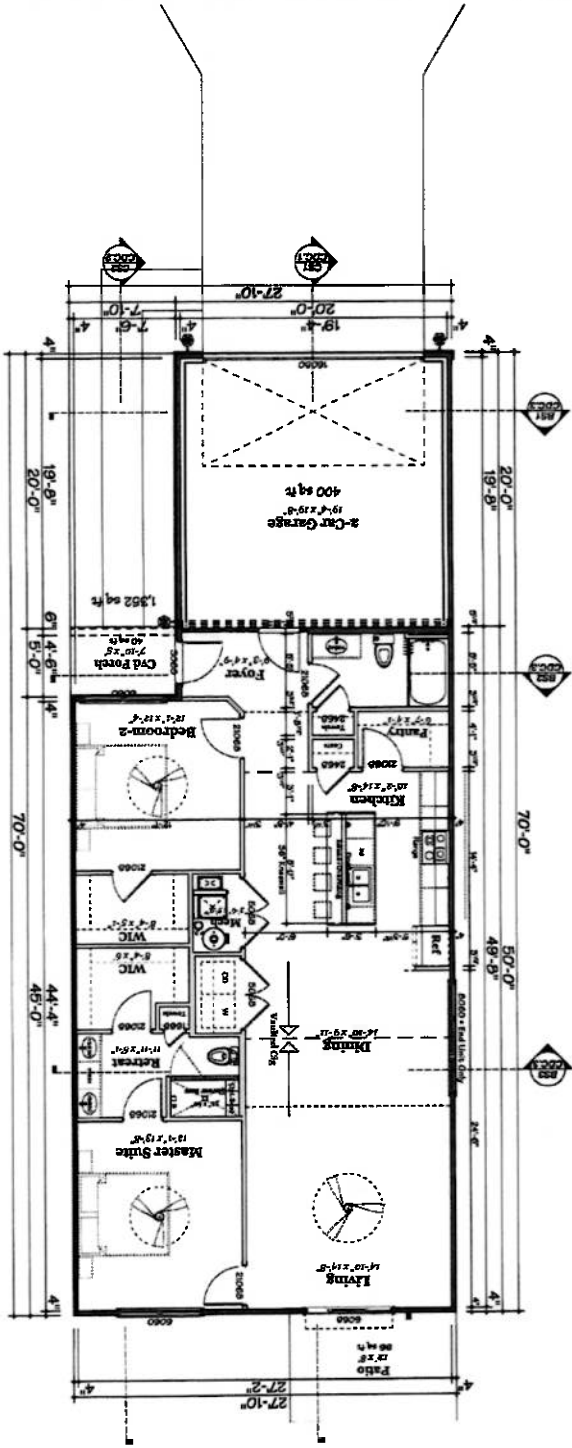
BROOKSTONE HOMES, LLC  
 7203 WOOTEN PIKE • CINCINNATI, OH 45227 • 513-946-7200  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
 E.A. • G. Varles

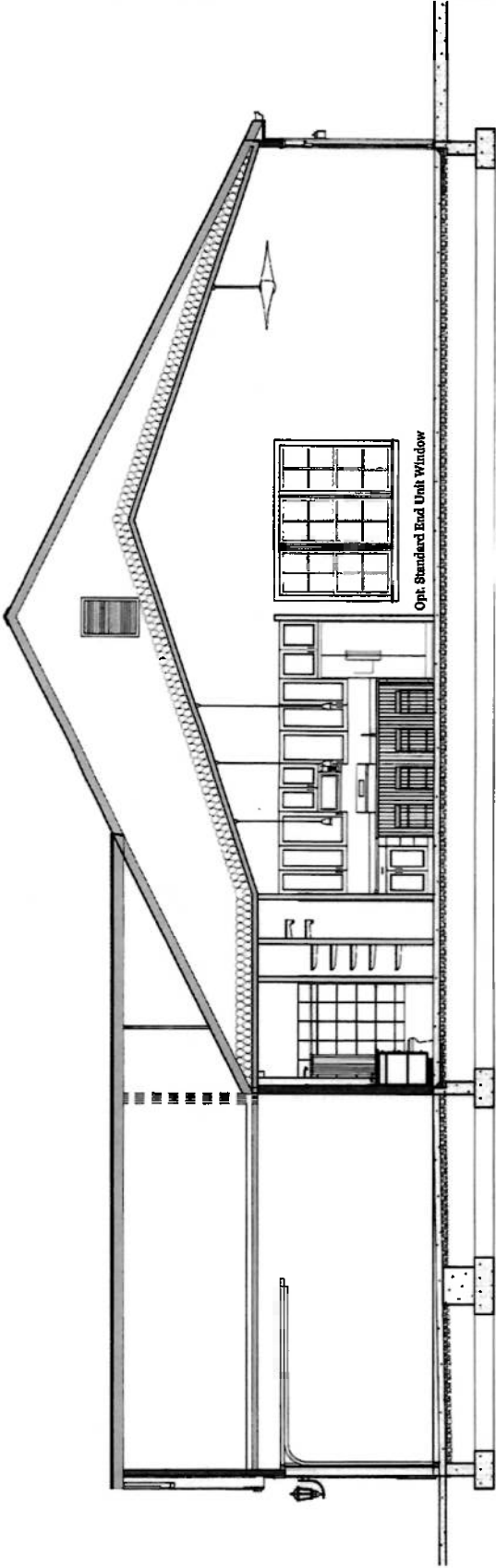
HB00 The Multi-Farm's  
 Vfx Holbrook 28' End • Virtual Tour - A  
 CD  
 A.5  
 SHEET NO. 0.1/0.2

**1.** Holbrook Ln • 28' End Unit • First Floor Plan  
1/8" = 1'-0"

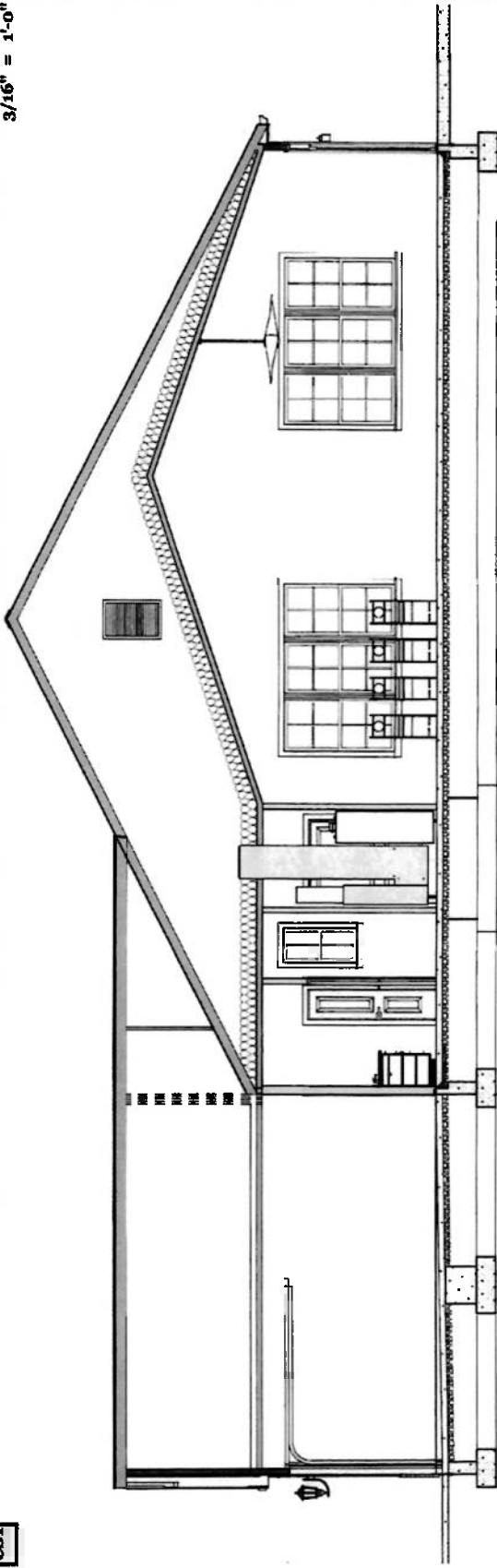


**1.** Holbrook Ln • 28' Unit • First Floor Plan  
1/8" = 1'-0"





**28'-Unit • Cross Section-1**  
 3/16" = 1'-0"  
 CSI



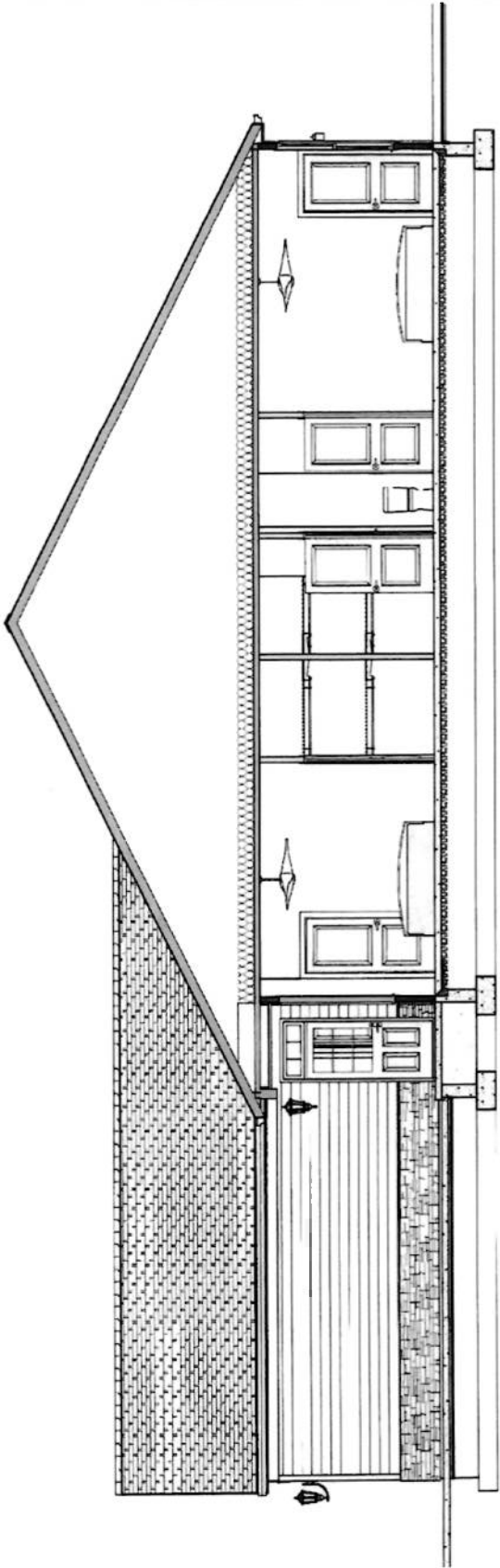
**28'-End Unit • Cross Section-1**  
 3/16" = 1'-0"  
 CSI

**Virtual f/x**  
 10011 Kennel Rd. • Suite 201 • Bay Ash, Ohio 44027  
 Designed By: P. Michael Fisher • (419) 993-0857  
 Email: virtual\_fx@icloud.com

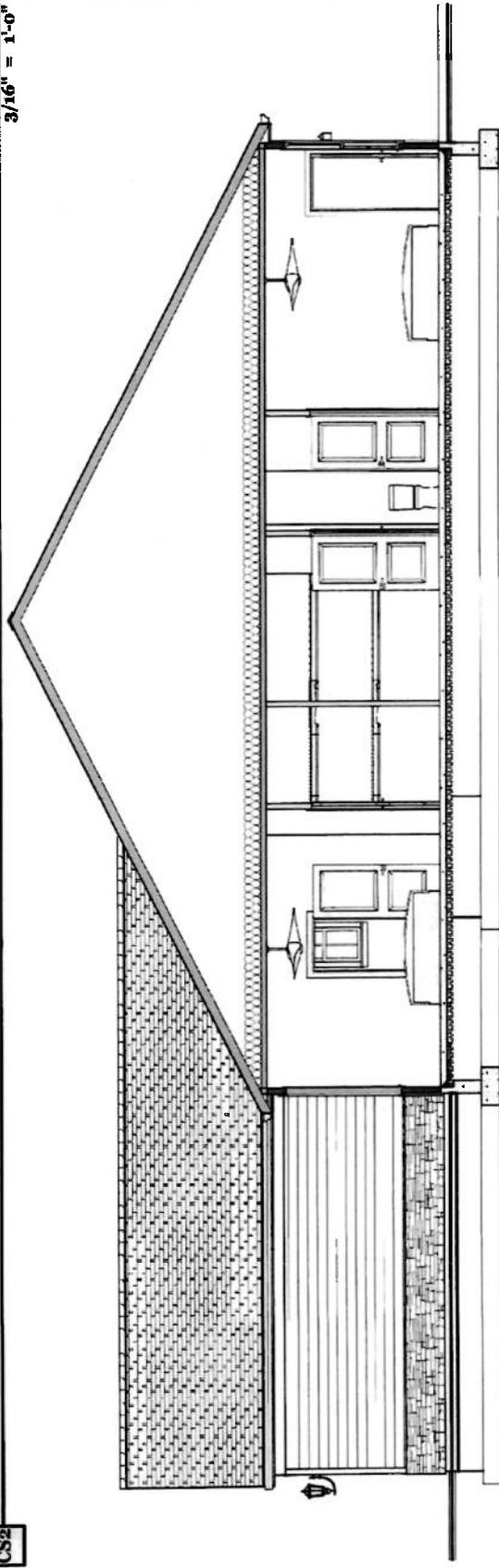
**Virtual f/x**  
 11111 Village Blvd. • Suite 100 • Bay Ash, Ohio 44027  
 440.297.4400  
 11111 Village Blvd. • Suite 100 • Bay Ash, Ohio 44027

**Brookstone**  
 The Multi-Farm's  
 El-A • G. Varies  
 Fort Delaware Crossing, LLC  
 1841 Holmden Lane • Florence, Kentucky 41042  
 Brookstone Homes, LLC.  
 7283 Woodspan Pike • Cincinnati, OH 45227 • 513-340-7288

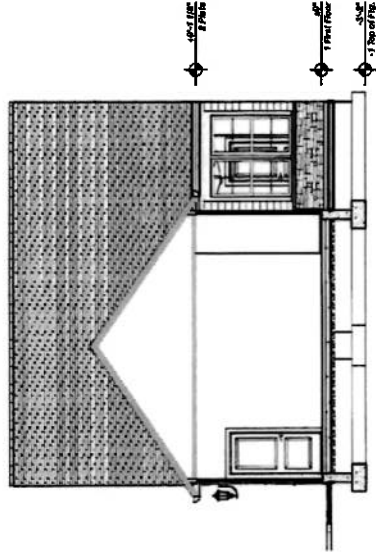
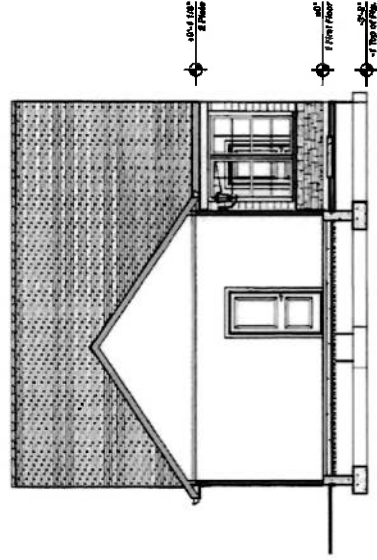
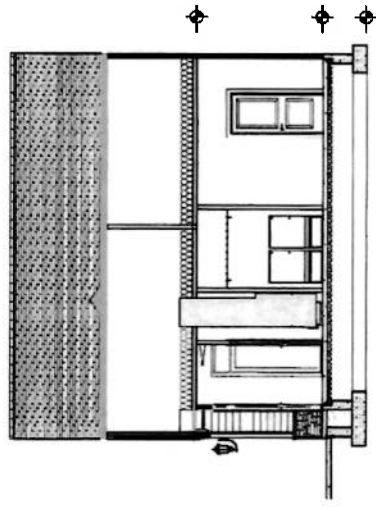
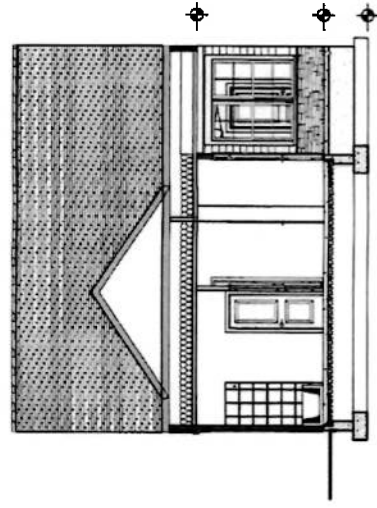
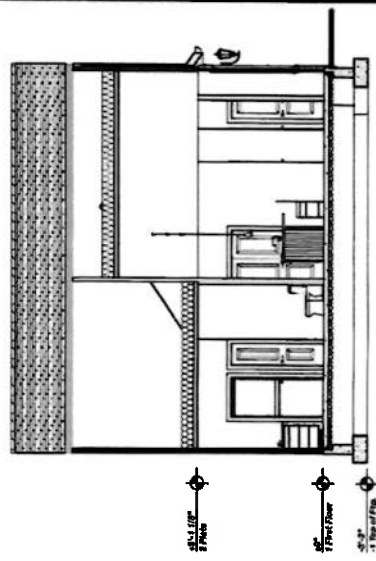
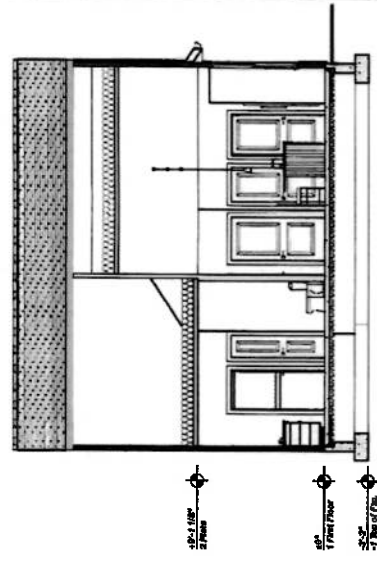
**CD**  
**C.2**  
 SHEET NO. 1412 OF 24

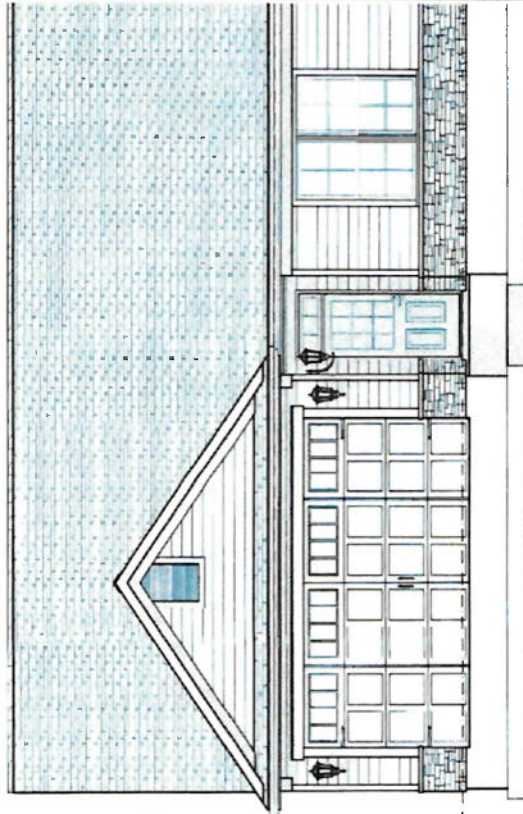


**CS2**  
 28'-Unit • Cross Section-2  
 3/16" = 1'-0"

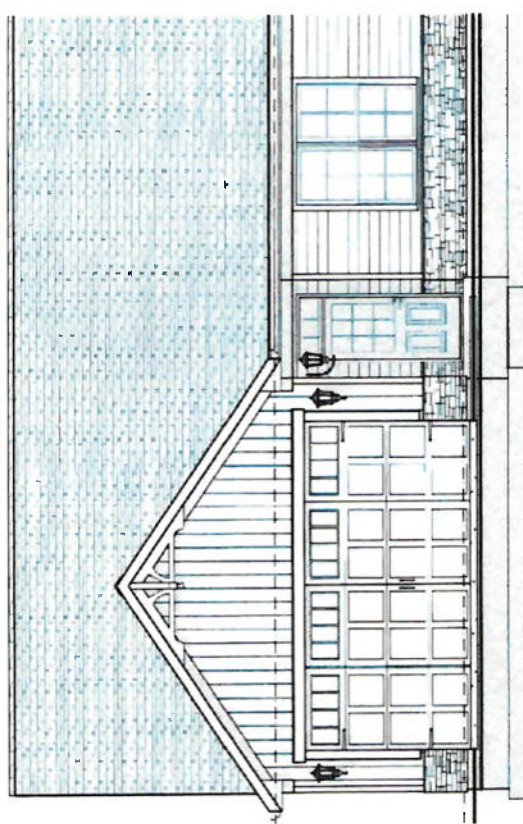


**CS2**  
 28'-End Unit • Cross Section-2  
 3/16" = 1'-0"





**Holbrook Ln. • 38' Unit • Front Elevation-B**  
 3/16" = 1'-0"



**Holbrook Ln. • 38' Unit • Front Elevation-A**  
 3/16" = 1'-0"

Help:  
 All Downward pointing arrows lead  
 to approved locations.

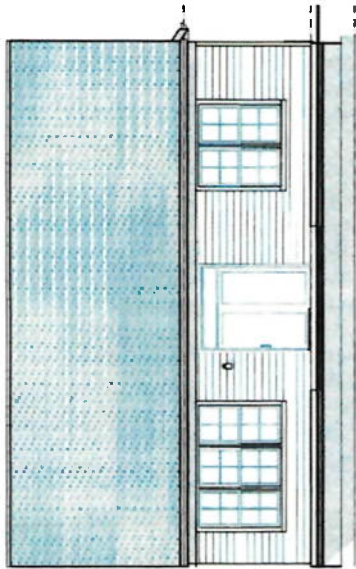
**WALL BRACING METHOD**  
 PER ICC 602.10.4  
 CS-WSP CONDUIT/CLY SHEATHED  
 WOOD STRUCTURAL PANEL, 1/2" MIN.  
 THICK CONNECTION REQUIREMENTS,  
 FASTENED TO STUDS AND JOISTS  
 INTERIOR SHEATHING ON BRACED WALLS  
 SHALL BE 5/8" MIN. GYPSUM BO. FASTENED  
 IN ACCORDANCE WITH TABLE 702.5.3

**EXHAUST FAN & SMOKE DETECTOR NOTE:**  
 ① IDENTIFY EXHAUST FAN LOCATIONS  
 (SEE NOTE 6 & 7 FOR CODE INFO ON SHEET A-0)  
 ② IDENTIFY SMOKE DETECTOR LOCATIONS  
 (SEE NOTE 11 & 12 ON SHEET A-2 & THE NOTE TO THE  
 LEFT FOR ALL SMOKE DETECTOR CODE INFO)  
 ③ IDENTIFY SMOKE/CO COMBO DETECTOR LOCATIONS  
 ④ PROGRAMMABLE THERMOSTAT REQ'D FOR ALL  
 FORCED AIR FINISHES

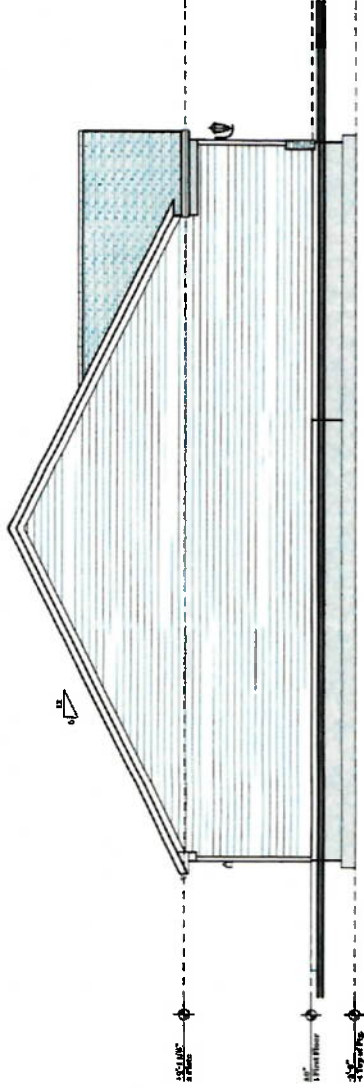
ALL SMOKE ALARMS SHALL BE LISTED IN  
 ACCORDANCE WITH UL 217, ON EACH LEVEL  
 WITHIN EACH DWELLING UNIT SMOKE  
 ALARMS UTILIZING PHOTOVOLTAIC AND  
 IONIZATION TECHNOLOGIES SHALL BE  
 INSTALLED. BEDROOMS TO USE IONIZATION  
 TYPE OUTSIDE BEDROOMS TO USE PHOTOVOLTAIC  
 SENSING SMOKE ALARMS MAY BE USED

E1

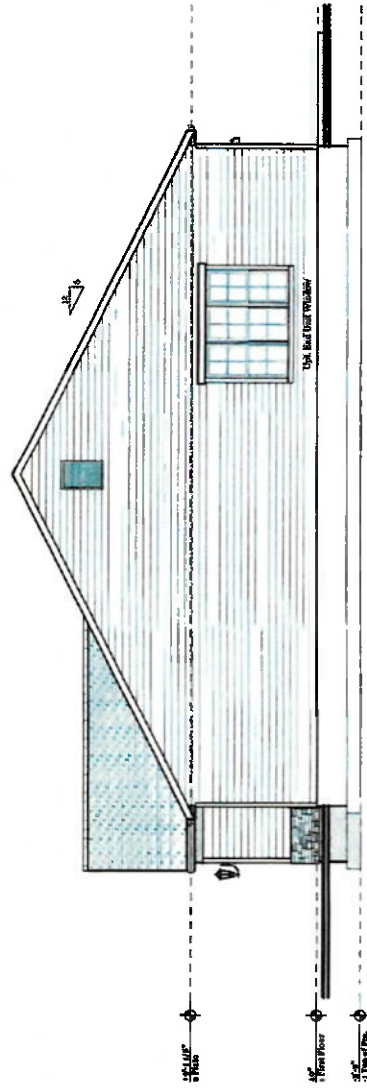
E1



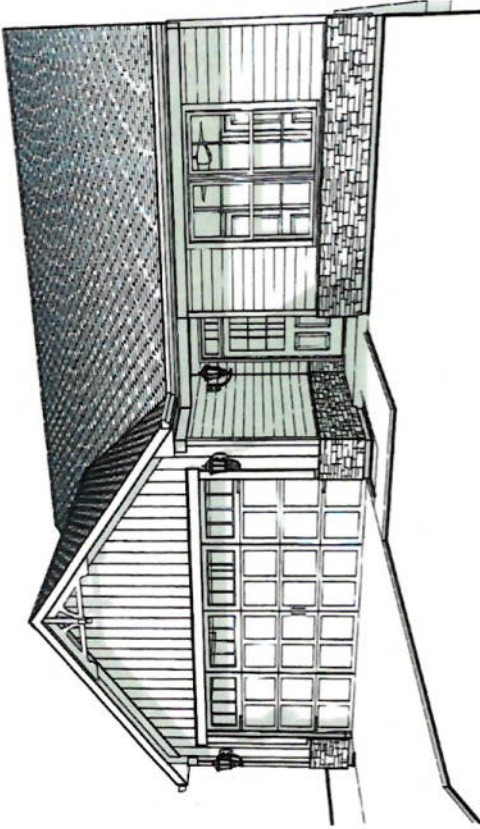
**E2** Holbrook Ln • 38' Unit • Rear Elevation-A  
1/8" = 1'-0"



**E3** Holbrook Ln • 38' Unit • Left Elevation-A  
1/8" = 1'-0"

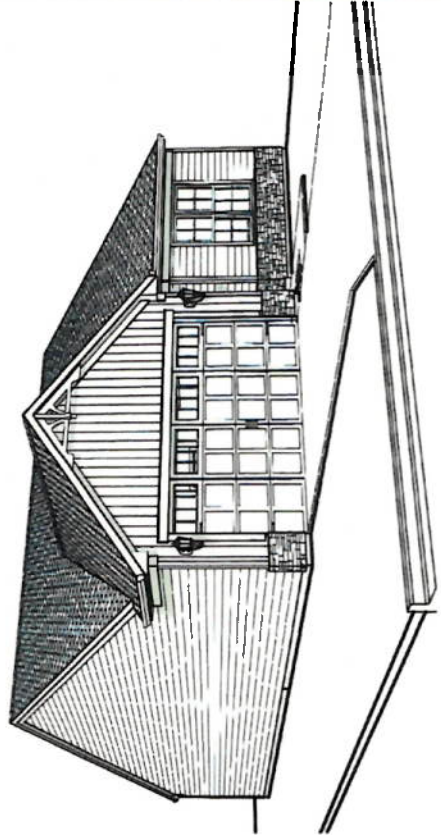


**E4** Holbrook Ln • 38' Unit • Right Elevation-A  
1/8" = 1'-0"



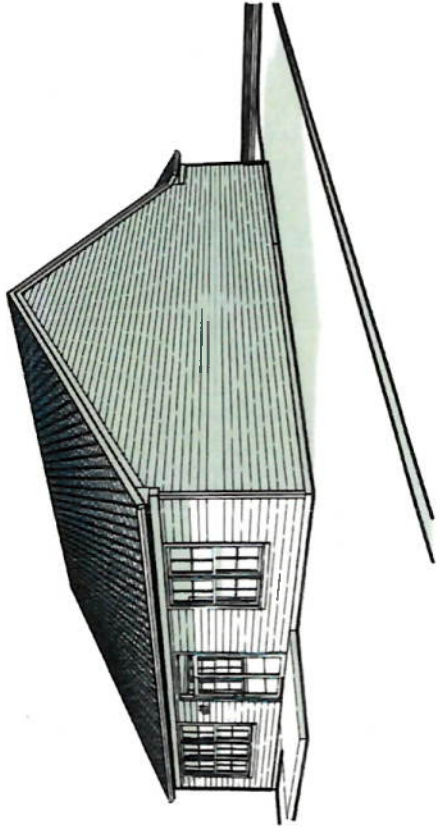
Vfx1

Holbrook 38' Ext • Vfx 3D Tour



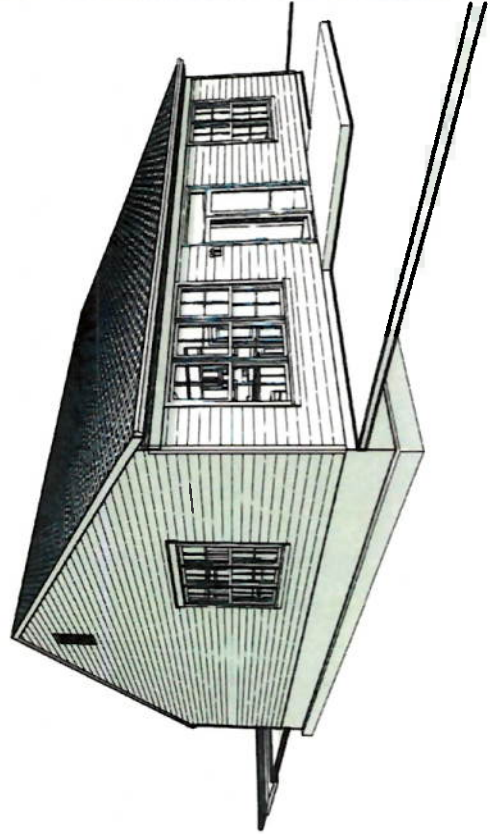
Vfx2

Holbrook 38' Ext • Vfx 3D Tour



Vfx3

Holbrook 38' Ext • Vfx 3D Tour



Vfx4

Holbrook 38' Ext • Vfx 3D Tour




1001 Kennel Rd. • Suite 301 • New Ark, Ohio 43042  
 Designed By: P. Michael Fisher • (614) 993-0877  
 Email: [virtual\\_fx@cloud.com](mailto:virtual_fx@cloud.com)

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 7783 WOOTEN PIKE • CHEVY Chase, OH 43027 • 513-340-7289  
**BROOKSTONE Homes, LLC**  
 1841 HOLBROOK LANE • FIDELITY, KENTUCKY 41042  
 For: Delaware Crossing, LLC  
 E-A • G. Varies

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**CD**  
**D.3**  
 Vfx Holbrook 38' Virtual Tour-A  
 SHEET NO. 012-P

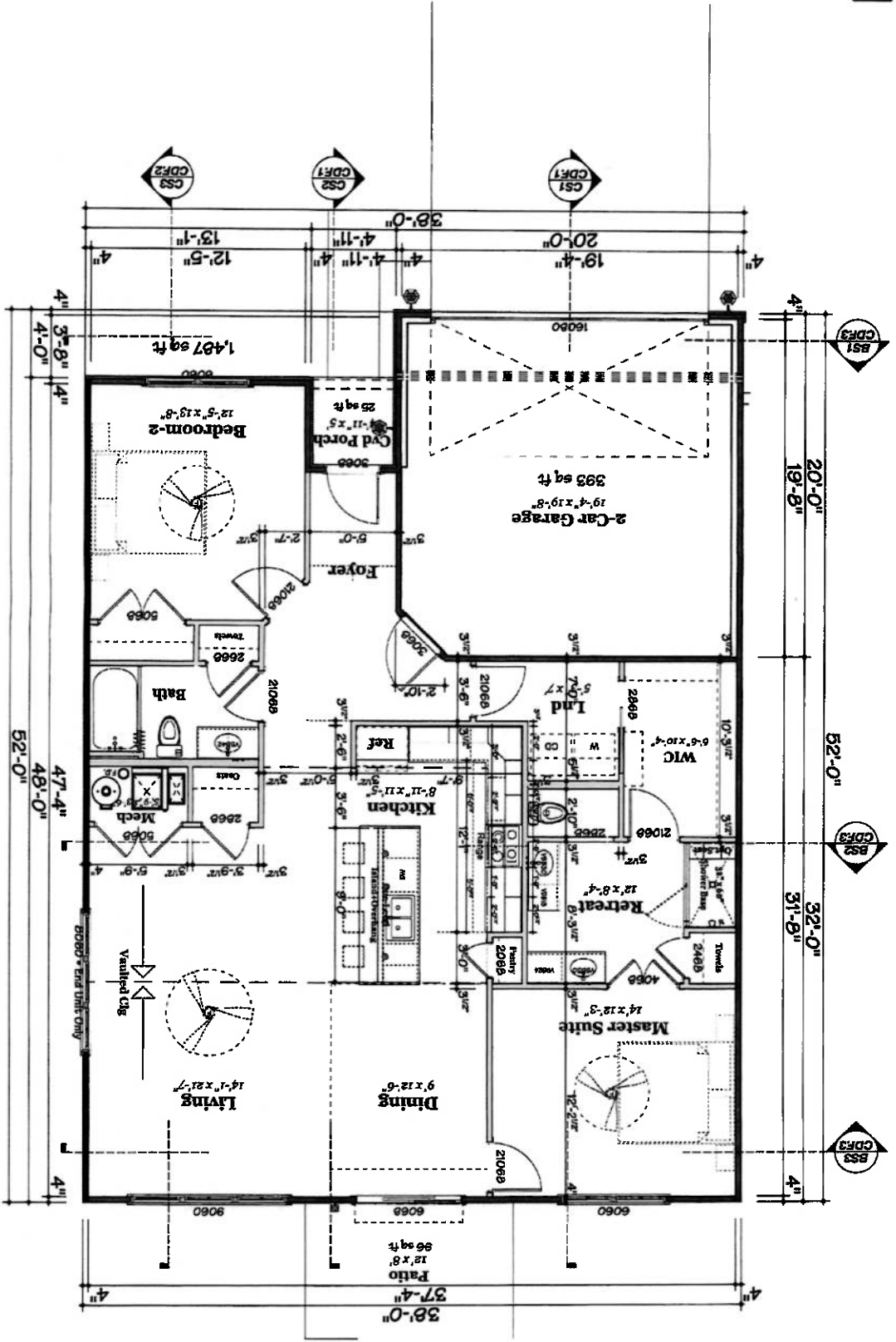
**HBOO The Multi-Farm's**  
 E.A. • G. Varies  
 For Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
**BROOKSTONE HOMES, LLC.**  
 7703 WOODRIDGE PIKE • CHICKASAW, MS 38927 • 513-340-7200  
 4474/10/09  
 10/10/09  
 10/10/09

**BROOKSTONE**

10131 Kennelwood Rd. • Sikeston • Missouri • 65750  
 Designed By: P. Michael Fisher • (513) 993-0857  
 Email: [vmf@hbood.com](mailto:vmf@hbood.com)  
**Virtual f/x**

**Virtual f/x**

1. Holbrook Ln • 38' Unit • First Floor Plan  
 3/16" = 1'-0"





SHEET NO. 24050

**CDF**  
City & County  
 Section 3

**.2**

**HBOO The Multi-Fam's**  
**BROOKSTONE HOMES, LLC.**  
 7201 WOODRUE PIKE • CINCINNATI, OH 45227 • 513-340-7200  
 1841 BOURBON LANE • FLORENCE, KENTUCKY 41042  
 For: Delaware Crossing, LLC  
 E.A. • G. Varies

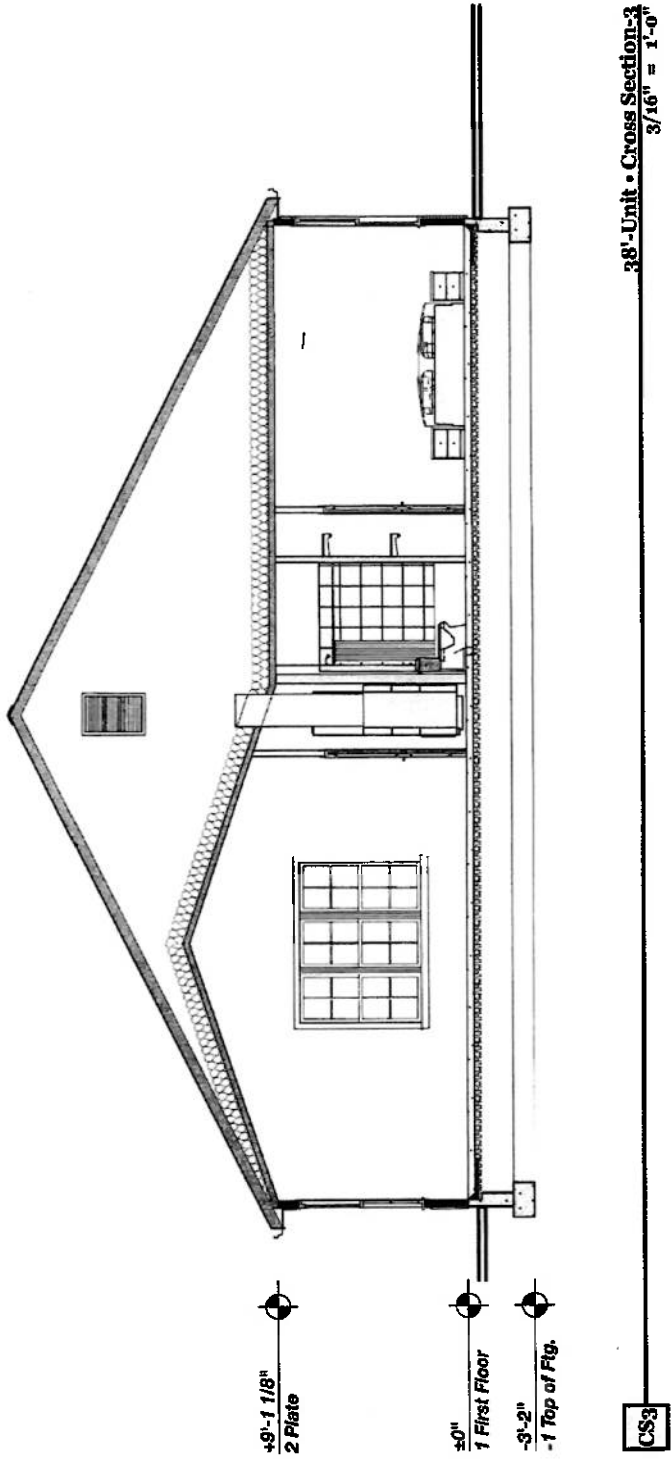
**BROOKSTONE**  
DESIGN

ARCHITECTURE  
 1616 W. WASHINGTON  
 CINCINNATI, OH 45202  
 TEL: 513-340-7200  
 FAX: 513-340-7200  
 WWW.BROOKSTONEDESIGN.COM

  
 Registered Professional Architect  
 Michael Fisher  
 Designated By: P. Michael Fisher • (513) 390-0857  
 10325 Kenwood Rd. • Ste. 2001 • Blue Ash, Ohio 45245  
 Email: [virtualfx@icloud.com](mailto:virtualfx@icloud.com)

**Virtual f/x**

**Vf/x**



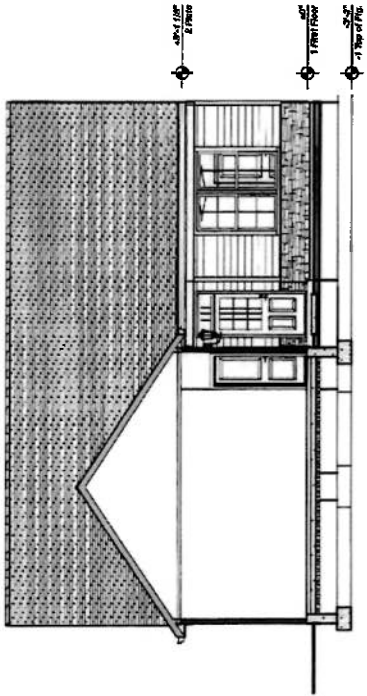
+9'-1 1/8"  
2 Plate

+0"  
1 First Floor

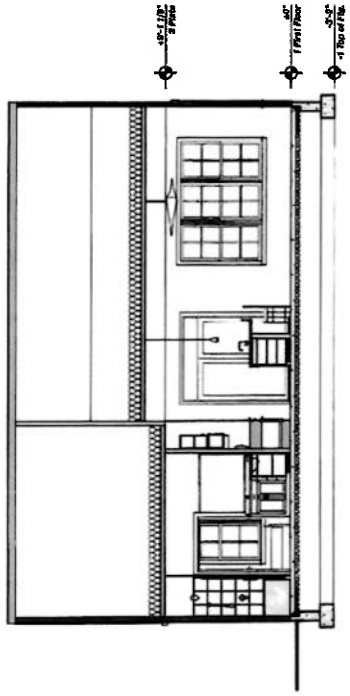
-3'-2"  
-1 Top of Fig.

CS3

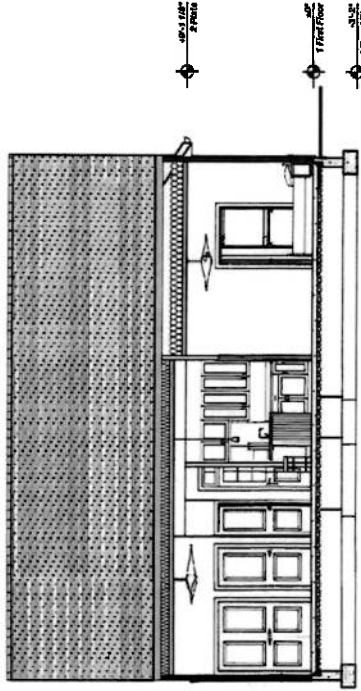
38'-Unit • Cross Section-3  
 3/16" = 1'-0"



BS1  
38'-Unit • Building Section-1  
1/8" = 1'-0"

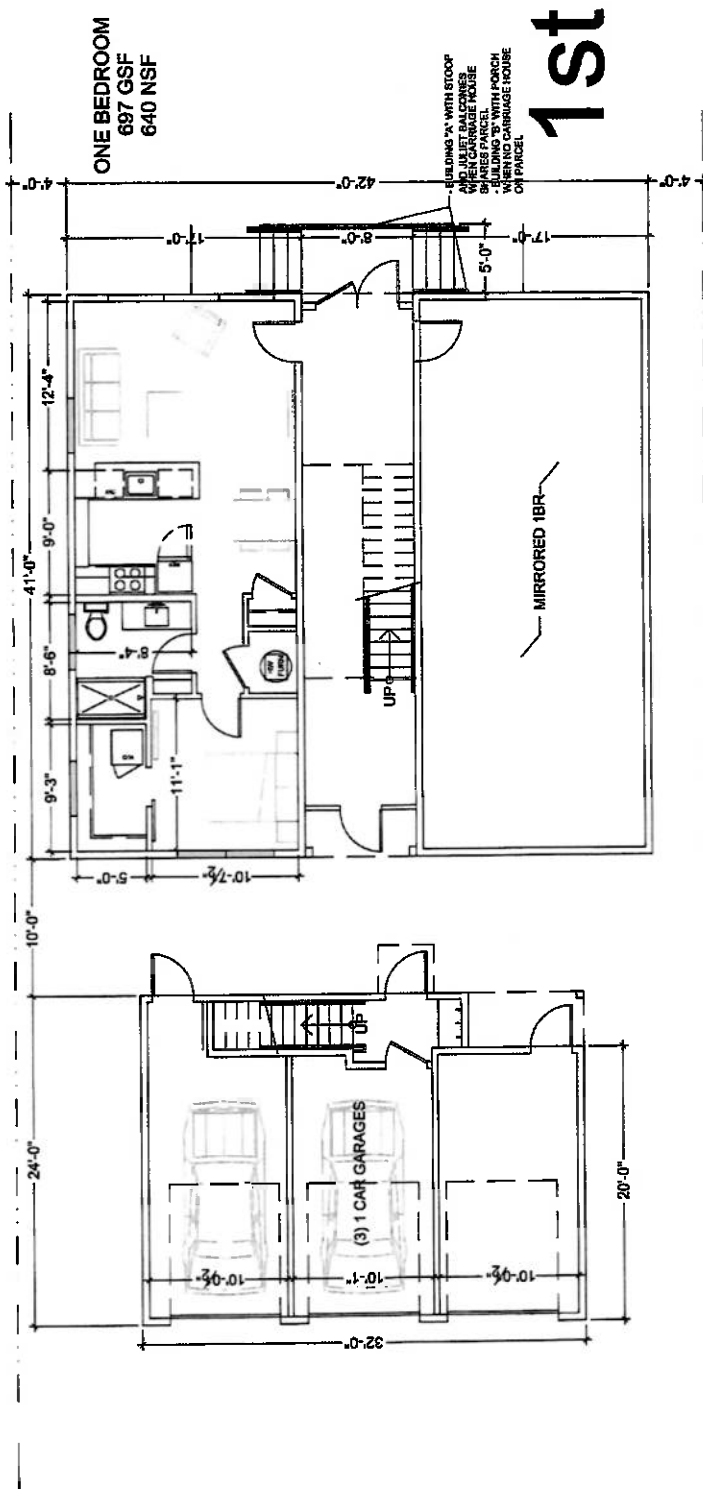


BS2  
38'-Unit • Building Section-2  
1/8" = 1'-0"



BS3  
38'-Unit • Building Section-3  
1/8" = 1'-0"

**POPLAR POINTE APARTMENTS  
SCHEMATIC DESIGN REVIEW SET**



ONE BEDROOM  
697 GSF  
640 NSF

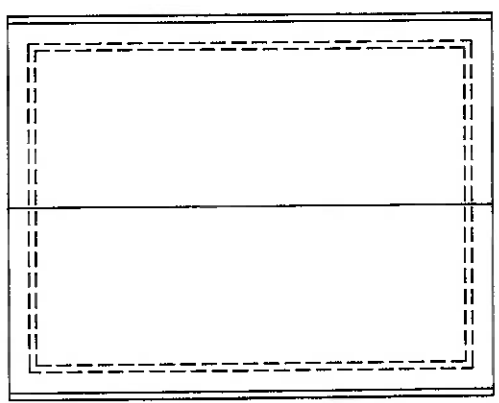
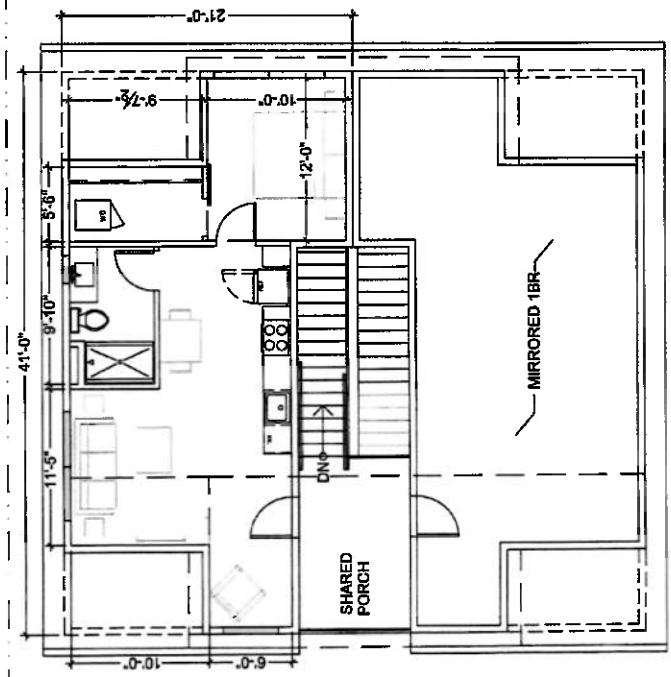
1st

CARRIAGE HOUSE AND BUILDING A (BUILDING B SIMILAR)

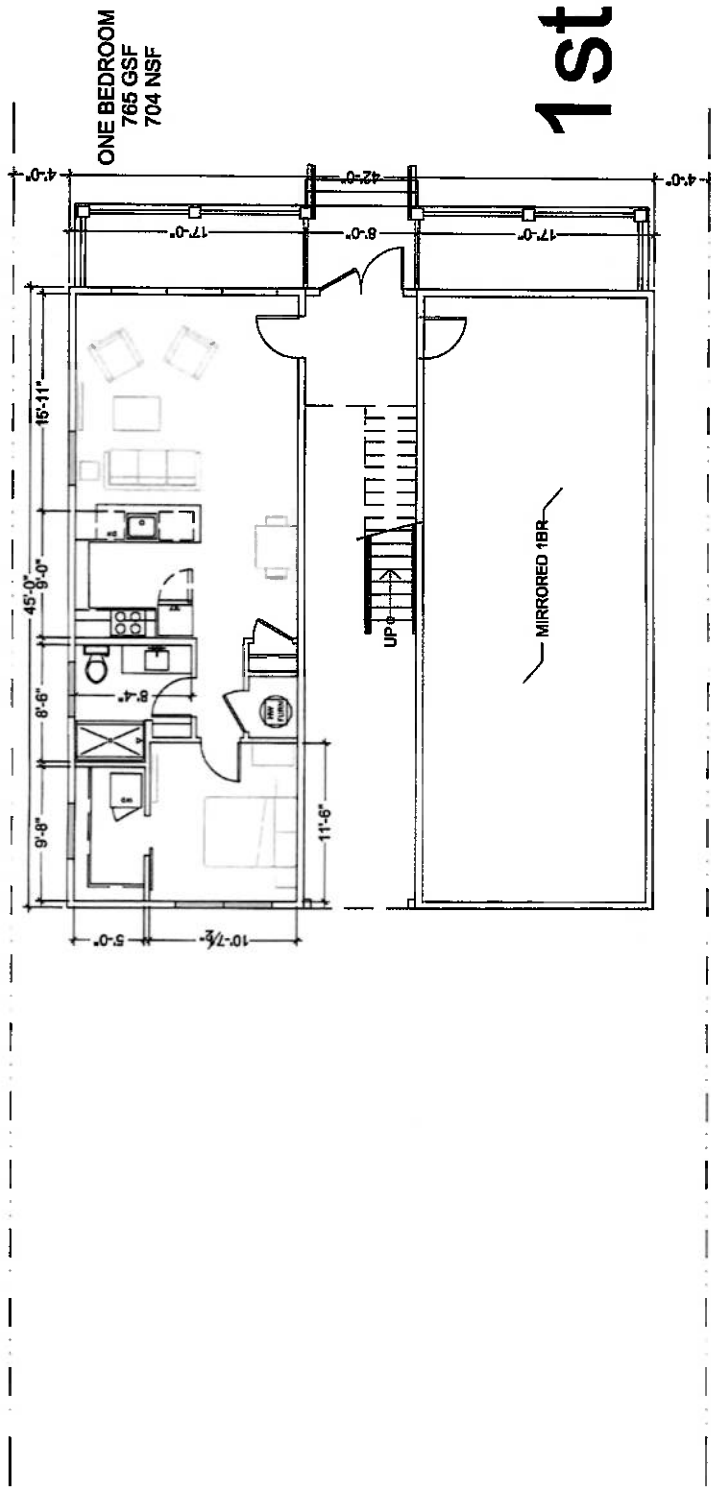


ONE BEDROOM  
624 GSF  
563 NSF

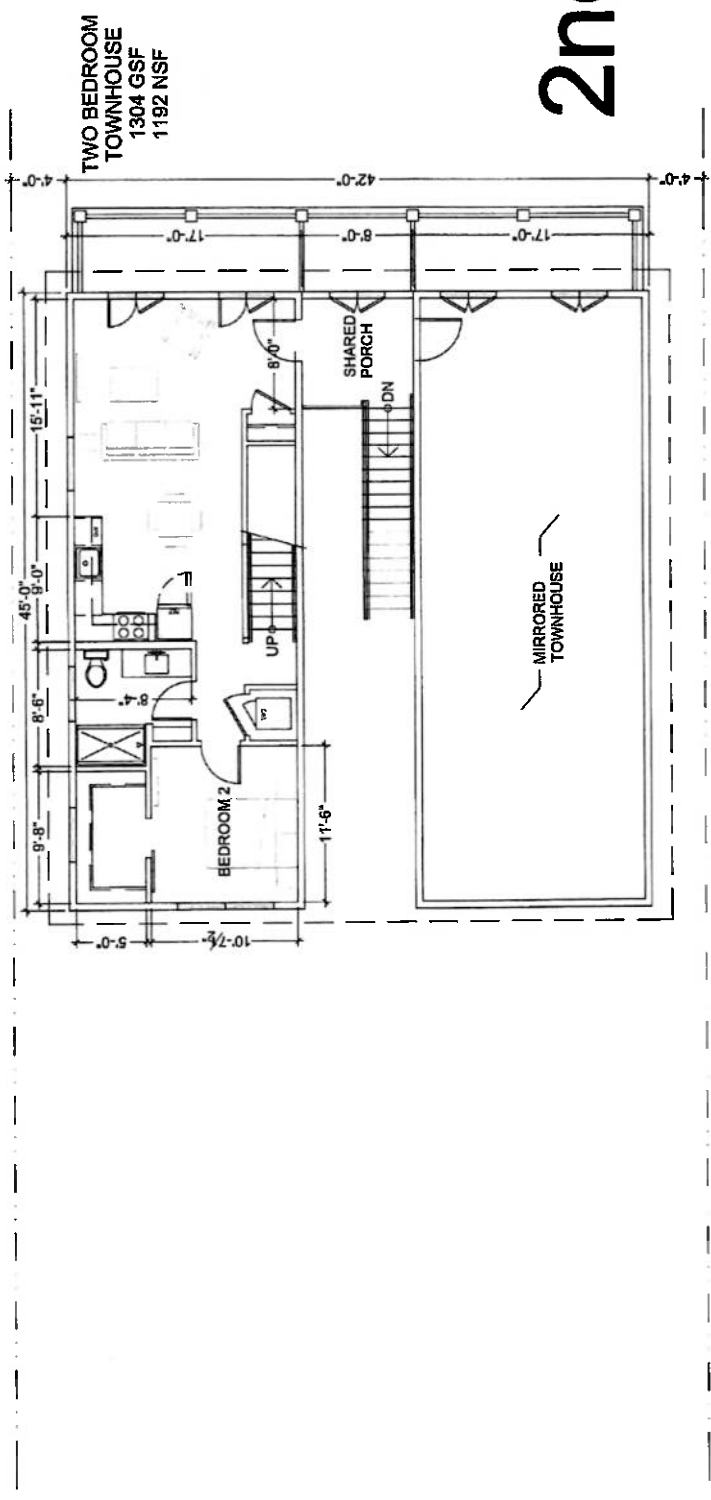
3rd



CARRIAGE HOUSE AND BUILDING A (BUILDING B SIMILAR)



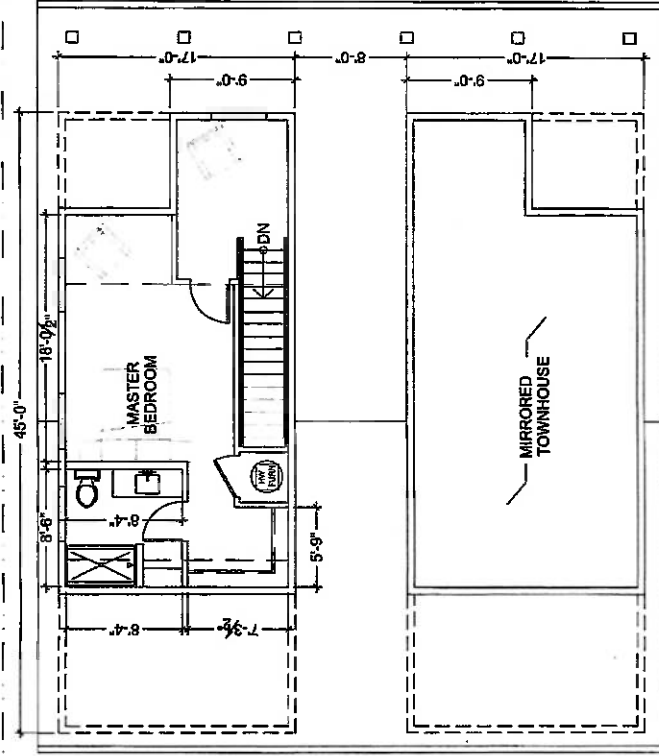
BUILDING C



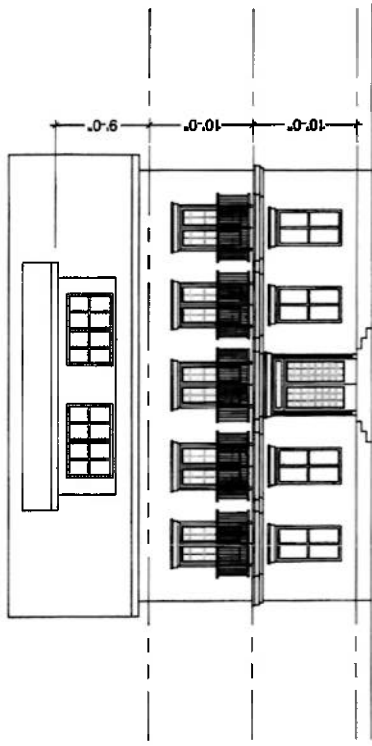
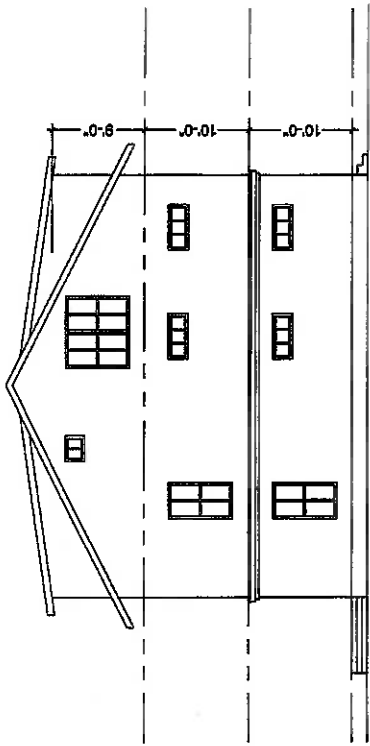
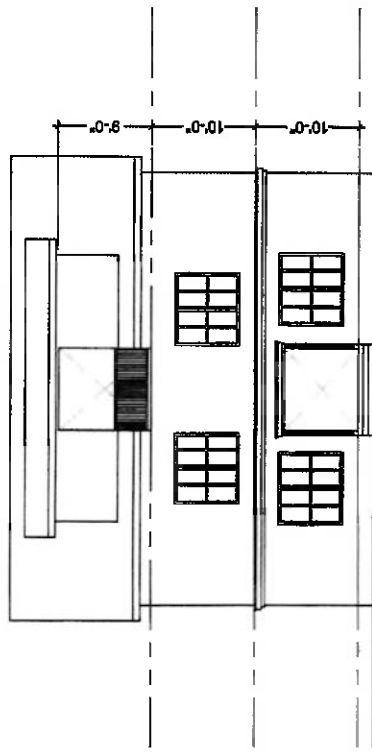
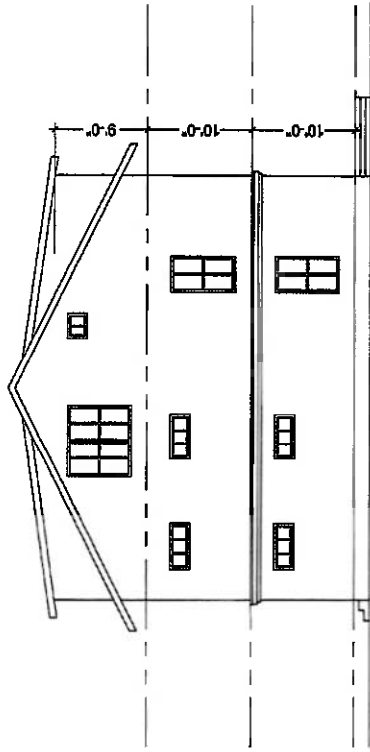
2nd

BUILDING C

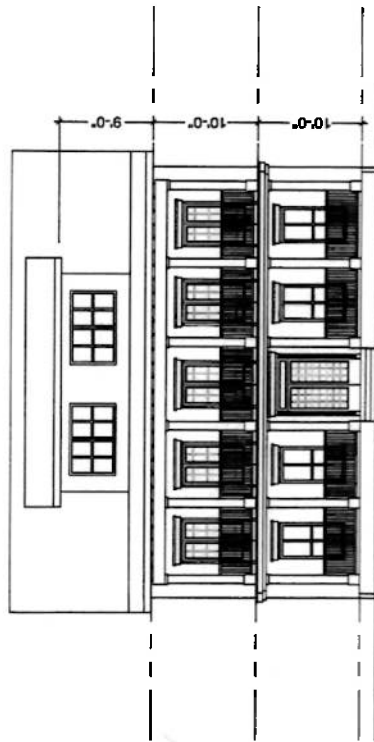
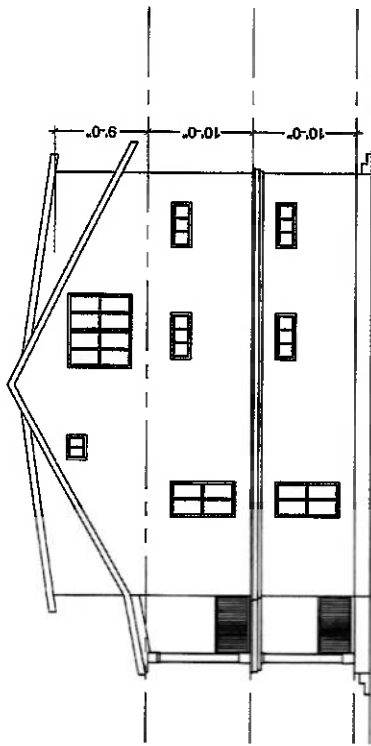
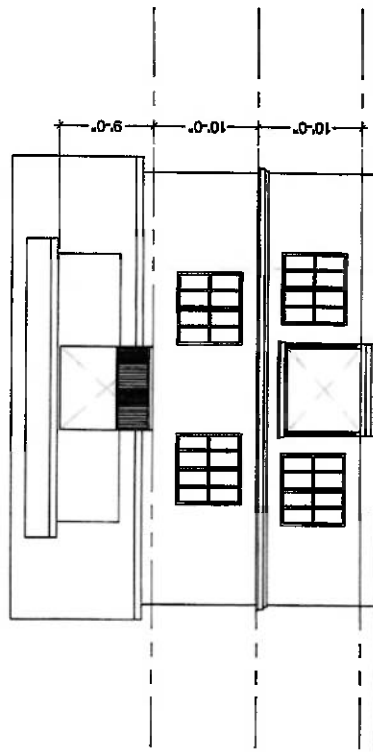
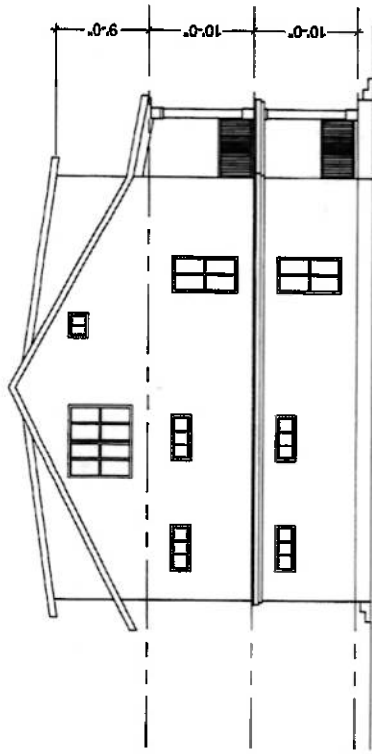
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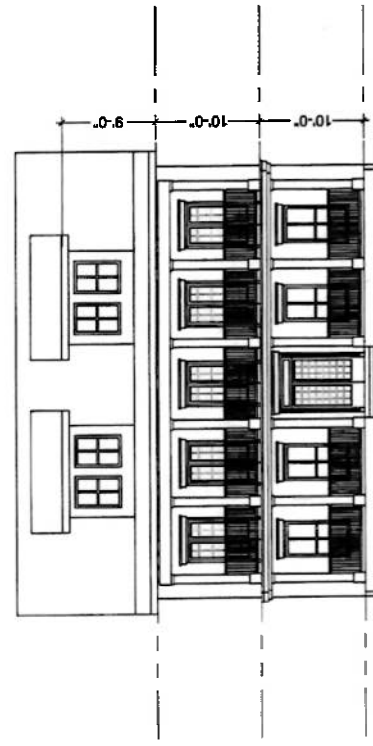
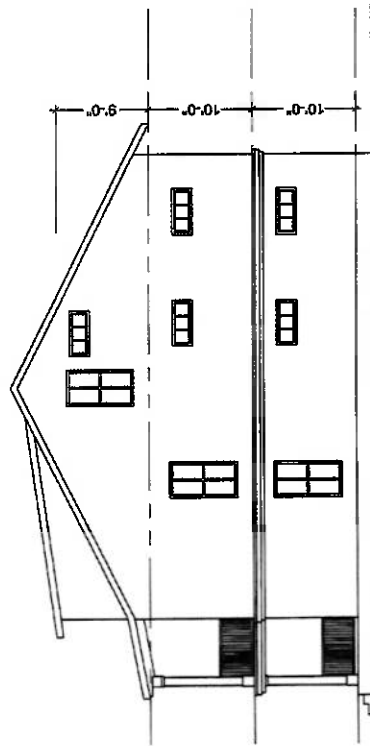
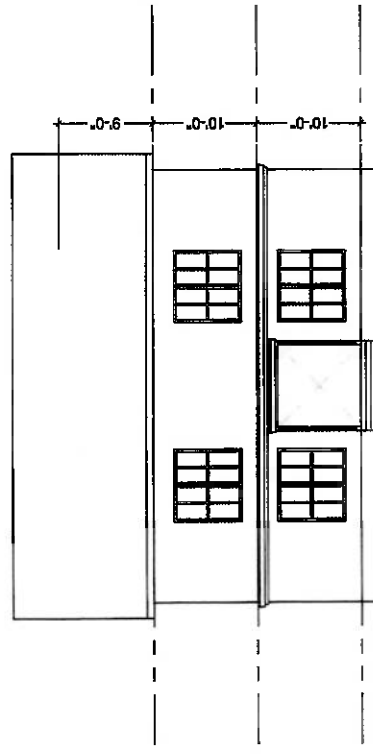
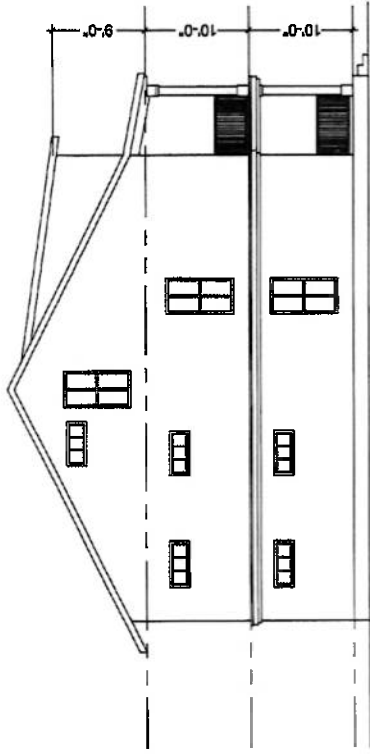
**BUILDING C**



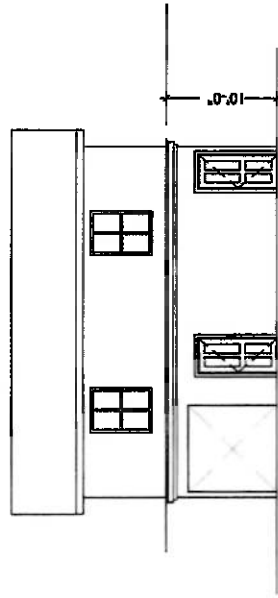
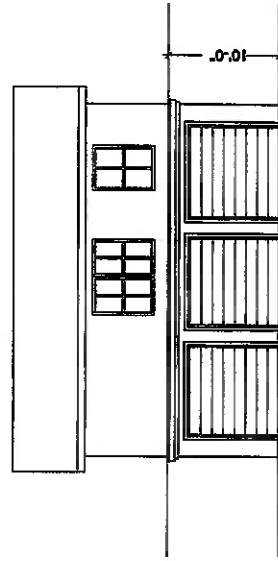
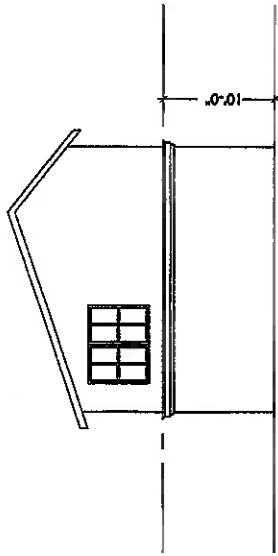
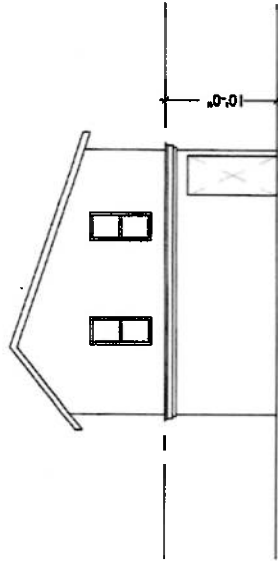
**BUILDING A - ELEVATIONS**



**BUILDING B - ELEVATIONS**



**BUILDING C - ELEVATIONS**



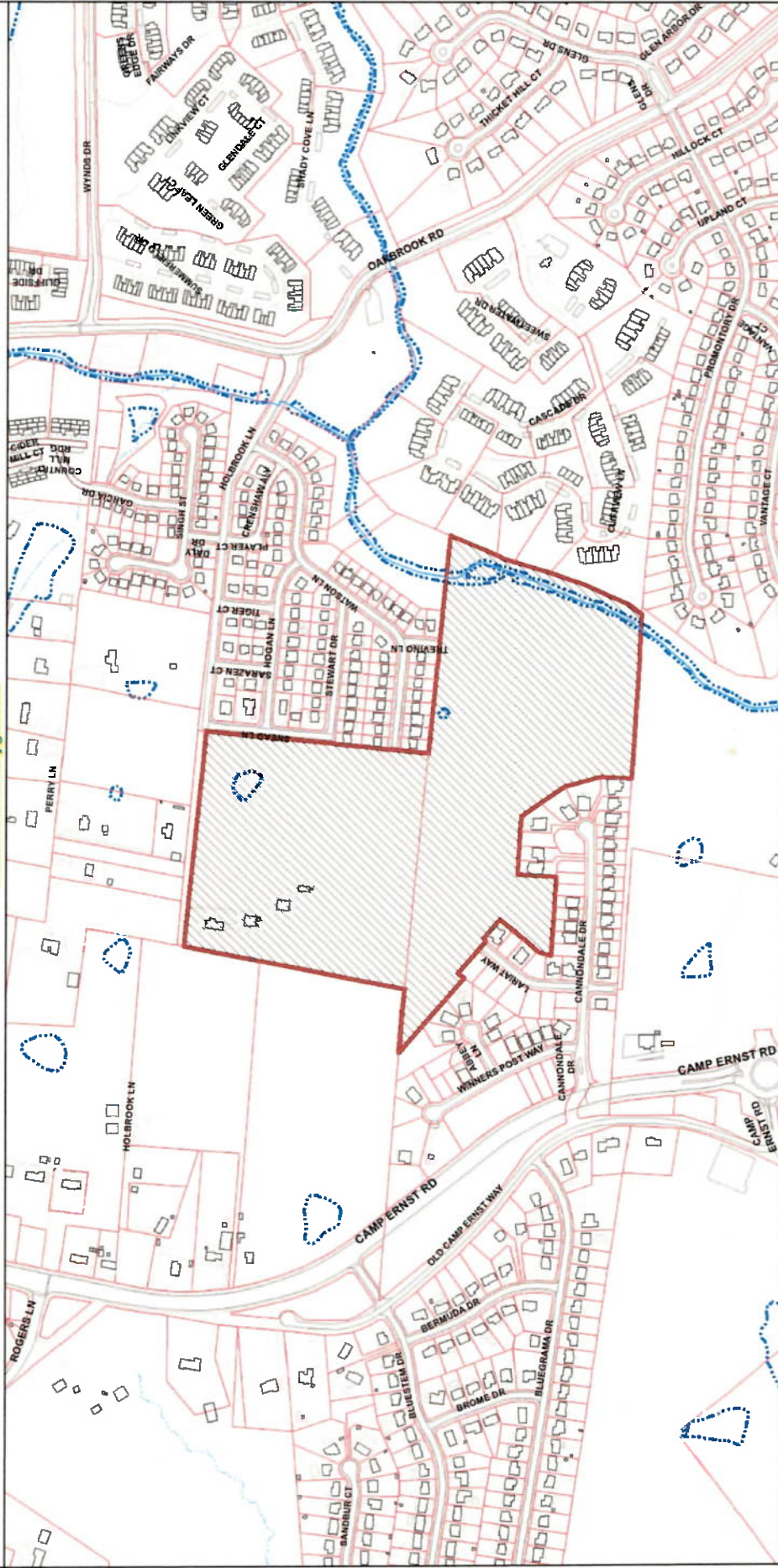
**CARRIAGE HOUSE - ELEVATIONS**

# Fiscal Court Meeting Presentation

# Vicinity Map

## Boone County GIS Map

[www.boonecountygis.com](http://www.boonecountygis.com)



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Map Created: 03/16/2024



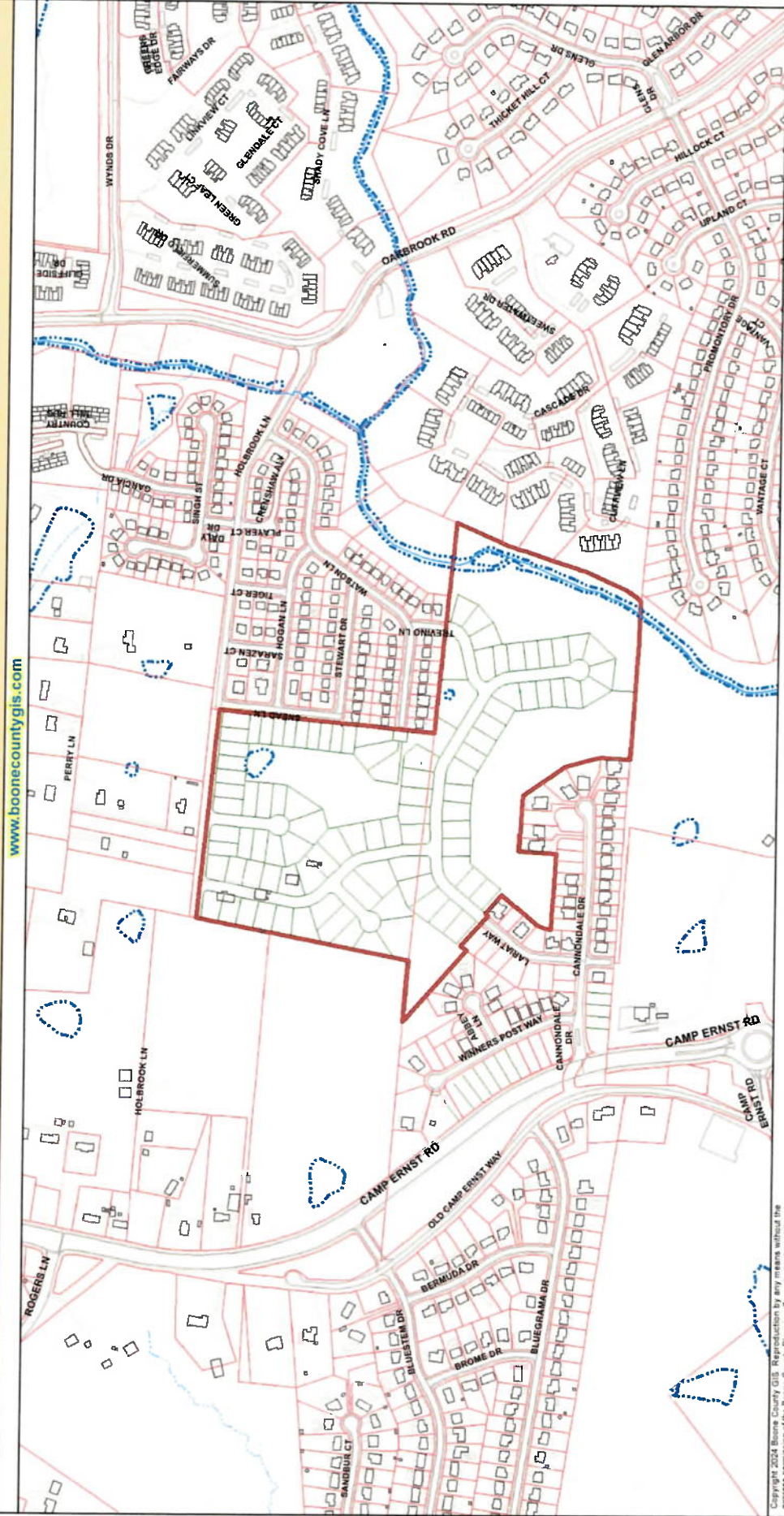
**Boone County GIS - Putting Northern Kentucky on the Map**



AcMap Document v. 11.01

# Valley Creek Farms Preliminary Plat Map

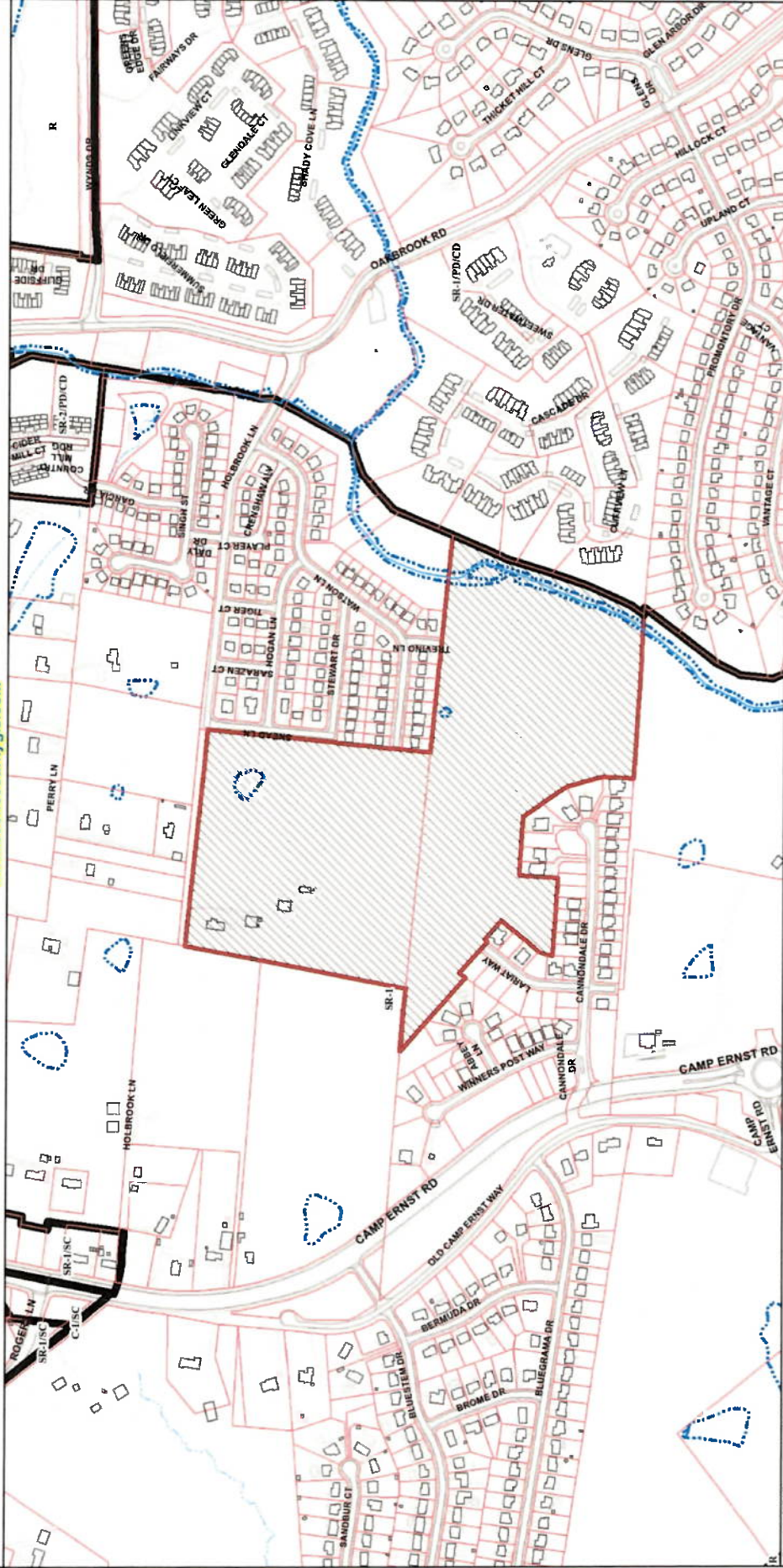
## Boone County GIS Map



# Zoning Map

## Boone County GIS Map

[www.boonecountygis.com](http://www.boonecountygis.com)



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Map Created: 11/16/2024



### Boone County GIS - Putting Northern Kentucky on the Map



AutoMap Document - 11/16/24

# 2040 Future Land Use Map

## Boone County GIS Map

[www.boonecountygis.com](http://www.boonecountygis.com)



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Map Created: 10/12/2024

0 412.5 825 1,650 2,475 3,300 Feet

1 inch = 400 feet

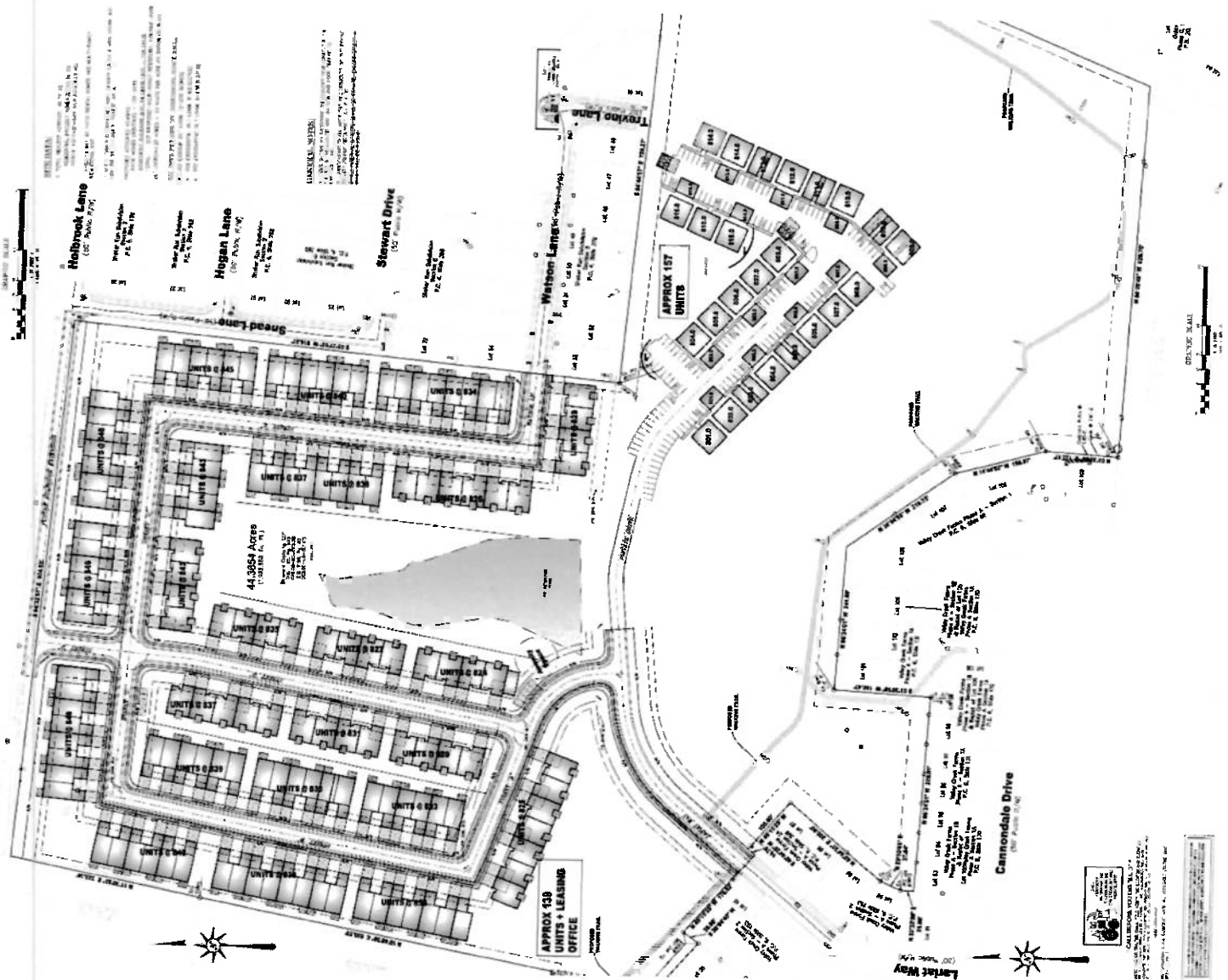
## Boone County GIS - Putting Northern Kentucky on the Map



Boone County GIS  
ArcMap, Document, \* .mxd

# Concept Plan

1. Construction of 139 attached single-family residential dwellings in twenty-four (24) single-story buildings.
2. Construction of 157 multi-family residential dwellings in twenty-four (24) three-story buildings and thirteen (13) two-story garages).
3. Construction of public streets within the northern portion of the site.
4. Street connections at Watson Lane, Holbrook Lane, and Lariat Way.
5. Construction of street extensions for Holbrook Lane and Lariat Way.
6. Provision for stormwater retention.
7. Provision for amenity space.
8. Construction of sidewalks.
9. Construction of a private access drive within the southern portion of the site, extending from the terminus of Lariat Way.
10. Provision for a walking train in the southern portion of the site.
11. Provision for public utilities.



# Public Hearing Summary

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## Proponents

- The development will allow the preservation of open green space
- Need for affordable housing

## Opponents

- Too much density
- There is no need for additional affordable housing
- Schools will be negatively impacted
- Lack of community support
- Additional traffic
- Existing developer has not completed the existing subdivision, and the proposed developer is the same company
- Will adversely affect property values
- The intersection at Camp Ernst Road will become more hazardous

# Recommendation/Findings Of Fact

## DENIAL (13-0)

1. The proposed Zoning Map Amendment is not consistent with the 2040 Future Land Use Map of the comprehensive plan, Our Boone County Plan 2040.  
  
The northern portion of the site is identified for Rural Density Residential, which is described as low density residential uses of up to one dwelling unit per acre. The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, would allow residential uses at a density of approximately 6 units per acre.  
  
The southern portion of the site is identified for Suburban Density Residential, which is described as single-family housing of up to four units per acre. The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, would allow multi-family residential uses at a density of approximately 6 units per acre.
2. The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, is not consistent with several Goals and Objectives contained in the comprehensive plan, Our Boone County Plan 2040.
3. The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, is not consistent with several Planned Development criteria, as outlined in Section 1506 of the zoning regulations.

# Recommendation/Findings Of Fact (continued)

4. The existing SR-1 zoning of the site in question is appropriate and the proposed SR-1/PD zoning for the site is inappropriate.

The existing SR-1 district, which permits detached single-family residential dwellings at a maximum intensity of four (4) units per acre, is consistent with the 2040 Future Land Use Map of the comprehensive plan, Our Boone County Plan 2040.

The existing SR-1 district provides for a land use and intensity which is compatible with the developed areas adjacent to the site in question.

The proposed SR-1/PD district, along with the submitted Concept Development Plan, would allow the development of attached single-family and multi-family residential dwellings at a maximum intensity of six (6) units per acre, representing an increase of fifty (50) percent over the recommended intensity as outlined in the comprehensive plan.

Lariat Way and Cannondale Drive have been designed and built as local streets, which are defined as a type of street to serve less than one hundred (100) residential lots or units. The proposed SR-1/PD district, along with the submitted Concept Development Plan, would create a situation where these streets would serve more than one hundred (100) residential units.
5. On April 27, 2018, a Preliminary Plat was approved for the development of the Valley Creek Farms subdivision showing the development of 181 detached single-family residential lots. To date, seventy-eight (78) of those lots have been platted. The remaining one hundred three (103) lots are encompassed within the site in question.

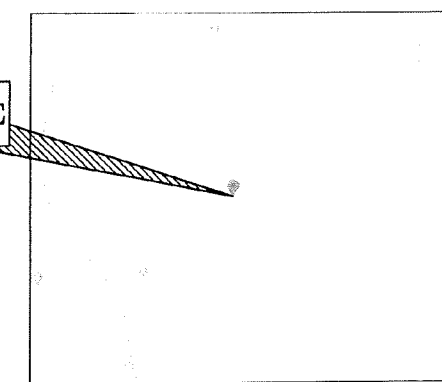
It seems reasonable to assume that the owners of the existing lots, and those that are considering purchase of the existing lots, would have done their due diligence and found that the area to the north and west of the existing development was going to be an extension of the subdivision.

The proposed Zoning Map Amendment, along with the submitted Concept Development Plan, significantly changes the character of what those existing/potential homeowners expected.
6. There have been no major changes of an economic, physical, or social nature within the area involved which were not anticipated in the adopted comprehensive plan and which have substantially altered the basic character of such area.

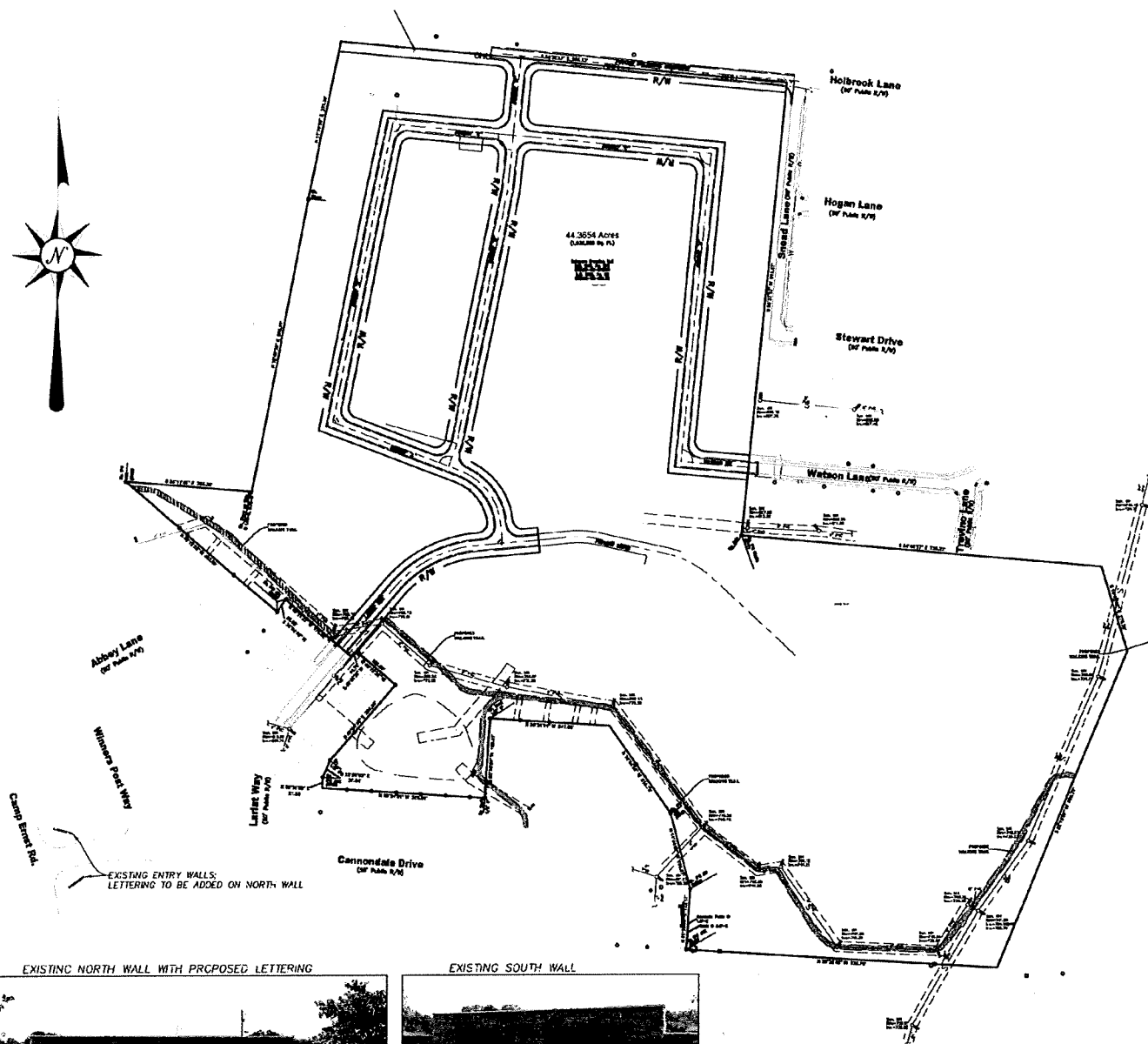
# PLANS FOR THE CONSTRUCTION OF A NEW RESIDENTIAL PLANNED DEVELOPMENT KNOWN AS: HICKORY BURLINGTON & POPLAR POINTE APARTMENTS

LARIAT WAY, BOONE COUNTY, KY  
JUNE, 2024

PROPOSED SITE

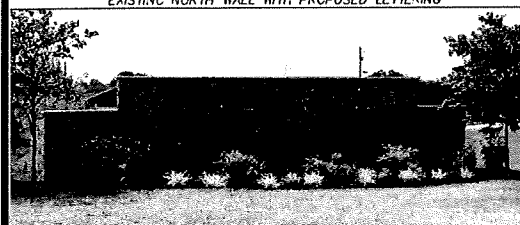


LOCATION MAP

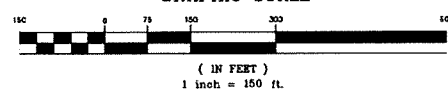


EXISTING NORTH WALL WITH PROPOSED LETTERING

EXISTING SOUTH WALL



GRAPHIC SCALE

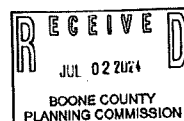


\* CALL BEFORE YOU DIG "B.U.D." \*

NOTE: THE CONTRACTOR SHALL FIELD VERIFY THE LOCATION AND ELEVATION OF ALL UNDERGROUND UTILITIES PRIOR TO ANY EXCAVATION AND SHALL CONTACT THE KENTUCKY UNDERGROUND PROTECTION, INC. A MINIMUM OF TWO (2) WORKING DAYS PRIOR TO DIGGING AT TELEPHONE NO. 1-800-752-6007

NO EXCAVATION SHALL COMMENCE UNTIL ALL APPLICABLE UTILITIES HAVE BEEN CLEARED.

NOTE: PRIOR TO ANY EXCAVATION FOR UNDERGROUND UTILITIES CONTRACTOR SHALL EXPOSE AND VERIFY LOCATIONS (HORIZONTAL AND VERTICAL) OF ALL EXISTING UTILITIES INCLUDING BUT NOT LIMITED TO GAS, WATER, AND SANITARY SEWER. ANY CONFLICTS SHALL BE REPORTED IMMEDIATELY TO THE ENGINEER AND THE APPROPRIATE AUTHORITIES.



**DEPARTMENT/ UTILITY TABLE:**

BOONE COUNTY BUILDING DEPARTMENT  
ROOM 312  
PO BOX 860  
2950 WASHINGTON STREET  
BURLINGTON KY 41005  
TEL: 859 334 2218

BOONE COUNTY PUBLIC WORKS DEPARTMENT  
5645 IDLEWILD ROAD  
BURLINGTON KY 41005  
TEL: 859-334-3600

MIKE ROUSE / PROJECT MANAGER  
BOONE COUNTY WATER DISTRICT  
2475 BURLINGTON PKE  
BURLINGTON, KY 41005  
OFFICE: 859-586-7270  
MOBILE: 859-991-5104

SANITATION DISTRICT NO. 1 (SD1) FOR SANITARY AND STORM  
ANDY AMAN  
PLAN REVIEW MANAGER  
1045 EATON DR.  
F1, WHICHI, KY 41017  
519-578-6880  
AAMAN@SD1.ORG

**OWNER:**

DELAWARE CROSSING LLC  
25 TOWN CENTER BLVD SUITE 104  
CRESTVIEW HILLS KY 41017

**CIVIL ENGINEERING:**

EVANS ENGINEERING  
4240 AIRPORT RD., SUITE 211  
CINCINNATI, OH 45226  
(513)-321-2168  
JONATHAN R. EVANS, P.E.

**SURVEYING:**

BERDING SURVEYING  
741 MAIN STREET  
MILLSBORO, OH 45150  
(513)-831-5505  
CLERY BERDING, P.S.

**IMPERVIOUS SURFACE RATIO:**

TOTAL SITE - 44.4 ACRES  
R/W TO BE DEDICATED = 4.65 ACRES  
IMPERVIOUS AREA - 10.45 ACRES  
PERVIOUS AREA - 29.3 ACRES  
ISR = 0.25

**ZONE INFORMATION:**

ZONE: SUBURBAN RESIDENTIAL ONE (SR-1)  
MAX INTENSITY = 4 D.U. PER ACRE + 50% INCREASE W/PD ~ 6 UNITS/ACRE  
\*\*6.60 UNITS/ACRE PROPOSED.  
MAX BUILDING HEIGHT = 45'  
MIN GREEN SPACE 30% (20% WITH PLANNED PAVED PATIS), 75% PROVIDED  
DETACHED AND PATIO HOUSING  
MIN LOT AREA = 4,000 SF  
MIN FRONTAGE = 50'  
MIN FRONT YARD SETBACK = 30' \*\* REQUESTING 20 FT.  
MIN REAR YARD SETBACK = 30' (ON PERIMETER ONLY) \*\* REQUESTING 20 FT.  
MIN SIDE YARD SETBACK = 5' MIN / 15' TOTAL

**NOTES:**

- OWNER: DELAWARE CROSSING LLC., 25 TOWN CENTER BLVD SUITE 104, CRESTVIEW HILLS KY 41017
- PROPERTY ADDRESS LARIAT WAY, WATSON LN., HOBROOK LN.
- PRESNT ZONING OF PROPERTY = SR-1
- PRESNT USE = VACANT
- SEWER - ALL SANITARY SEWER WILL BE PUBLIC SEWER CONSTRUCTED TO THE STANDARDS OF SANITATION DISTRICT NO. 1 AND WILL BE OWNED BY SANITATION DISTRICT NO. 1, WITH A 20' EASEMENT OR AS DETERMINED BY SANITATION DISTRICT NO. 1.
- WATER PROPOSED PUBLIC WATER MAINS WILL BE EXTENDED FROM LARIAT WAY AND WATSON LN. & CONSTRUCTED IN ACCORD WITH THE STANDARDS OF THE BOONE COUNTY WATER DISTRICT.
- GAS - DUKE ENERGY
- ELECTRIC - OWEN ELECTRIC & DUKE
- TELEPHONE - CINCINNATI BELL
- ALL PROPOSED UTILITIES TO BE UNDERGROUND
- EXISTING SOIL TYPES = ROSSMOYNE SILT LOAM (RSB, HSC); JESSUP SILT LOAM (JED); CYNTHIANA SILTY CLAY LOAM (CYF).
- PARKING WILL BE ON ONE SIDE OF THE STREET ONLY FOR ALL STREETS.
- ALL STREETS TO BE PUBLIC STREETS. DRIVE TO POPLAR POINTE TO BE PRIVATE DRIVE.
- THIS PROJECT LIES WITHIN FLOOD ZONE X (AREAS OF MINIMAL FLOODING) AND NEAR FLOOD ZONE A (NO FLOOD ELEVATIONS DETERMINED) AS SHOWN ON FLOOD INSURANCE MAP FOR BOONE COUNTY, COMMUNITY-PANEL NUMBER 21015C0115D, EFFECTIVE DATE, 3/21/2023.
- ALL STORM SEWER TO BE CONSTRUCTED IN ACCORD WITH THE RULES AND REGULATIONS OF SANITARY DISTRICT NO. 1.
- EXISTING TOPOGRAPHIC PLAN BASED ON A SURVEY BY BERDING SURVEYING.
- ALL BOUNDARY INFORMATION IS BASED A FIELD SURVEY BY BERDING SURVEYING.
- TREES WILL BE PLANTED IN ACCORD WITH SECTION 3619 OF THE BOONE COUNTY ZONING REGULATIONS.

**SHEET INDEX:**

- C-1 COVER SHEET
- C-2 OVERALL ZONING PLAN
- C-2.1 OVERALL ZONING PLAN
- C-3 EXISTING CONCEPT DEVELOPMENT PLAN
- C-3.1 EXISTING CONCEPT DEVELOPMENT PLAN
- C-4 PROPOSED CONCEPT DEVELOPMENT PLAN
- C-4.1 PROPOSED CONCEPT DEVELOPMENT PLAN
- C-5 UTILITY & GRADING PLAN
- C-5.1 UTILITY & GRADING PLAN

**LEGEND**

- MAILBOX
- CATV BOX
- TELEPHONE BOX
- ELECTRIC BOX
- TRANSFORMER
- GAS VALVE
- WATER SPIGOT
- WATER METER
- WATER VALVE
- FIRE HYDRANT
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- SLMH STORM MANHOLE
- SMH SANITARY MANHOLE-APPROXIMATE LOCATION SHOWN PER RECORD
- OSMH SANITARY MANHOLE
- MANHOLE-TYPE UNDETERMINED
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- OVERHEAD UTILITY(S)
- UNDERGROUND TELEPHONE LINE-APPROXIMATE LOCATION SHOWN PER RECORD
- GAS LINE/SIZE-APPROXIMATE LOCATION SHOWN PER RECORD
- WATER LINE-APPROXIMATE LOCATION SHOWN PER RECORD
- WATER LINE PER MARKINGS IN RESPONSE TO KY811 REQUEST ON 5-3-2024
- STORM LINE/SIZE/TYPE-FIELD VERIFIED
- SANITARY LINE/SIZE/TYPE-APPROXIMATE LOCATION SHOWN PER RECORD
- SANITARY LINE/SIZE/TYPE-FIELD VERIFIED

**PRELIMINARY**  
NOT FOR CONSTRUCTION

REVISIONS	NO. & DESCRIPTION
DATE	BY

**EVANS ENGINEERING**  
4240 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45226  
(513) 321-2168

HICKORY BURLINGTON & POPLAR POINTE  
APARTMENTS  
**COVER SHEET**  
LARIAT WAY,  
BURLINGTON, BOONE COUNTY, KY

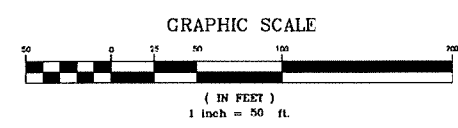
SCALE:	HORIZ.	VERT.
	1"=150'	N/A

JOB NO.	24-101
DATE	JUL 2, 2024

**SHEET NO.**

C-1

**PRELIMINARY**  
NOT FOR CONSTRUCTION



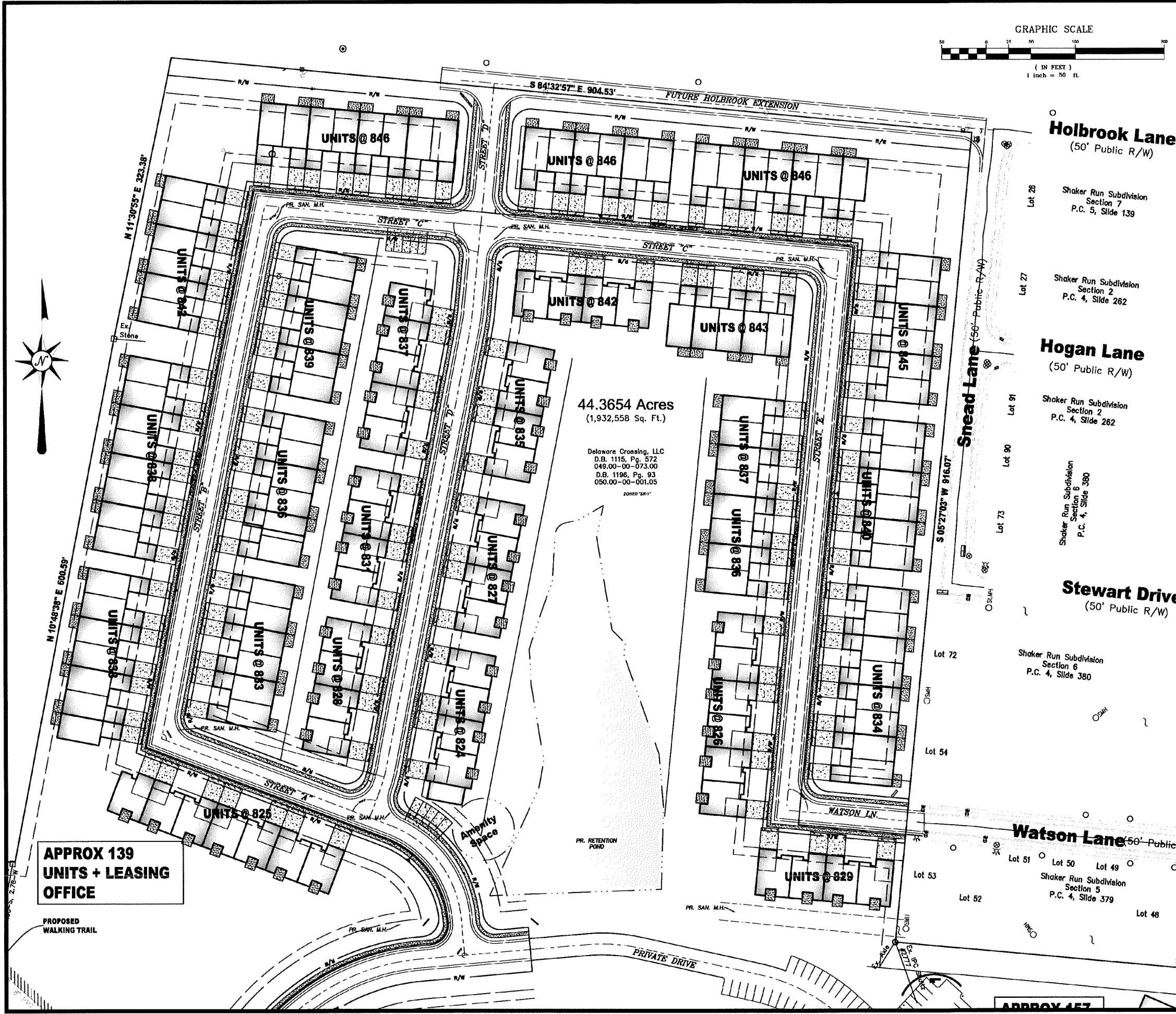
**SITE DATA:**

- TOTAL PROJECT ACREAGE: 44.37 AC.  
RESIDENTIAL PROJECT ACREAGE: 40.08 AC.  
STREET RIGHT-OF-WAY ACREAGE: 4.29 AC.
  - PROJECT WILL BE PATIO RENTAL HOMES AND MULTI-FAMILY RESIDENTIAL USE.
  - MULTI-FAMILY RESIDENTIAL: NOTE DENSITY CALCULATIONS SHOWN ARE FOR THE MULTI-FAMILY PROJECT AREA.
- PROPOSED ATTACHED HOUSING:  
PATIO HOMES (RENTALS) - 139 UNITS  
APARTMENT BUILDINGS WITH 67 UNITS EACH - 128 UNITS  
TOTAL: 267 PROPOSED MULTI-FAMILY RESIDENTIAL HOUSING UNITS  
267 UNITS/44.37 ACRES = 6± UNITS PER ACRE AS SHOWN ON PLAN.

- SOIL TYPES (PER USDA SOIL CONSERVATION SERVICE MAPS):
- ED (JESSUP SILT LOAM, 12-20% SLOPES)
  - RSB (ROSSMOYNE SILT LOAM, 0-6% SLOPES)
  - RSC (ROSSMOYNE SILT LOAM, 6-12% SLOPES)

**GENERAL NOTES:**

- ITEMS SHOWN AS PROPOSED ON CONCEPT DEVELOPMENT PLAN SHOULD BE RELOCATED AND OR NEW AND LOOK SIMILAR TO EXISTING ITEMS.
- LANDSCAPING SHALL MEET THE REQUIREMENTS OF THE BOONE COUNTY ZONING ORDINANCE, ARTICLE 36.
- ALL INTERNAL STREETS SHALL BE PRIVATE, EXCEPT STREET A WHICH WILL BE PUBLIC.



44.3654 Acres  
(1,932,558 Sq. Ft.)

Delaware Crossing, LLC  
D.B. 1115, Pg. 572  
049.00-00-073.00  
D.B. 1196, Pg. 93  
050.00-00-001.05  
ZONED "SR-1"

**APPROX 139 UNITS + LEASING OFFICE**



\* CALL BEFORE YOU DIG "B.U.D." \*

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REVISIONS	DATE	BY	NO. & DESCRIPTION

**EVANS ENGINEERING**  
4240 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45226  
(513) 321-2168



HICKORY BURLINGON & POPLAR APARTMENTS  
**OVERALL ZONING PLAN**  
LAWIAT WAY,  
BERLINGTON, BOONE COUNTY, KY

SCALE:	HORIZ.	VERT.
	1"=50'	N/A
JOB NO.	24-101	
DATE	Jul. 2, 2024	

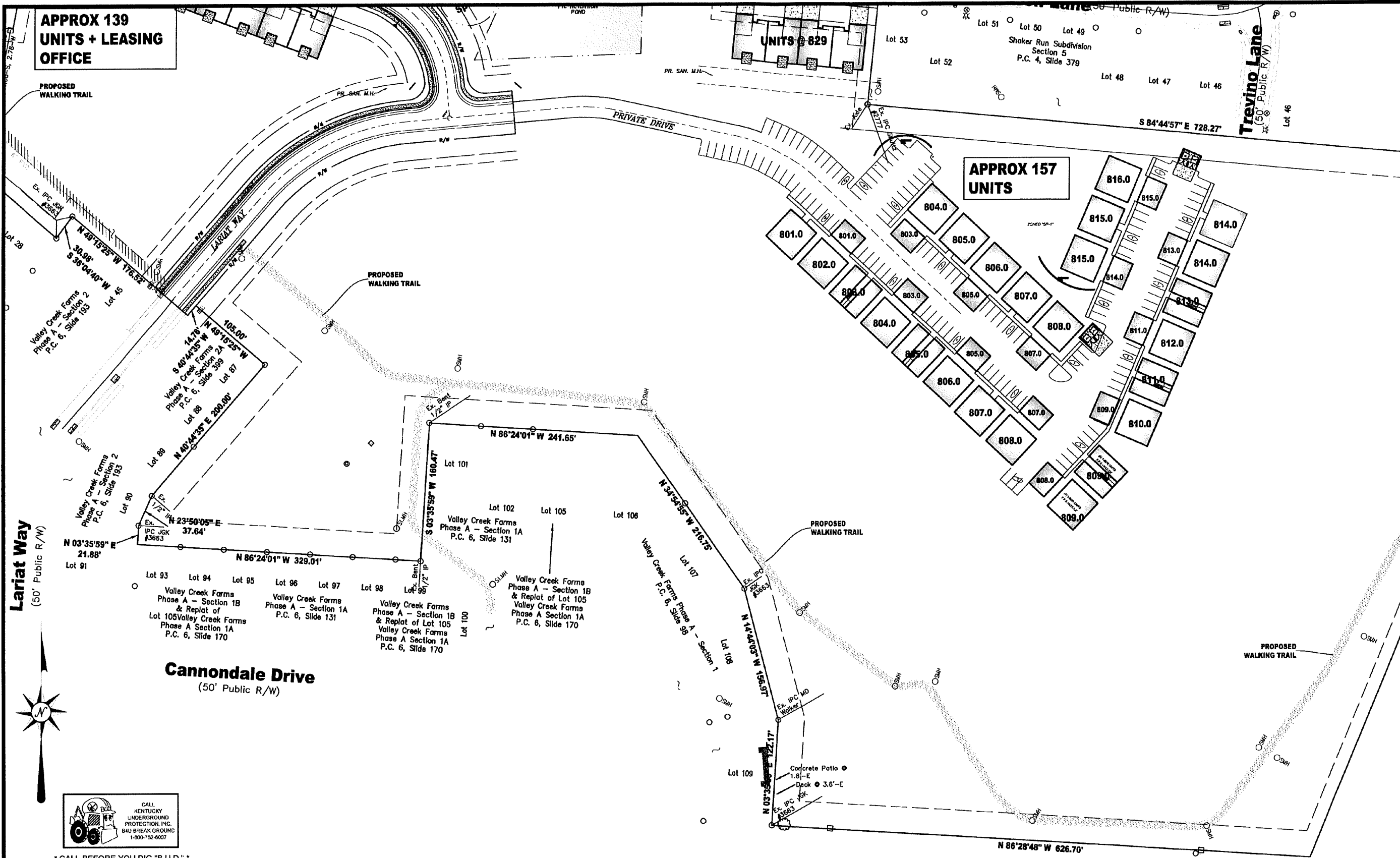
**SHEET NO.**  
C-2

**APPROX 139  
UNITS + LEASING  
OFFICE**

**UNITS @ 829**

**APPROX 157  
UNITS**

**Trevino Lane**  
(50' Public R/W)



**PRELIMINARY  
NOT FOR CONSTRUCTION**

REVISIONS	NO. & DESCRIPTION
BY	
DATE	

**EVANS ENGINEERING**  
4240 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45226  
(513) 321-2168



HICKORY BURLINGTON & POPLAR POINT  
APARTMENTS  
**OVERALL ZONING PLAN**  
LARIAT WAY,  
BURLINGTON, BOONE COUNTY, KY

SCALE:	HORIZ.	VERT.
	1"=50'	N/A

JOB NO.	24-101
DATE	J.I. 2, 2024

**SHEET NO.**

C-2.1

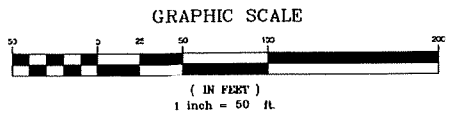


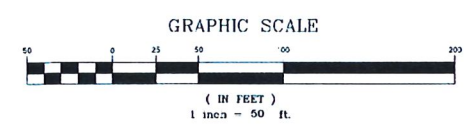
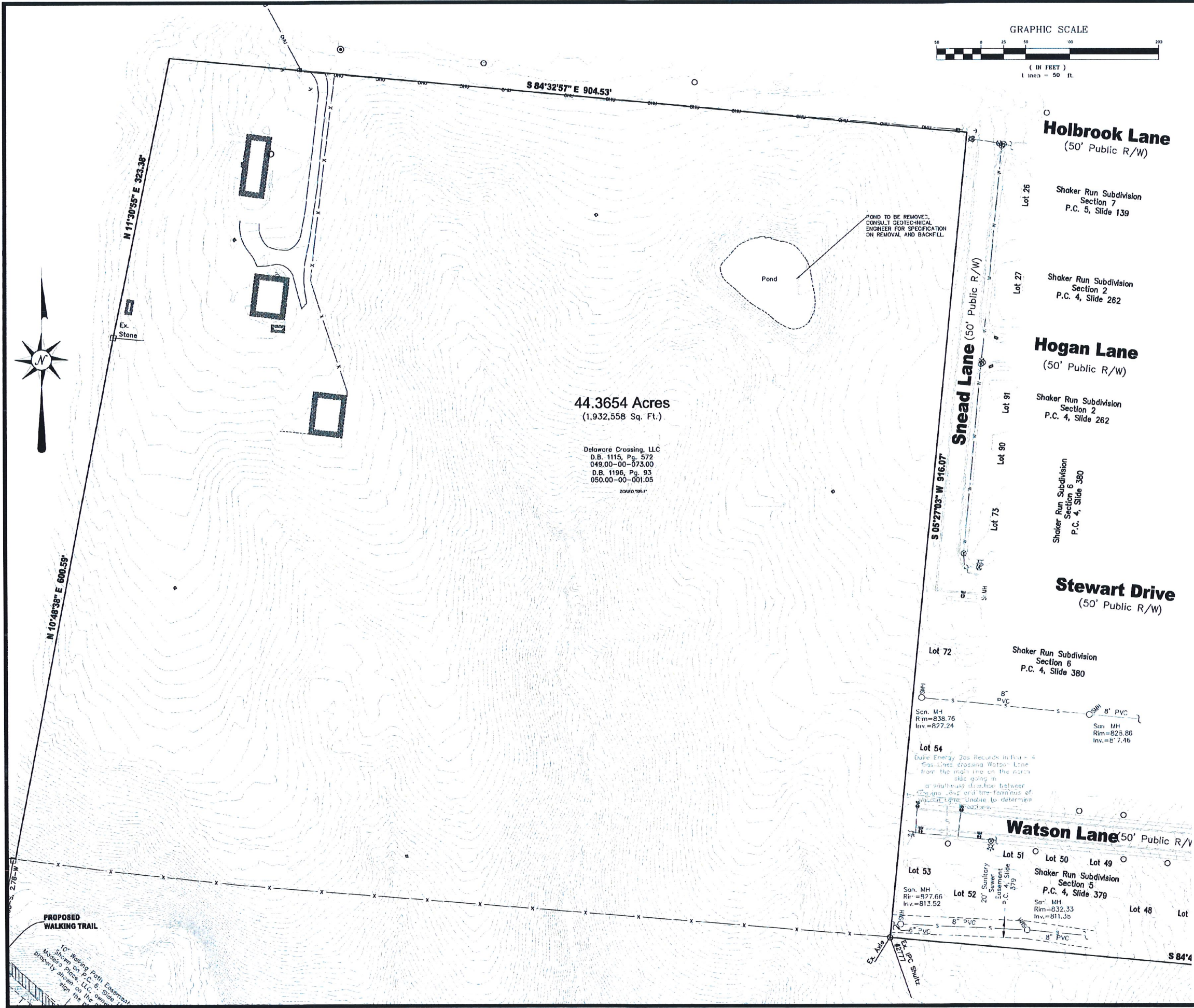
"CALL BEFORE YOU DIG "B.U.D." "

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44.3654 Acres  
(1,932,558 Sq. Ft.)

Delaware Crossing, LLC  
D.B. 1115, Pg. 572  
049.00-00-073.00  
D.B. 1196, Pg. 93  
050.00-00-001.05  
ZONED R1.1

POND TO BE REMOVED.  
CONSULT GEOTECHNICAL  
ENGINEER FOR SPECIFICATION  
ON REMOVAL AND BACKFILL.

- LEGEND**
- 1-1 MAILBOX
  - m CATV BOX
  - TELEPHONE BOX
  - ELECTRIC BOX
  - TRANSFORMER
  - GAS VALVE
  - WATER SPOGOT
  - WATER METER
  - WATER VALVE
  - FIRE HYDRANT
  - CATCH BASIN
  - SINGLE GRATE INLET
  - DOUBLE GRATE INLET
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  - SMH SANITARY MANHOLE
  - MANHOLE-TYPE UNDETERMINED
  - CLEANOUT
  - GUY WIRE
  - UTILITY POLE
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  - STORM LINE/SIZE/TYPER-FIELD VERIFIED
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\* CALL BEFORE YOU DIG "B.U.D." \*

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**PRELIMINARY**  
NOT FOR CONSTRUCTION

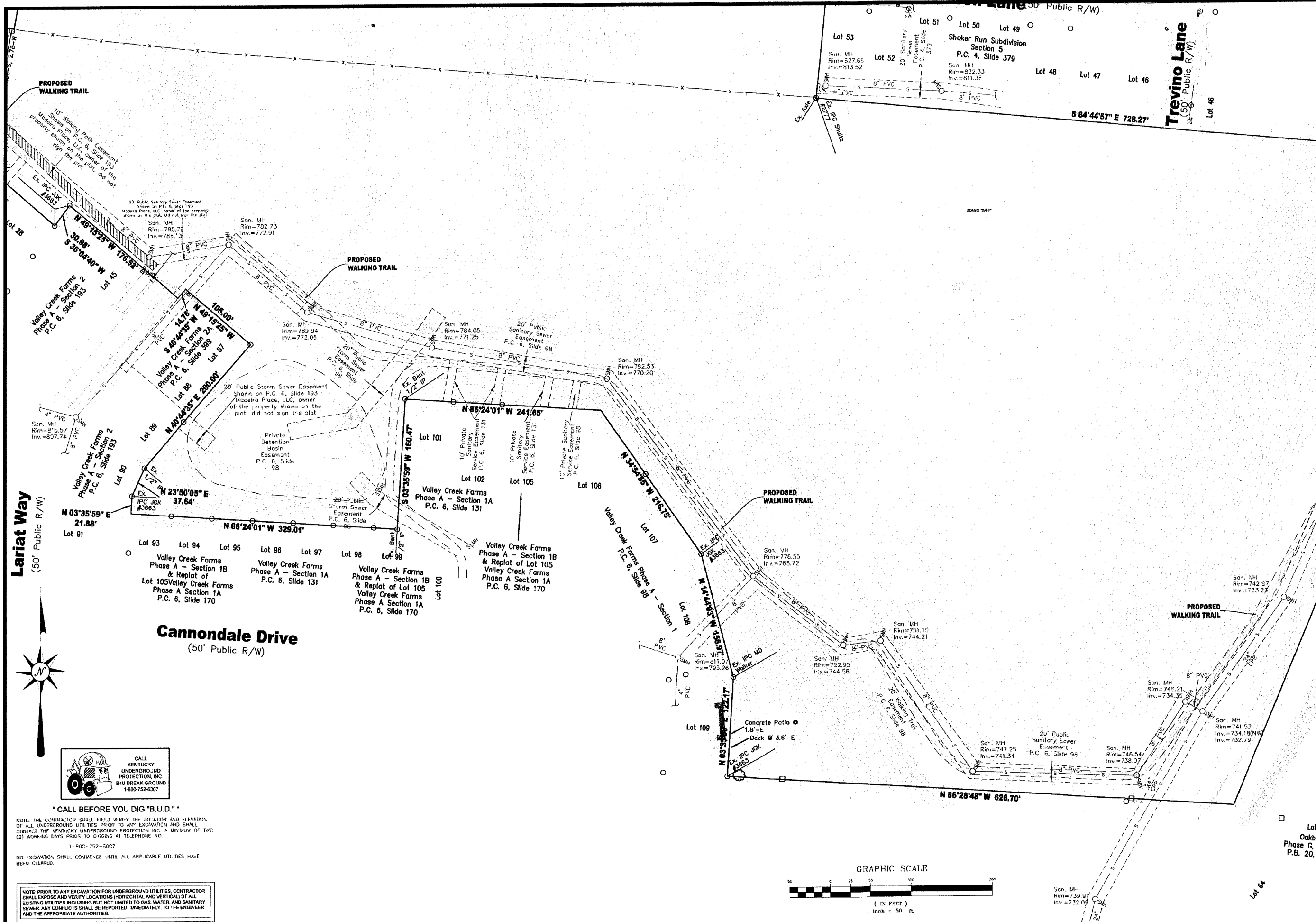
REVISIONS	
NO. & DESCRIPTION	DATE

**EVANS ENGINEERING**  
4240 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45226  
(513) 321-2168

HICKORY BURLINGTON & POPLAR POINTE  
APARTMENTS  
**EXISTING CONCEPT  
DEVELOPMENT PLAN**  
LARIAT WAY,  
BURLINGTON, ROONE COUNTY, KY

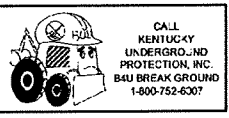
SCALE:	HORIZ.	VERT.
	1"=50'	N/A
JOB NO.	24-101	
DATE	Jul. 2, 2024	

SHEET NO.  
C-3



**Lariat Way**  
(50' Public R/W)

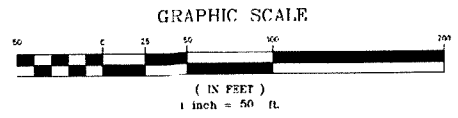
**Cannondale Drive**  
(50' Public R/W)



CALL KENTUCKY UNDERGROUND PROTECTION, INC. BEFORE YOU DIG "B.U.D."  
1-800-752-5007

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**PRELIMINARY**  
NOT FOR CONSTRUCTION

REVISIONS	BY	NO. & DESCRIPTION

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4240 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45226  
(513) 321-2168



HICKORY BURLINGTON & NORLAK POINT  
APARTMENTS  
**EXISTING CONCEPT  
DEVELOPMENT PLAN**  
LARIAT WAY,  
BURLINGTON, BOONE COUNTY, KY

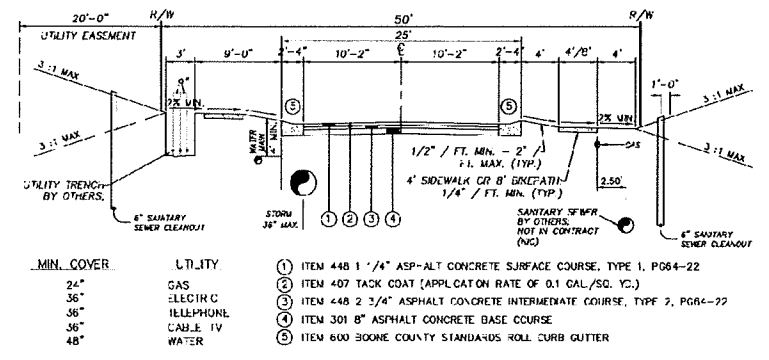
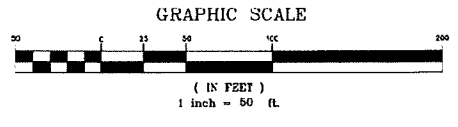
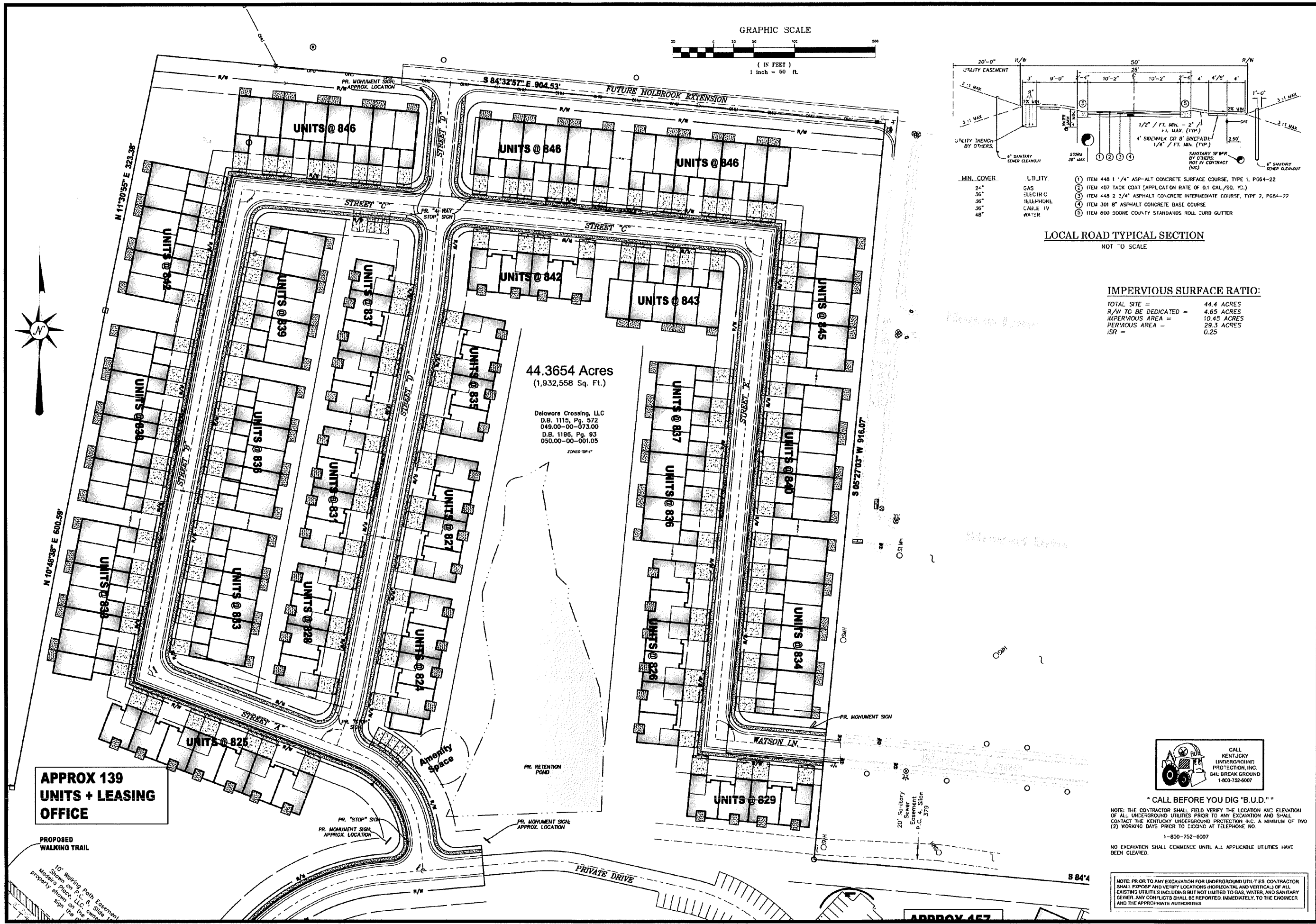
SCALE:	HORIZ.	VERT.
	1"=50'	N/A

JOB NO.	24-101
DATE	Jul. 2, 2024

**SHEET NO.**

C-3.1

Lot  
Oakbr  
Phase C,  
P.B. 20,



LOCAL ROAD TYPICAL SECTION  
NOT TO SCALE

IMPERVIOUS SURFACE RATIO:  
TOTAL SITE = 44.4 ACRES  
R/W TO BE DEDICATED = 4.65 ACRES  
IMPERVIOUS AREA = 10.45 ACRES  
PERVIOUS AREA = 29.3 ACRES  
ISR = 0.25

44.3654 Acres  
(1,932,558 Sq. Ft.)

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D.B. 1115, Pg. 572  
049.00-00-073.00  
D.B. 1186, Pg. 93  
050.00-00-001.05  
ZONED "R-1"

APPROX 139  
UNITS + LEASING  
OFFICE

PROPOSED  
WALKING TRAIL



\* CALL BEFORE YOU DIG "B.U.D."\*

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**PRELIMINARY**  
NOT FOR CONSTRUCTION

REVISIONS	NO.	DESCRIPTION
BY		
DATE		

**EVANS ENGINEERING**  
4240 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45226  
(513) 321-2168



HICKORY BURLINGTON & POPLAR POINTE  
APARTMENTS  
**PROPOSED CONCEPT  
DEVELOPMENT PLAN**  
LARIAT WAY,  
BURLINGTON, BOONE COUNTY, KY

SCALE:	HORIZ.	VERT.
	1"=50'	N/A
JOB NO.	24-101	
DATE:	Jul. 2, 2024	

SHEET NO.

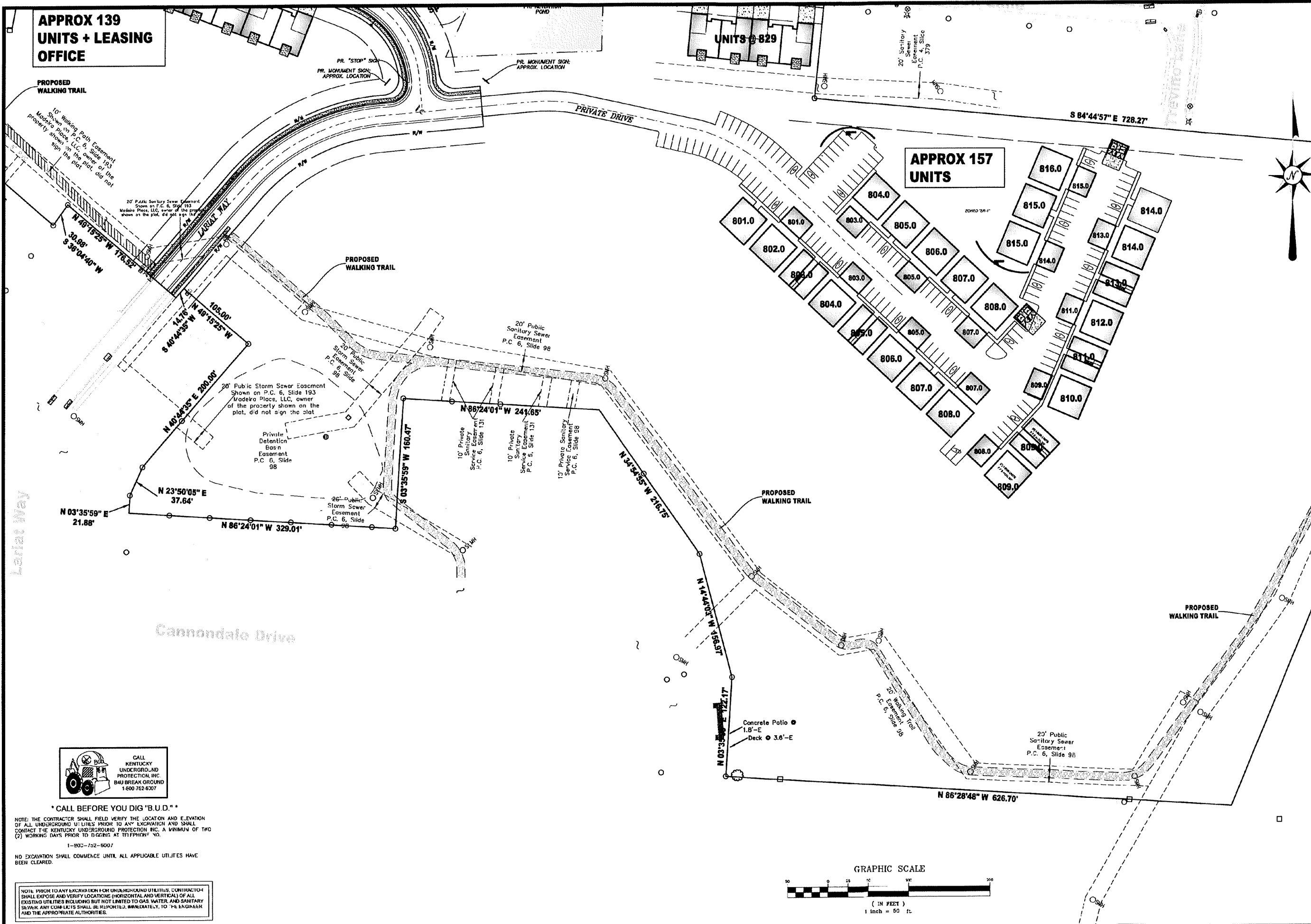
C-4

**APPROX 139  
UNITS + LEASING  
OFFICE**

**UNITS 829**

**APPROX 157  
UNITS**

**PRELIMINARY  
NOT FOR CONSTRUCTION**



REVISIONS	NO. & DESCRIPTION
DATE	BY

**EVANS ENGINEERING**  
4240 AIRPORT ROAD, SUITE 211  
CINCINNATI, OHIO 45226  
(513) 321-2168



HICKORY BURLINGTON & POPULAR POINTE  
APARTMENTS  
**PROPOSED CONCEPT  
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LARIAT WAY,  
BURLINGTON, BOONE COUNTY, KY

SCALE:	HORIZ.	VERT.
	1"=50'	N/A
JOB NO.	24-101	
DATE	Jul. 2, 2024	

SHEET NO.

C-4.1



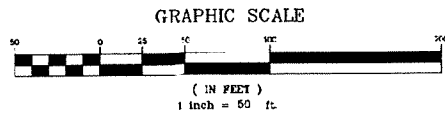
**\* CALL BEFORE YOU DIG "B.U.D." \***

NOTE: THE CONTRACTOR SHALL FIELD VERIFY THE LOCATION AND ELEVATION OF ALL UNDERGROUND UTILITIES PRIOR TO ANY EXCAVATION AND SHALL CONTACT THE KENTUCKY UNDERGROUND PROTECTION INC. A MINIMUM OF TWO (2) WORKING DAYS PRIOR TO DIGGING AT TELEPHONE NO.

1-802-752-8807

NO EXCAVATION SHALL COMMENCE UNTIL ALL APPLICABLE UTILITIES HAVE BEEN CLEARED.

NOTE: PRIOR TO ANY EXCAVATION FOR UNDERGROUND UTILITIES, CONTRACTOR SHALL EXPOSE AND VERIFY LOCATIONS (HORIZONTAL AND VERTICAL) OF ALL EXISTING UTILITIES INCLUDING BUT NOT LIMITED TO GAS, WATER, AND SANITARY SEWER. ANY CONFLICTS SHALL BE REPORTED IMMEDIATELY TO THE ENGINEER AND THE APPROPRIATE AUTHORITIES.







PROPOSED 1-STORY RESIDENCES FOR: DELAWARE CROSSING, LLC

# HOLBROOK LN • LOT 00

1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042 • BOONE Co. PARCEL ID: 049.00-00-073.00

"THE BTR COLLECTION" • CONSTRUCTION DOCUMENTS-1

## BROOKSTONE HOMES, LLC.

7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200



*Virtual f/x*

10351 KENWOOD RD.

BLUE ASH, OHIO 45242

(513) 393-0857



Virtual f/x View • Elevation-A

PROPERTY INFORMATION

20 AC ON HOLBROOK LN (49-73)

SHEET INDEX

ID	Layout Name	Published
T1	Title Sheet	<input checked="" type="checkbox"/>
CDA.1	E1 28' Unit • Front Elevation-A & B	<input checked="" type="checkbox"/>
CDA.2	E2 28' Unit • Other Elevations	<input checked="" type="checkbox"/>
CDA.3	E2 28' End Unit • Elevations	<input checked="" type="checkbox"/>
CDA.4	Vfx Holbrook 28' • Virtual Tour-A	<input checked="" type="checkbox"/>
CDA.5	Vfx Holbrook 28' End • Virtual Tour-A	<input checked="" type="checkbox"/>
CDB.1	1. 28' Unit • First Floor Plans	<input checked="" type="checkbox"/>
CDB.2	-1. 28' Unit • Foundation Plans	<input checked="" type="checkbox"/>
CDB.3	-1. 28' Unit • Roof Plans	<input checked="" type="checkbox"/>
CDB.4.1	Vfx Holbrook 28' • 3D-vfx Int Tour-1	<input checked="" type="checkbox"/>
CDB.4.2	Vfx Holbrook 28' • 3D-vfx Int Tour-2	<input checked="" type="checkbox"/>
CDB.5.1	Vfx Holbrook 28' End • 3D-vfx Int ...	<input checked="" type="checkbox"/>
CDB.5.2	Vfx Holbrook 28' End • 3D-vfx Int ...	<input checked="" type="checkbox"/>
CDC.1	CS1 • Cross Section-1	<input checked="" type="checkbox"/>
CDC.2	CS2 • Cross Section-2	<input checked="" type="checkbox"/>
CDC.3	BS1 • Building Sections	<input checked="" type="checkbox"/>
CDD.1	E1 38' Unit • Front Elevation-A & B	<input checked="" type="checkbox"/>
CDD.2	E2 38' Unit • Other Elevations	<input checked="" type="checkbox"/>
CDD.3	Vfx Holbrook 38' • Virtual Tour-A	<input checked="" type="checkbox"/>
CDE.1	1. 38' Unit • First Floor Plans	<input checked="" type="checkbox"/>
CDE.2	-1. 38' Unit • Foundation Plans	<input checked="" type="checkbox"/>
CDE.3	-1. 38' Unit • Roof Plans	<input checked="" type="checkbox"/>
CDE.4.1	Vfx Holbrook 38' • 3D-vfx Int Tour-1	<input checked="" type="checkbox"/>
CDE.4.2	Vfx Holbrook 38' • 3D-vfx Int Tour-2	<input checked="" type="checkbox"/>
CF.1	CS1 • Cross Section-1 & 2	<input checked="" type="checkbox"/>
CF.2	CS2 • Cross Section-3	<input checked="" type="checkbox"/>
CF.3	BS1 • Building Sections	<input checked="" type="checkbox"/>
CF.4	Typical Wall Section	<input type="checkbox"/>
CDG	Notes & Specs	<input checked="" type="checkbox"/>
VfxA	HB00 Holbrook 28' • Render-A	<input type="checkbox"/>

Square Footages:	28' Units	38' Units
• First Floor-A	= 1,352 sqft	= 1,487 sqft
• First Floor-A (End Unit)	= 1,382 sqft	= N/A sqft
• 1st Floor Slab	= 1,301 sqft	= 1,430 sqft
• 1st Floor Slab (End Unit)	= 1,329 sqft	= N/A sqft
• Garage Slab	= 372 sqft	= 372 sqft
• Front Porch Slab	= 40 sqft	= 25 sqft
• Rear 12 x 8 Patio	= 96 sqft	= 96 sqft

CODE INFORMATION

- REFERENCED CODE: RESIDENTIAL CODE OF KENTUCKY • KRC-2019
- BUILDING CLASSIFICATION: USE GROUP - • SFR
- BUILDING CONSTRUCTION TYPE: • 5B (UNPROTECTED)
- ROOF LIVE LOAD = 25 PSF
- SNOW LOAD = 25 PSF
- WIND LOAD @ 20 PSF
- FLOOR LOAD @ 40 PSF

OWNER DELAWARE CROSSING, LLC  
 RESIDENTIAL SERVICES VIRTUAL F/X  
 MICHAEL P. FISHER  
 CONTRACTOR BROOKSTONE HOMES, LLC.

DATE: 06/24/24 • DD1  
 REVISED:  
 RELEASE:  
 PERMIT RELEASE:

Vf/x

Virtual f/x  
 10351 Kenwood Rd. • Suite 201 • Blue Ash, Ohio 45242  
 Designed By: P. Michael Fisher • (513) 393-0857  
 Email: virtual.fx@icloud.com

REVISIONS:  
 Job Request 06/07/24  
 Design Dev-1 06/24/24



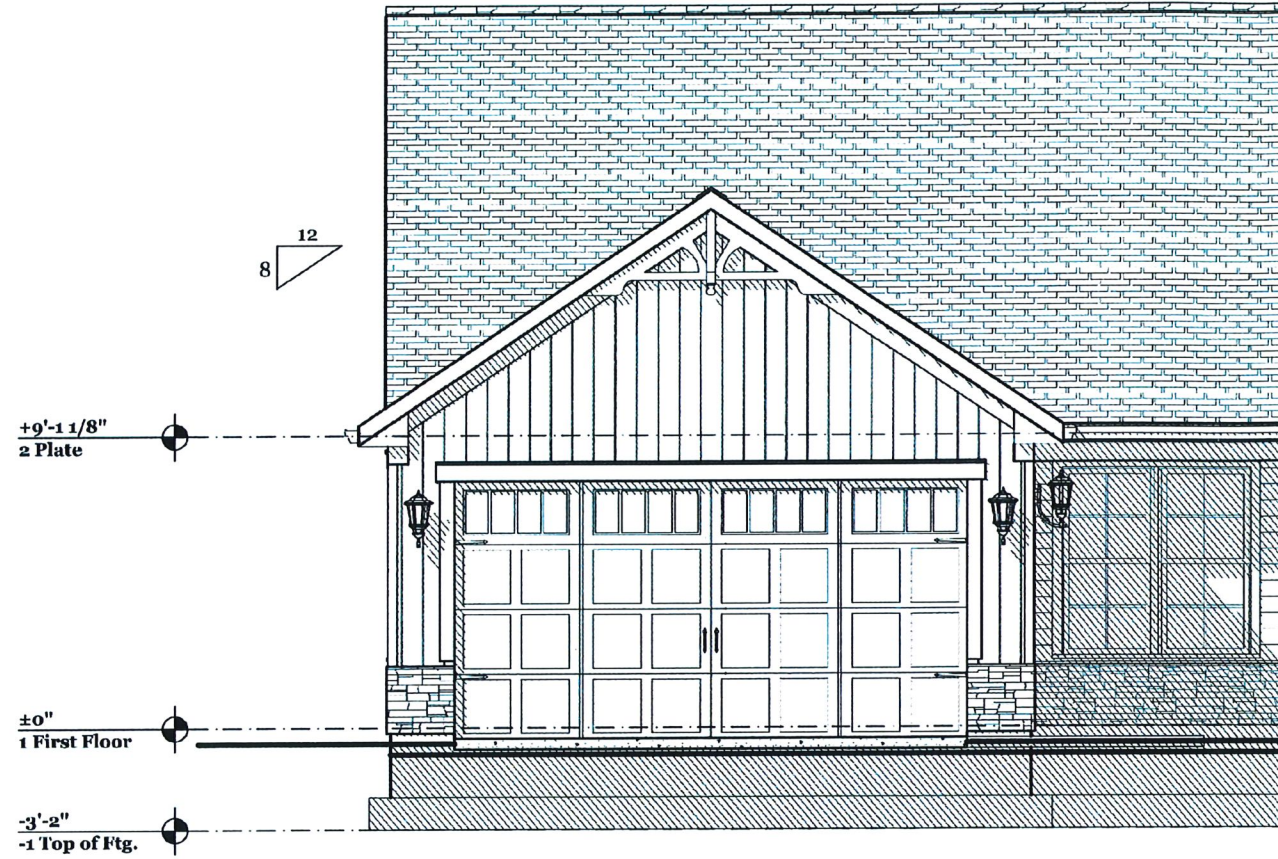
2024-0007

HB00 The Multi-Fam's  
 EL-A • G. Varies  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
 BROOKSTONE HOMES, LLC.  
 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200

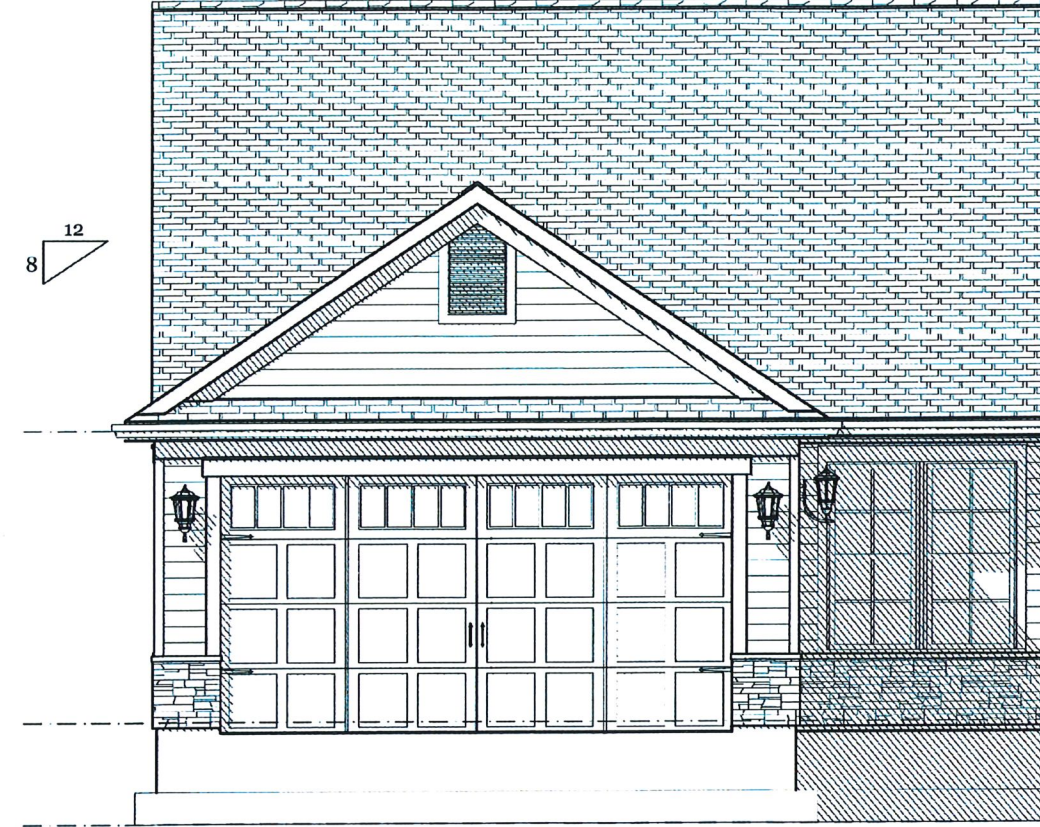
SHEET TITLE  
 Title Sheet

T1

SHEET NO. 1 of 30



**E1** **Holbrook Ln • 28' Unit • Front Elevation-A**  
3/16" = 1'-0"



**E1** **Holbrook Ln • 28' Unit • Front Elevation-B**  
3/16" = 1'-0"

ALL SMOKE ALARMS SHALL BE LISTED IN ACCORDANCE WITH UL-217. ON EACH LEVEL WITHIN EACH DWELLING UNIT SMOKE ALARMS UTILIZING PHOTOVOLTAIC AND IONIZATION TECHNOLOGIES SHALL BE INSTALLED. BEDROOMS TO USE IONIZATION TYPE. OUTSIDE BEDROOMS TO BE PHOTOVOLTAIC TYPE. SEPARATE OR DUEL-SENSING SMOKE ALARMS MAY BE USED

**EXHAUST FAN & SMOKE DETECTOR NOTE:**  
 ● DENOTES EXHAUST FAN LOCATIONS (SEE NOTE 9, D FOR CODE INFO ON SHEET A-5)  
 Ⓢ DENOTES SMOKE DETECTOR LOCATIONS (SEE NOTE 11, b ON SHEET A-5 & THE NOTE TO THE LEFT FOR ALL SMOKE DETECTOR CODE INFO)  
 Ⓢ/CO DENOTES SMOKE/CO COMBO DETECTOR LOCATIONS  
 PROGRAMMABLE THERMOSTAT REQ'D FOR ALL FORCED AIR FURNACES

**WALL BRACING MEATHOD PER RCO 602.10.4**  
 CS-WSP: CONTINUOUSLY SHEATHED WOOD STRUCTURAL PANEL. 3/8" MIN. THK., CONNECTION REQUIREMENTS; FASTENERS COMPLY WITH RCO 602.3(3) FASTENED 6" ON EDGES AND 12" IN FIELD. INTERIOR SHEATHING ON BRACED WALLS SHALL BE 1/2" MIN. GYPSUM BD. FASTENED IN ACCORDANCE WITH RCO TABLE 702.3.5

Note:  
All Downspouts connected to approved locations

**CODE INFORMATION CONT...**  
**STRUCTURAL MEMBER ALLOW ABLE DEFLECTION**  
 • Rafters having slopes greater than 3:12 with no finished ceiling attached to rafters L/180  
 • Interior walls and partitions H/180  
 • Floors and plastered ceilings L/360  
 • All other structural members L/240  
 • Exterior walls with plaster or stucco finish H/360  
 • Exterior walls—wind loads with brittle finishes H/240  
 • Exterior walls—wind loads with flexible finishes L/120d  
 • Lintels supporting masonry veneer walls L/600

Vf/x

Virtual f/x

10351 Kenwood Rd. • Suite 2001 • Blue Ash, Ohio 45242  
 Designed By: P. Michael Fisher • (513) 393-0857  
 Email: virtual.fx@icloud.com

**REVISIONS:**  
 Job Request 06/07/24  
 Design Dev-1 06/24/24

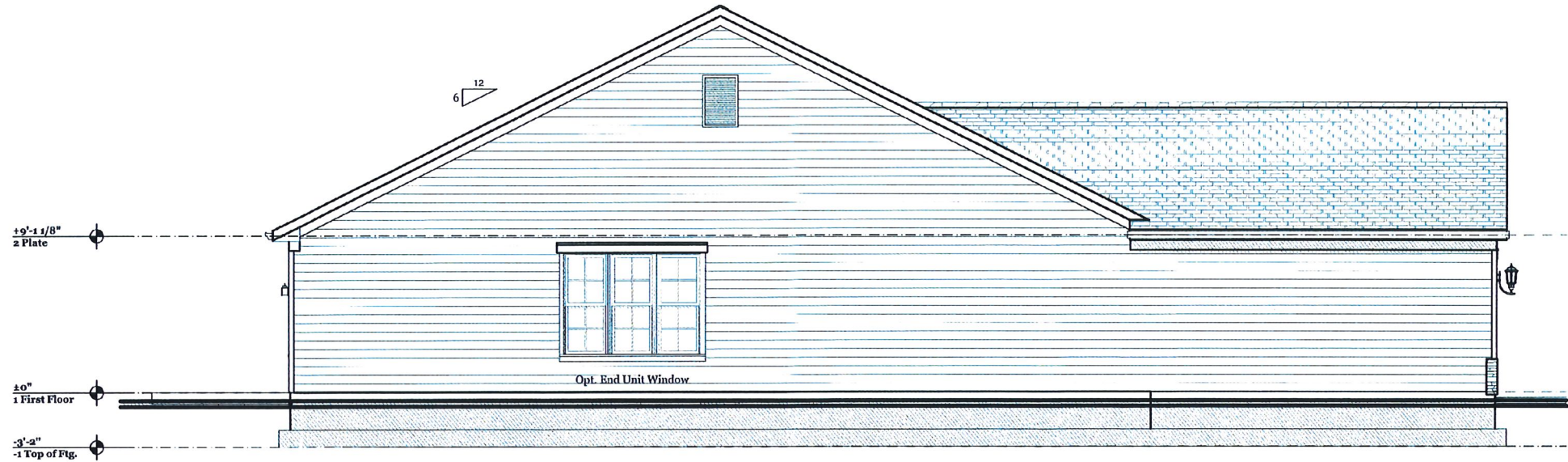
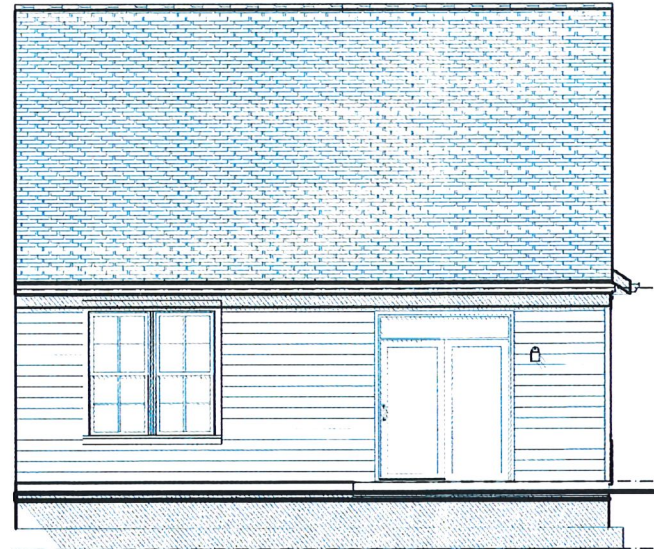
BROOKSTONE  
H O M E S

2024-0607

**HBOO The Multi-Fam's**  
 El-A • G. Varies  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
**BROOKSTONE HOMES, LLC.**  
 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200

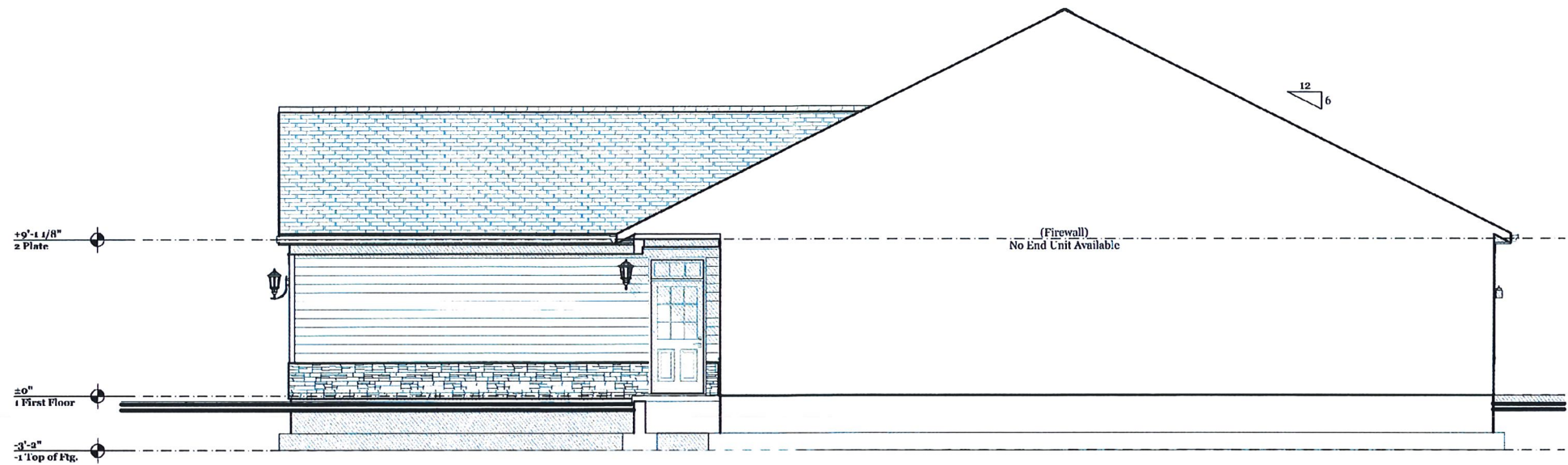
CD  
A.1

SHEET NO. 2 of 30



**E2** 28' Unit • Rear Elevation-A  
1/8" = 1'-0"

**E3** Holbrook Ln • 28' Unit • Left Elevation-A  
1/8" = 1'-0"



**E4** Holbrook Ln • 28' Unit • Right Elevation-A  
1/8" = 1'-0"

**Vf/x**

*Virtual f/x*  
10351 Kenwood Rd. • Suite 2001 • Blue Ash, Ohio 45242  
Designed By: P. Michael Fisher • (513) 393-0857  
Email: virtual.fx@icloud.com



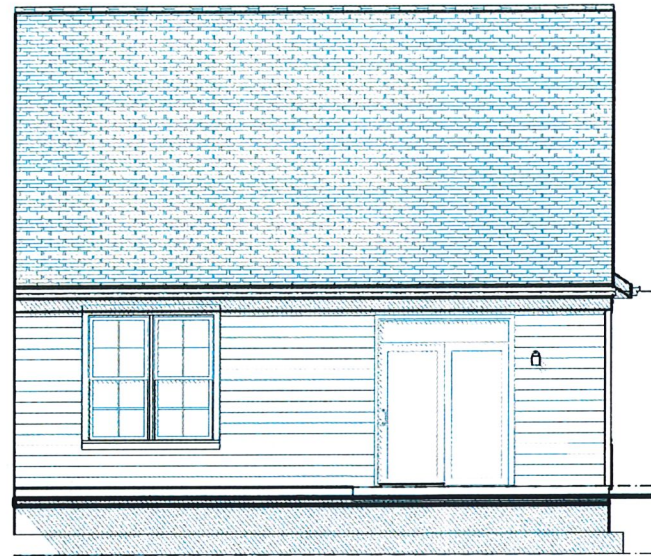
REVISIONS:  
 Job Request 06/07/24  
 Design Dev-1 06/24/24

**BROOKSTONE**  
P O M E S

2024-0607

**HB00** The Multi-Fam's  
 EL-A • G. Varies  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
**BROOKSTONE Homes, LLC.**  
 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200

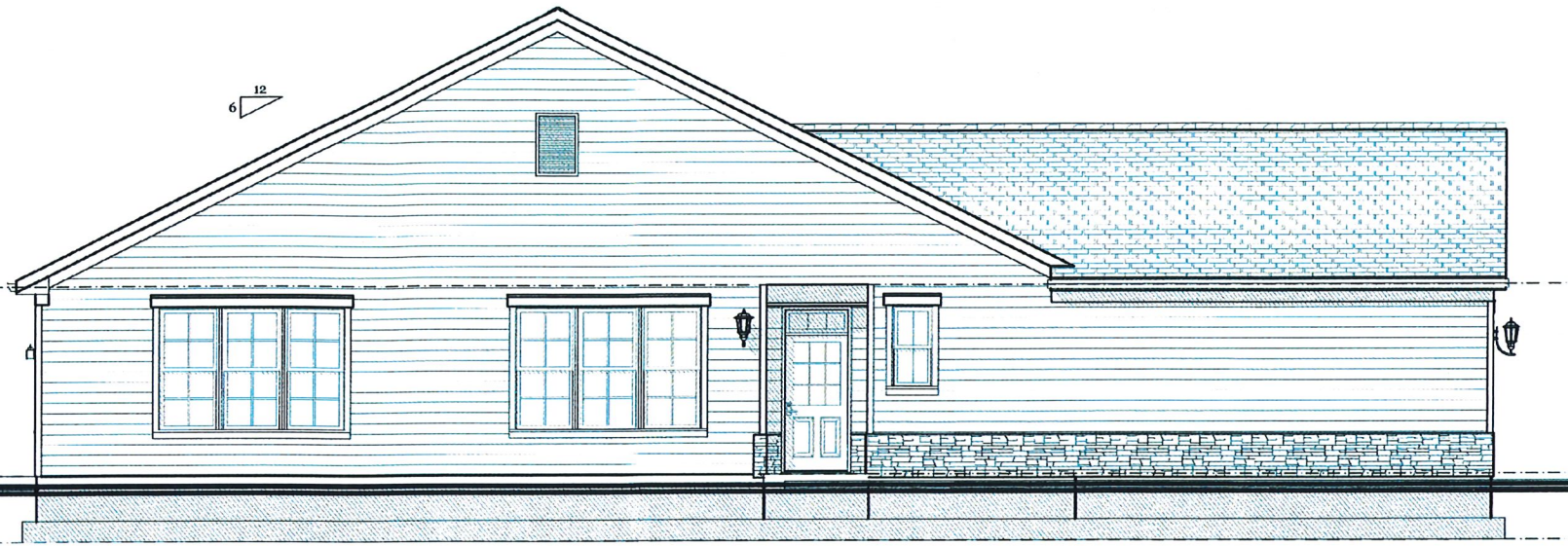
E2 28' Unit • Other Elevations  
**CD**  
**A.2**  
 SHEET NO. 3 of 30



+9'-1 1/8"  
2 Plate

=0"  
1 First Floor

-3'-2"  
-1 Top of Fig.



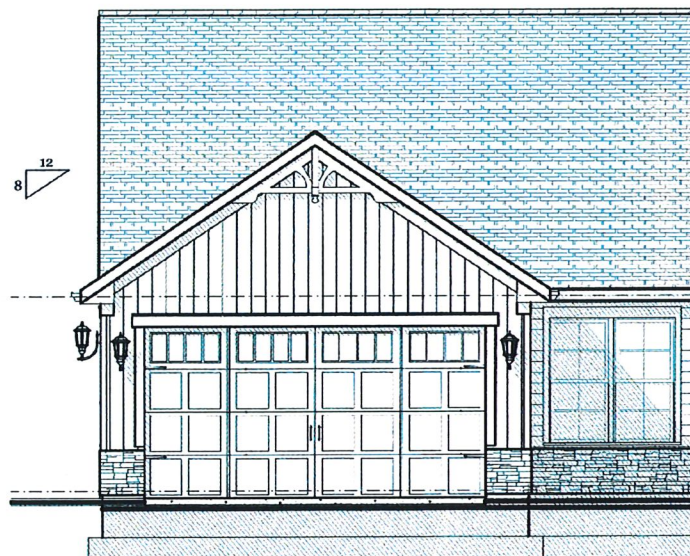
+9'-1 1/8"  
2 Plate

=0"  
1 First Floor

-3'-2"  
-1 Top of Fig.

**E2** 28' Unit • Rear Elevation-A  
1/8" = 1'-0"

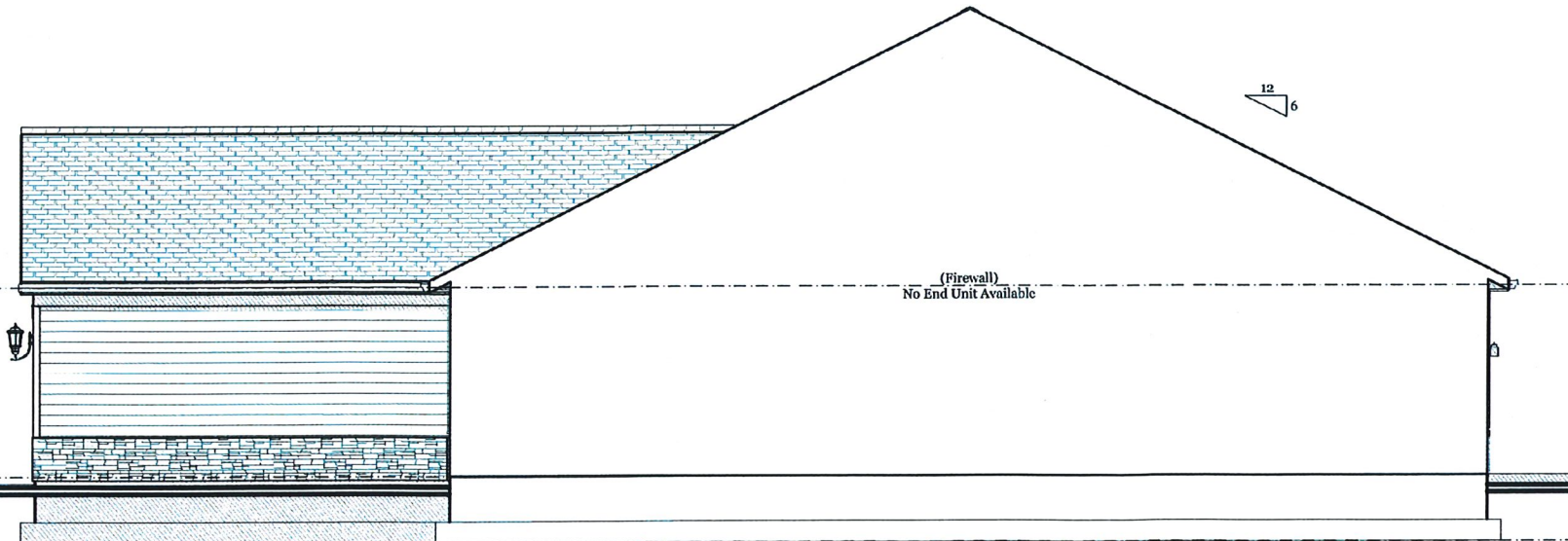
**E3** Holbrook Ln • 28' End Unit • Left Elevation-A  
1/8" = 1'-0"



+9'-1 1/8"  
2 Plate

=0"  
1 First Floor

-3'-2"  
-1 Top of Fig.



+9'-1 1/8"  
2 Plate

=0"  
1 First Floor

-3'-2"  
-1 Top of Fig.

**E1** 28' End Unit • Front Elevation-A  
1/8" = 1'-0"

**E4** Holbrook Ln • 28' End Unit • Right Elevation-A  
1/8" = 1'-0"

**Virtual f/x**

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 Designed By: P. Michael Fisher • (513) 393-0857  
 Email: virtual.fx@icloud.com

REVISIONS:

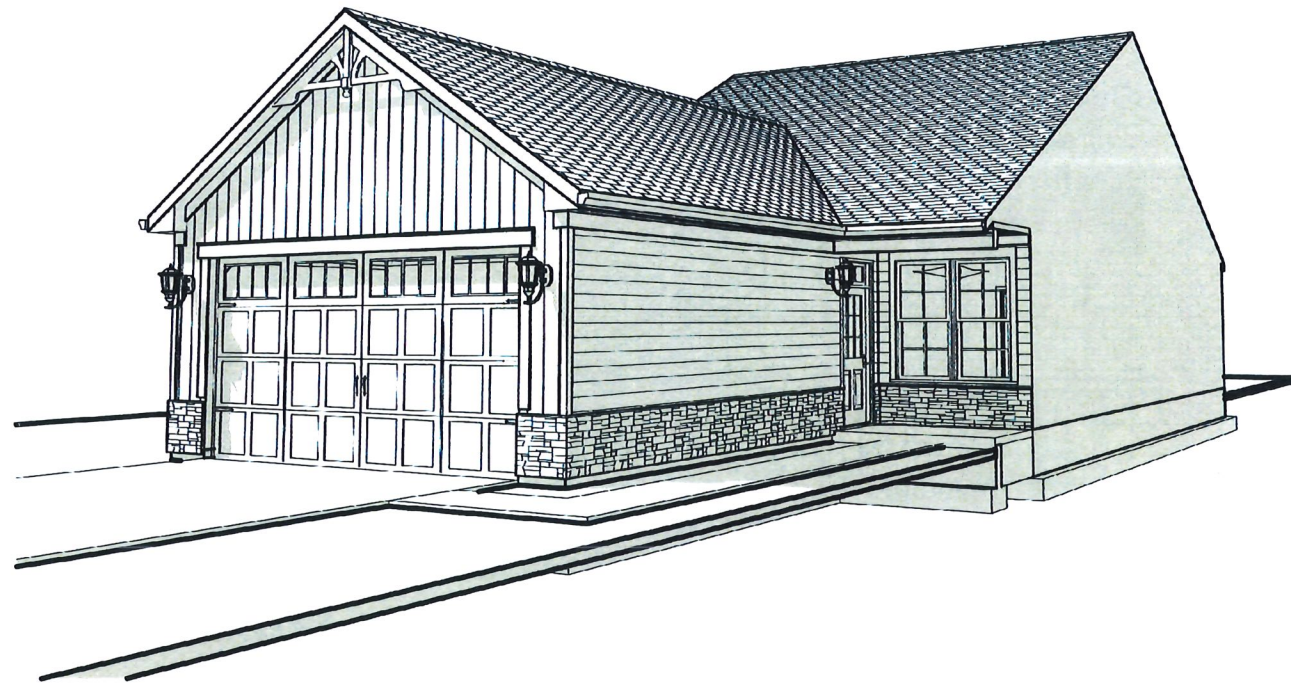
Job Request	06/07/24
Design Dev-1	06/24/24

**BROOKSTONE**  
H O M E S

2024-0607

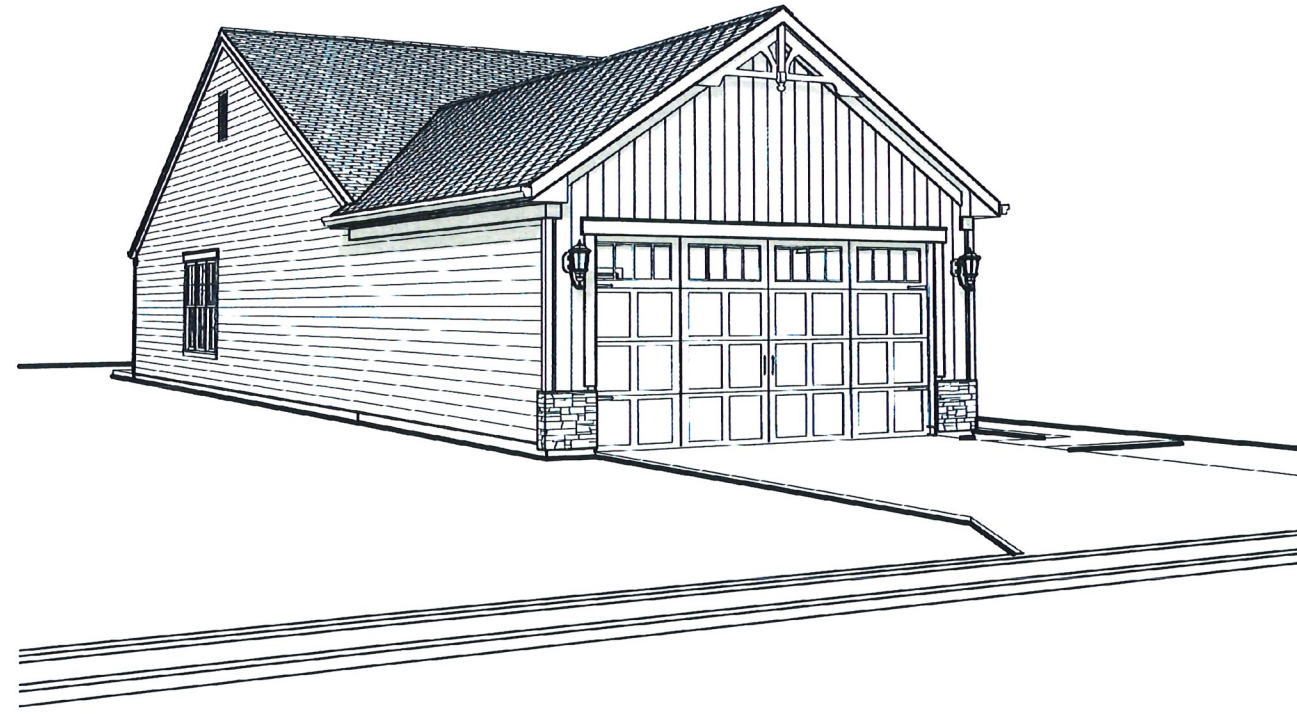
**The Multi-Fam's**  
 E1-A • G. Varies  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
**BROOKSTONE HOMES, LLC**  
 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200

E2 28' End Unit • Elevations  
**CD**  
**A.3**  
 SHEET NO. 4 of 30



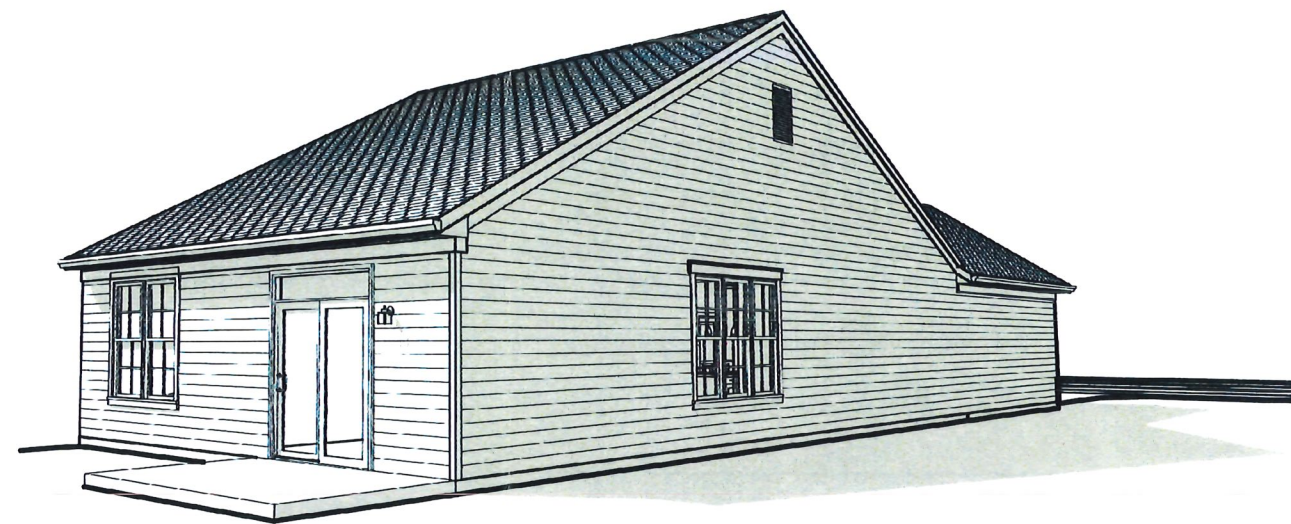
Vfx1

Holbrook 28' Ext • Vfx 3D Tour



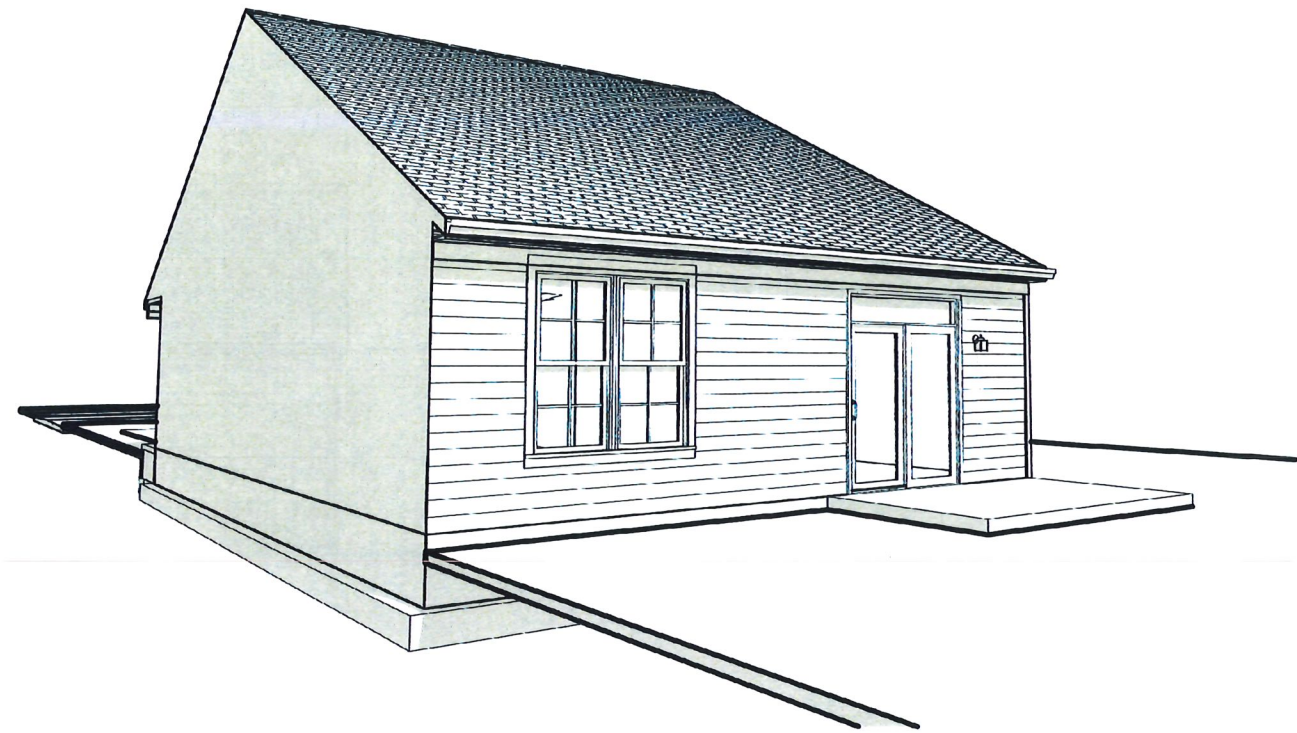
Vfx2

Holbrook 28' Ext • Vfx 3D Tour



Vfx3

Holbrook 28' Ext • Vfx 3D Tour



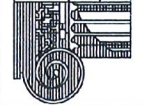
Vfx4

Holbrook 28' Ext • Vfx 3D Tour

Vfx/x

Virtual f/x

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 Email: virtual.fx@icloud.com



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 Job Request 06/07/24  
 Design Dev-1 06/24/24

BROOKSTONE  
 h o m e s

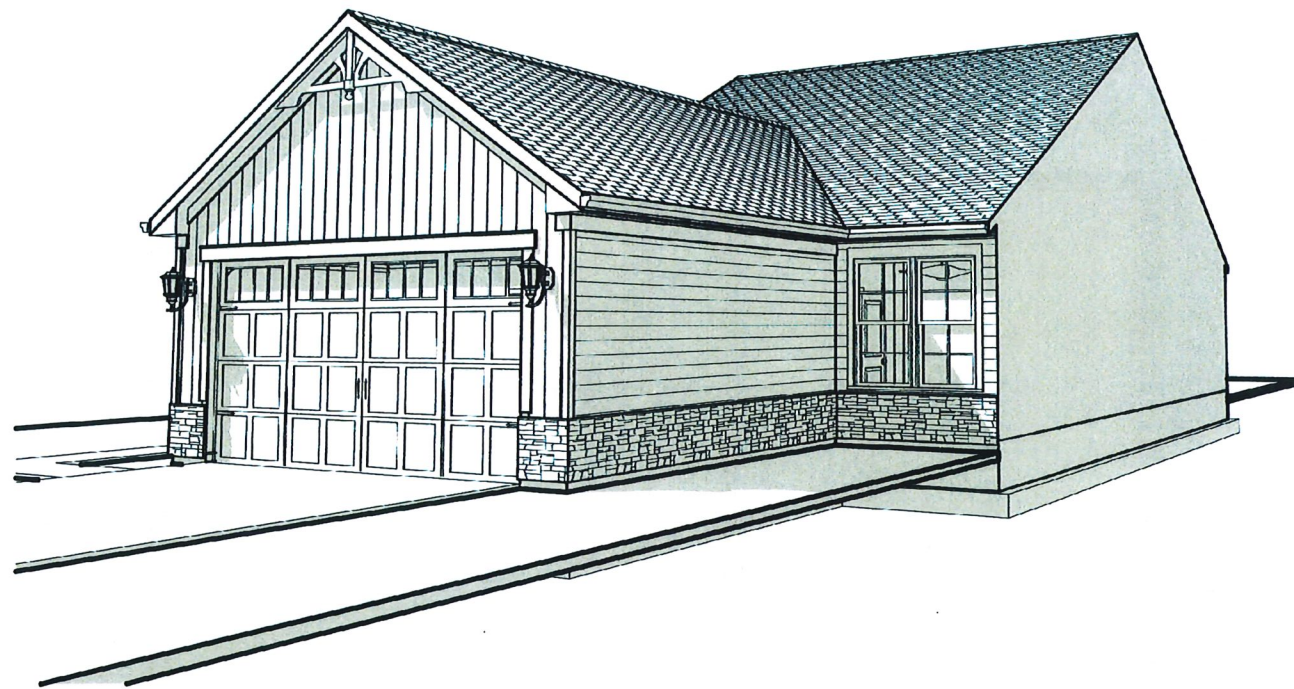
2024-0607

HB00 The Multi-Fam's  
 El-A • G. Varies  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
 BROOKSTONE HOMES, LLC.  
 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200

Vfx Holbrook 28' •  
 Virtual Tour-A

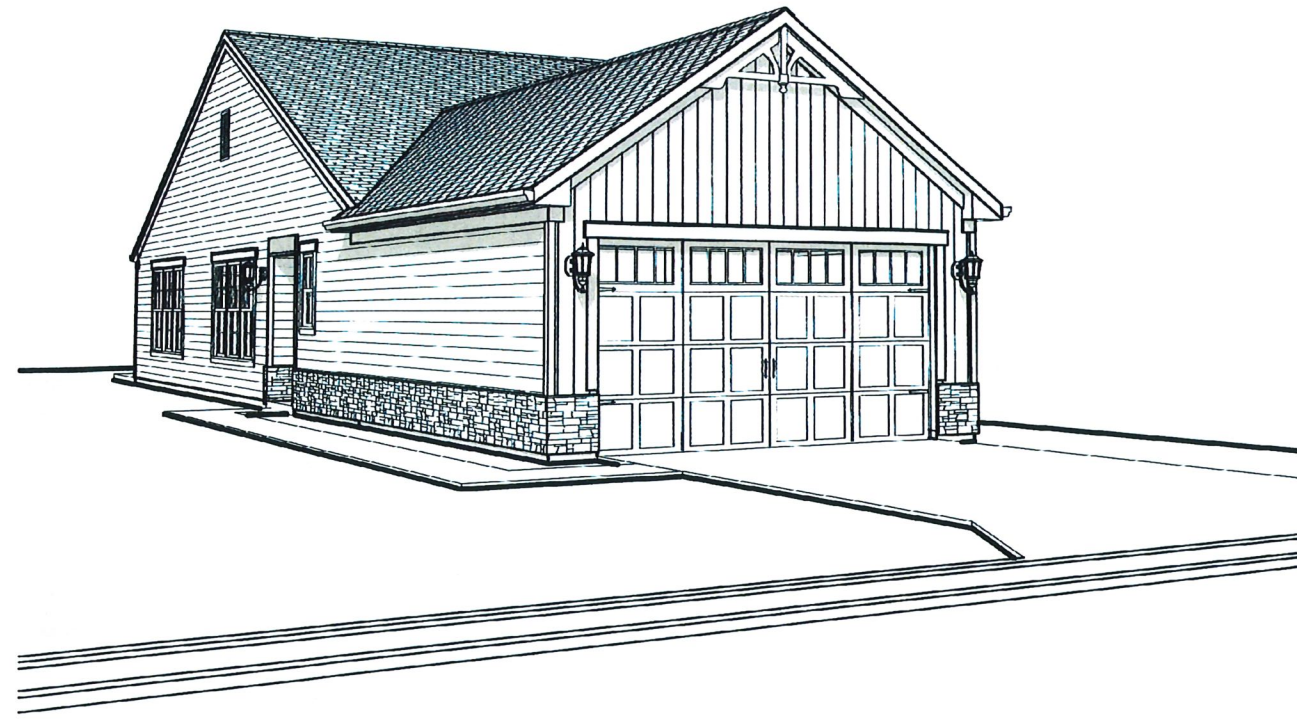
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SHEET NO. 5 of 30



Vfx1

Holbrook 28' End Ext • Vfx 3D Tour



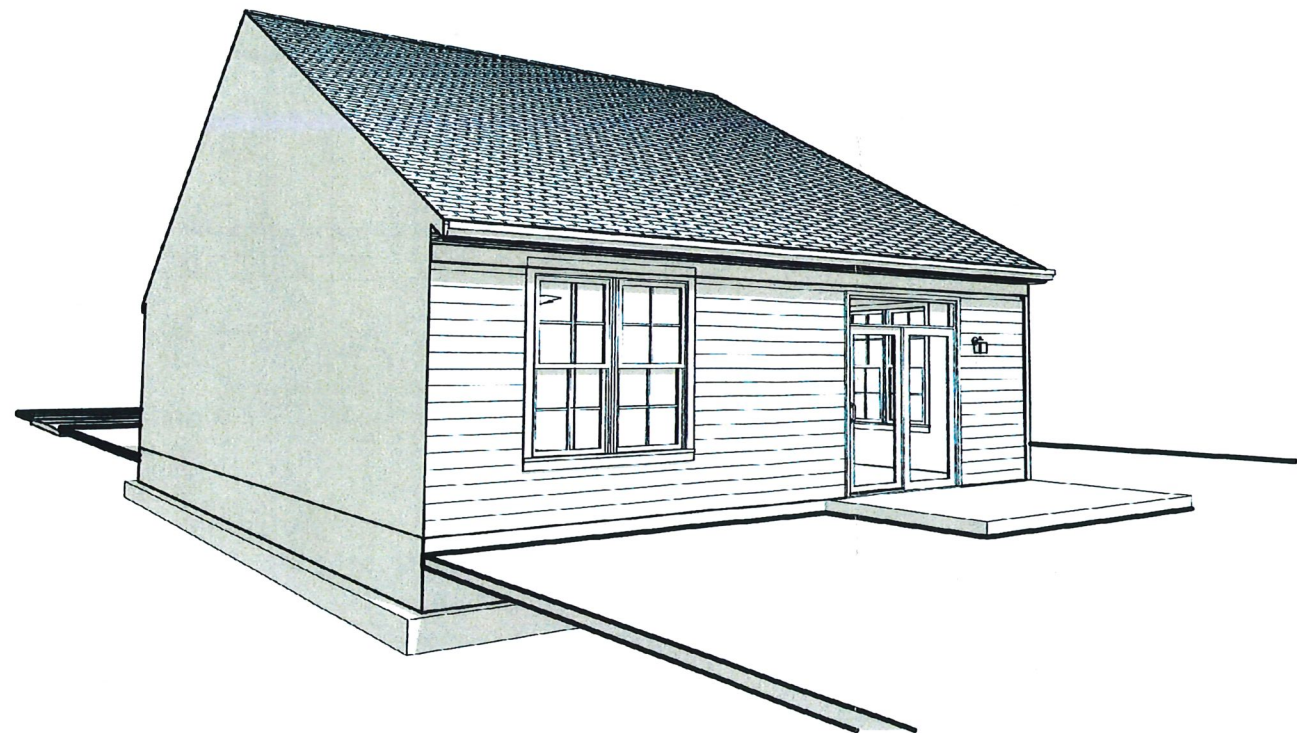
Vfx2

Holbrook 28' End Ext • Vfx 3D Tour



Vfx3

Holbrook 28' End Ext • Vfx 3D Tour

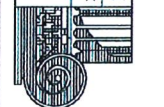


Vfx4

Holbrook 28' End Ext • Vfx 3D Tour

Vfx/x

Virtual f/x  
 10351 Kenwood Rd. • Suite 2001 • Blue Ash, Ohio 45242  
 Designed By: P. Michael Fisher • (513) 393-0857  
 Email: virtual.fx@icloud.com



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 Design Dev-1 06/24/24

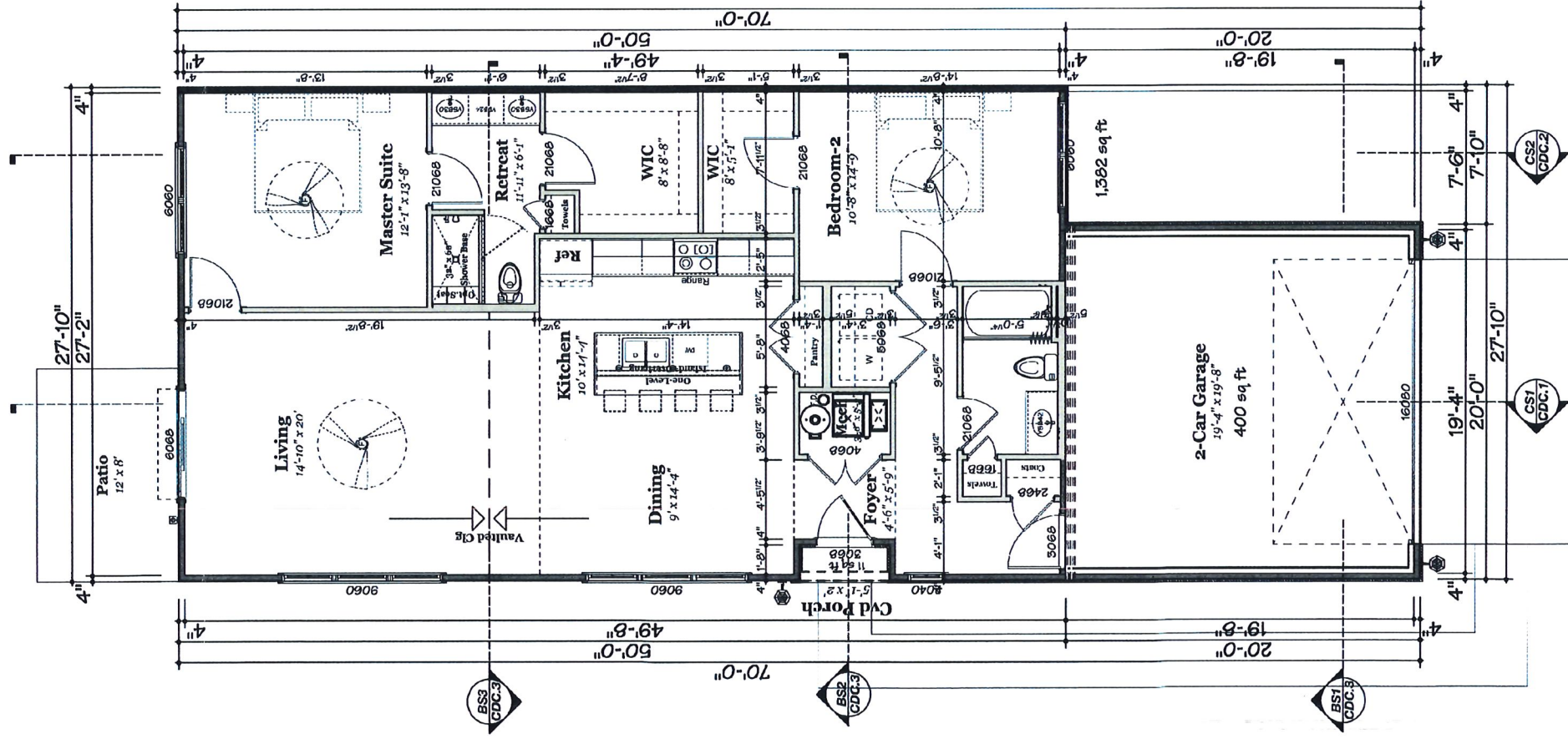
BROOKSTONE  
 HOMES

2024-0607

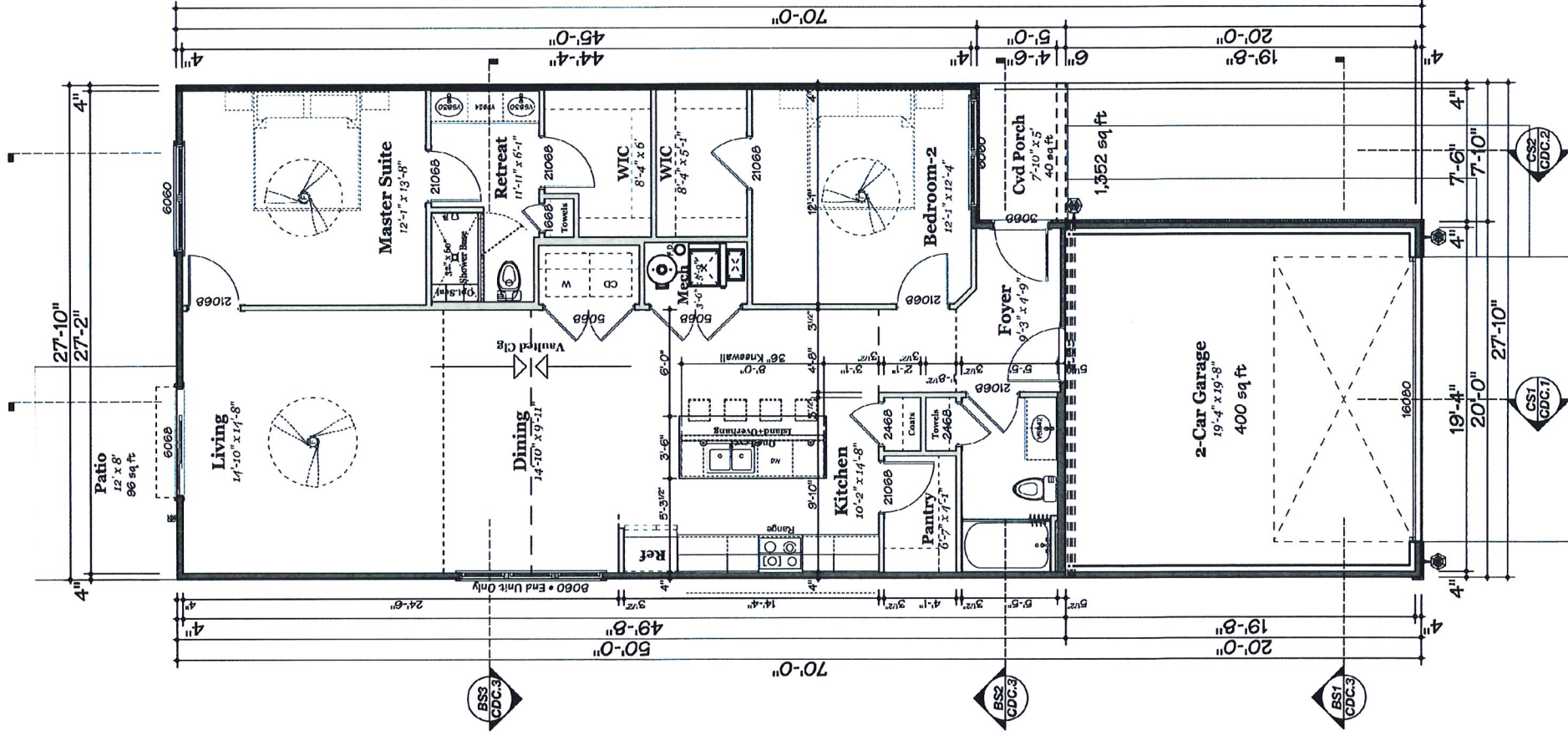
**HB00** The Multi-Fam's  
 El-A • G. Varies  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
**BROOKSTONE HOMES, LLC.**  
 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200

Vfx Holbrook 28'  
 End • Virtual Tour-A

CD  
 A.5  
 SHEET NO. 6 of 30



1. Holbrook Ln • 28' End Unit • First Floor Plan  
1/8" = 1'-0"

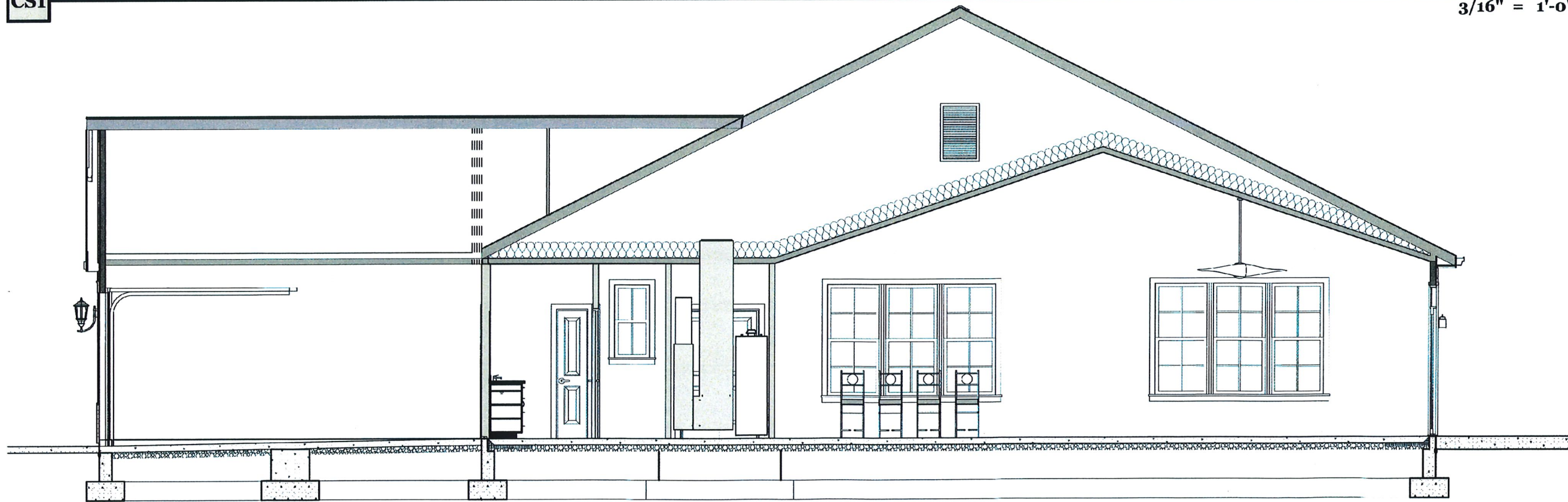


1. Holbrook Ln • 28' Unit • First Floor Plan  
1/8" = 1'-0"



CS1

28'-Unit • Cross Section-1  
3/16" = 1'-0"



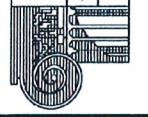
CS1

28'-End Unit • Cross Section-1  
3/16" = 1'-0"

*Vf/x*

*Virtual f/x*

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Designed By: P. Michael Fisher • (513) 393-0857  
Email: virtual.fx@icloud.com



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 Job Request 06/07/24  
 Design Dev-1 06/24/24

*B*  
BROOKSTONE  
INCORPORATED

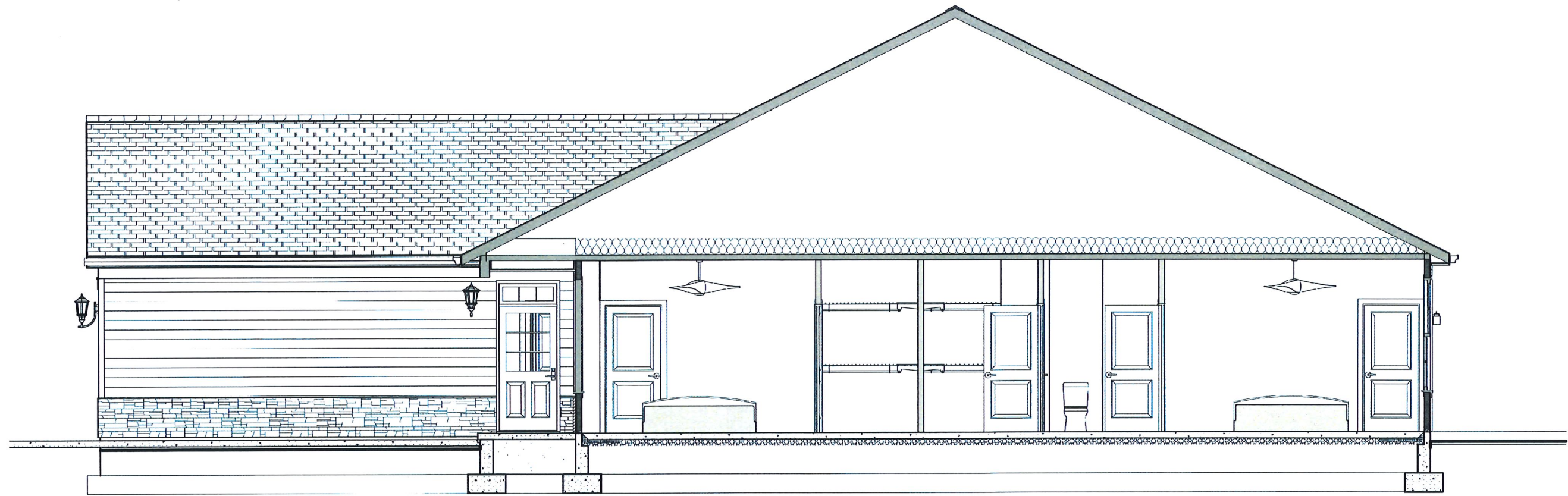
2024-0607

**HBOO** The Multi-Fam's  
 El-A • G. Varies  
 FOT: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
**BROOKSTONE Homes, LLC.**  
 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200

CS1 • Cross Section-1

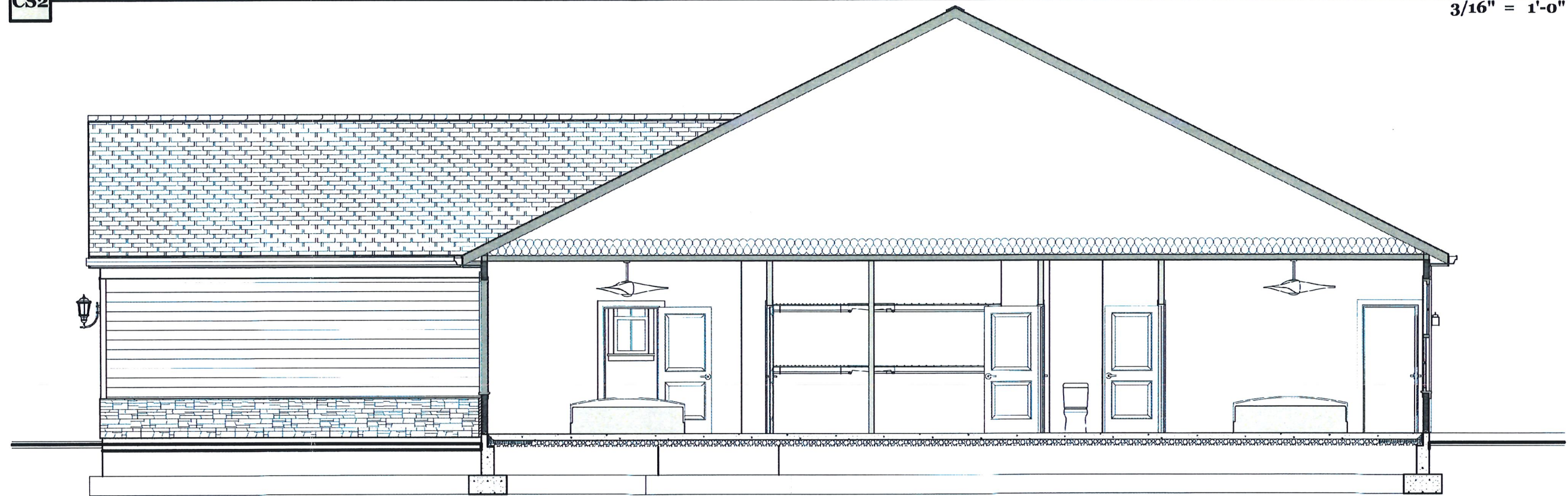
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SHRRT NO. 14 of 30



CS2

28'-Unit • Cross Section-2  
3/16" = 1'-0"



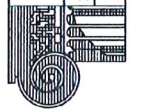
CS2

28'-End Unit • Cross Section-2  
3/16" = 1'-0"

Vf/x

Virtual f/x

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Designed By: P. Michael Fisher • (513) 393-0857  
Email: virtual.fx@icloud.com



REVISIONS:  
 Job Request 06/07/24  
 Design Dev-1 06/24/24

BROOKSTONE  
H O M E S

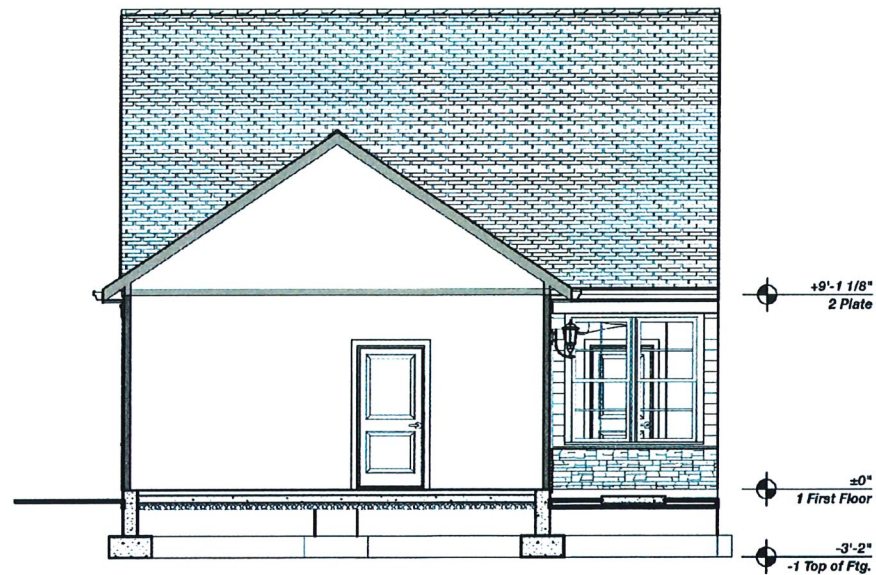
2024-0607

**HB00** The Multi-Fam's  
 EL-A • G. Varies  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
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 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200

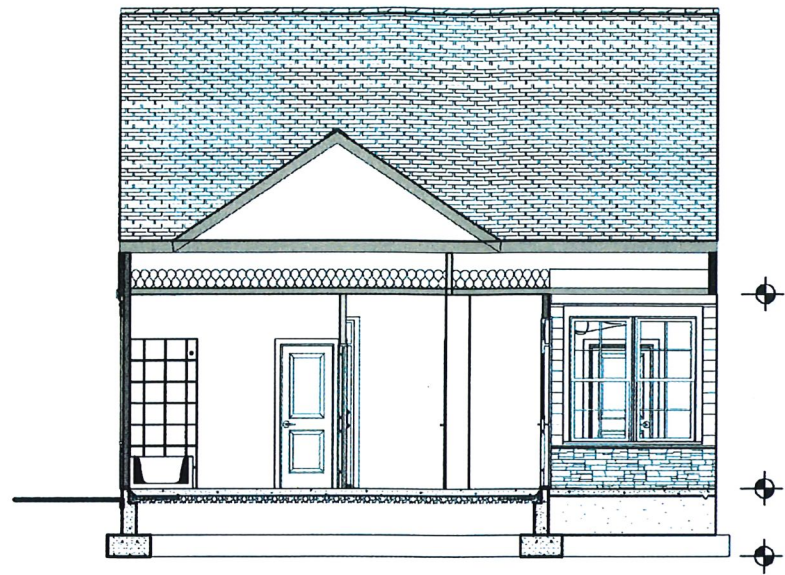
CS2 • Cross  
Section-2

CD  
C.2

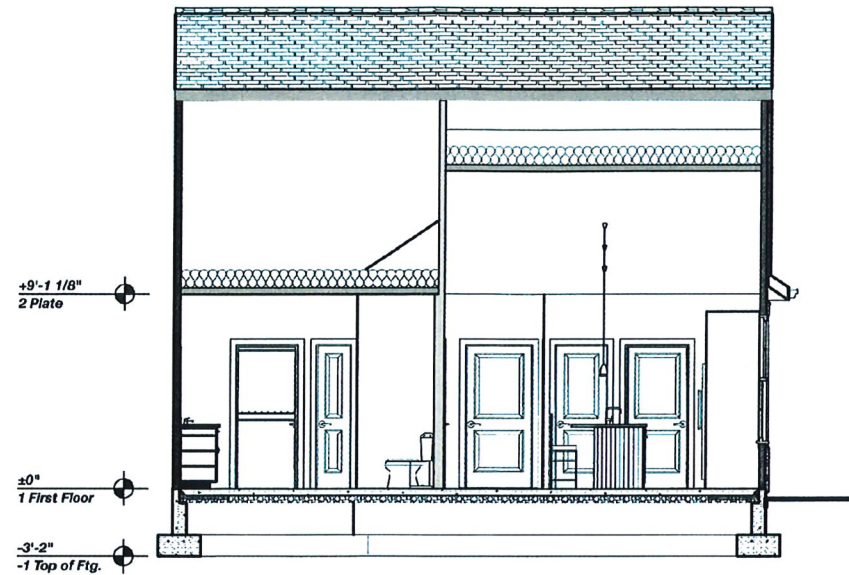
SHEET NO. 15 of 30



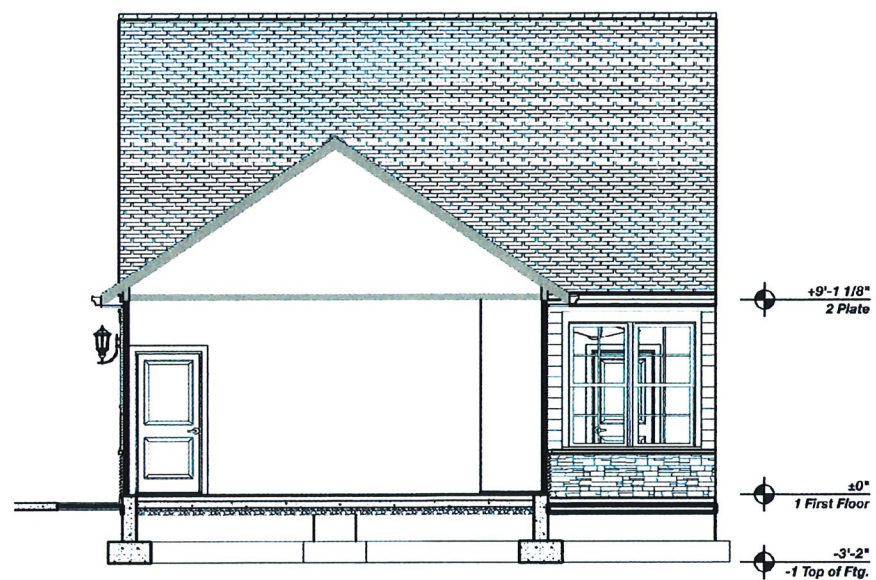
**BS1** ————— **28'-Unit • Building Section-1**  
1/8" = 1'-0"



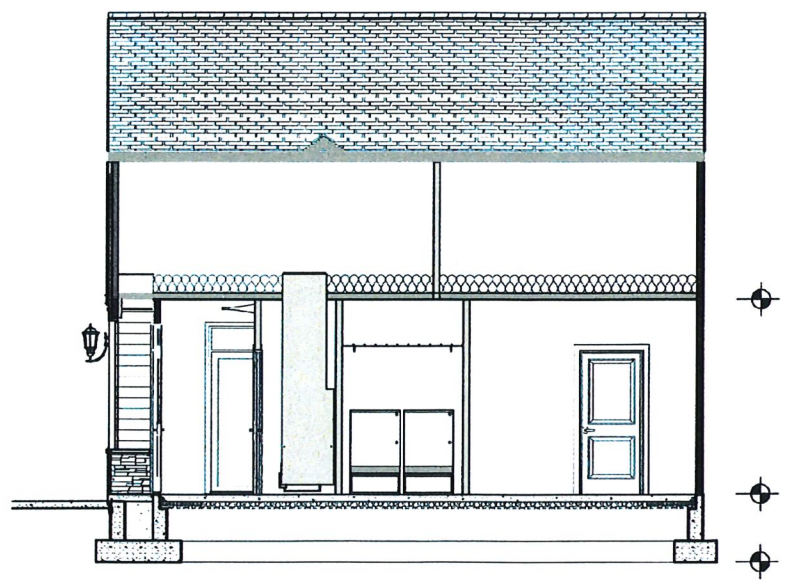
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1/8" = 1'-0"



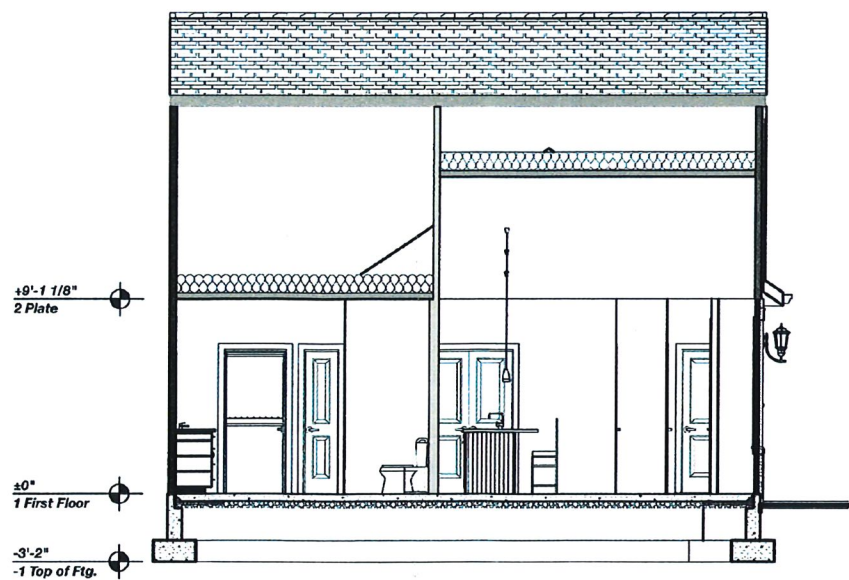
**BS3** ————— **28'-Unit • Building Section-3**  
1/8" = 1'-0"



**BS1** ————— **28'-End Unit • Building Section-1**  
1/8" = 1'-0"



**BS2** ————— **28'-End Unit • Building Section-2**  
1/8" = 1'-0"



**BS3** ————— **28'-End Unit • Building Section-3**  
1/8" = 1'-0"

**Vf/x**

*Virtual f/x*

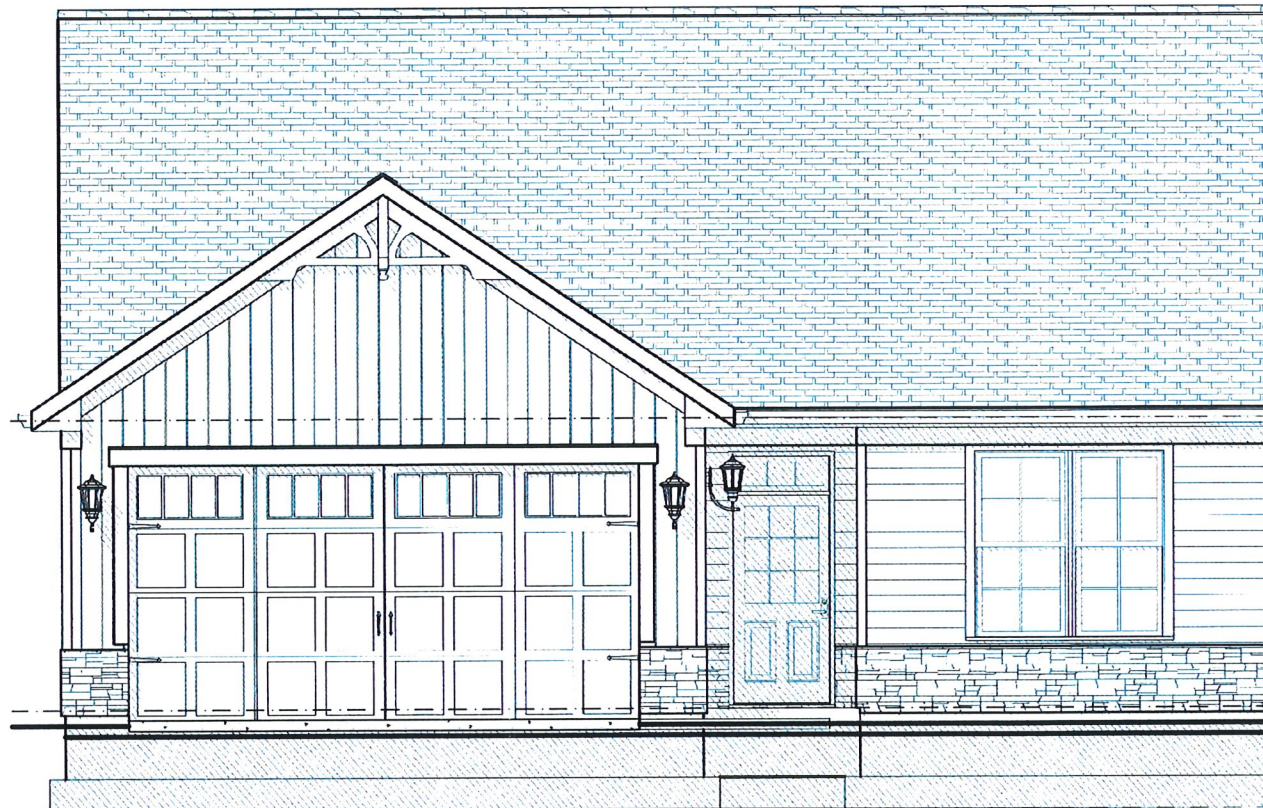
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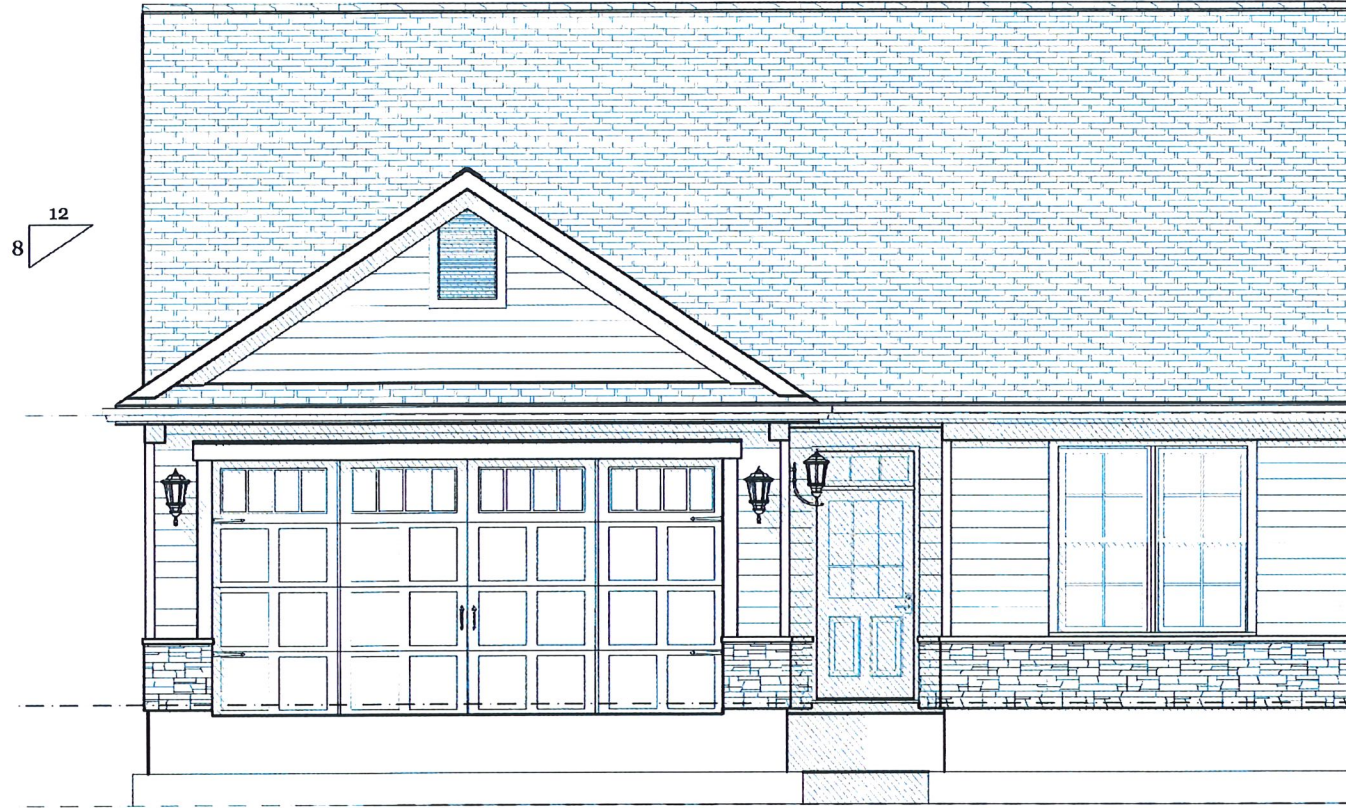
**B**  
BROOKSTONE  
c o n s  
2024-0607

**HB00** The Multi-Fam's  
 E-I-A • G. Varies  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
**BROOKSTONE Homes, LLC.**  
 7203 WOOSTER PIKE • CINCINNATI, OH 45227 • 513-340-7200

BS1 • Building Sections  
**CD**  
**C.3**  
 SHEET NO. 16 of 30



**Holbrook Ln • 38' Unit • Front Elevation-A**  
3/16" = 1'-0"



**Holbrook Ln • 38' Unit • Front Elevation-B**  
3/16" = 1'-0"

ALL SMOKE ALARMS SHALL BE LISTED IN ACCORDANCE WITH UL-217. ON EACH LEVEL WITHIN EACH DWELLING UNIT SMOKE ALARMS UTILIZING PHOTOVOLTAIC AND IONIZATION TECHNOLOGIES SHALL BE INSTALLED. BEDROOMS TO USE IONIZATION TYPE. OUTSIDE BEDROOMS TO BE PHOTOVOLTAIC TYPE. SEPARATE OR DUAL-SENSING SMOKE ALARMS MAY BE USED

**EXHAUST FAN & SMOKE DETECTOR NOTE:**  
 ● DENOTES EXHAUST FAN LOCATIONS (SEE NOTE 9, D FOR CODE INFO ON SHEET A-5)  
 Ⓢ DENOTES SMOKE DETECTOR LOCATIONS (SEE NOTE 11, b ON SHEET A-5 & THE NOTE TO THE LEFT FOR ALL SMOKE DETECTOR CODE INFO)  
 Ⓢ/CO DENOTES SMOKE/CO COMBO DETECTOR LOCATIONS PROGRAMMABLE THERMOSTAT REQ'D FOR ALL FORCED AIR FURNACES

**WALL BRACING MEATHOD PER RCO 602.10.4**  
 CS-WSP: CONTINUOUSLY SHEATHED WOOD STRUCTURAL PANEL. 3/8" MIN. THK.. CONNECTION REQUIREMENTS; FASTENERS COMPLY WITH RCO 602.3(3) FASTENED 6" ON EDGES AND 12" IN FIELD. INTERIOR SHEATHING ON BRACED WALLS SHALL BE 1/2" MIN. GYPSUM BD. FASTENED IN ACCORDANCE WITH RCO TABLE 702.3.5

Note:  
All Downspouts connected to approved locations

E1

E1

**Vf/x**

*Virtual f/x*

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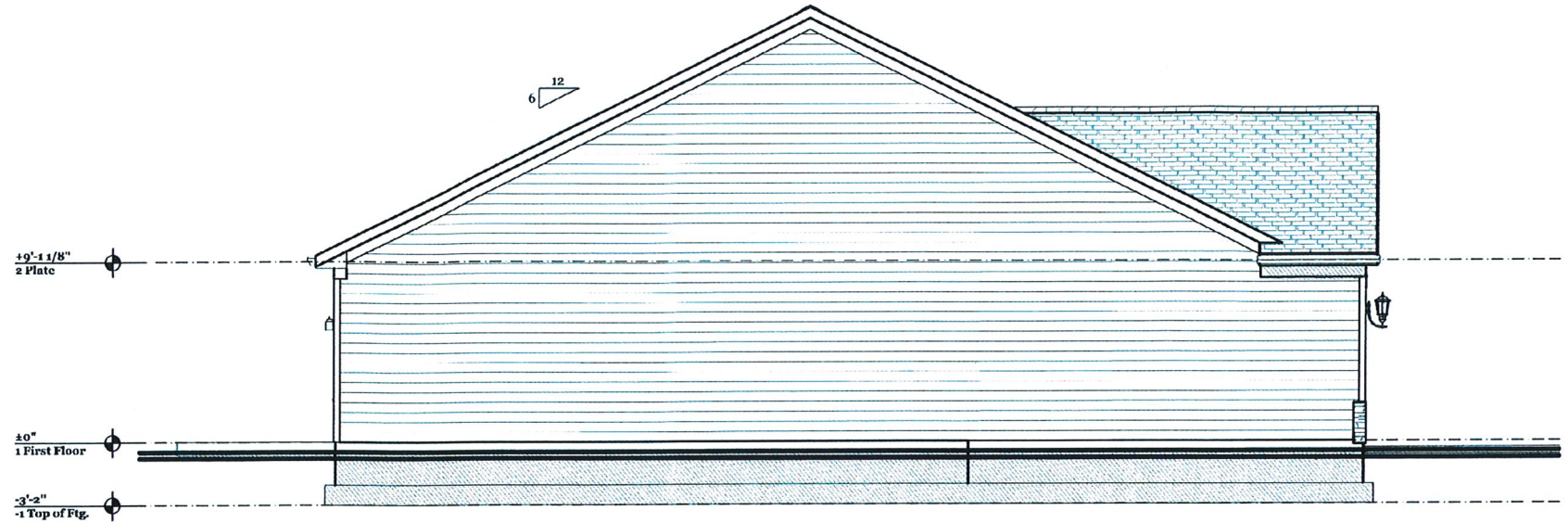
**HB**  
**BROOKSTONE**  
 HOMES

**The Multi-Fam's**  
 El-A • G. Varies  
 For: Delaware Crossing, LLC  
 1841 HOLBROOK LANE • FLORENCE, KENTUCKY 41042  
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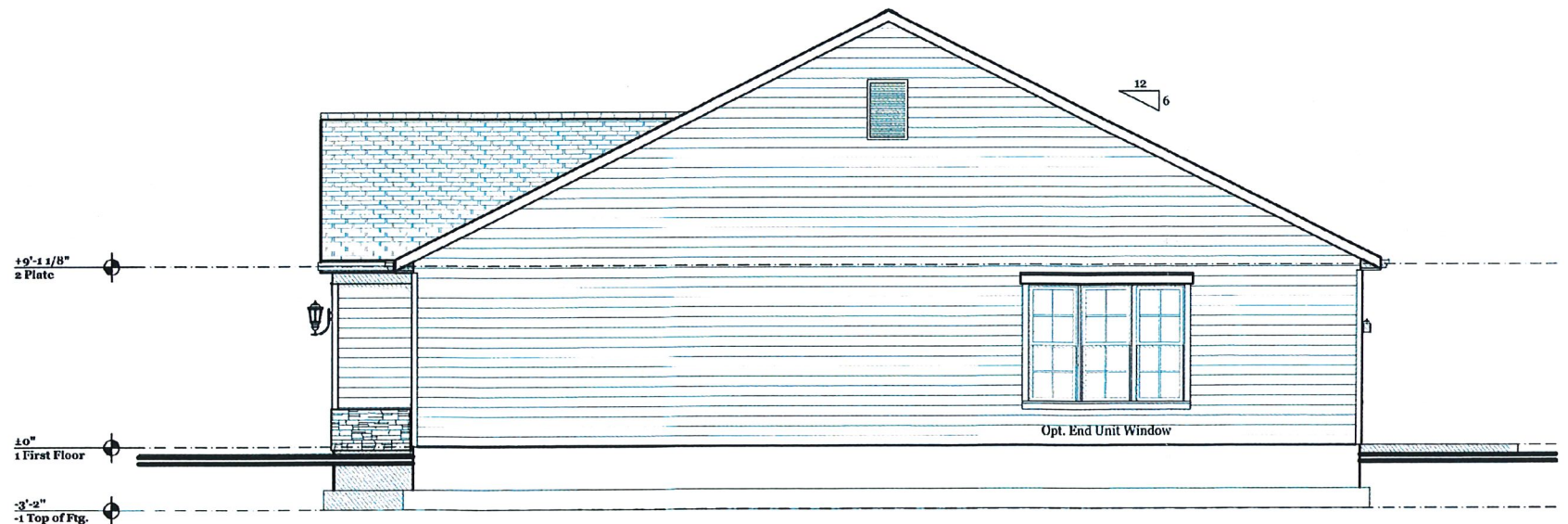
E1 38' Unit • Front Elevation-A & B  
**CD**  
**D.1**  
 SHEET NO. 17 of 30



**E2** — **Holbrook Ln • 38' Unit • Rear Elevation-A**  
1/8" = 1'-0"



**E3** — **Holbrook Ln • 38' Unit • Left Elevation-A**  
1/8" = 1'-0"

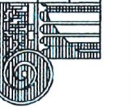


**E4** — **Holbrook Ln • 38' Unit • Right Elevation-A**  
1/8" = 1'-0"

Vf/x

Virtual f/x

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 B R O O K S T O N E  
 I I O O M M C C S

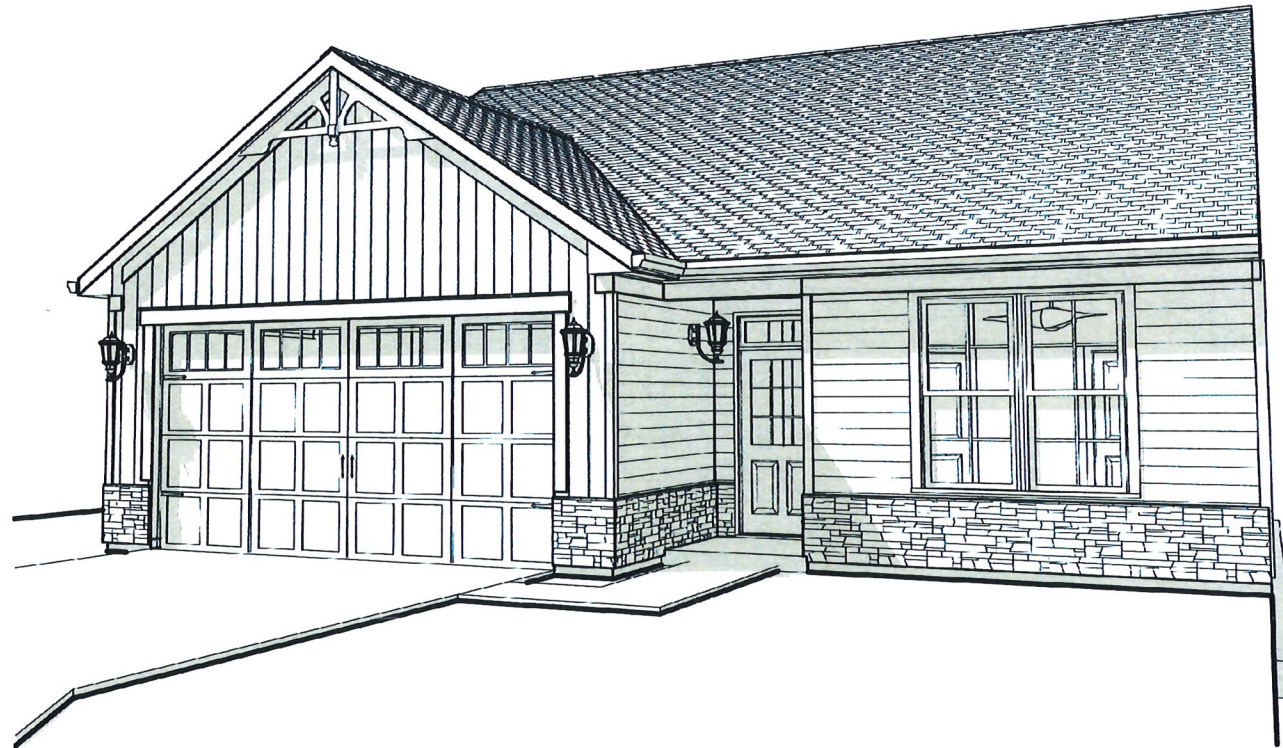
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E2 38' Unit • Other Elevations

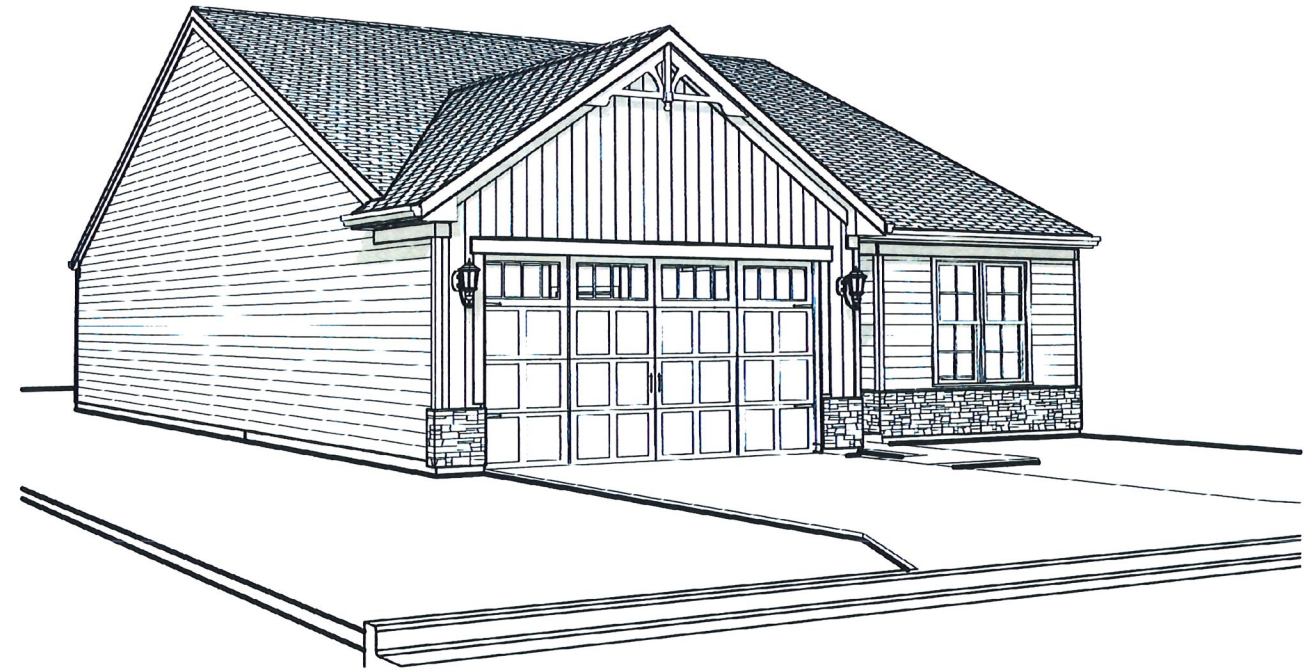
CD  
 D.2

SHEET NO. 18 of 30



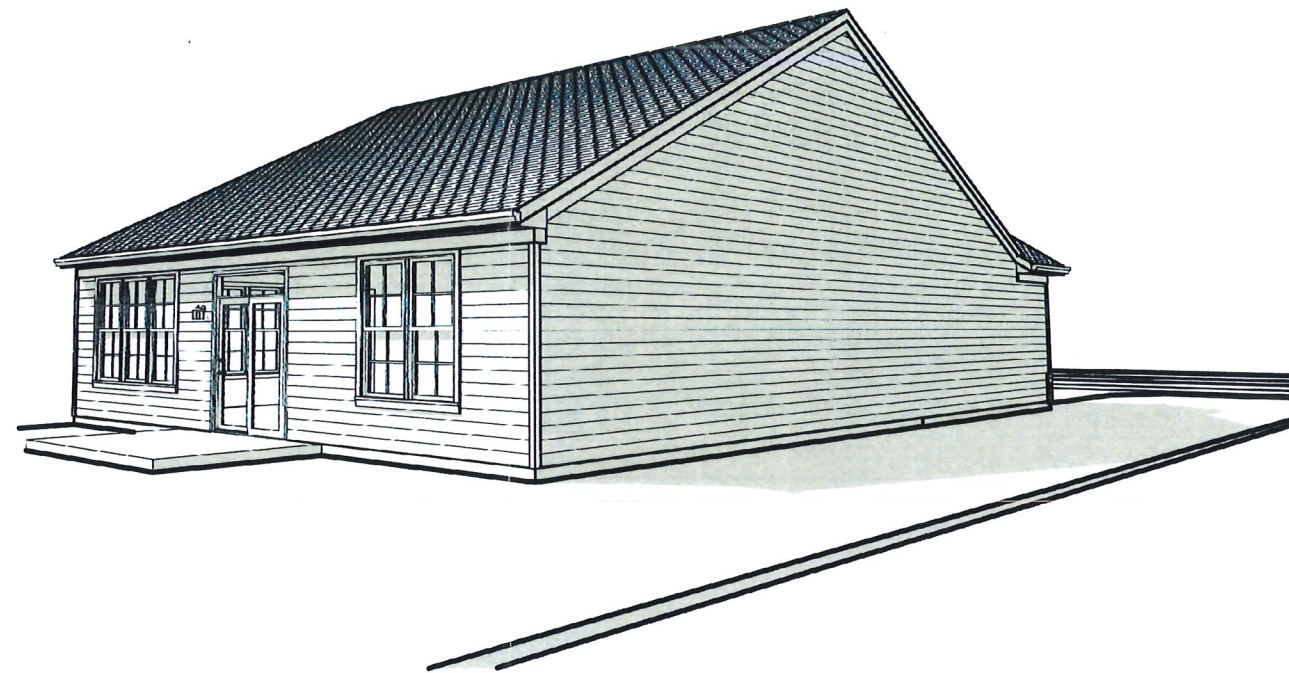
Vfx1

Holbrook 38' Ext • Vfx 3D Tour



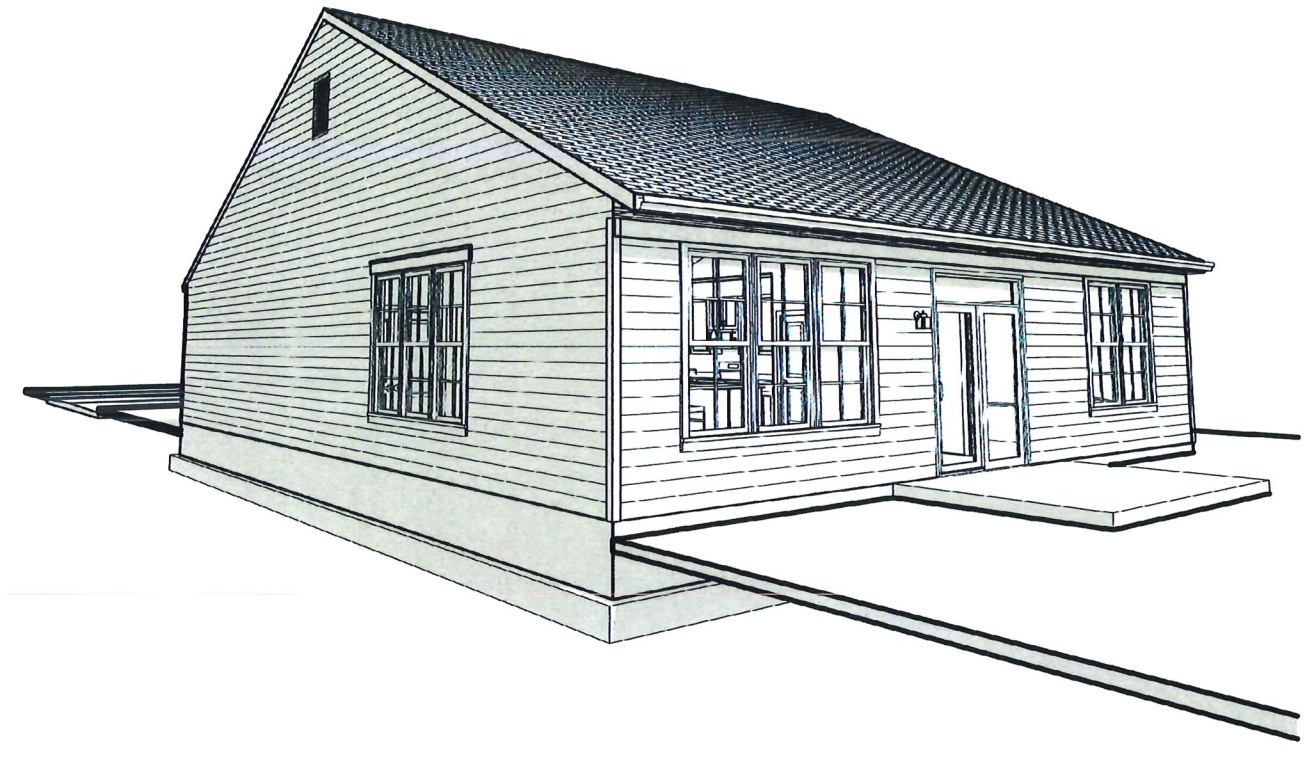
Vfx2

Holbrook 38' Ext • Vfx 3D Tour



Vfx3

Holbrook 38' Ext • Vfx 3D Tour



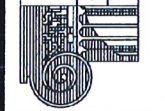
Vfx4

Holbrook 38' Ext • Vfx 3D Tour

Vfx/x

Virtual f/x

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 Design Dev-1 06/24/24

BROOKSTONE  
 HOMES

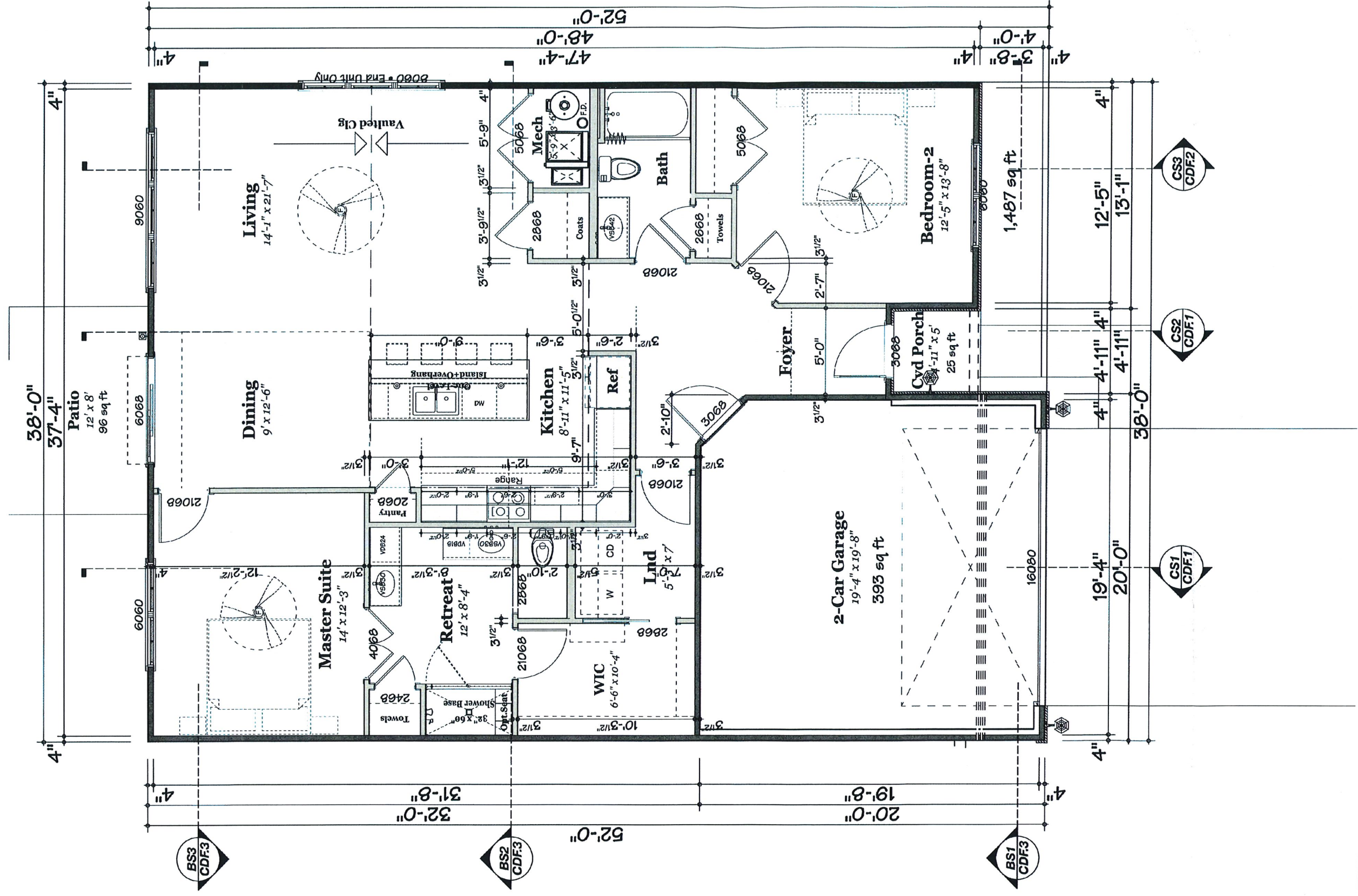
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Vfx Holbrook 38' •  
 Virtual Tour-A

CD  
 D.3

SHEET NO. 19 of 30



1.

Holbrook Ln • 38' Unit • First Floor Plan  
3/16" = 1'-0"

SHEET NO. 20 of 30

E.1  
CD

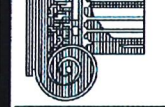
1. 38' Unit • First Floor Plans

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El-A • G. Varies  
For: Delaware Crossing, LLC  
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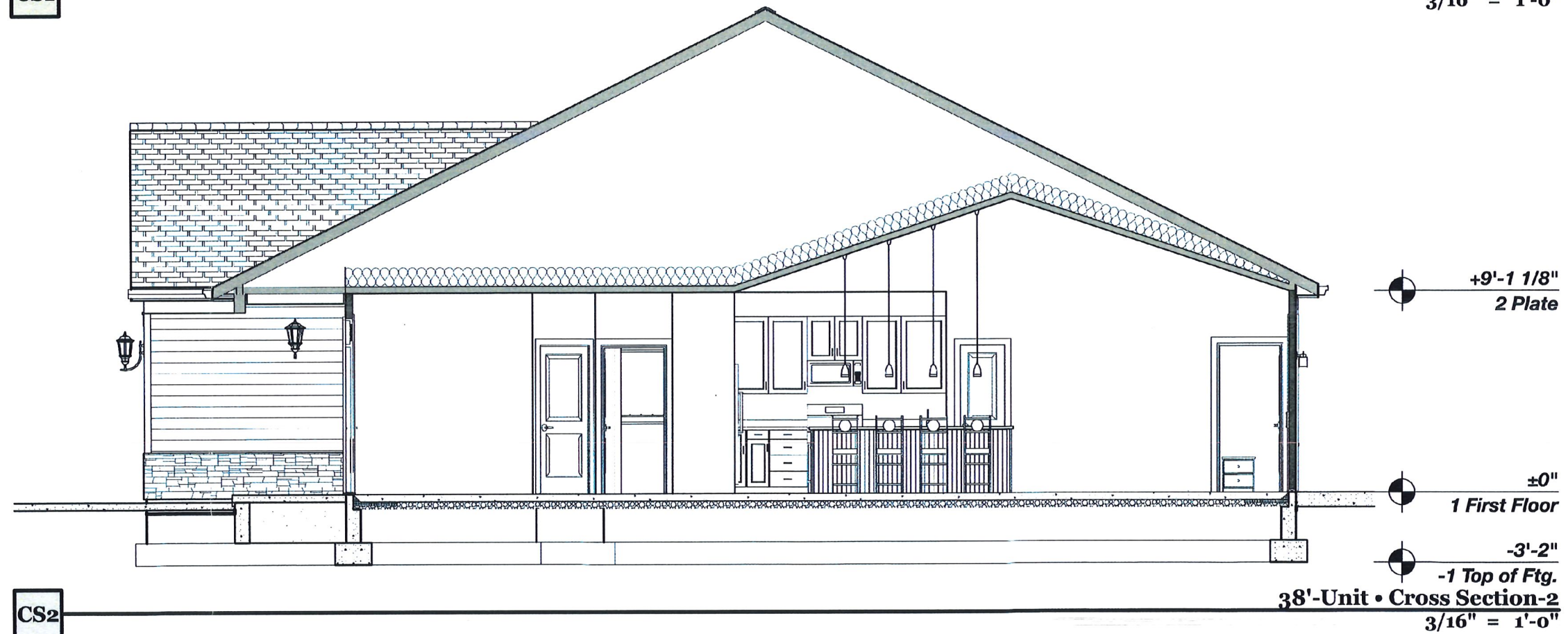
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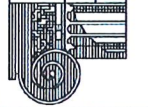
**Vf/x**



Vf/x

Virtual f/x

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 1 2 3 4 5 6 7 8 9

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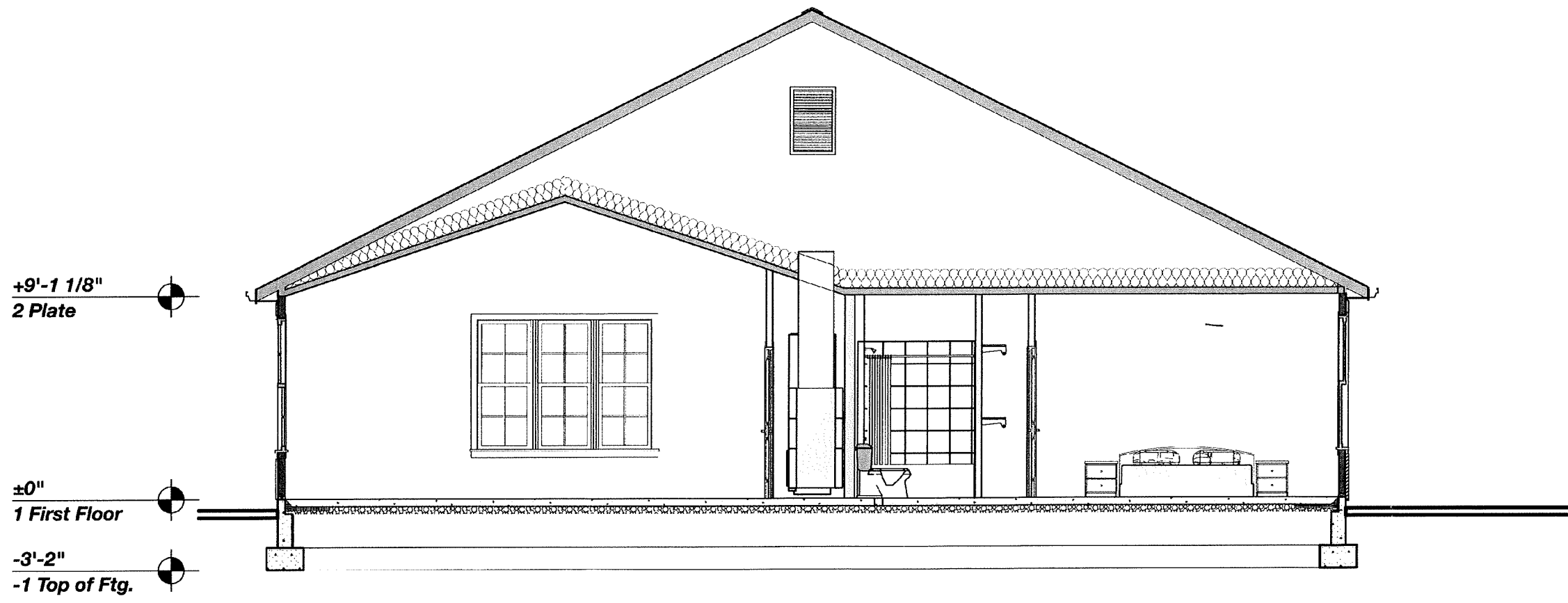
**HBOO** The Multi-Fam's  
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CS1 • Cross Section-1  
 & 2

CDF

.1

SHEET NO. 25 of 30



+9'-1 1/8"  
2 Plate

±0"  
1 First Floor

-3'-2"  
-1 Top of Ftg.

CS3

38'-Unit • Cross Section-3  
3/16" = 1'-0"

Vf/x

*Virtual f/x*

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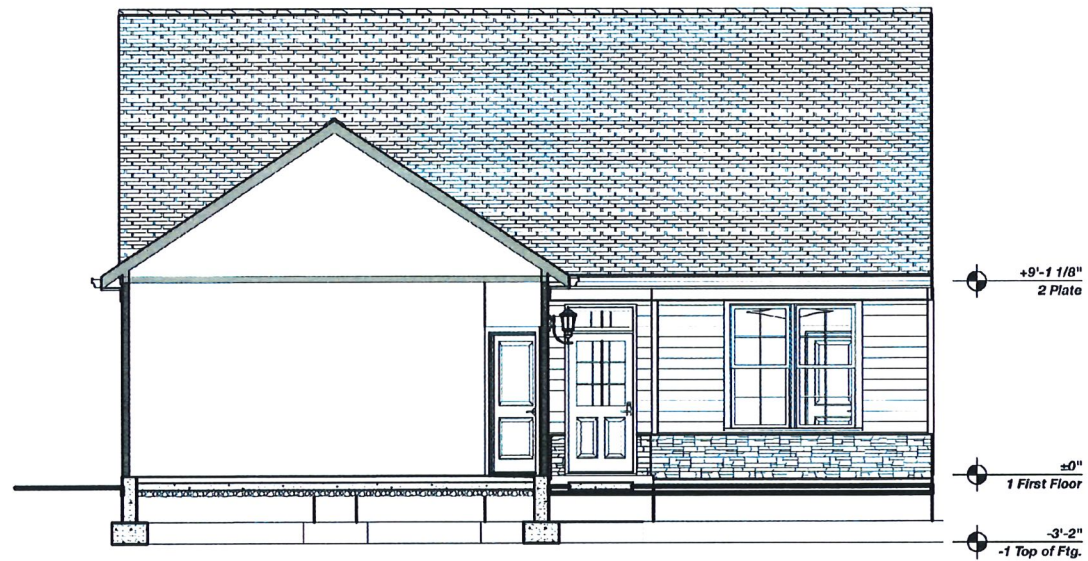
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 Design Dev-1 06/24/24

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2024-0607

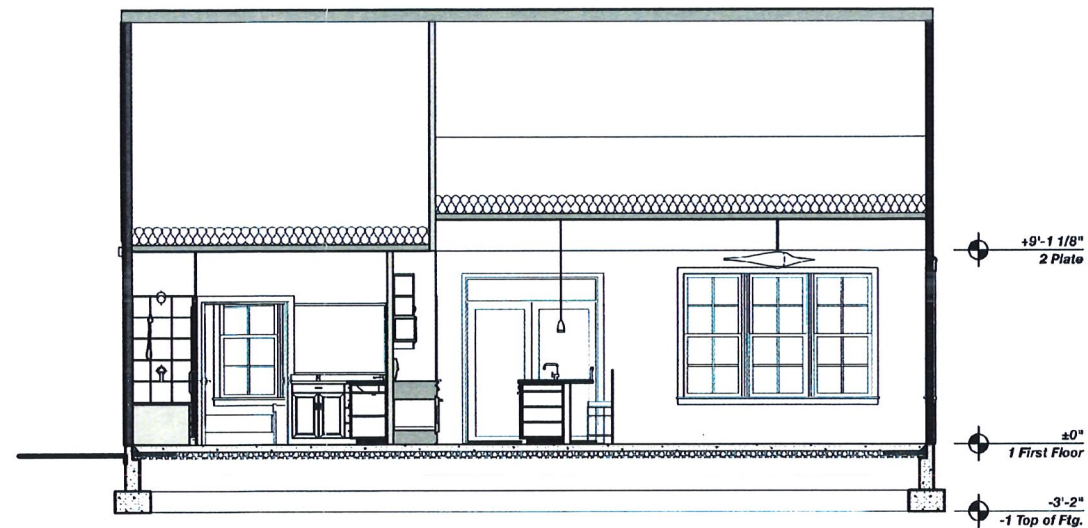
**HBOO** The Multi-Fam's  
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CS2 • Cross Section-3  
**CDF**  
 .2  
 SHEET NO. 26 of 30



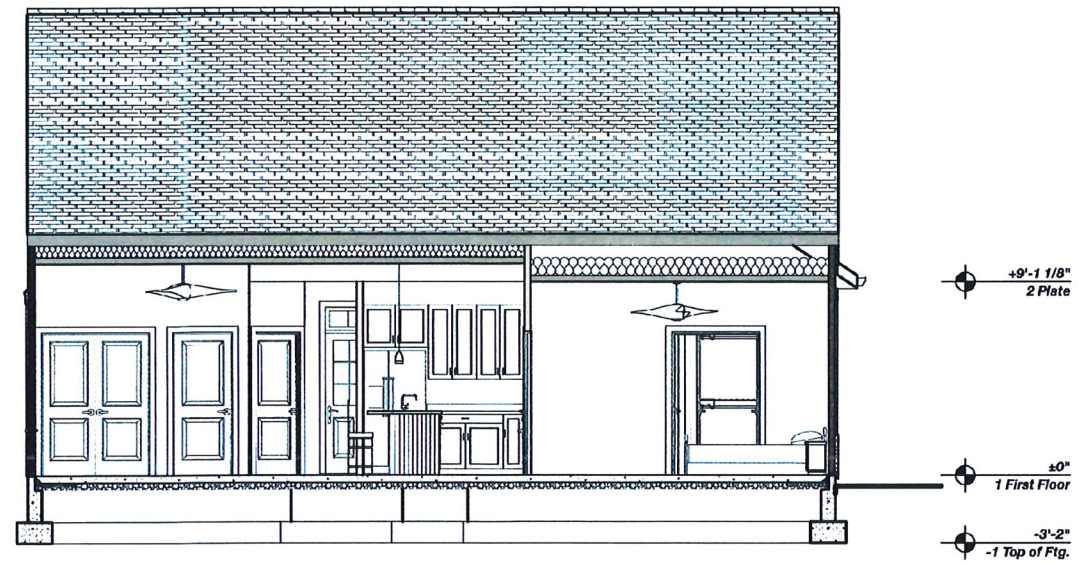
**BS1**

**38'-Unit • Building Section-1**  
1/8" = 1'-0"



**BS2**

**38'-Unit • Building Section-2**  
1/8" = 1'-0"



**BS3**

**38'-Unit • Building Section-3**

**Vf/x**

*Virtual f/x*

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Email: virtual.fx@icloud.com

**REVISIONS:**

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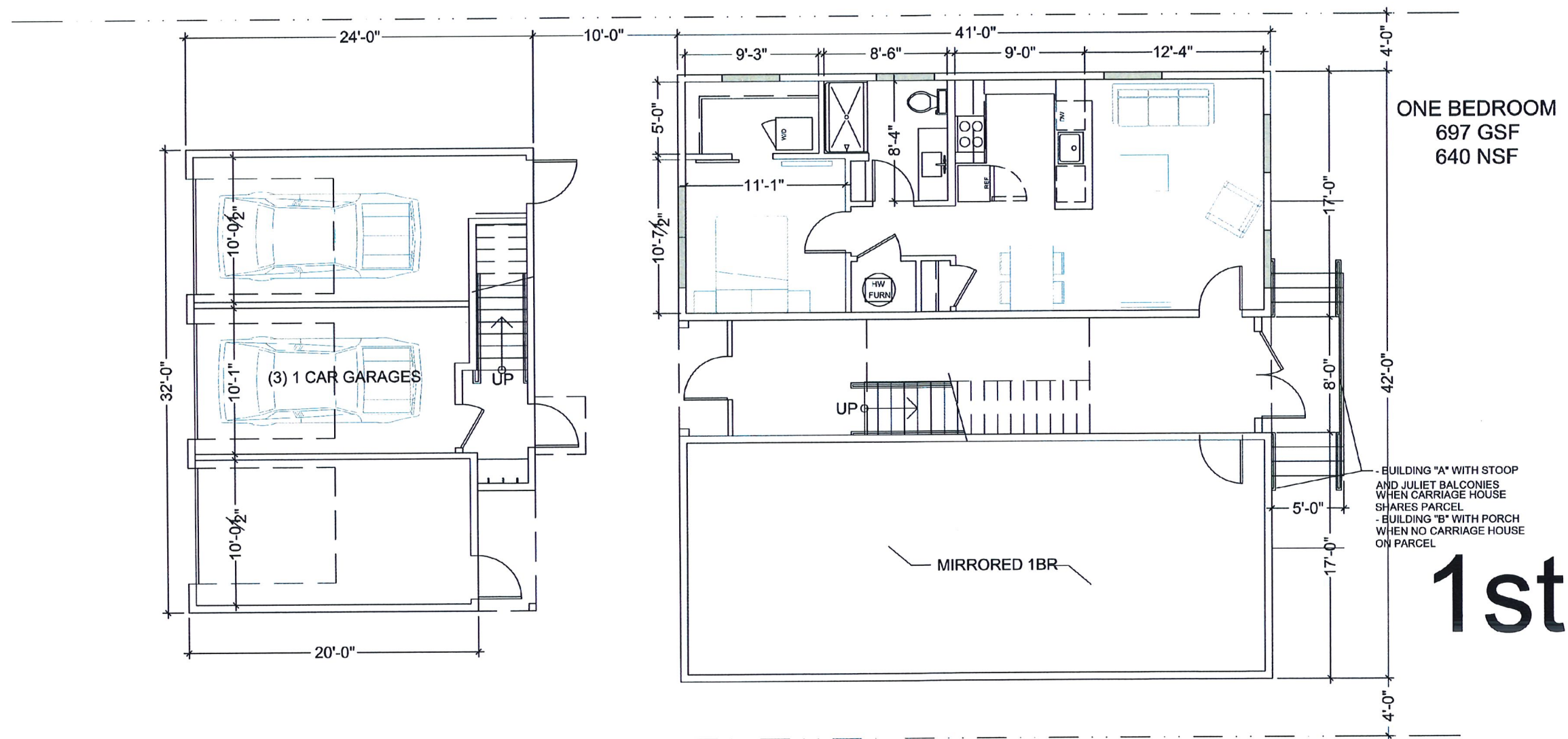
**BROOKSTONE**  
H O M E S

2024-0607

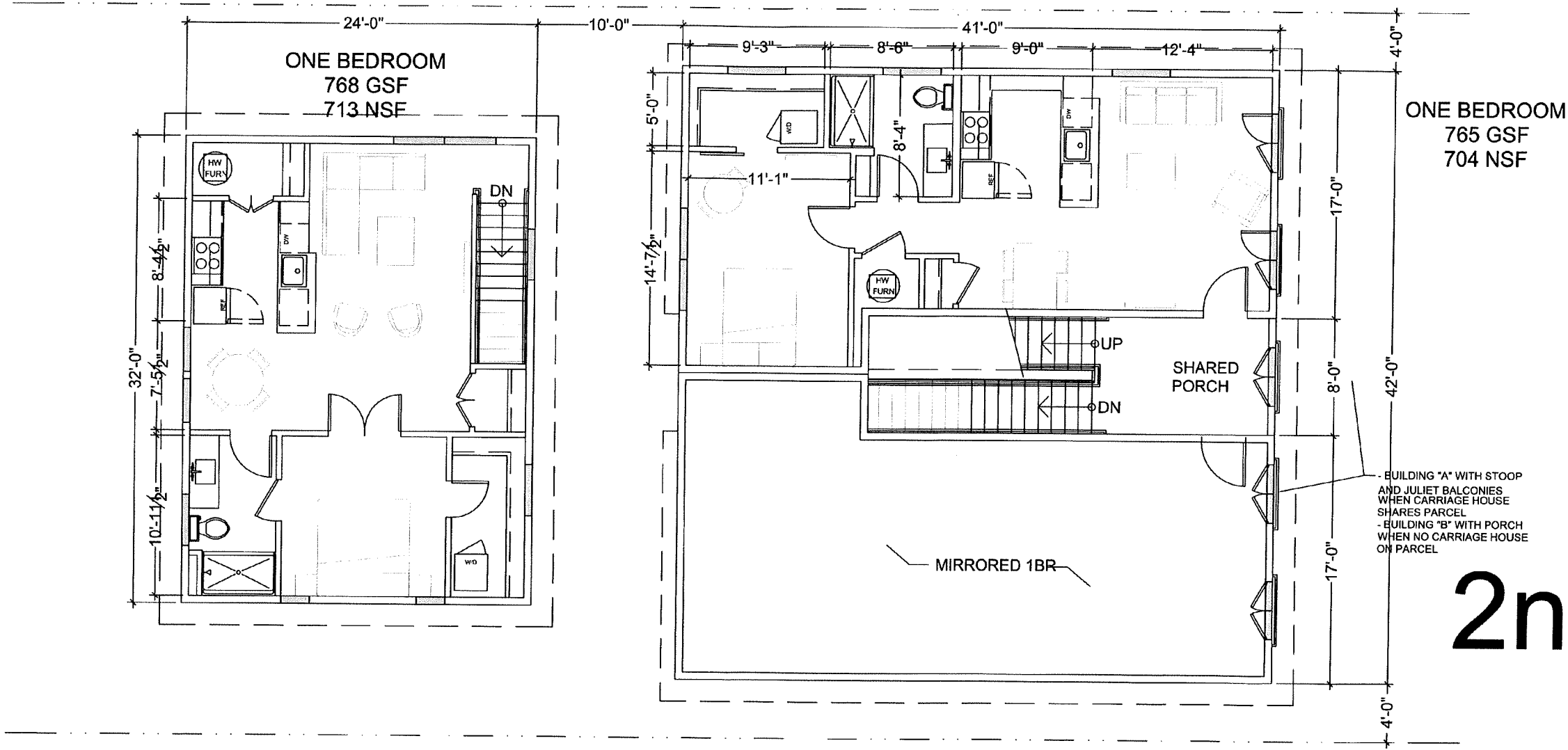
**HBOO** The Multi-Fam's  
El-A. G. Varies  
For: Delaware Crossing, LLC  
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BS1 • Building Sections  
**CDF**  
3  
SHEET NO. 27 of 30

POPLAR POINTE APARTMENTS  
SCHEMATIC DESIGN REVIEW SET



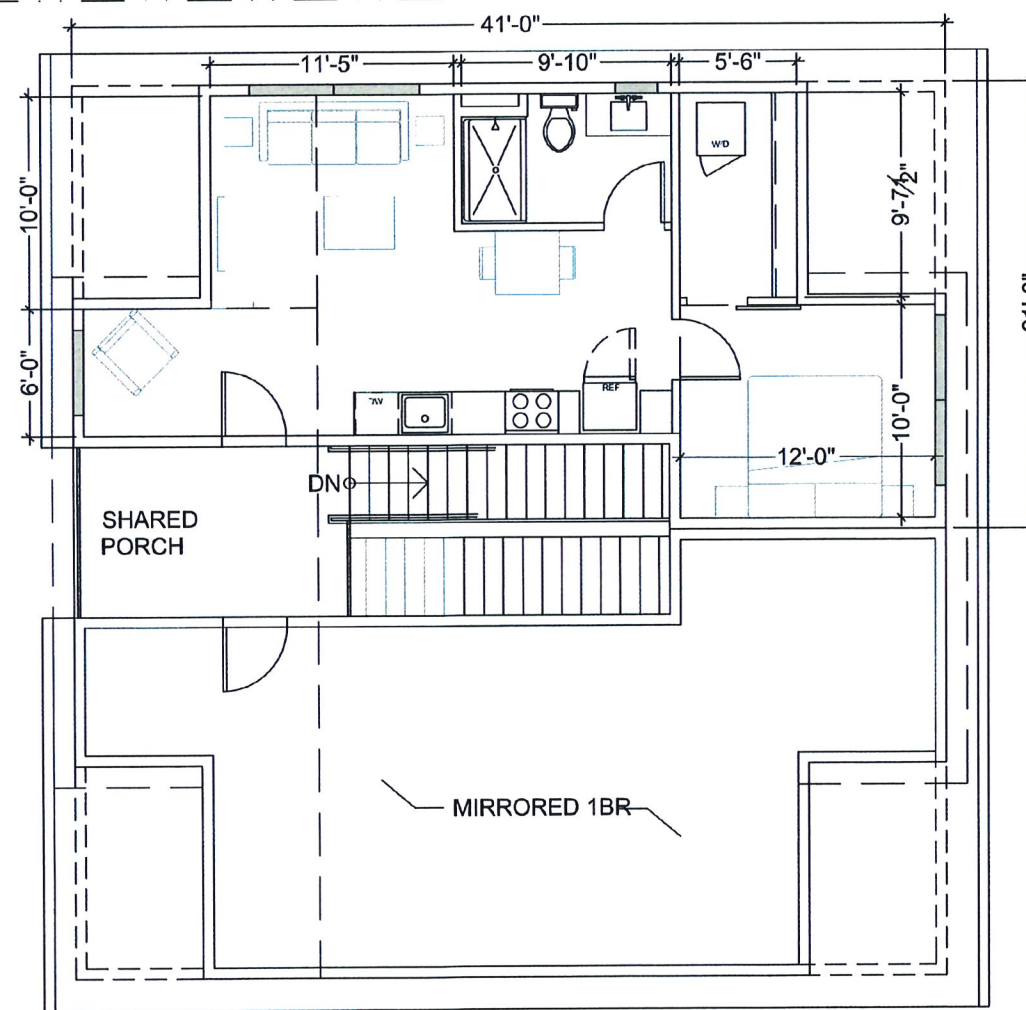
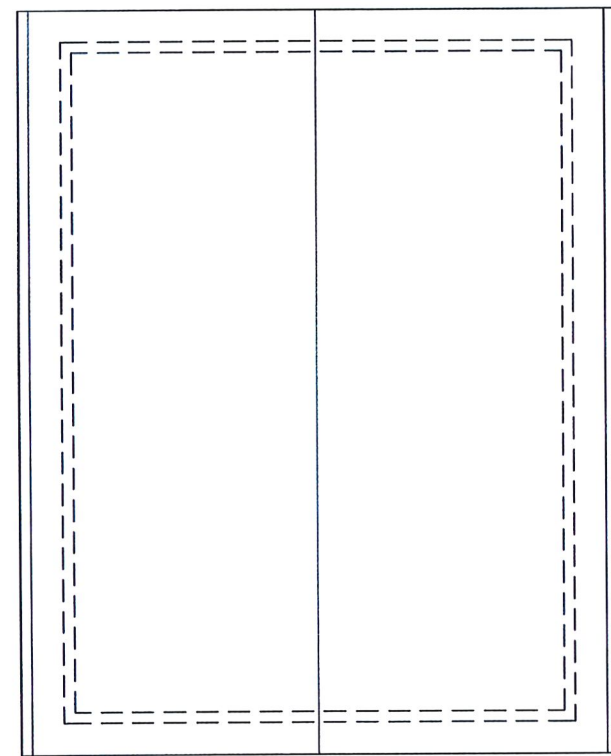
CARRIAGE HOUSE AND BUILDING A (BUILDING B SIMILAR)



- BUILDING "A" WITH STOOP AND JULIET BALCONIES WHEN CARRIAGE HOUSE SHARES PARCEL  
 - BUILDING "B" WITH PORCH WHEN NO CARRIAGE HOUSE ON PARCEL

**2nd**

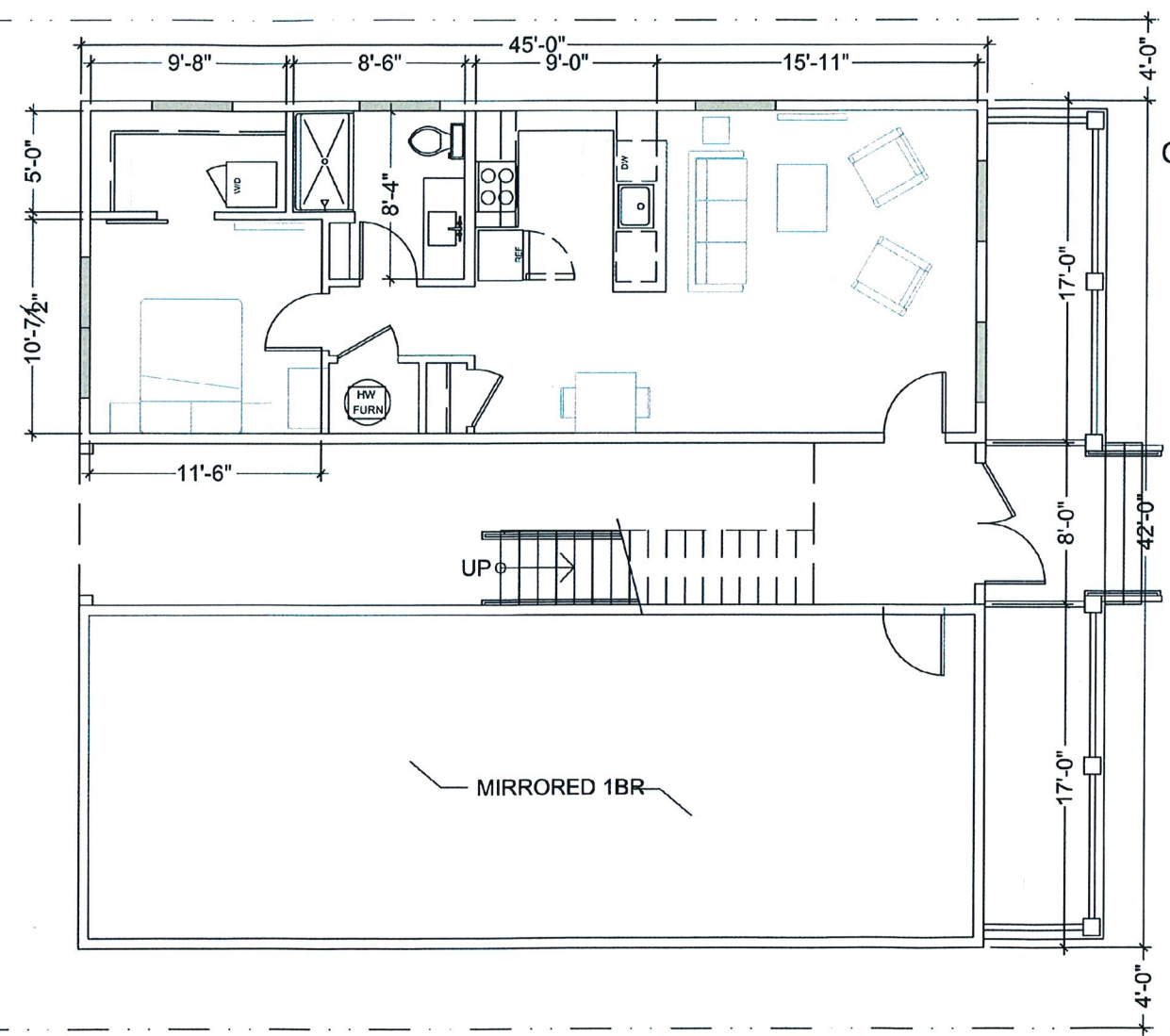
CARRIAGE HOUSE AND BUILDING A (BUILDING B SIMILAR)



ONE BEDROOM  
624 GSF  
563 NSF

3rd

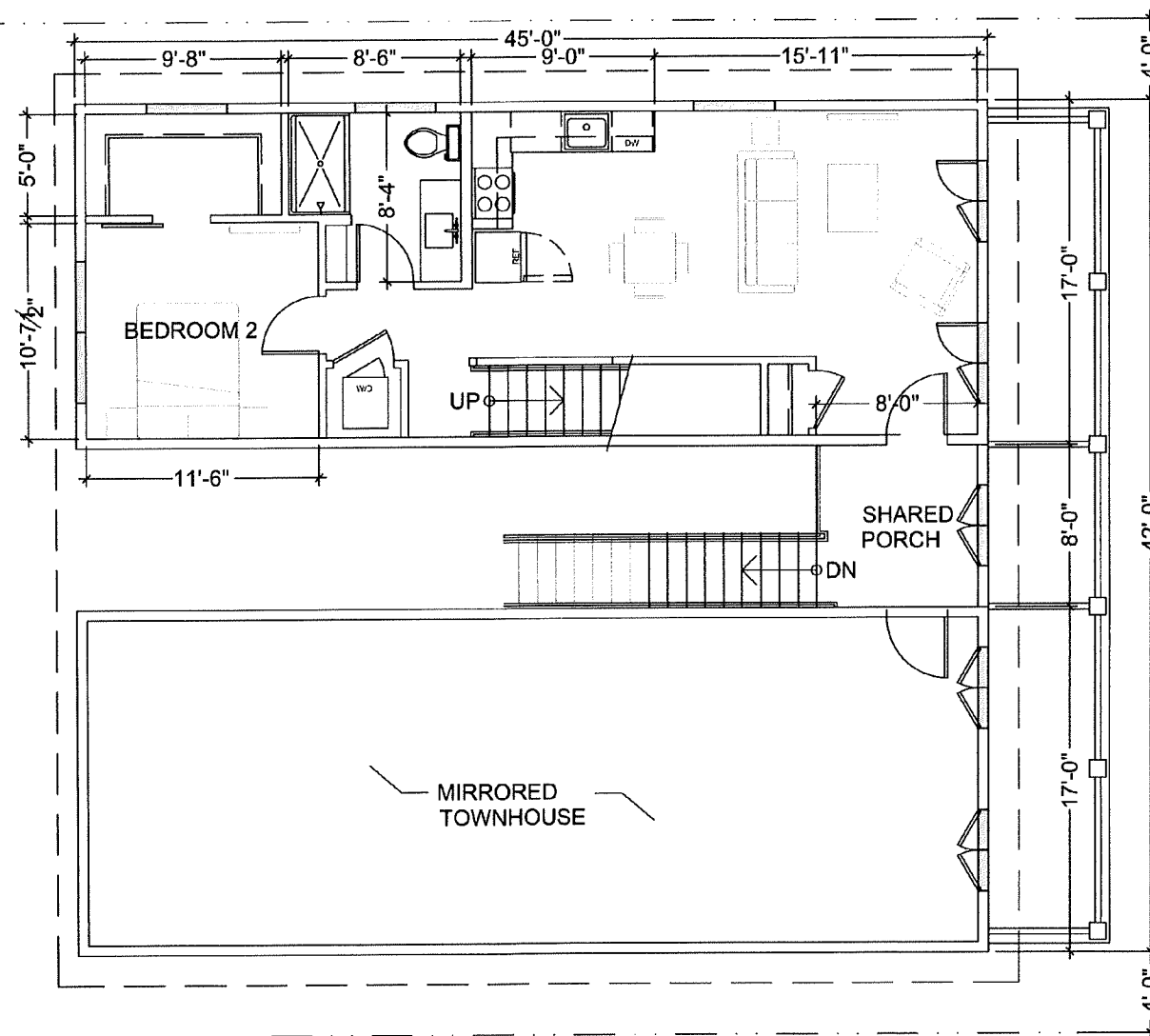
CARRIAGE HOUSE AND BUILDING A (BUILDING B SIMILAR)



ONE BEDROOM  
765 GSF  
704 NSF

1st

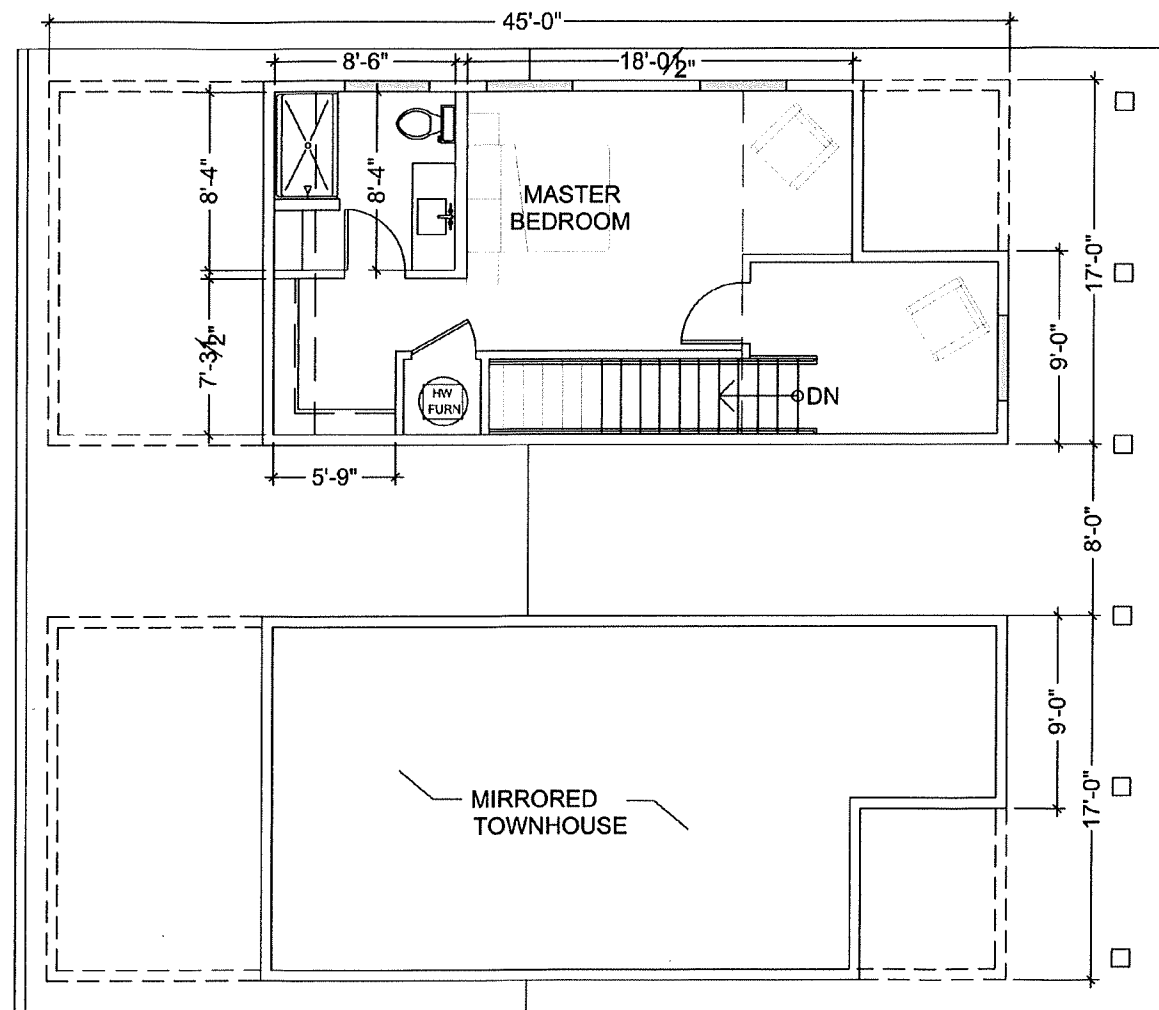
BUILDING C



TWO BEDROOM  
TOWNHOUSE  
1304 GSF  
1192 NSF

2nd

BUILDING C

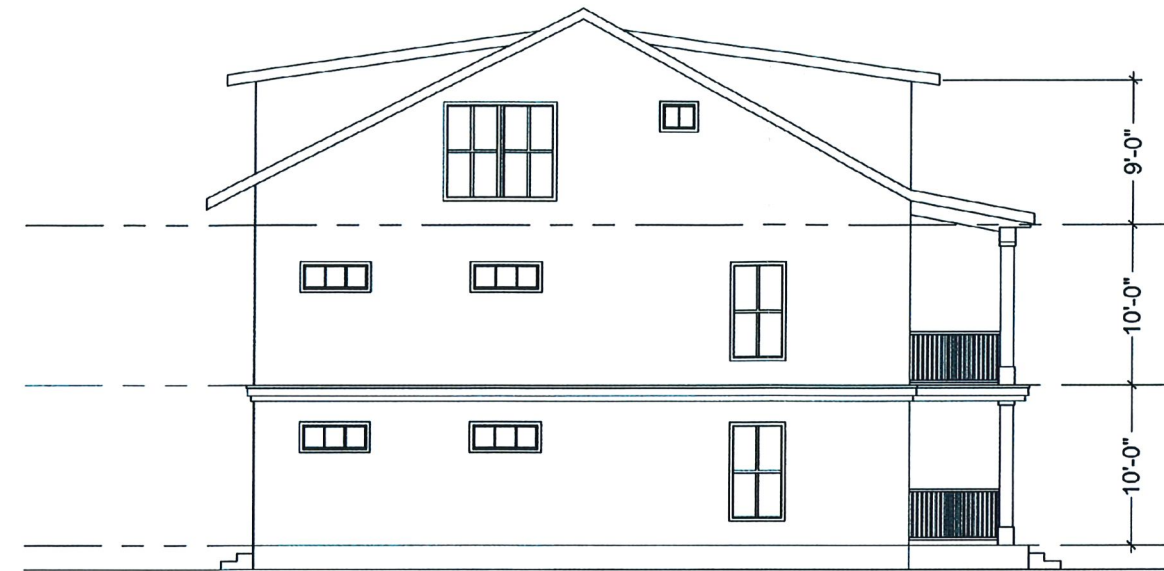


3rd

BUILDING C



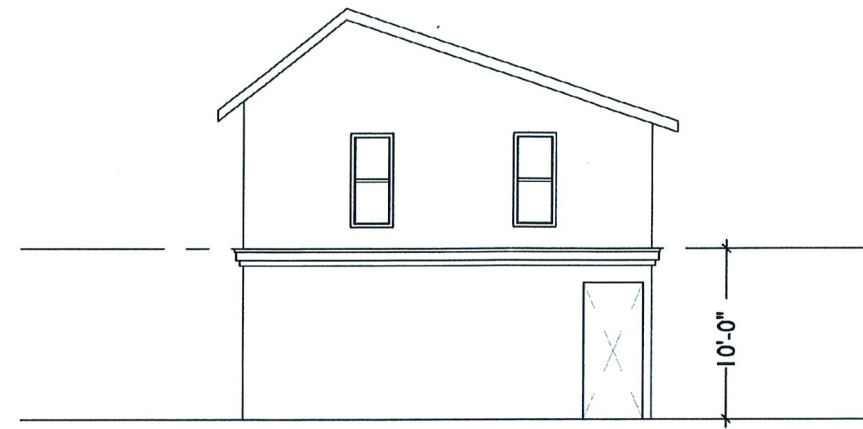
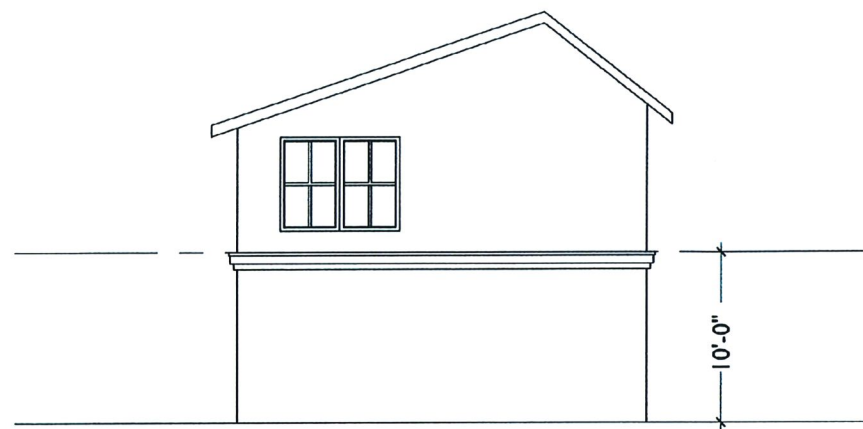
BUILDING A - ELEVATIONS



BUILDING B - ELEVATIONS



BUILDING C - ELEVATIONS



CARRIAGE HOUSE - ELEVATIONS

